

Narrative Response to Applicable Criteria

99.325 EXTENSIONS OF APPROVAL

A. The Planning Director may grant an extension from the effective date of approval of two years pertaining to applications listed in Section 99.060(A) upon finding that:

1. The applicant has demonstrated, and staff and the Planning Commission concur, that the application is in conformance with applicable CDC provisions and relevant approval criteria enacted since the application was initially approved; and

2. There are no demonstrated material misrepresentations, errors, omissions, or changes in facts that directly impact the project, including, but not limited to, existing conditions, traffic, street alignment and drainage; or

3. The applicant has modified the approved plans to conform with current approval criteria and remedied any inconsistency with subsection (A)(2) of this section, in conformance with any applicable limits on modifications to approvals established by the CDC.

Response: Please see Section 85.000 for response to approval criteria that has changed since the initial submittal.

B. The Planning Commission may grant an extension from the effective date of approval of two years pertaining to applications listed in Section 99.060(B), consistent with subsections (A)(1) through (3) of this section.

Response: This extension application requests a two year extension from the date of our approval expiration, June 6th, 2010.

C. The Historic Review Board may grant an extension from the effective date of approval of two years for applications listed in Section 99.060(D), consistent with subsections (A)(1) through (3) of this section.

Response: This section is not applicable to this project.

D. Eligibility for Extensions.

1. Only those applications approved between July 1, 2006, and December 31, 2009, shall be eligible for an extension.

2. Any application eligible for an extension under subsection (D)(1) of this section that would expire by June 30, 2010, shall be exempt from expiration pending a decision regarding the extension application; provided, that a complete application and deposit fee have been submitted to the Planning Director prior to that date. However, the extension shall begin on the date that the application's initial approval lapsed.

Response: This Subdivision was initially approved June 6th 2007 and therefore is eligible for an extension.

E. Extension Procedures.

1. The application for extension of approval may be submitted only after a pre-application meeting under Section 99.030(B).

Response: Pre-application conference was held May 20, 2010 at the City of West Linn per Section 99.030.

2. The application shall satisfy the neighborhood meeting requirements of Section 99.038 for those cases that require compliance with that section.

Response: Notice for a public neighborhood meeting was sent and the property posted with an 11" x 17" notification sign on May 19 2010 per CDC 99.038. A public neighborhood meeting was held to discuss the extension application on June 8th 2010 at 7pm at Emmanuel Presbyterian Church.

Site Information:

The applicant proposes a six lot subdivision on the site. The site is zoned Single Family Residential, R-10. Approximately 100 feet to the Southwest, on Cedar Oak Drive, is Cedaroak Park Primary School. On the Southeast side of Cedar Oak Drive are more single family detached houses. Approximately 700 feet to the East of the site is the Cedaroak Boat Ramp along the Willamette River. To the East, homes front along Nixon Drive. The rear portions of the two lots directly across from the site are vacant or heavily treed. One lot appears to contain a swimming pool. The related dwelling on the East portion of that site fronts on Nixon Street. The surrounding streets are improved only with side-strip paving. There are no sidewalks, shoulders or other street improvements on Elmran Drive, which wraps around the site from the East through the South and up the West side.

3. Applications for extensions must be submitted along with the appropriate deposit to the Planning Department.

4. Applications for extensions will be processed if the initial approval lapses prior to issuance of a decision, consistent with subsection (D)(2) of this section.

5. Notice of the decision shall be issued consistent with Section 99.080.

6. The decision shall not become effective until resolution of all appeal periods, including an opportunity for City Council call-up pursuant to this chapter. (ORD. 1589 § 1 (Exh. A), 2010)

85.000 GENERAL PROVISIONS

The following sections of the CDC have changed since our initial submittal November 7, 2007:

85.160(F)(3)

85.170(B)(2)(F)

85.200(A), the streets criteria section, has changed in (1-3) and (22).

85.200(B)(2)

85.200(J)(10)

85.160 SUBMITTAL REQUIREMENTS FOR THE TENTATIVE PLAN

F. The following proposed improvements shall be shown on the tentative plan or supplemental drawings:

3. Any proposed infrastructure improvements that address those identified in the City Transportation System Plan. (ORD. 1544)

Response: The pedestrian pathway that we are providing along Elmran Drive is part of a medium priority project listed on table 5-2 of the Transportation system plan. This is the only known infrastructure improvement included in this project that is addressed in the City Transportation System Plan.

85.170 SUPPLEMENTAL SUBMITTAL REQUIREMENTS FOR A TENTATIVE SUBDIVISION OR PARTITION PLAN

B. Transportation.

2. Traffic Impact Analysis (TIA).

A. Purpose. The purpose of this section of the code is to implement Section 660-012-0045 (2) (e) of the State Transportation Planning Rule that requires the City to adopt a process to apply conditions to development proposals in order to minimize adverse impacts to and protect transportation facilities. This section establishes the standards for when a proposal must be reviewed for potential traffic impacts; when a Traffic Impact Analysis must be submitted with a development application in order to determine whether conditions are needed to minimize impacts to and protect transportation facilities; what must be in a Traffic Impact Study; and who is qualified to prepare the Study.

B. Typical Average Daily Trips. The latest edition of the Trip Generation manual, published by the Institute of Transportation Engineers (ITE) shall be used as the standards by which to gauge average daily vehicle trips.

C. When Required. A Traffic Impact Analysis may be required to be submitted to the City with a land use application, when the following conditions apply:

a. The development application involves one or more of the following actions:

- (1) A change in zoning or a plan amendment designation; or
- (2) Any proposed development or land use action that ODOT states may have operational or safety concerns along a state highway; and
- (3) The development shall cause one or more of the following effects, which can be determined by field counts, site observation, traffic impact analysis or study, field measurements, crash history, Institute of Transportation Engineers Trip Generation manual; and information and studies provided by the local reviewing jurisdiction and/or ODOT:

(a.) An increase in site traffic volume generation by 250 Average Daily Trips (ADT) or more (or as required by the City Engineer); or

(b.) An increase in use of adjacent streets by vehicles exceeding the 20,000 pound gross vehicle weights by 10 vehicles or more per day; or

(c.) The location of the access driveway does not meet minimum intersection sight distance requirements, or is located where vehicles entering or leaving the property are restricted, or such vehicles queue or hesitate on the State highway, creating a safety hazard; or

(d.) The location of the access driveway does not meet the access spacing standard of the roadway on which the driveway is located; or

(e.) A change in internal traffic patterns that may cause safety problems, such as back-up onto the highway or traffic crashes in the approach area.

D. Traffic Impact Analysis Requirements.

1. Preparation. A Traffic Impact Analysis shall be prepared by a professional engineer in accordance with OAR 734-051-180. The City shall commission the traffic analysis and it will be paid for by the applicant,

2. Transportation Planning Rule Compliance. See Section 105.050 Transportation Planning Rule Compliance.

3. Pre-application Conference. The applicant will meet with West Linn Public Works prior to submitting an application that requires a Traffic Impact Application. This meeting will determine the required elements of the TIA and the level of analysis expected.

E. Approval Criteria.

1. Criteria. When a Traffic Impact Analysis is required, approval of the development proposal requires satisfaction of the following criteria:

(a) The Traffic Impact Analysis was prepared by a professional traffic engineer in accordance with OAR 734-051-180; and

(b) If the proposed development shall cause one or more of the effects in Section 55.125(A)(3), above, or other traffic hazard or negative impact to a transportation facility, the Traffic Impact Analysis includes mitigation measures that meet the City's Level-of-Service and satisfactory to the City Engineer, and ODOT when applicable; and

(c) The proposed site design and traffic and circulation design and facilities, for all transportation modes, including any mitigation measures, are designed to:

(1.) Have the least negative impact on all applicable transportation facilities; and

(2.) Accommodate and encourage non-motor vehicular modes of transportation to the extent practicable; and

(3.) Make the most efficient use of land and public facilities as practicable; and

(4.) Provide the most direct, safe and convenient routes practicable between on-site destinations, and between on-site and off-site destinations; and

(5.) Otherwise comply with applicable requirements of the City of West Linn Community Development Code.

F. Conditions of Approval. The City may deny, approve, or approve the proposal with appropriate conditions.

1. Dedication of land for streets, transit facilities, sidewalks, bikeways, paths, or accessways shall be required where the existing transportation system will be impacted by or is inadequate to handle the additional burden caused by the proposed use.

2. Improvements such as paving, curbing, installation or contribution to traffic signals, construction of sidewalks, bikeways, accessways, paths, or streets that serve the proposed use where the existing transportation system may be burdened by the proposed use may be required. (ORD. 1584)

Response: Traffic Impact Analysis is not required by the City Engineer for this project, therefore this section does not apply to this application.

85.200 APPROVAL CRITERIA

No tentative subdivision or partition plan shall be approved unless adequate public facilities will be available to provide service to the partition or subdivision area prior to final plat approval and the Planning Commission or Planning Director, as applicable, find that the following standards have been satisfied, or can be satisfied by condition of approval. (ORD 1544)

A. Streets

1. General. The location, width and grade of streets shall be considered in their relation to existing and planned streets, to the generalized or reasonable layout of streets on adjacent undeveloped parcels, to topographical conditions, to public convenience and safety, to

accommodate various types of transportation (automobile, bus, pedestrian, bicycle), and to the proposed use of land to be served by the streets. The functional class of a street aids in defining the primary function and associated design standards for the facility. The hierarchy of the facilities within the network in regards to the type of traffic served (through or local trips), balance of function (providing access and/or capacity), and the level of use (generally measured in vehicles per day) are generally dictated by the functional class. The street system shall assure an adequate traffic or circulation system with intersection angles, grades, tangents, and curves appropriate for the traffic to be carried. Streets should provide for the continuation, or the appropriate projection, of existing principal streets in surrounding areas and should not impede or adversely affect development of adjoining lands or access thereto.

To accomplish this, the emphasis should be upon a connected continuous pattern of local, collector, and arterial streets rather than discontinuous curvilinear streets and cul-de-sacs. Deviation from this pattern of connected streets should only be permitted in cases of extreme topographical challenges including excessive slopes (35 percent plus), hazard areas, steep drainageways, wetlands, etc. In such cases, deviations may be allowed but the connected continuous pattern must be reestablished once the topographic challenge is passed. Streets should be oriented with consideration of the sun, as site conditions allow, so that over 50 percent of the front building lines of homes are oriented within 30 degrees of an east-west axis. (ORD. 1382; ORD. 1584)

Internal streets are the responsibility of the developer. All streets bordering the development site are to be developed by the developer with, typically, half-street improvements or to City standards prescribed by the City Engineer. Additional travel lanes may be required to be consistent with adjacent road widths or to be consistent with the adopted Transportation System Plan and any adopted updated plans. (ORD. 1544)

An applicant may submit a written request for a waiver of abutting street improvements if the Transportation System Plan prohibits the street improvement for which the waiver is requested. Those areas with numerous (particularly contiguous) under-developed or undeveloped tracts will be required to install street improvements. When an applicant requests a waiver of street improvements and the waiver is granted, the applicant shall propose a fee amount that will be reviewed by the City manager or the Manager's designee. The City Manager or the Manager's designee will revise the proposed fee as necessary and establish the amount to be paid on a case by case basis. The applicant shall pay an in-lieu fee for improvements to the nearest street identified by the City Manager or Manager's designee as necessary and appropriate. The amount of the in-lieu fee shall be roughly proportional to the impact of the development on the street system as determined in the CDC 85.200 (A) (22) below. (ORD. 1442) (ORD.1544)

Streets shall also be laid out to avoid and protect clusters and significant trees, but not to the extent that it would compromise connectivity requirements per CDC Section 85.200(A)(1), or bring the density below 70 percent of the maximum density for the developable net area. The developable net area is calculated by taking the total site acreage and deducting Type I and II lands; then up to 20 percent of the remaining land may be excluded as necessary for the purpose of protecting significant tree clusters or stands as defined in CDC Section 55.100(B)(2). (ORD. 1408) (ORD.1544)

Response: This development includes a private street, Jackson Court and was designed to curve to preserve some existing trees. Jackson Court is not a through street because of the existing steep slope on the East side of the site. The private street and the half street improvement both include green street elements. Jackson Court's right of way includes a 16 foot paved roadway with a 1 foot gravel shoulder and a 7 foot vegetated storm water swale on each side. The half street improvements to upper Elmran Drive include public pedestrian path and bikeway, a vegetated storm water swale and street trees.

