





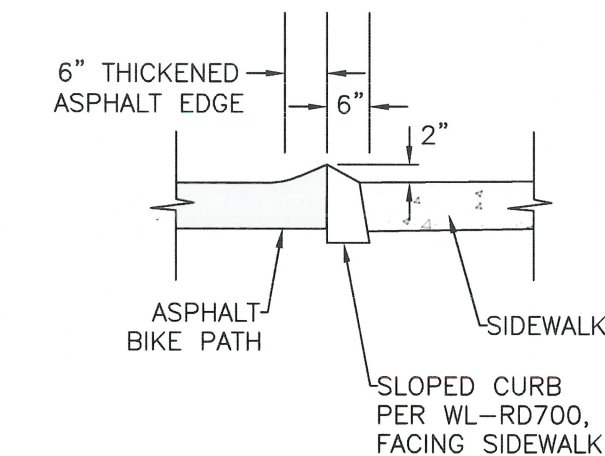


LEGEND:

-  = CURB EXPOSURE IN INCHES
-  = SLOPED CURB EXPOSED IN INCHES (SEE WL-RD700)
-  = ACCESSIBLE TURNING SPACE (6' X 6' TYP. (5' X 5' MIN.), MAX. 2% CROSS-SLOPE)
-  = TRUNCATED DOMES (MINIMUM 2' IN DIRECTION OF TRAVEL)
-  = CONCRETE SIDEWALK SURFACE
-  = ASPHALT SEPARATED BIKE PATH SURFACE

NOTES:

1. 15' CURB RADIUS IS TYPICAL. LARGER CURB RADIUS LIKE 30' SHALL ONLY BE USED AT INTERSECTIONS WITH RECORDED HIGH VOLUMES OF TRUCK, BUS, AND LARGE VEHICLE TURNING MOVEMENTS AS SHOWN ON PLANS.
2. MOUNTABLE CURB PER WL-RD700 SHALL ONLY BE USED ON THE STREET EDGE OF THE ISLAND BETWEEN THE PROTECTED BIKE PATHS. "E" AS SPECIFIED.
3. FLARES SHALL BE PROVIDED ON ARTERIALS, COLLECTORS, AND NEIGHBORHOOD ROUTES. ON LOCAL ROADS, PROVIDE TYPICAL RETURN CURBS. MAXIMUM FLARE SLOPE = 7.5% PLUS/MINUS RUNNING STREET SLOPE
4. THE PROTECTIVE ISLAND SHALL BE CONSTRUCTED OF 6" CONCRETE WITH 8" AGG. BASE.



Standard Drawing No. WL-BI011	Effective Date: 02/22/22
SEPARATED BIKE PATH AT INTERSECTION DUAL PATH	
 <p>West Linn CITY OF PUBLIC WORKS DEPARTMENT - STANDARD DRAWINGS 22500 Salamo Road, West Linn OR 97068 Phone: 503-722-5500 www.westlinnoregon.gov</p>	
NOTE: All material and workmanship shall be in accordance with the City of West Linn Standard Construction Specifications.	
The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.	
Approved  City Engineer	
File WL-BI011.dwg	