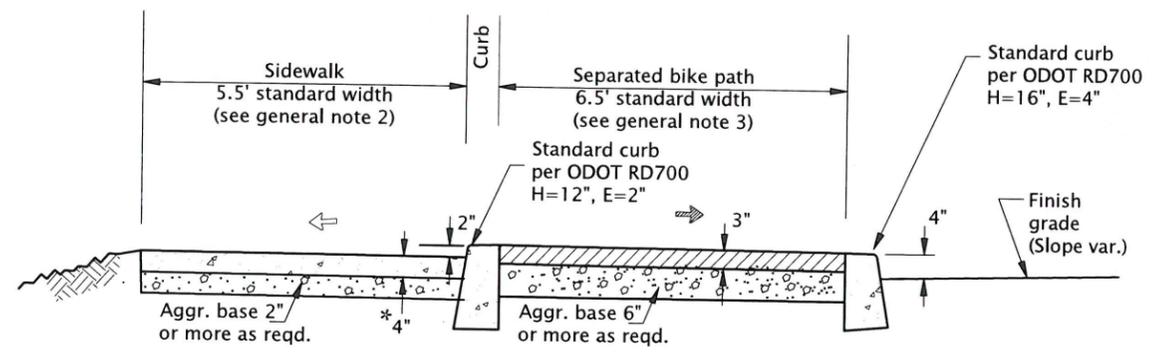
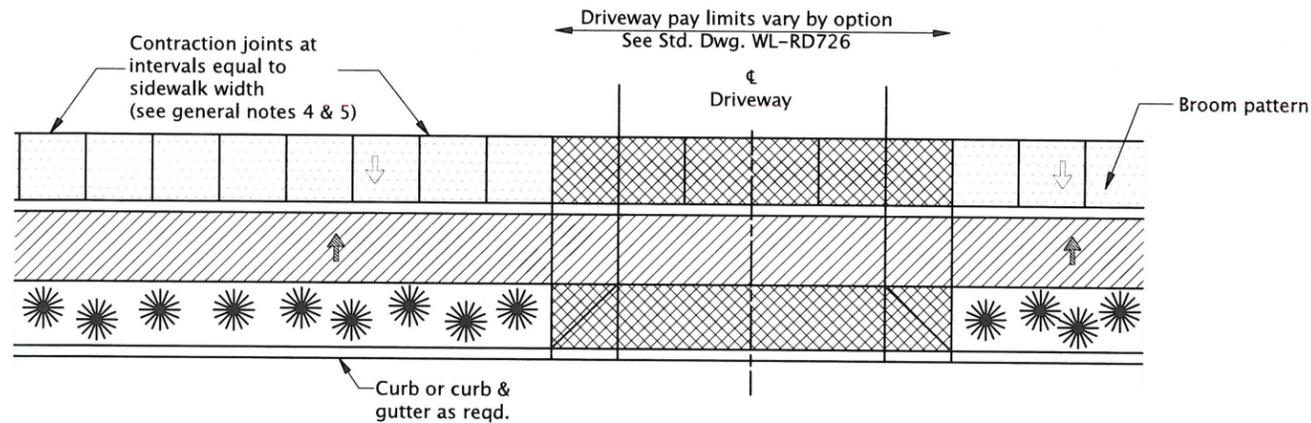