2. Right of way and Roadway Widths. In order to accommodate larger tree lined boulevards and sidewalks, particularly in residential areas, the standard right-of-way widths for the different street classifications shall be within the range listed below. But, instead of filling in the right-of-way with pavement, they shall accommodate the amenities (e.g., boulevards, street trees, sidewalks). The exact width of the right-of-way shall be determined by the City Engineer or the approval authority. The following ranges will apply:

Street Classification	Right-of-Way
Highway 43	60-80
Major arterial	60-80
Minor arterial	60-80
Major collector	60-80
Collector	60-80
Local street	40-60
Cul-de-sac	40-60
Radii of cul-de-sac	48-52
Alley	16

Additional right-of-ways for slopes may be required. Sidewalks shall not be located outside of the right-of-way unless to accommodate significant natural features or trees.

3. Street Widths. Street widths shall depend upon which classification of street is proposed. The classifications and required cross sections are established in Chapter 8 of the adopted TSP. Streets are classified as follows. (ORD 1584)

Freeways are state or interstate facilities that provide regional travel connections. These routes have the highest capacity and the most restrictive access requirements. Two local freeway interchanges at 10th Street and at Highway 43 serve the entire city of West Linn. Interchanges are grade-separated facilities with arterial or principal arterial streets. No intermediate vehicular or pedestrian access is allowed.

Principal Arterials are typically state highways that provide the high level roadway capacity to local land uses. These routes connect over the longest distance (sometimes miles long) and are less frequent than other arterial or collectors. These highways generally span

several jurisdictions and often have statewide importance (as defined in the ODOT State Highway Classification).

These facilities should provide for a high level of transit service and include transit priority measures to expedite bus travel.

Arterial Streets serve to interconnect the City. These streets link major commercial, residential, industrial and institutional areas. Arterial streets are typically spaced about one mile apart to assure accessibility and reduce the incidence of traffic using collectors or local streets for through traffic in lieu of a well placed arterial street. Access control is the key feature of an arterial route. Arterials are typically multiple miles in length.

Collector streets provide both access and circulation within and between residential and commercial/industrial areas. Collectors differ from arterials in that they provide more of a citywide circulation function, do not require as extensive control of access and that they penetrate residential neighborhoods, distributing trips from the neighborhood and local street system. Collectors are typically greater than 0.5 to 1.0 miles in length.

Neighborhood Routes are usually long relative to local streets and provide connectivity to collectors or arterials. Since neighborhood routes have greater connectivity, they generally have more traffic than local streets and are used by residents in the area to access the neighborhood, but do not serve citywide/large area circulation. They are typically about a quarter to a half-mile in total length. Traffic from cul-de-sacs and other local streets may drain onto neighborhood routes to gain access to collectors or arterials. Because traffic needs are greater than a local street, certain measures should be considered to retain the neighborhood character and livability of these streets. Neighborhood traffic management measures are often appropriate (including devices such as speed humps, traffic circles and other devices – refer to later section in this chapter). However, it should not be construed that neighborhood routes automatically get speed humps or any other measures. While these streets have special needs, neighborhood traffic management is only one means of retaining neighborhood character and vitality.

Local Streets have the sole function of providing access to immediate adjacent land. Service to “through traffic movement” on local streets is deliberately discouraged by design.

The following table identifies appropriate street width (curb to curb) in feet for various street classifications. The desirable width shall be required unless the applicant or his engineer can demonstrate that site conditions, topography, or site design require the reduced minimum width. (ORD. 1584)

City of West Linn Roadway Cross-Section Standards

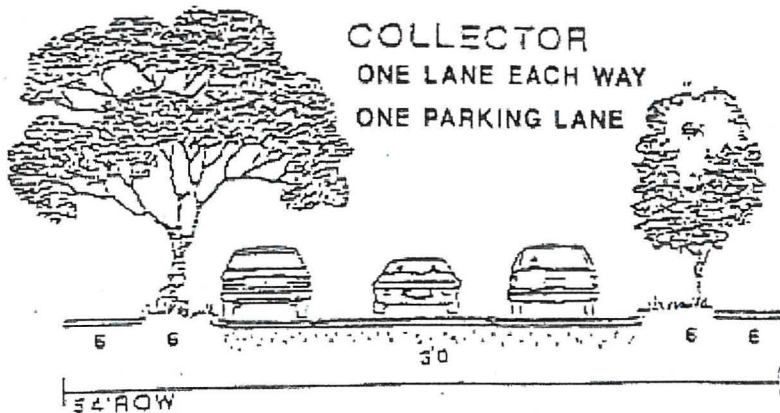
Street Element	Characteristic	Width/Options
Vehicle Lane Widths: (minimum widths)	Arterial	11 feet
	Collector	10 feet
	Neighborhood	10 feet
	Local	12 feet

	Turn Lane	10-14 feet
On-Street Parking	Arterials Collectors Neighborhood Local	Limited (in commercial areas) Some (unstriped) Some (8 feet) Some (unstriped)
Bicycle Lanes (minimum widths)	New Construction Reconstruction	5 to 6 feet 5 to 6 feet
Sidewalks (minimum width) (See note below)	Arterial Collector Neighborhood/Local	6 feet 6 feet 6 feet
Landscape Strips	Can be included in all streets	6 feet
Medians:	5-Lane 3-Lane 2-Lane	Optional Optional Consider if appropriate
Neighborhood Traffic Mgmt	Arterials Collectors Neighborhood Local	Not Recommended Under Special Conditions Should consider if appropriate Should consider if appropriate
Transit	Arterial/Collectors Neighborhood Route Local	Appropriate Only in special circumstances Not Recommended

NOTE: Commercial/OBC zone development on arterials requires a 12-foot wide sidewalk which includes three feet for street trees, hydrants, street furniture, etc. Commercial/OBC zone development on local streets requires an 8-foot wide sidewalk with no planter strip, but shall include cut-outs for street trees. In both commercial and residential areas where site constraints exist, sidewalks and planter strips may be reduced to the minimum necessary (e.g., 4 feet for sidewalks and no planter strip) to accommodate walking and significant natural features such as mature trees, steep embankment, grade problems, and existing structures, or to match existing sidewalks or right-of-way limitations. These natural features are to be preserved to the greatest extent possible. Requests for this configuration shall require the endorsement of the City Engineer. City Engineer has the authority to require that street widths match adjacent street widths.

Sidewalk Location	Sidewalk Width
Arterial in GC/OBC zone	12 feet
Collector/Local in GC/OBC zone	8 feet
Storefront on arterial	12 feet
Storefront on collector/local	8 feet
Residential Development	6 feet (+6-foot planter strip)

(GC = General Commercial; OBC = Office Business Center) (ORD.



Response: The private street, Jackson Court includes a 16 foot paved roadway with a 1 foot gravel shoulder and a 7 foot vegetated storm water swale on each side. Elmran Drive is considered a Collector Street. The half street improvements increase the right of way to 54 feet, requiring a 4 foot dedication on the development side of Elmran Drive. The half street improvements include a 5 foot pedestrian path, 7 foot vegetated storm water swale, 2 foot gravel shoulder and 14 foot roadway.

4. The decision-making body shall consider the City Engineer's recommendations on the desired right of way width, pavement width and street geometry of the various street types within the subdivision after consideration by the City Engineer of the following criteria:

- a. The type of road as set forth in the Transportation Master Plan.
- b. The anticipated traffic generation.
- c. On-street parking requirements.
- d. Sidewalk and bikeway requirements.
- e. Requirements for placement of utilities.
- f. Street lighting.
- g. Drainage and slope impacts.
- h. Street trees.
- i. Planting and landscape areas.
- j. Existing and future driveway grades.
- k. Street geometry.

I. Street furniture needs, hydrants.

Response: The city engineers have determined that Elmran Drive is considered a Collector street.

This proposed development will only increase the traffic by 4 additional single family homes. This proposal will provide a 5 foot pedestrian walkway separated from the roadway by a 2 foot gravel shoulder and 7 foot bioswale. The applicant will replace the existing water line along Elmran Drive and place all utilities along the street frontage underground.

The proposed work will occur set back from the steeply sloping area of the property and the steepest area will be protected with an easement, as shown on the plans.

Existing light poles to be relocated and one additional light fixture to be provided per City of West Linn and utility company.

The proposed development includes landscaped bio-swales along Elmran Drive and the new access drive. Street trees will be provided along Elmran Drive.

The existing fire hydrant located on the upper section of Elmran Drive will be relocated per the street improvements.

5. Additionally, when determining appropriate street width, the decision-making body shall consider the following criteria:

a. When a local street is the only street serving a residential area and is expected to carry more than the normal local street traffic load, the designs with two travel and one parking lane are appropriate.

b. Streets intended to serve as signed but unstriped bike routes should have the travel lane widened by two feet.

c. Collectors should have two travel lanes and may accommodate some parking. Bike routes are appropriate.

d. Arterials should have two travel lanes. On-street parking is not allowed unless part of a Street Master Plan. Bike lanes are required as directed by the Parks Master Plan and Transportation Master Plan.

Response: Elmran Drive is designated a Collector and includes two travel lanes.

10. Additional Right-of-Way for Existing Streets. Wherever existing street right-of-ways adjacent to or within a tract are of inadequate widths based upon the standards of this chapter, additional right-of-way shall be provided at the time of subdivision or partition.

Response: As part of this Approval Extension Application, we are providing 4 feet of dedication that will increase the existing right of way to 54 feet.

22. Based upon the determination of the City Manager or the Manager's designee, the applicant shall construct or cause to be constructed, or contribute a proportionate share of the costs, for all necessary off-site improvements identified by the transportation analysis commissioned to address CDC 85.170.B.2. that are required to mitigate impacts from the proposed subdivision. Proportionate share of the costs shall be determined by the City Manager or Manager's designee who shall assume that the proposed subdivision provides improvements in rough proportion to identified impacts of the subdivision. Off-site transportation improvements will include bicycle and pedestrian improvements as identified in the adopted City of West Linn TSP. (ORD. 1526) (ORD. 1544) (ORD. 1584)

Response: No transportation analysis has been required by the City Engineer for this project, and no off-site improvements have been required. Based on the decision of the planning

commission we are providing half street improvements along upper Elmran drive along with a wheel-chair assessable pedestrian pathway. The applicant is also providing a 12 foot easement at the intersection of Elmran and Cedar Oak to provide better sight distance around the corner.

B. Blocks and Lots

2. **Sizes.** The recommended block size is 400 feet in length to encourage greater connectivity within the subdivision. Blocks shall not exceed 800 feet in length between street lines, except for blocks adjacent to arterial streets or unless topographical conditions or the layout of adjacent streets justify a variation. Designs of proposed intersections shall demonstrate adequate sight distances to the City Engineer’s specifications. Block sizes and proposed accesses must be consistent with the adopted TSP.

(ORD. 1584)

Response: Block size is not applicable to this development as this project site is not large enough to require multiple blocks. The approved intersection design is per the City Engineers specifications.

3. **Lot Size and Shape.** Lot size, width, shape, and orientation shall be appropriate for the location of the subdivision, for the type of use contemplated, for potential utilization of solar access, and for the protection of drainageways, trees, and other natural features. No lot shall be dimensioned to contain part of an existing or proposed street. All lots shall be buildable, and the buildable depth should not exceed two and one half times the average width. Buildable describes lots that are free of constraints such as wetlands, drainageways, etc., that would make home construction impossible. Lot sizes shall not be less than the size required by the zoning code unless as allowed by Planned Unit Development (PUD). (ORD. 1401)

Depth and width of properties reserved or laid out for commercial and industrial purposes shall be adequate to provide for the off street parking and service facilities required by the type of use proposed.

