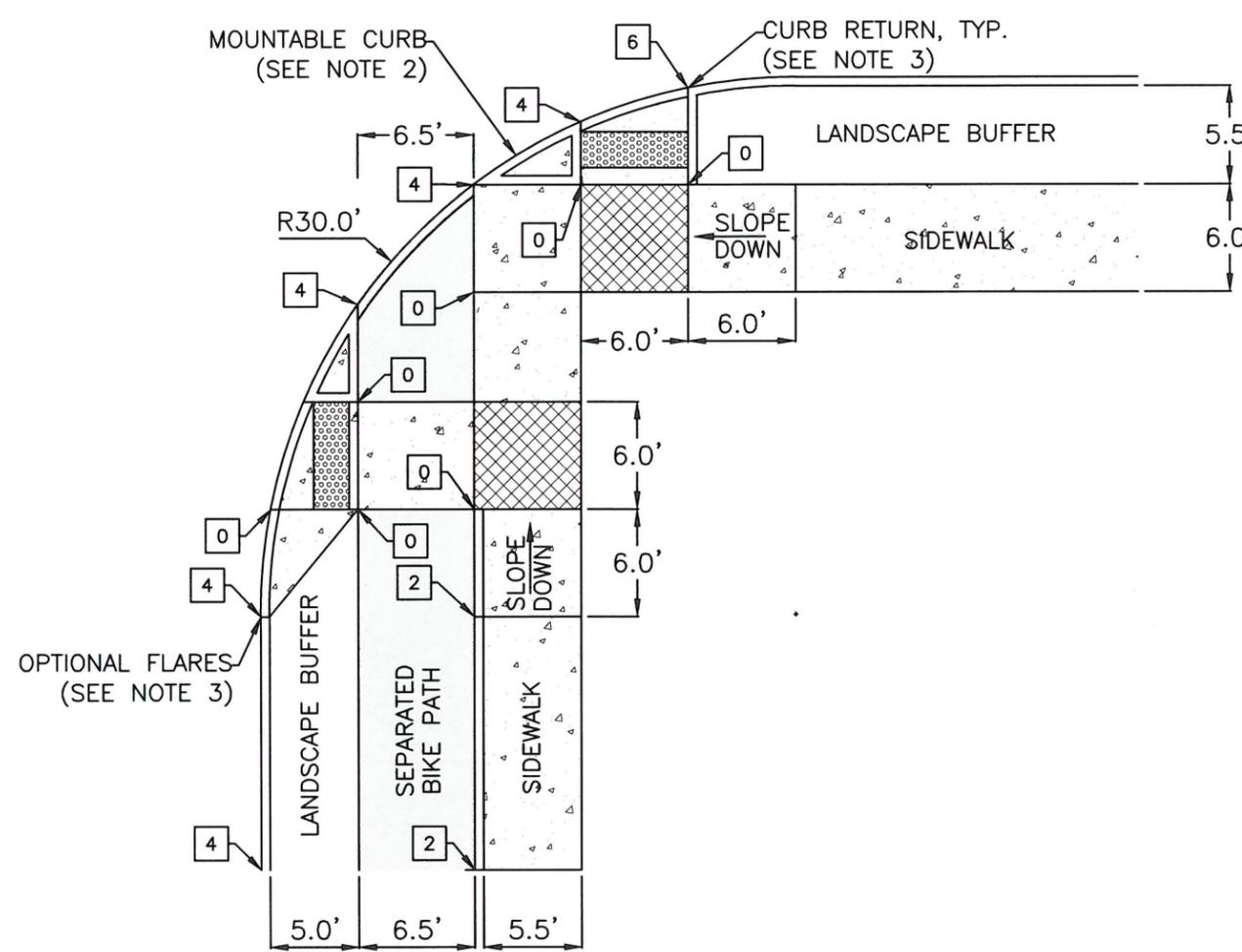


15' CURB RADIUS
(TYPICAL)



30' CURB RADIUS
(SEE NOTE 1)

LEGEND:

-  = CURB EXPOSURE IN INCHES
-  = ACCESSIBLE TURNING SPACE (6' X 6' TYP. (5' X 5' MIN.), MAX. 2% CROSS-SLOPE)
-  = TRUNCATED DOMES (MINIMUM 2' IN DIRECTION OF TRAVEL)
-  = CONCRETE SIDEWALK SURFACE
-  = ASPHALT SEPARATED BIKE PATH SURFACE

NOTE:

1. 15' CURB RADIUS IS TYPICAL. LARGER CURB RADIUS LIKE 30' SHALL ONLY BE USED AT INTERSECTIONS WITH RECORDED HIGH-VOLUMES OF TRUCK, BUS, AND LARGE VEHICLE TURNING MOVEMENTS.
2. MOUNTABLE CURB PER ODOT RD700 SHALL ONLY BE USED ON THE STREET EDGE OF THE ISLAND BETWEEN THE PROTECTED BIKE PATHS. "E" AS SPECIFIED.
3. FLARES SHALL BE PROVIDED IN AREAS WITH RECORDED HIGH-VOLUMES OF TRUCK, BUS, AND LARGE VEHICLE TURNING MOVEMENTS. WHERE THESE TURNING MOVEMENTS ARE NOT COMMON, PROVIDE TYPICAL RETURN CURBS.
4. IN AREAS WHERE FLARES ARE PROVIDED TO ACCOMODATE TURNING MOVEMENTS, THE PROTECTIVE ISLAND SHALL BE CONSTRUCTED OF CONCRETE. IN ALL OTHER AREAS, IT MAY BE PLANTED WITH LOW-GROWING SHRUBS AND GRASSES.

Standard Drawing No.
WL-B1012

Title:
SEPARATED BIKE PATH AT INTERSECTION
SINGLE PATH

Approved:  City Engineer

Effective Date:
02/22/19

 **West Linn**
CITY OF

PUBLIC WORKS DEPARTMENT - STANDARD DRAWINGS
22500 Salamo Road, West Linn OR 97068
Phone: 503-722-5500 www.westlinnoregon.gov

NOTE:
All material and workmanship shall be in accordance with the City of West Linn Standard Construction Specifications.

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

File
WL-B1012.dwg