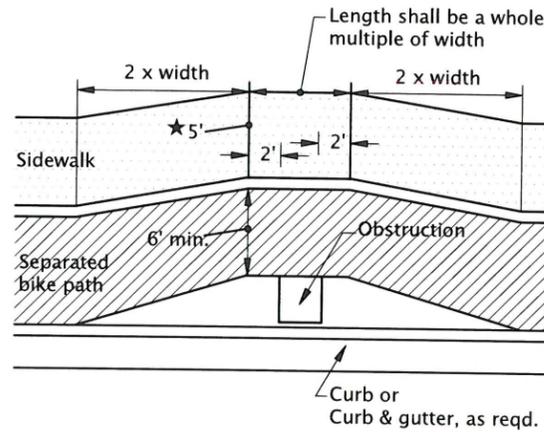
REVERSE SLOPE SETBACK SEPARATED BIKE PATH AND SIDEWALK



REVERSE SLOPE CURB LINE SEPARATED BIKE PATH AND SIDEWALK

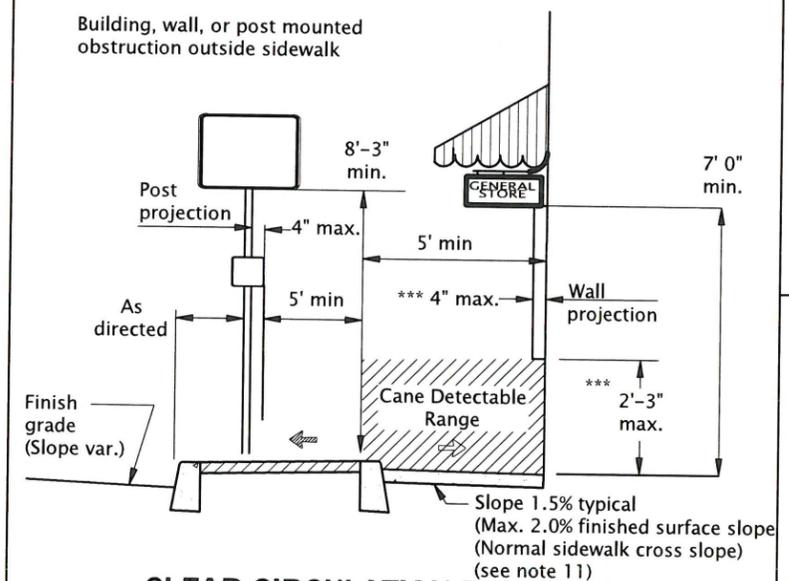


PLAN VIEW - REVERSE SLOPE SETBACK SEPARATED BIKE PATH AND SIDEWALK



★ When site constraints prohibit a 5' passage, the Engineer may direct this to be reduced, but no less than 4' (as shown on plans).

REQUIRED CURB LINE SEPARATED BIKE PATH AND SIDEWALK WIDENING AROUND OBSTRUCTIONS

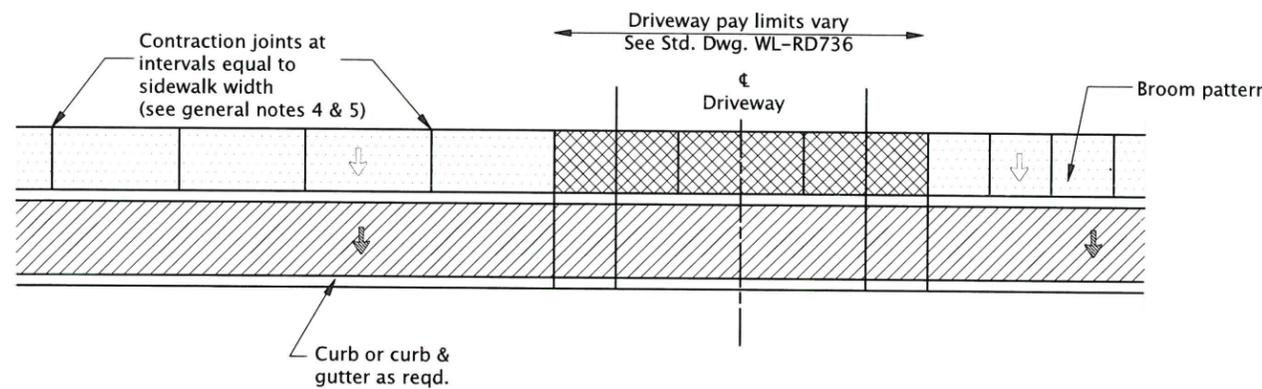


CLEAR CIRCULATION PATH FOR REVERSE SLOPE CURB LINE SEPARATED BIKE PATH AND SIDEWALK

(Only when path widening around an obstruction is not feasible)

- Sidewalk (PCC)
- Separated bike path (AC)
- Driveway (PCC) (See general note 8) (See project plans for details not shown)
- Slope 1.5% typical (Max. 2.0% finished surface slope)
- Slope 2.0% typical (Min. 1.0% finished surface slope)

- * As specified in plans, min. 4". If sidewalk is intended as portion of a driveway or mountable curb is used min. thickness 6".
- ** Provide compacted backfill adjacent to curb and separated bike path
- *** Objects with base below 2'-3" may protrude any distance as long as the 5' circulation path is maintained. When an object with a base higher than 2'-3" protrudes further than 4" provide a detection below protrusion to delineate edge.



PLAN VIEW - REVERSE SLOPE CURB LINE SEPARATED BIKE PATH AND SIDEWALK

GENERAL NOTES FOR ALL DETAILS:

1. Include additional paved or unpaved 2' clearance to vertical faces higher than 5' such as retaining walls, sound walls, fences and buildings.
2. Curb type and sidewalk width as shown on plans or as directed. On sidewalks 8' and wider, provide a longitudinal joint at the midpoint.
3. Storm drain weep hole pipes are not permitted in sidewalks with separated bike paths.
4. Const. expansion joints at 200' maximum spacing, and at points of tangency, and at ends of each driveway. For monolithic curb & sidewalk, const. expansion joints at 45' maximum spacing.
5. Const. contraction joints at 15' maximum spacing, and at ends of each driveway and curb ramp.
6. For curb details, see ODOT Std. Dwgs. RD700 & RD701.
7. Sidewalk details are based on ODOT applicable standards.
8. For driveway details not shown, see Std. Dwgs. WL-RD726 and WL-RD736.
9. See project plans for details not shown.
10. The reverse slope curb line layout for separated bike path and sidewalk is only allowed with City Engineer approval.
11. Provide positive drainage away from any building entrances.

Standard Drawing No.
WL-RD722
Effective Date:
02/22/19

Title: REVERSE SLOPE SEPARATED BIKE PATH AND SIDEWALK

City Engineer
Approved: City Engineer
West Linn
CITY OF
PUBLIC WORKS DEPARTMENT - STANDARD DRAWINGS
22500 Salamo Road, West Linn OR 97068
Phone: 503-722-5500
www.westlinnoregon.gov

NOTE: All material and workmanship shall be in accordance with the City of West Linn Standard Construction Specifications without consulting a Registered Professional Engineer.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

File
WL-RD722.dwg