Response: All lots are in excess of the size required by the R-10 zoning, 10,000 SF. The lot areas reflected below do not include the area of each lot included in the access drive, Jackson Court.

Lot Number	Lot Area	Average Width (approx.)	Average Depth (approx.)
Lot 1	11,429 SF	85 feet	140 feet
Lot 2	11,307 SF	81 feet	124 feet
Lot 3	12,525 SF	88 feet	128 feet
Lot 4	10,554 SF	85 feet	120 feet
Lot 5	10,197 SF	78 feet	100 feet
Lot 6	10,046 SF	78 feet	100 feet

J. Supplemental Provisions

10. **Annexation and street lights.** Developer and/or homeowners’ association shall, as a condition of approval, pay for all expenses related to street light energy and maintenance

costs until annexed into the City, and state that: "This approval is contingent on receipt of a final order by the Portland Boundary Commission, approving annexation of the subject property." This means, in effect, that any permits, public improvement agreements, final plats, and certificates of occupancy may not be issued until a final order is received. (ORD. 1408; ORD. 1590 § 1, 2009)

Response: This criteria does not apply to this development. The street lights are already a part of the City of West Linn.

End of Extension of Subdivision Approval SUB-07-01 Narrative Response to Criteria

NOTICE OF
SUBDIVISION
APPROVAL
EXTENSION
APPLICATION
4111 ELMRAN DR.

APPLICANT:

CANYON
DEVELOPMENT, LLC

PHONE:

503-239-2000

Neighborhood Meeting Sign-in List Placeholder

May 17, 2010

Robinwood Neighborhood Association and Neighbors within 500 feet of
4111 Elmran Drive
West Linn, Oregon 97068

Re: *Bella Flats Subdivision Approval Extension at 4111 Elmran Drive*

All interested parties,

Please find attached a notice to inform you of a chance to discuss the Subdivision Approval Extension for the project site at 4111 Elmran Drive. We plan to develop the existing (2) lots in this location into (6) separate lots each over 10,000 square feet. We received approval from the City of West Linn for this subdivision June 6th 2007. We received construction permit approvals May 5th 2010.

We will be attending the next Robinwood Neighborhood Association meeting and would like to invite you to discuss our proposal in further detail. This may not be the only topic on the agenda for this meeting.

The meeting will be held:

DATE/TIME: Tuesday June 8th, 2010, 7pm
LOCATION: Emmanuel Presbyterian Church
19200 Willamette Drive
West Linn, Oregon 97045
APPLICANT: Melynda Retallack/Garrin Royer, Canyon Development

We look forward to seeing you at the meeting or hearing from you. If you are unable to attend the meeting and wish to make additional comments, please contact your Neighborhood Association President or Designee:

Thomas Boes, President
18717 Upper Midhill Drive, West Linn Oregon 97068
presidentRNA@gmail.com
Kevin Bryck, Designee
18840 Nixon Ave. West Linn, Oregon 97068

Sincerely,
Canyon Development, LLC



Melynda Retallack, AIA, NCARB
Development and Facility Services

File: 100510.BellaFlats-NAmtg.doc

C A N Y O N D E V E L O P M E N T

PO Box 42310 Portland, OR 97242-2310 ▪ T: 503.239.2000 ▪ F: 503.296.5707

Subdivision Approval Extension Application

4111 Elmran Drive, West Linn Oregon 97068

Applicant: Garrin Royer, Canyon Development LLC
221 Molalla Ave Suite 220
Oregon City, Oregon 97045
503-239-2000
groyer@redsidedevelopment.com

Representative: Melynda Retallack, Canyon Development LLC
221 Molalla Ave Suite 220
Oregon City, Oregon 97045
503-239-2000
mel@redsidedevelopment.com

Property Information: 4111 Elmran Drive, West Linn Oregon 97068
Assessor's Map: 21E13CD, Tax Lots 3100 and 3201
Site Size: 1.62 Acres
Comprehensive Plan: Low Density Residential
Zoning: Single Family Residential, Detached, R-10

The applicant is required to apply for a Subdivision Approval Extension since the site work may not be completed within three years of the initial approval. The applicant has received construction permit approval for the public and private site improvements through the City of West Linn.

For more information, please join us at your next Neighborhood Association Meeting being held:

Tuesday June 8th, 7pm
Emmanuel Presbyterian Church
19200 Willamette Drive
West Linn, Oregon 97045



Development Features:

- Rain Gardens** Cleanses all roof and hardscape storm water run off
- Native Plants** Landscaping uses native and drought tolerant plantings that require minimal irrigation once established
- Street Bio-swales** Cleanses all public and semi-public hardscape storm water run off
- Tree Preservation** Preservation of existing large trees on the site



Siteplan

Custom Home Features:

- Efficient Fixtures** Water efficient fixtures and Energy star appliances equal immediate cost savings for the home owner
- Windows** Uses high R-value insulation and efficient windows for lower heating and cooling costs for the home owner
- Sustainable Finishes** Sustainable and recycled content finishes for the home interiors to reduce off gassing and improve indoor air quality
- Lighting** Incorporates energy efficient lighting
- Recycle Construction Waste** Construction team recycles over 95% of the construction waste

21E13CC01200
Ray & Andrea Scofield
4114 Calaroga Cir
West Linn, OR 97068

21E13CC01300
David Bean
4154 Calaroga Cir
West Linn, OR 97068

21E13CC01400
Barbara Johnson
4270 Calaroga Cir
West Linn, OR 97068

21E13CC01500
James & Berdella Elliot
4722 Calaroga Dr
West Linn, OR 97068

21E13CC01600
Jack & Wendy Gold
4778 Calaroga Dr
West Linn, OR 97068

21E13CC08600
Peggy Shaffer & Russell Coker
3950 Elmran Dr
West Linn, OR 97068

21E13CC08900
Gino & Germaine Ius
4040 Glen Ter
West Linn, OR 97068

21E13CC09000
Artan Hoxha
4064 Glen Ter
West Linn, OR 97068

21E13CD00500
Clifford & Marni Gerber
18406 Nixon Ave
West Linn, OR 97068

21E13CD00600 & 00601
Craig Jacobsen
18444 Nixon Ave
West Linn, OR 97068

21E13CD00700
Peter & Pamela Jameson
18480 Nixon Ave
West Linn, OR 97068

21E13CD00800
Martin Robert Ralston
18490 Nixon Ave
West Linn, OR 97068

21E13CD01201
Dawn Meaney
4745 Calaroga Dr
West Linn, OR 97068

21E13CD01202
Kristin McCoy
18428 Steamboat Way
West Linn, OR 97068

21E13CD01203
Patrick & Victoria Smith
4448 Mapleton Dr
West Linn, OR 97068

21E13CD01204
Ross & Connie Wescott
18463 Nixon Ave
West Linn, OR 97068

21E13CD01205
Linda Parman
18472 Steamboat Way
West Linn, OR 97068

21E13CD01206
Christopher Swigart
18489 Nixon Ave
West Linn, OR 97068

21E13CD01300
Gregory & Anne Morse
18335 Nixon Ave
West Linn, OR 97068

21E13CD01900
Randy & Linda Tomic
Po Box 522
Marylhurst, OR 97036

21E13CD02000
Robert Langman
4151 Calaroga Cir
West Linn, OR 97068

21E13CD02100
Robert Stout
18455 Steamboat Way
West Linn, OR 97068

21E13CD02200
John Milas & Gretchen Richardson
4211 Calaroga Cir
West Linn, OR 97068

21E13CD02300
Richard Manson
4233 Calaroga Cir
West Linn, OR 97068

21E13CD02400
Diane Kearns
4255 Calaroga Cir
West Linn, OR 97068

21E13CD02500
Dawn Meaney
4745 Calaroga Dr
West Linn, OR 97068

21E13CD02600
Darin Holm, Trustee
4785 Calaroga Dr
West Linn, OR 97068

21E13CD02700
Margaret Teske O'Reilly
4025 Elmran Dr
West Linn, OR 97068

21E13CD02800
David Fosberg
54876 Juniper Flats Rd
Maupin, OR 97037

21E13CD02900
James & Sheila Nichols
4073 Elmran Dr
West Linn, OR 97068

21E13CD02901
Michael & Peggy Berger
4081 Elmran Dr
West Linn, OR 97068

21E13CD03000
Donald Fisher
5882 NW Highland Pl
Corvallis, OR 97330

21E13CD03201
Canyon Development LLC
PO Box 42310
Portland, OR 97242

21E13CD03300
Richard & Sheila Wiitanen
4092 Elmran Dr
West Linn, OR 97068

21E13CD03400
Steven & Susan Schelot
4167 Glen Ter
West Linn, OR 97068

21E13CD03500
Rodney & Jeanette Greiling
4040 Elmran Dr
West Linn, OR 97068

21E13CD03600
Florence Guimary
4022 Elmran Dr
West Linn, OR 97068

21E13CD03700
Brian & Naomi Denekas
4131 Glen Ter
West Linn, OR 97068

21E13CD03800
Judith Grant
Po Box 647
West Linn, OR 97068

21E13CD03900
Tony & Linda Spears
4138 Elmran Dr
West Linn, OR 97068

21E13CD04000
Carl Kurzenberger
4148 Elmran Dr
West Linn, OR 97068

21E13CD04100
Kevin Donovan & Diana Kendall
4250 Elmran Dr
West Linn, OR 97068

21E13CD04101
Marshall & Nancy King
18687 Nixon Ave
West Linn, OR 97068

21E13CD04200
Carson Francis & Norma Weddle
18675 Nixon Ave
West Linn, OR 97068

21E13CD04300
Thomas Baker
4111 Elmran Dr
West Linn, OR 97068

21E13CD04301
Stephan & Dawn Gunther
18665 Nixon Ave
West Linn, OR 97068

21E13CD04400
Miles Miller
18595 Nixon Ave
West Linn, OR 97068

21E13CD04401 & 04501
Walter & Bonita Ostergard
4450 Elmran Dr
West Linn, OR 97068

21E13CD04500
Denaire & Scott Sommerset
PO Box 62
Wheeler, OR 97147

21E13CD04502
Neil & Susan Robins
4468 Elmran Dr
West Linn, OR 97068

21E13CD04600
Stephanie Ann Kidd
4550 Elmran Dr
West Linn, OR 97068

21E13CD04700
Diana Craine
18560 Nixon Ave
West Linn, OR 97068

21E13CD04800
Donna Louise Beckett
18670 Nixon Ave
West Linn, OR 97068

21E13CD04900
Katy & Kevan Lesser
18690 Nixon Ave
West Linn, OR 97068

21E24BA01201
Richard & Sharon Kilian
18837 Nixon Ave
West Linn, OR 97068

21E24BA01300
Esther Lorance & Michael Monical
18735 Nixon Ave
West Linn, OR 97068

21E24BA01301
David & Janice Palmer
18741 Nixon Ave
West Linn, OR 97068

21E24BA01400
Craig & Kathleen Jaeger
18705 Nixon Ave
West Linn, OR 97068

21E24BA01500
Soderquist Bell Living Trust
4552 Cedar Oak Dr
West Linn, OR 97068

21E24BA01600
Judity Citterman & David Harmon
4312 Stanford St
Chevy Chase, MD 20815

21E24BA01700
Robert Hayes, Trustee
4424 Cedar Oak Dr
West Linn, OR 97068

21E24BA01800
West Linn-Wils Sch Dist #3j
Po Box 35
West Linn, OR 97068

21E13CD02101
Janusz & Barbara Bogdan
16872 Cherry Crest Dr
Lake Oswego, OR 97034

21E13CD02102
Richard & Kelly Wilhelmi
4153 Calaroga Cir
West Linn, OR 97068

21E13CD02201 & 02202
David & Marlynn Pass
Po Box 382
West Linn, OR 97068

Thomas Boes
18717 Upper Midhill Drive
West Linn, OR 97068

Kevin Bryck
18840 Nixon Avenue
West Linn, OR 97068

Jennifer Hartung
19448 Wilderness Drive
West Linn, OR 97068

Robert Bjere
18378 Upper Midhill Drive
West Linn, OR 97068

Anthony Bracco
2716 Robinwood Way
West Linn, OR 97068

Andy Harris
2270 Arbor Drive
West Linn, OR 97068

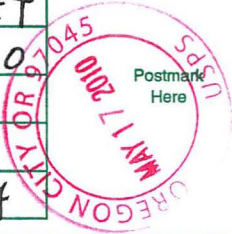
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 City, State, ZIP+4 West Linn OR 97068

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 Street, Apt. No.: **18717 Upper Midhill Dr.**
 or PO Box No.: **West Linn OR 97068**
 City, State, ZIP+4

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PLACE STICKER AT TOP OF ENVELOPE TO THE RIGHT OF THE RETURN ADDRESS, FOLD AT DOTTED LINE

SENDER: COMPLETE THIS SECTION	COMPLETE THIS SECTION ON DELIVERY	
<ul style="list-style-type: none"> ■ Complete items 1, 2, and 3. Also complete item 4 if Restricted Delivery is desired. ■ Print your name and address on the reverse so that we can return the card to you. ■ Attach this card to the back of the mailpiece, or on the front if space permits. 	A. Signature <input type="checkbox"/> Agent <input type="checkbox"/> Addressee X	
1. Article Addressed to: <p style="text-align: center; font-size: 1.2em;">Thomas Boes 18717 Upper Midhill Dr West Linn OR 97148</p>	B. Received by (Printed Name)	C. Date of Delivery
2. Article Number (Transfer from service label)	D. Is delivery address different from item 1? <input type="checkbox"/> Yes If YES, enter delivery address below: <input type="checkbox"/> No 3. Service Type <input checked="" type="checkbox"/> Certified Mail <input type="checkbox"/> Express Mail <input type="checkbox"/> Registered <input type="checkbox"/> Return Receipt for Merchandise <input type="checkbox"/> Insured Mail <input type="checkbox"/> C.O.D. 4. Restricted Delivery? (Extra Fee) <input type="checkbox"/> Yes	
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102595-02-M-1540

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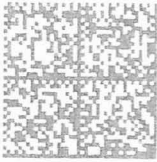
- A mailing receipt
- A unique identification number
- A record of date and time of delivery

Important Reminders

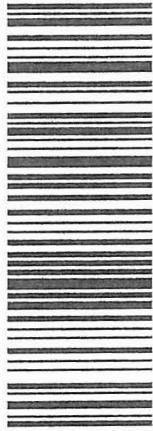
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1. Article Addressed to: <p style="text-align: center;">KEVIN BRYCK 18840 NIXON AVE. WEST Linn OR 97148</p>	B. Received by (<i>Printed Name</i>)	C. Date of Delivery
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Meeting Summary– Bella Flats Subdivision - **DRAFT**

<i>Project:</i>	Cedar Oak – Bella Flats Subdivision	<i>Meeting Date:</i>	June 8, 2010
<i>Location:</i>	4111 Elmran Drive West Linn, OR 97068	<i>Owner:</i>	Canyon Development LLC 221 Molalla Ave. Suite 220 Oregon City, Oregon 97045
<i>Distribution</i>	Robinwood Neighborhood Association City of West Linn	<i>Issued by:</i>	Melynda Retallack 2 pages

Meeting Summary from Robinwood Neighborhood Association Meeting, June 8, 2010, 7pm
Discussion pertaining to this Subdivision Approval Extension lasted approximately 35 minutes and then the Neighborhood Association moved on to the next item on their agenda.

- | <i>Date:</i> | <i>Description:</i> |
|--------------|--|
| 1.00 | Canyon Development presented the nature of the proposed development and the status of the initial Subdivision Approvals, construction permits and Extension Application. |
| 2.00 | Neighbors noted that the intersection of A Steamboat Way (Lower Elmran) and Elman Dr. with S. Cedar Oak Dr. should have better demarcation of crosswalks, it is felt that the existing conditions are unsafe for students. Methods to improve conditions may include the addition of stop signs and striping the crosswalks. The Neighborhood Association will discuss these intersection improvements further with the City of West Linn. |
| 3.00 | More off-street parking is desired by the neighbors. Events at the school necessitate parking on streets. Question of whether the design allows cars to park off the street on the grass. Noted that the current street standards for the City of West Linn do not include a parking lane on Elmran Drive. The neighbors would have preferred the school add a parking lane along their property. |
| 4.00 | Concern was raised as to whose responsibility it will be to maintain the bioswales – the City of West Linn or the homeowners. It is our understanding that the bioswales will be the responsibility of the City of West Linn, since they will be handling the stormwater from the city street. |
| 5.00 | Concern was raised about the steep hillside and performing any work close to that slope. Was noted that there is a geo-technical report available as part of the public record. |
| 6.00 | It is noted that there are no sidewalks along A Steamboat Way (lower Elmran Drive). A neighbor suggested that a Non-remonstrance Agreement be included as a condition so that the City could hold the developer responsible for future improvements. It would be intended to be included as a condition for the final approval. This motion was raised and seconded. There was internal disagreement within the Association on this issue and discussion continued later on in the meeting. It was brought to a motion, with 4 in favor and 8 against. |
| 7.00 | Concern was raised as to whom would enforce parking issues relative to keeping the streets clear for fire access. It was noted that the Fire Marshall required the houses at the end of the access drive be fully fire sprinklered. |

- 8.00 Discussed the rain gardens being provided for each property. There was some concern as to whether they would infiltrate and affect the steep slope. Upon reviewing the Civil Drawings, it was agreed that the proposed method that connects the rain gardens to the bio swales and storm system was adequate.
- 9.00 The adjacent property owner inquired as to how the trees will be selected for removal. He has a tree that he would like the City to remove as it is a hazard to his property. It was noted that as part of the conditions of approval the City arborist will be involved on site at the time of the home construction to decide whether additional trees will be removed. A letter from the Owner to the City about this issue was provided to Canyon Development for review at this meeting. Was noted that Canyon Development was aware of this letter.
- 10.00 Question was raised as to whether the signage on the site met the City of West Linn Signage standards. Neighbors noted that the signage are temporary and that there were similar size signs around the neighborhood currently.

END OF MEETING SUMMARY

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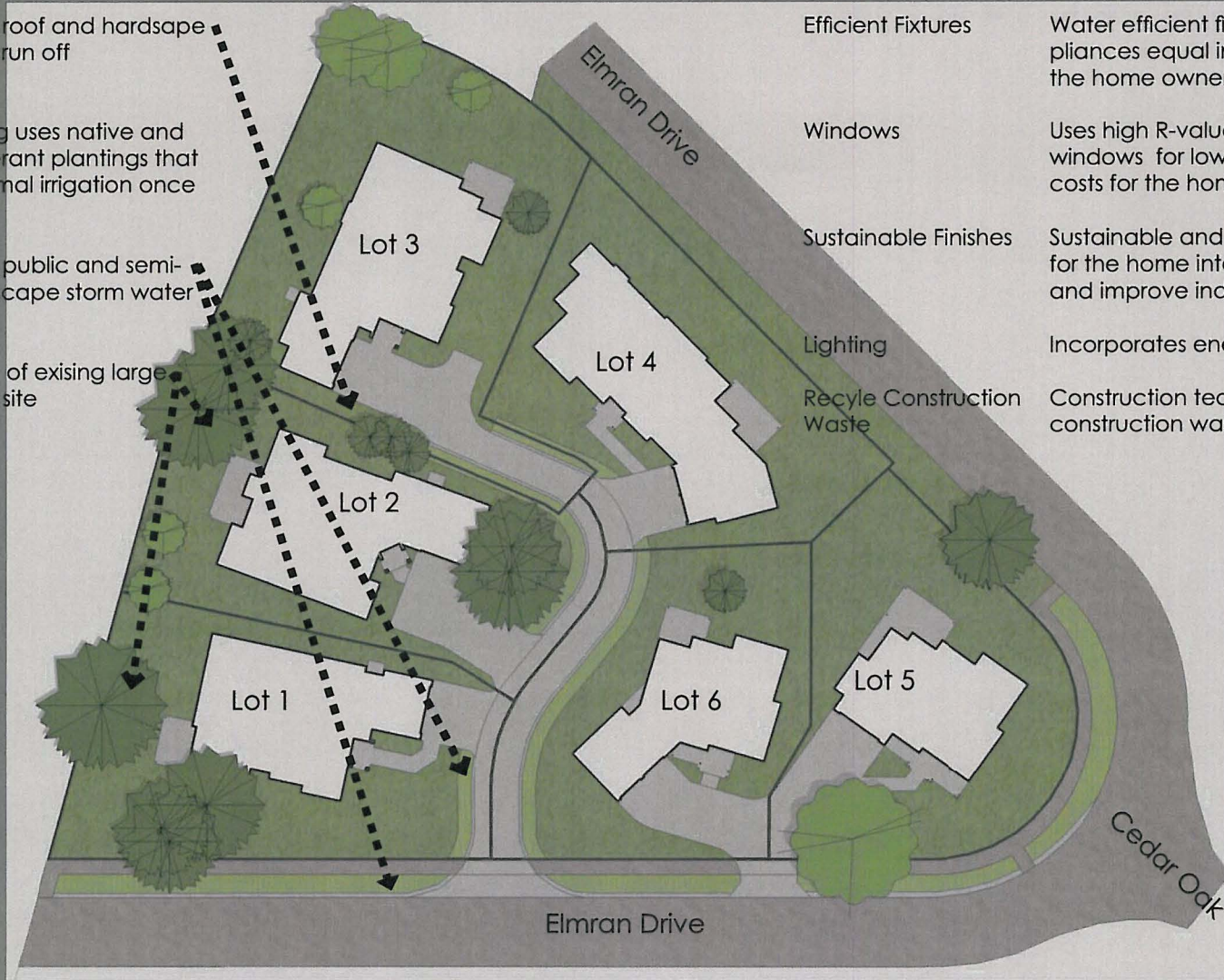
Development Features:

Rain Gardens Cleanses all roof and hardscape storm water run off

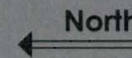
Native Plants Landscaping uses native and drought tolerant plantings that require minimal irrigation once established

Street Bio-swales Cleanses all public and semi-public hardscape storm water run off

Tree Preservation Preservation of existing large trees on the site



Siteplan



Custom Home Features:

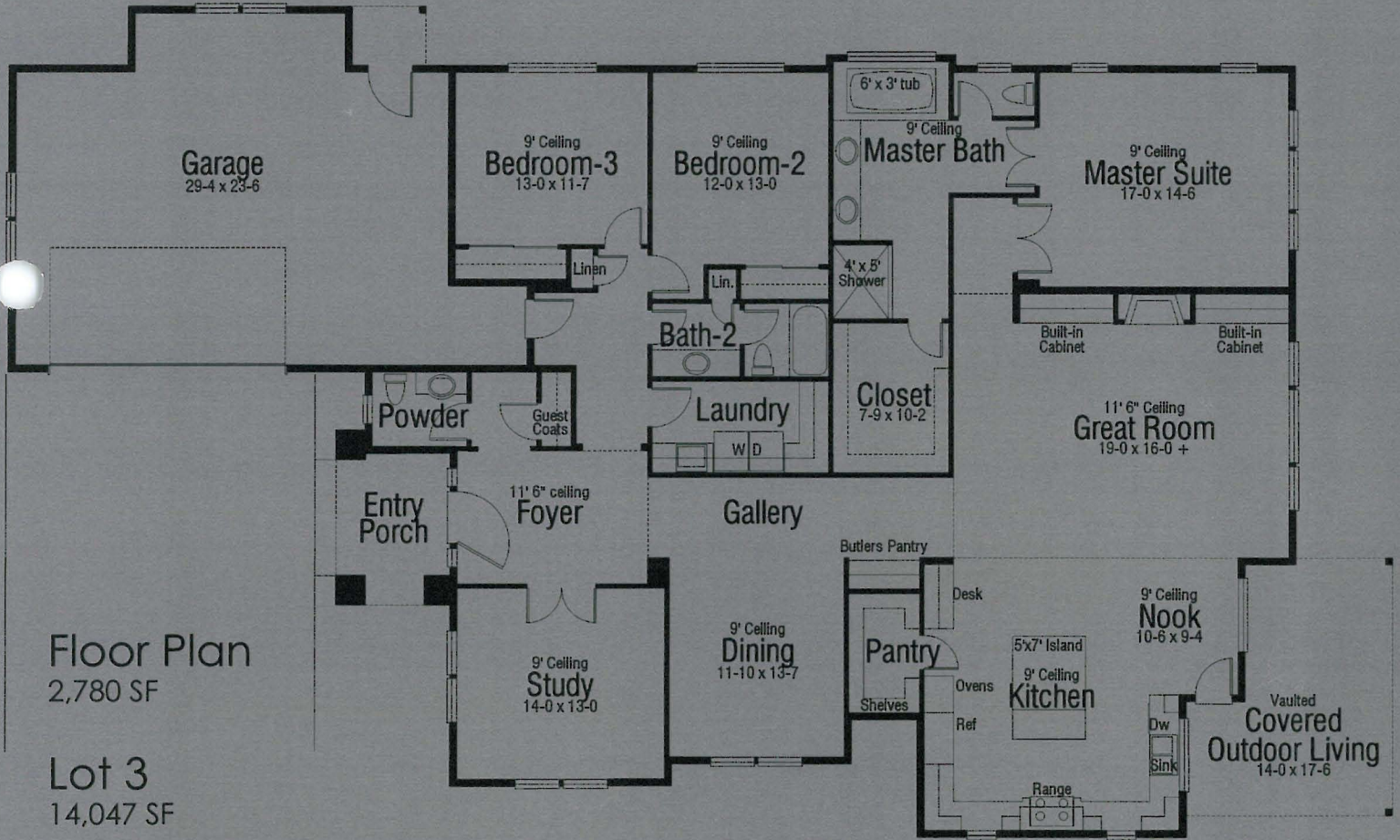
Efficient Fixtures Water efficient fixtures and Energy star appliances equal immediate cost savings for the home owner

Windows Uses high R-value insulation and efficient windows for lower heating and cooling costs for the home owner

Sustainable Finishes Sustainable and recycled content finishes for the home interiors to reduce off gassing and improve indoor air quality

Lighting Incorporates energy efficient lighting

Recycle Construction Waste Construction team recycles over 95% of the construction waste



Contact:

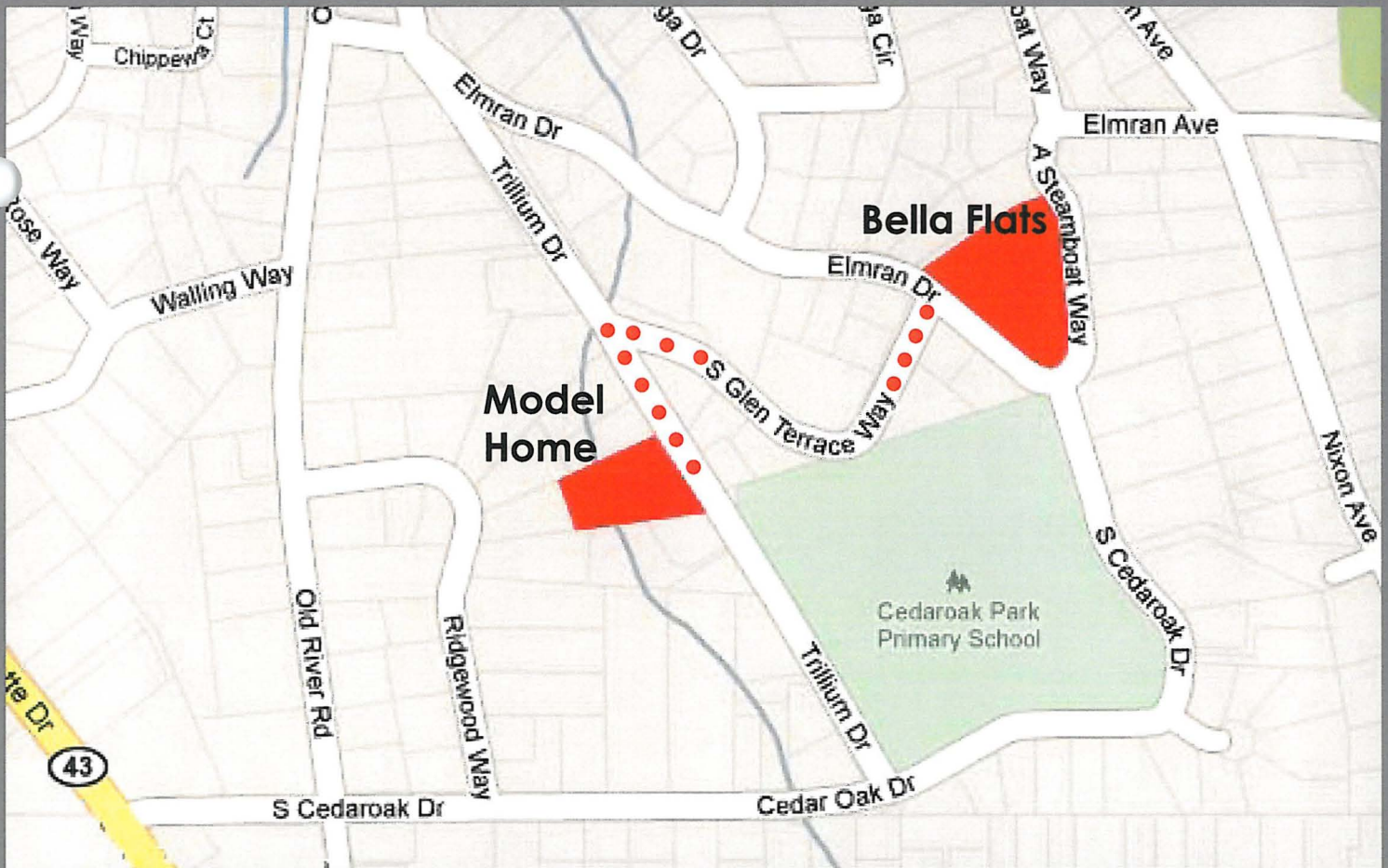
Hymark Custom Homes
Greg Sams 503.655.2466
linksetc@msn.com

Redside Development
Garrin Royer 503.239.2000
groyer@redsidedevelopment.com



BELLA FLATS CUSTOM HOMES 4111 Elmran Drive, West Linn

Directions:
Between Bella Flats and
Model Home (18811 Trillium Drive):



Please see the full sized 24x36 drawings of the site work for the proposed subdivision.

Please see the 11x17 drawings of the site work for the proposed subdivision.

Please see the attached CD .

City of West Linn
PRE-APPLICATION CONFERENCE MEETING
DRAFT
Notes
May 20, 2010

SUBJECT: Extension of previous approval for subdivision at 4111-4125 Elmran Drive

ATTENDEES: Applicants: Garrin Royer, Melinda Retallack
Staff: Tom Soppe (Planning Department), Khoi Le (Engineering Department)

The following is a summary of the meeting discussion provided to you from staff meeting notes. Additional information may be provided to address any "follow-up" items identified during the meeting. These comments are PRELIMINARY in nature. Please contact the Planning Department with any questions regarding approval criteria, submittal requirements, or any other planning-related items. Please note disclaimer statement below.

Project Details

On May 10, 2007, the Planning Commission approved Canyon Development's request for a 6-lot subdivision at 4111-4125 Elmran Drive in the Robinwood neighborhood. The subdivision request was to turn 2 existing parcels into 6 lots on a triangular shaped piece of land surrounded by Elmran Drive on the east and west and other residential parcels to the north, at the intersection of Elmran Drive and Cedaroak Drive. This was file number SUB-07-01. The decision became effective on June 6, 2007, and therefore the 3-year expiration date for the decision will be June 6, 2010.

Community Development Code (CDC) 99.325(D)(1) states "Only those applications approved between July 1, 2006 and December 31, 2009 shall be eligible for an extension." The previous application therefore qualifies the applicant to be able to apply for an extension. CDC 99.325(D)(2) allows applicants with applications that expire before June 30, 2010 to apply for extensions by June 30, 2010 even if the application has already passed its 3-year expiration by this date. Therefore, despite the 3-year expiration date set to occur on June 6, the applicant can still apply for the extension by June 30. The two-year extension, if approved, would be measured from the original expiration date. Therefore if the Extension application is approved, the expiration date would be June 6, 2012.

The two-year extension application would require the final plat to be recorded by June 6, 2012 as the two-year extension application is a new application to which all new code applies. 85.090 states "The final plat map shall be submitted to the Planning Director and

The deposit for an Extension is half the deposit of the original application. The original application had a deposit of \$5,400 so the deposit for the Extension application will be \$2,700. Any cost overruns to the Extension deposit will result in additional billings.

Once the submittal is deemed complete, the staff will schedule a hearing with the Planning Commission and will send out public notice of the hearing at least 20 days before it occurs. The Planning Commission's decision may be appealed to City Council by the applicant or anyone with standing.

Pre-application notes are void after 18 months. After 18 months with no application approved or in process, a new pre-application conference is required.

Typical land use applications can take 6-10 months from beginning to end.

DISCLAIMER: This summary discussion covers issues identified to date. It does not imply that these are the only issues. The burden of proof is on the applicant to demonstrate that all approval criteria have been met. These notes do not constitute an endorsement of the proposed application. Staff responses are based on limited material presented at this pre-application meeting. New issues, requirements, etc. could emerge as the application is developed.

Pre-app2010/Preapp 2010-5-20/PA-10-14 Elmran Drive

Another apparent error in how this was proposed and reviewed previously is that the lot sizes appear to have been calculated as if the minimum lot size is allowed to be 10,000 square feet including access easements, when actually the 10,000 square foot minimum is required to be calculated excluding any access easements. CDC Chapter 2 defines lot area as *“The total area of a lot measured in a horizontal plane within the lot boundary lines exclusive of public and private roads, and easements of access to other property or the private driveway area of a flag lot.”* (Emphasis mine) It appears lot lines will have to be adjusted to ensure that the non-access-easement areas of each lot have at least 10,000 square feet. Because of this needed change, the applicant should respond also to the criterion 85.200(B)(3) Lot Size and Shape.

Process

The Extension permit is required.

A neighborhood meeting following the provisions of 99.038 is required for an Extension permit for a subdivision per 99.325(E)(2). Contact Thomas Boes, President of the Robinwood Neighborhood Association, at (503) 699-6112 or presidentrna@gmail.com. The applicant is required to provide the neighborhood association with conceptual plans and other material at least 10 days prior to the meeting. The Extension application cannot be accepted unless the neighborhood meeting provisions are fulfilled by the time the application is submitted.

Follow the instructions under Project Details above as to which selected Chapter 85 criteria should be responded to in a narrative. The CDC is online at <http://westlinnoregon.gov/planning/community-development-code-cdc>.

Follow the submittal requirements for Chapter 85. Submittal requirements may be waived but the applicant must first identify the specific submittal requirement and request, in letter form, that it be waived by the Planning Director and must identify the specific grounds for that waiver. The waiver may or may not be granted by the Planning Director. Per above, the applicant should respond to the following criteria in 85.200 and any other criteria the applicant discovers to be necessary:

- 85.200(A)(1-5)
- 85.200(A)(10)
- 85.200(A)(22)
- 85.200(B)(2-3)
- 85.200(J)(10)

Also see the paragraph above in the Project Details section regarding which non-narrative submittal requirements have changed since the submittal of SUB-07-01.

N/A is not an acceptable response to the approval criteria. Prepare the application and submit to the Planning Department with deposit fees and signed application form.

recorded with the County within three years from the date of approval of the tentative plan, or as approved under CDC Section 99.325. If the final plat is not recorded by that time, the approval expires.” Section 99.325 is the new section of the CDC regarding Extension applications.

The extension application must respond to all criteria that may have changed since the time of the original application, and comply with any necessary changes based on this.

Chapter 85 Land Division has not changed substantially since the original application. For responses to Chapter 85 criteria, the applicant needs to respond only to those that have changed due to code amendments since the original application (and any related to errors or omissions in the review of SUB-07-01, which are addressed below). Some of the changed code sections may not have changed in the context of what they require from this particular subdivision application, in which case the applicant shall respond to the criteria by explaining this. For any that do necessitate changes in the site plan or other aspects of the subdivision proposal, in order for the proposal to meet current code, this should be explained in the narrative and plans should be altered accordingly for the submittal of the Extension application. The criteria of Chapter 85.200 that appear to have undergone code changes since the original application are (A)(1) Streets (General), (A)(3) Street Widths, (A)(22) off-site improvements, (B)(2) Block Sizes, and 85.200(J)(10) Annexation and Street Lights. If changes instigated by these affect the nature of the submittal as it relates to other criteria in 85.200 as well, than those criteria should be responded to in the narrative also, even if these other criteria themselves have not changed since the original application.

Submittal requirements that have changed since the original application was made include 85.160(D)(1) (proposed street names). This may require or suggest, in this case, a change in the proposed private street name, depending on how the proposed name was chosen. CDC 85.160(F)(3) (Transportation System Plan-related infrastructure improvements) has also changed, as has 85.170(B)(2)(F), also regarding transportation improvements. If these necessitate changes in the submittal for the Extension application as compared to the original submittal, they should be reflected in the new submittal.

CDC 99.325 states that an extension may be granted if “there are no demonstrated material misrepresentations, errors, omissions, or changes in facts that directly impact the project, including, but not limited to, existing conditions, traffic, street alignment and drainage”. One apparent error in how the project was reviewed previously was that Elmran Drive was treated as a neighborhood street when it is classified as a collector street in the City’s Transportation System Plan. Since Elmran Drive is a collector street, one more foot of dedication along the entire project frontage will be required, compared to what has been proposed and approved until this point. This should be reflected in site plan submittals and lot size calculations in the Extension submittal. Because Elmran Drive was not treated as a collector street previously, the applicant should also respond to the sections of 85.200(A) (the streets section of 85.200) that address street width and street classification, in addition to the changed sections of 85.200(A) listed above. These are 85.200(A)(2), (4), (5), and (10).

PRE-APPLICATION CONFERENCE MINUTES Bella Flats Subdivision

<i>Project:</i>	Cedar Oak – Bella Flats Subdivision	<i>Meeting Date:</i>	May 20, 2010
<i>Location:</i>	4111 Elmran Drive West Linn, OR 97068	<i>Owner:</i>	Canyon Development LLC 221 Molalla Ave. Suite 220 Oregon City, Oregon 97045
<i>Distribution:</i>	Garrin Royer, Canyon Development Jack Hoffman, Dunn Carney Don Cushing, Cushing and Associates Melynda Retallack, Canyon Development	<i>Issued by:</i>	Melynda Retallack 2 pages
<i>Attendees:</i>	Tom Soppe, City of West Linn Khoi Le, City of West Linn Jack Hoffman, Dunn Carney		Don Cushing, Cushing and Associates Melynda Retallack, Canyon Development

Attachments: Pre-application notes from project planner, Tom Soppe, dated May 20, 2010, Revised Timeline, dated May 20, 2010

The following items were discussed, not necessarily in the following order:

Date: *Description:*

- 1.01** Tom gave us a copy of his notes regarding the extension application and its requirements, attached.
- 1.02** The City of West Linn considers Elmran a “collector” street. The plans currently plan for the public street improvements per the requirements of a “local or neighborhood” street. The City requires this to be corrected. The city will require an additional 1’ of right of way (ROW) on the lot side of sidewalk. This will affect the lot sizes for Lots 1, 5 and 6 and change the plat. The pavement width can remain the same.
In addition, a “collector” street requires a more substantial cross section. We will have to increase the cross section of paving from 4” to 5” and the base rock from 12” to 14”.
Don Cushing will put together a proposal to address this change.
- 1.03** The City of West Linn requires that we correct the lot sizes to exclude the area of the new private drive, Jackson Court. Per the CDC definition of Lot Area:
Lot area. The total area of a lot measured in a horizontal plane within the lot boundary lines exclusive of public and private roads, and easements of access to other property or the private driveway area of a flag lot.
Mel has looked into the history of why the lot area was not calculated this way previously. It seems that the City wanted the property owners to be responsible for their area of the private drive, in lieu of Jackson Court being its own tract. However, it appears that per the CDC we still cannot count the area of access in the lot area as it pertains to the 10,000SF minimum requirement. This requirement will cause Lot 6 to be less than 10,000 SF, so some engineering may be required to adjust the lot boundaries.
Don Cushing will put together a proposal to address this change.

Tom noted that the only way to adjust the lot size below 10,000 SF in this R-10 zone is to go through a PUD process or obtain a variance. However, the Extension process does not allow changes to the application. In order to get a variance we

would have to go through a new application process.

Note that I also spoke to Matt Hastie today and he does not recall any way to get around this, he assumes that previously this definition was overlooked by staff.

- 1.04** Don feels he could get these changes done in 1 ½ weeks, therefore potentially we could still be on track to submit the application 6/10/10. We suggested to Khoi and Tom that we have a meeting post application submittal with all parties that review for completeness (Parks, Engineering, Fire and Planning) to try to address any questions or issues quickly and they agreed this was a possibility.
- 1.05** Mel will obtain CAD plans from the home designer to assist Don in adjusting the lot lines.
- 1.06** Don asked Khoi to discuss potentially pulling the public work permit approval immediately after the Planning Commission hearing with Dennis. Khoi will get back to us on how that process might work.
- 1.07** We need to revise the application narrative to include a response to 85.170 B.1.F. in lieu of 85.170 F.1. This section pertains to Traffic Impact Analysis that were not required for this project so the changes to this section do not affect the application. However, since this section has changed since the initial submittal we must address it as part of the Extension Application.
- 1.08** We need to add a response to 85.160 D.1:
1. Proposed name of the subdivision and streets; these names shall not duplicate nor resemble the name of any other subdivision or street in the City and shall be determined by the City Manager or designee. Street names should be easily spelled, pronounced, and of limited length. All new street names must, to the greatest extent possible, respect and be representative of the surrounding geography and existing street names. Street names should consider any prominent historical City figures or neighborhood themes that exist. Subdivision street names may not reference names of the builder or developer. (ORD. 1565)
I am not clear how the name Jackson Court was chosen. We will need to address this item in the narrative and defend the choice of this name.
- 1.09** Tom noted that our timeline was aggressive and noted that they have to give the public 20 days notice from the time an application is deemed complete to the Planning Commission Hearing date. We pointed out that if we can shorten the completion review time that potentially we can still get to a hearing 7/21/10, which could still allow us to begin construction the beginning of August.
- 1.10** Jack plans to talk to Chris Jordan again when the team has a plan of how we are proceeding to see if he will help us expedite this process.

IF YOU HAVE ANY QUESTIONS OR COMMENTS REGARDING THE ABOVE MEETING MINUTES PLEASE FEEL FREE TO CONTACT THIS OFFICE IN WRITING WITHIN ONE WEEK FOR ANY CORRECTIONS.

END OF MEETING NOTES REPORT



DEVELOPMENT REVIEW APPLICATION

TYPE OF REVIEW (Please check all boxes that apply):

- | | |
|---|---|
| <input type="checkbox"/> Annexation | <input type="checkbox"/> Non-Conforming Lots, Uses & Structures |
| <input type="checkbox"/> Appeal and Review * | <input checked="" type="checkbox"/> ^{Two} One-Year Extension * |
| <input type="checkbox"/> Conditional Use | <input type="checkbox"/> Planned Unit Development |
| <input type="checkbox"/> Design Review | <input type="checkbox"/> Pre-Application Meeting * |
| <input type="checkbox"/> Easement Vacation | <input type="checkbox"/> Quasi-Judicial Plan or Zone Change |
| <input type="checkbox"/> Extraterritorial Ext. of Utilities | <input type="checkbox"/> Street Vacation |
| <input type="checkbox"/> Final Plat or Plan | <input type="checkbox"/> Subdivision |
| <input type="checkbox"/> Flood Plain Construction | <input type="checkbox"/> Temporary Uses * |
| <input type="checkbox"/> Hillside Protection and Erosion Control | <input type="checkbox"/> Tualatin River Greenway |
| <input type="checkbox"/> Historic District Review | <input type="checkbox"/> Variance |
| <input type="checkbox"/> Legislative Plan or Change | <input type="checkbox"/> Water Resource Area Protection/Wetland |
| <input type="checkbox"/> Lot Line Adjustment */** | <input type="checkbox"/> Willamette River Greenway |
| <input type="checkbox"/> Minor Partition (Preliminary Plat or Plan) | <input type="checkbox"/> Other/Misc |

Home Occupation, Pre-Application, Sidewalk Use Application *, Permanent Sign Review *, Temporary Sign Application * require different application forms available in the forms and application section of the City Website or at City Hall.

TOTAL FEES/DEPOSIT \$ 2,700.00

<u>CANTON DR. LLC</u>	<u>221 MOLALLA AVE STE 220</u>	<u>OREGON CITY</u>	<u>OR 97045</u>	<u>503-239-2000</u>
OWNER (PRINT)	ADDRESS	CITY	ZIP	PHONE &/OR E-MAIL

<u>MELYNDA RETALLACK</u>	<u>221 MOLALLA AVE STE 220</u>	<u>OREGON CITY</u>	<u>OR 97045</u>	<u>503-239-2000</u>
APPLICANT (PRINT)	ADDRESS	CITY	ZIP	PHONE &/OR E-MAIL

<u>DON CUSHING</u>	<u>1677 SE PINEHURST AVE SUITE 201</u>	<u>MILWAUKIE</u>	<u>OR 97267</u>	<u>503-887-5331</u>
CONSULTANT (PRINT)	ADDRESS	CITY	ZIP	PHONE &/OR E-MAIL

SITE LOCATION/ADDRESS 411 ELMRAN DRIVE WEST LINN OR 97068

Assessor's Map No.: MAP 2-1E-13CD Tax Lot(s): 3100 & 3201 Total Land Area: 1.7 ACRES

- All application fees are non-refundable (excluding deposit).
- The owner/applicant or their representative should be present at all public hearings.
- A denial or approval may be reversed on appeal. No permit will be in effect until the appeal period has expired.
- Four (4) complete hard-copy sets (single sided) of application materials must be submitted with this application. One (1) complete set of digital application materials must also be submitted on CD in PDF format.
* No CD required / ** Only one copy needed

The undersigned property owner(s) hereby authorizes the filing of this application, and authorizes on site review by authorized staff. I hereby agree to comply with all code requirements applicable to my application.

SIGNATURE OF PROPERTY OWNER(S) [Signature] Date 6/9/10

SIGNATURE OF APPLICANT(S) [Signature] Date 6/9/10

ACCEPTANCE OF THIS APPLICATION DOES NOT INFER A COMPLETE SUBMITTAL. THE APPLICANT WAIVES THE RIGHT TO THE PROVISIONS OF ORS 94.020. ALL AMENDMENTS TO THE COMMUNITY DEVELOPMENT CODE AND TO OTHER REGULATIONS ADOPTED AFTER THE APPLICATION IS APPROVED SHALL BE ENFORCED WHERE APPLICABLE. APPROVED APPLICATIONS AND SUBSEQUENT DEVELOPMENT IS NOT VESTED UNDER THE PROVISIONS IN PLACE AT THE TIME OF INTIAL APPLICATION.

CONTACT: PLANNING AND BUILDING; 22500 SALAMO RD #1000; WEST LINN, OR 97068; PHONE: 656-4211 FAX: 656-4106 PLANNING@WESTLINNOREGON.GOV

CITY OF WEST LINN
 22500 Salamo Rd.
 West Linn, OR. 97068
 (503) 656-4211

PLANNING RECEIPT
 Receipt: # 935803
 Date : 06/15/2010
 Project: #MI-10-15
 BY: JN

NAME : CANYON DEVELOPMENT LLC

ADDRESS : PO BOX 42310

CITY/STATE/ZIP: PORTLAND OR 97242

PHONE # : 239-2000

SITE ADD. : 4111 ELMRAN DR / BELLA FLATS

TYPE I HOME OCCUPATIONS		HO	\$
PRE-APPLICATIONS	Level I (), Level II ()	DR	\$
HISTORIC REVIEW	Residential Major (), Minor (), New ()	DR	\$
	Commercial Major (), Minor (), New ()		
SIGN PERMIT	Face (), Temporary (), Permanent ()	DR	\$
SIDEWALK USE PERMIT		DR	\$
APPEALS	Plan. Dir. Dec. (), Subdivsion (),	DR	\$
	Plan Comm./City Coun. (), Nbhd ()		
LOT LINE ADJUSTMENT		LA	\$
CITY/METRO BUSINESS LICENSE		BL	\$


The following items are paid by billing against the up-front deposit estimate.
 If the amount of time billed to your project exceeds the amount covered by the
 deposit, additional payment may be required.

DESIGN REVIEW	Class I (), Class II ()	RD	\$
VARIANCE	Class I (), Class II ()	RD	\$
SUBDIVISION	Standard (), Expedited ()	RD	\$
ANNEXATION	"Does Not Include Election Cost"	RD	\$
CONDITIONAL USE		RD	\$
ZONE CHANGE		RD	\$
MINOR PARTITION		RD	\$
MISCELLANEOUS PLANNING		RD	\$ 2700.00
Boundry Adjustments	()		
Modification to approval	()	Water Resource	
Code Amendments	()	Area Protection	()
Comp. Plan Amendments	()	Street Vacations	()
Temporary Permit Admin.	()	Easement Vacations	()
Temporary Permit Council	()	Will. River Greenway	()
Flood Management	()	Tualatin River Grwy.	()
Inter-Gov. Agreements N/C	()	Street Name Change	()
Alter Non-Conforming Res.	()	Code Interpretations	()
Alter Non-Conforming Comm.	()	Type II Home Occ.	()
Measure 37 Claims	()	Planned Unit Dev. PUD	()

TOTAL REFUNDABLE DEPOSIT RD \$ 2700.00
 GENERAL MISCELLANEOUS Type: PM \$


 TOTAL Check # 100003 Credit Card () Cash () \$ 2700.00



 Chicago Title
Development Services
"Helping you build the future"

4111 Elmran, West Linn
21E13CD 3100
5/11/10

Tax Lots
All Street

500' Radius 

NOTICE OF
SUBDIVISION
APPROVAL
EXTENSION
APPLICATION
4111 ELMRAN DR.

APPLICANT:

CANYON
DEVELOPMENT, LLC

PHONE:

503-239-2000

Symbol	Species	Symbol	Species	Common Name	Code	Scientific Name
1	SS	26	BLM	Algar Maple	BLM	<i>Acer macrophyllum</i>
2	WRC	27	BLM	Western Red Cedar	WRC	<i>Thuja plicata</i>
3	WRC	28	BLM	Betel spuce	SB	<i>Picea sitchensis</i>
4	WRC	29	BLM	English spuce	ES	<i>Picea engelmannii</i>
5	WRC	30	BLM	Magnolia	M	<i>Magnolia cordata</i>
6	WRC	31	BLM	Holly	HO	<i>Ilex aquifolium</i>
7	SP	32	BLM	Gringo	G	<i>Quercus bicolor</i>
8	SB	33	BLM	Decodar cedar	DC	<i>Cedrus deodara</i>
9	HO	34	BLM	Oregon Ash	OA	<i>Fraxinus latifolia</i>
10	SS	35	BLM	Oregon White oak	OYO	<i>Quercus garryana</i>
11	WL	36	BLM	Douglas Fir	DF	<i>Pseudotsuga menziesii</i>
12	DM	37	BLM	Hemlock	H	<i>Cristataque macrocarpa</i>
13	SB	38	BLM	California hazelnut	HZ	<i>Corylus cornuta</i>
14	WF	39	ES	White birch	WB	<i>Betula populifolia</i>
15	DC	40	BLM	English Ivy	ET	<i>Hedera helix</i>
16	DC	41	WRC	Serviceberry	SB	<i>Amelanchier alnifolia</i>
17	M	42	BLM	Pedon oak	PO	<i>Prunus oregonensis</i>
18	O	43	SP	Willow	W	<i>Salix lasiocarpa</i>
19	DC	44	DF	Cherry	CH	<i>Prunus sp.</i>
20	ES	45	WRC	Mountain Ash	MA	<i>Sorbus aucuparia</i>
21	SP	46	DF	Horse Chestnut	HC	<i>Fagus Hippocastanum</i>
22	DC	47	BLM	Dwarfberry	DB	<i>Dryobalanus glauca</i>
23	O	48	BLM	Noble Fir	NF	<i>Abies procera</i>
24	WRC	49	HO	Pine	P	<i>Pinus contorta</i>
25	WRC			Scotch Broom	SB	<i>Cytisus scoparius</i>

DAVID D. HUNTER
 ASCA REGISTERED CONSULTING ARBORIST #408
 ISA CERTIFIED ARBORIST #PN-1058 A
 PNW ISA CERTIFIED TREE RISK ASSESSOR #0160
 PROFESSIONAL FORESTER
 PO BOX 624
 FOREST GROVE, OR 97119
 OFFICE: 503-357-4344
 CELL: 503-918-0396
 FAX: 503-357-0169
 EMAIL: DDHUNTERARBORIST@AOL.COM

- LEGEND**
- CONSTRUCTION ENTRANCE
 - TREE PROTECTION FENCE, SIZE VARIES
 - SILT FENCE
 - CONSTRUCTION FENCE
 - CATCH BASIN WITH BIO BAG
 - BIO BAG

REVISIONS

6/1/20

PROFESSIONAL
 ENGINEER
 1999
 01000
 CIVIL
 CUSHING

REVISION DATE: 12/29/11

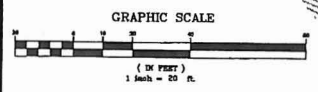
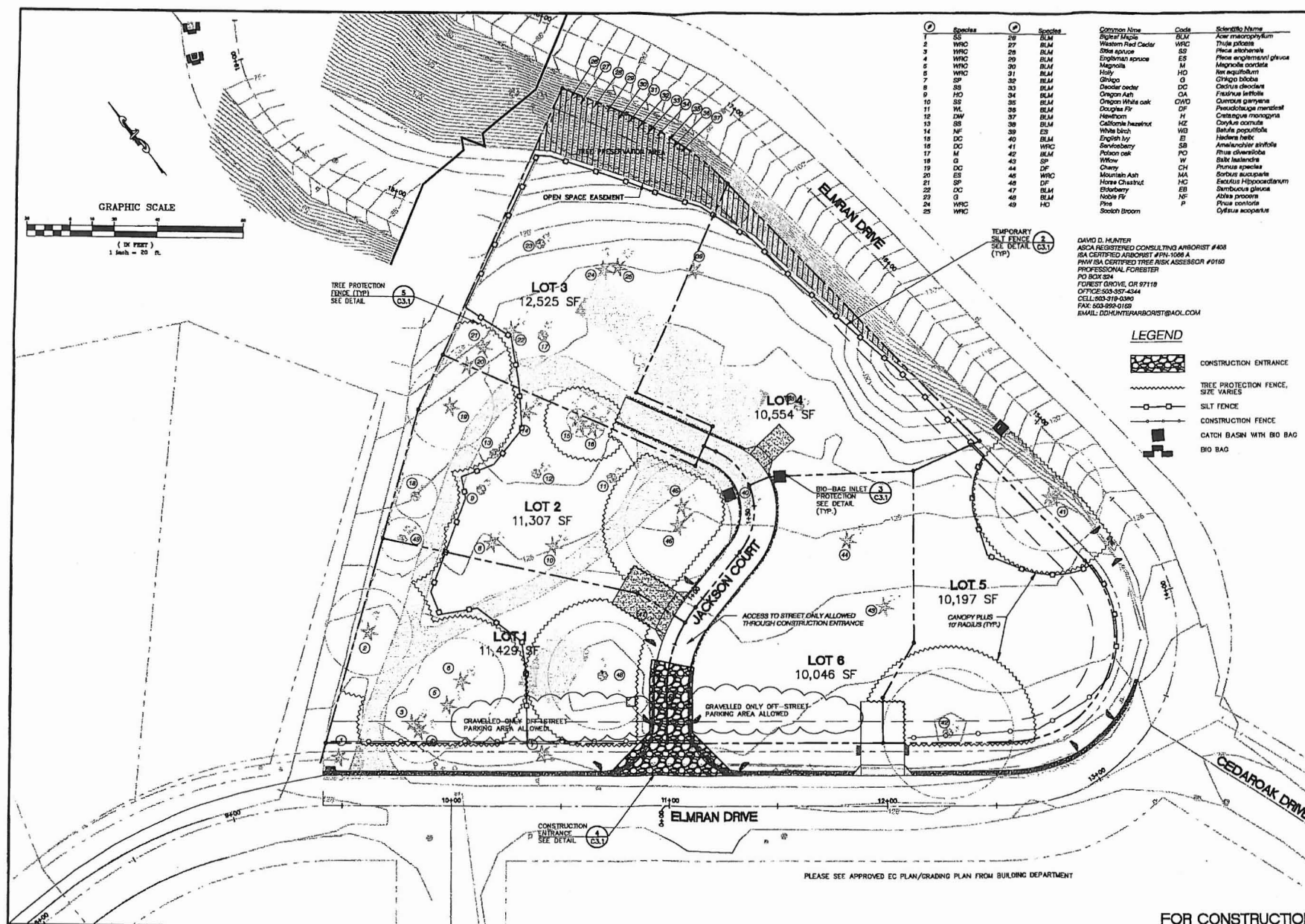
BELLA FLATS SUBDIVISION
 4TH ELMFRAN DRIVE
 WEST LINN, OREGON

EROSION CONTROL AND
 TREE PROTECTION PLAN

cushing
 Dana Cushing, Associate
 Civil Engineer
 4677 SE Pinchurst Ave
 Suite 201
 Milwaukie, OR 97267
 Voice: (503) 387-5331
 Fax: (503) 286-2164

DATE: 6/1/2010
 SCALE: 1" = 20'
 DRAWN: JEG
 JOB: 06-011
 SHEET

C3.0
 OF 19



PLEASE SEE APPROVED EC PLAN/GRADING PLAN FROM BUILDING DEPARTMENT

FOR CONSTRUCTION

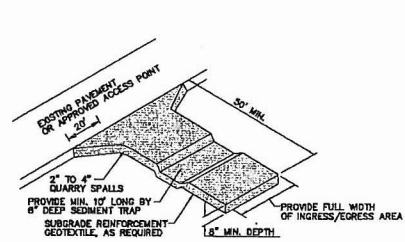
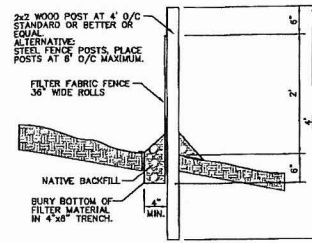
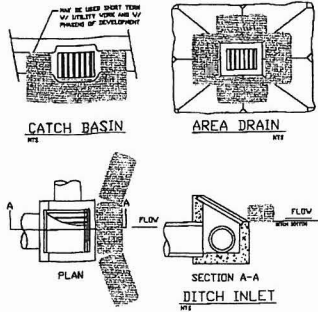
EROSION CONTROL NOTES

1. OWNER OR DESIGNATED PERSON SHALL BE RESPONSIBLE FOR PROPER INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES, IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS.
2. THE IMPLEMENTATION OF THESE ESC PLANS AND CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED BY THE LOCAL JURISDICTION, AND VEGETATION/LANDSCAPING IS ESTABLISHED.
3. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY MARKED IN THE FIELD PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE CLEARING LIMITS SHALL BE PERMITTED. THE MARKINGS SHALL BE MAINTAINED BY THE APPLICANT/CONTRACTOR FOR THE DURATION OF CONSTRUCTION.
4. THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT-LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
5. THE ESC FACILITIES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT-LADEN WATER DOES NOT LEAVE THE SITE.
6. THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.
7. AT NO TIME SHALL SEDIMENT BE ALLOWED TO ACCUMULATE MORE THAN 1/3 THE BARRIER HEIGHT. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATIONS SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNSTREAM SYSTEM.
8. STABILIZED ROCK CONSTRUCTION ENTRANCES SHALL BE INSTALLED PER DETAIL AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT.
9. STORM DRAIN INLETS, BASINS, AND AREA DRAINS SHALL BE PROTECTED UNTIL PAVEMENT SURFACES ARE COMPLETED AND/OR VEGETATION IS RE-ESTABLISHED. PAVEMENT SURFACES AND VEGETATION ARE TO BE PLACED AS RAPIDLY AS POSSIBLE.
10. SEEDING SHALL BE PERFORMED NO LATER THAN SEPTEMBER 1 FOR EACH PHASE OF CONSTRUCTION. SEEDING SHALL TAKE PLACE NO EARLIER THAN APRIL 1 AND NO LATER THAN SEPTEMBER 1.
11. IF THERE ARE EXPOSED SOILS OR SOILS NOT FULLY ESTABLISHED FROM OCTOBER 1ST THROUGH APRIL 30TH, THE WET WEATHER EROSION PREVENTION MEASURES WILL BE IN EFFECT. SEE THE CURRENT CITY OF WEST LINN'S CURRENT EROSION AND SEDIMENTATION CONTROL STANDARDS FOR ADDITIONAL WET WEATHER MEASURES.
12. THE DEVELOPER SHALL REMOVE ESC MEASURES WHEN VEGETATION IS FULLY ESTABLISHED.
13. REFER TO SECTION 208 FOR CITY OF WEST LINN SURFACE RESTORATION REQUIREMENTS. USE HOBBS & HOPKINS COMPANION MIX FOR THE GRASS SEED MIXTURE.

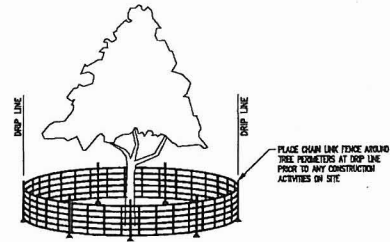
1 EROSION CONTROL NOTES + SEQUENCING
N.T.S.

2 FILTER FABRIC SEDIMENT FENCE
N.T.S.

3 BIO-BAG INLET PROTECTION
N.T.S.



4 CONSTRUCTION ENTRANCE W/ WASH
N.T.S.



5 TREE PROTECTION FENCING
N.T.S.

REVISIONS

6/21/10

REGISTERED PROFESSIONAL ENGINEER
DON CUSHING
LICENSE NO. 3506
STATE OF OREGON
EXPIRES 12/31/11

REVISION DATE: 12/21/11

BELLA FLATS SUBDIVISION
4th ELMPAN DRIVE
WEST LINN, OREGON

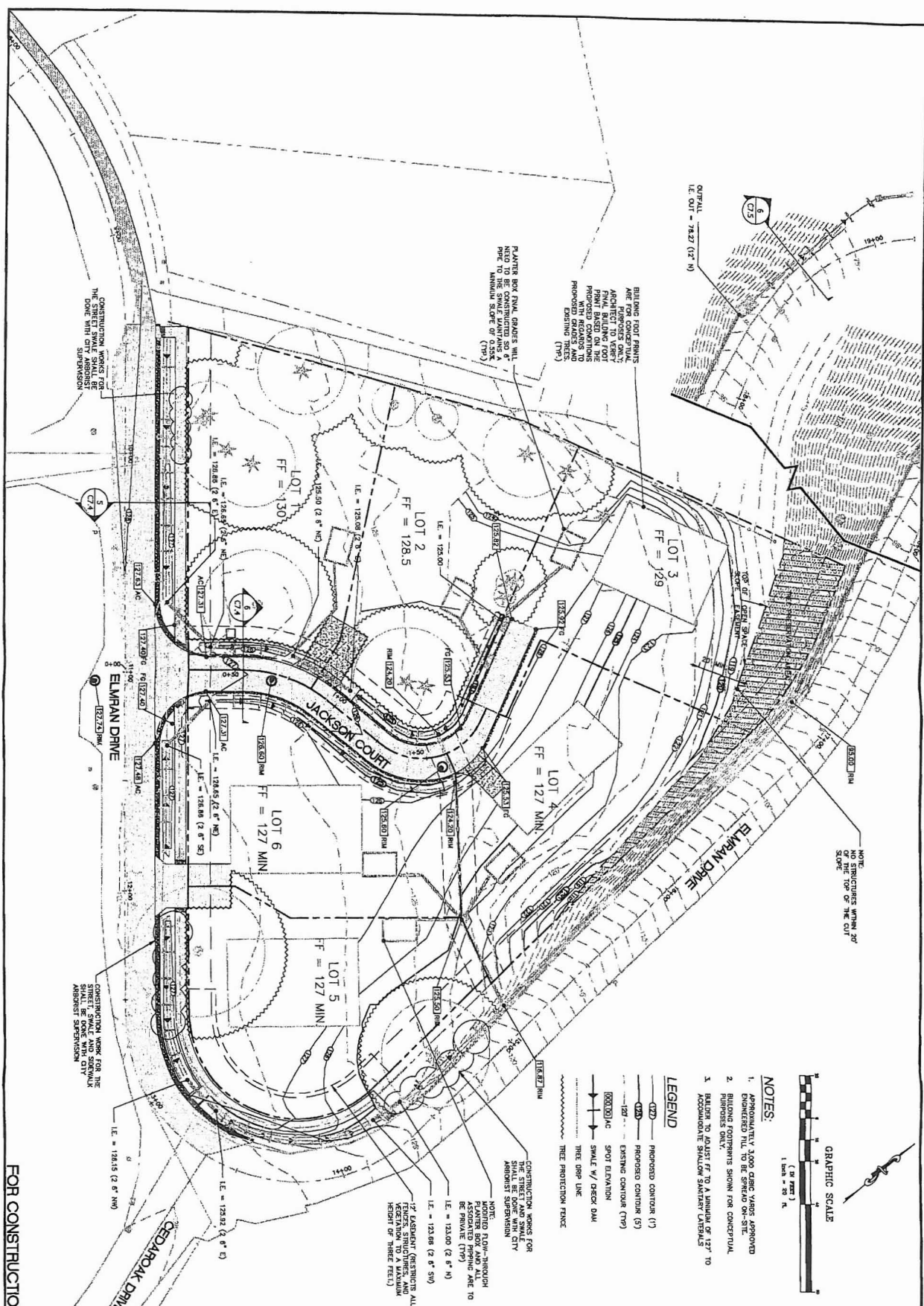
EROSION CONTROL DETAILS

cushing
Don Cushing Associates
Civil Engineers
4677 SE Pinehurst Ave
Suite 201
Milwaukie, OR 97267
Voice: (503) 387-5331
Fax: (360) 286-2164

DATE: 6/1/2010
SCALE: NONE
DRAWN: JEG
JOB: 06-011
SHEET

C3.1

FOR CONSTRUCTION



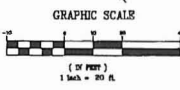
REVISIONS 1 2 3	GRADING PLAN BELLA FLATS SUBDIVISION 4111 ELMRAN DRIVE WEST LINN, OREGON		DATE: 5/7/2010 SCALE: 1" = 20' DRAWN: ESO CHECKED: C4.0 SHEET: 1 OF 18
---------------------------------	---	--	--

EXISTING CATCH BASIN
 RM = (VERIFY)
 NEW I.E. N = 89.28 (12" SW, VERIFY)
 I.E. OUT = (12" NE, VERIFY)

DITCH INLET
 RM TOP = (VERIFY)
 RM BOTTOM = 72.15 (VERIFY)
 I.E. OUT = 85.46 (12" NE, VERIFY)
 SEE DETAIL 3 ON SHEET C7.4,
 SECTION C-C

LEGEND

- EXIST. UTILITY POLE
- EXIST. UTILITY POLE WITH LIGHT
- EXIST. UTILITY POLE WITH UNDERGROUND DROP
- EXIST. GUY ANCHOR
- EXIST. OVERHEAD LINE
- EXIST. SANITARY MAN HOLE
- EXIST. SANITARY SEWER LINE
- EXIST. STORM CATCH BASIN
- EXIST. STORM SEWER LOCATES
- EXIST. TELEPHONE RISER
- EXIST. COMMUNICATION LOCATES
- EXIST. WATER METER
- EXIST. WATER VALVE
- EXIST. FIRE HYDRANT
- EXIST. WATER LOCATES
- EXIST. GAS METER
- EXIST. GAS LOCATES
- EXIST. BOLLARD
- EXIST. MAIL BOX
- EXIST. SIGN
- EXIST. EVCON
- EXIST. EVCON
- EXIST. EVCON
- EXIST. WOOD FENCE
- EXIST. VEGETATION LINE
- EXIST. PROPERTY LINE
- PROPOSED 8" x 8" ALUMINUM HOT TAPPING SLEEVE W/ THRUST RESTRAINT & 8" G.V.
- PROPOSED WATER METER
- PROPOSED FIRE HYDRANT
- PROPOSED SANITARY SEWER MANHOLE
- PROPOSED SANITARY SEWER SHALLOW MANHOLE
- PROPOSED STORM DRAIN MANHOLE
- PROPOSED CATCH BASIN
- PROPOSED DITCH INLET
- PROPOSED STORM DRAIN, SIZE
- PROPOSED STORM FLOW ARROW
- PROPOSED CHECK DAM
- PROPOSED RUP-RAP
- PROPOSED DOMESTIC WATER LINE, SIZE
- PROPOSED SANITARY SEWER, SIZE
- PROPOSED SWALE
- MODIFIED FLOW-THROUGH PLANTER (PRIVATE)
- 8" SS
- 8" SANITARY SEWER



SEWER MH DATA

- 1 SS MH OVER EXISTING LINC
 STA 0+13.3 8.84' RT
 RM = 127.74
 I.E. N = 121.89 (8" NE)
 EXISTING FLOW = 121.49 (12" VERIFY)
- 2 SS MH STA 0+77.42 5.48' RT
 RM = 128.82
 I.E. N = 122.33 (8" NE)
 I.E. OUT = 122.13 (8" SW)
- 3 SS SHALLOW MANHOLE
 STA 1+46.83 3.69' RT
 RM = 125.58
 I.E. N = 123.00 (4" E)
 I.E. N = 123.00 (4" N)
 I.E. N = 123.00 (4" SE)
 I.E. OUT = 122.80 (8" SW)
 COVER = 2.5'

STORM MH DATA

- 1 STORM MANHOLE
 RM = 123.50
 I.E. N = 118.50 (12" NW)
 I.E. N = 118.28 (8" W)
 I.E. N = 118.28 (8" S)
 I.E. N = 118.28 (8" W)
 I.E. OUT = 118.30 (12" SE)
- 2 STORM MANHOLE
 RM = 133.00
 I.E. N = 90.68 (12" S)
 I.E. OUT = 90.48 (12" N)

CATCH BASIN DATA

- 1 CATCH BASIN
 RM = 124.55
 I.E. OUT = 128.00 (12" SE)
 25 LF 8" S = 0.52X
 SEE DETAILS 1, 2, 3
 (C7.4, C7.5)
- 2 CATCH BASIN
 RM = 124.55
 I.E. N = 118.88 (12" NW)
 I.E. OUT = 118.67 (12" W)

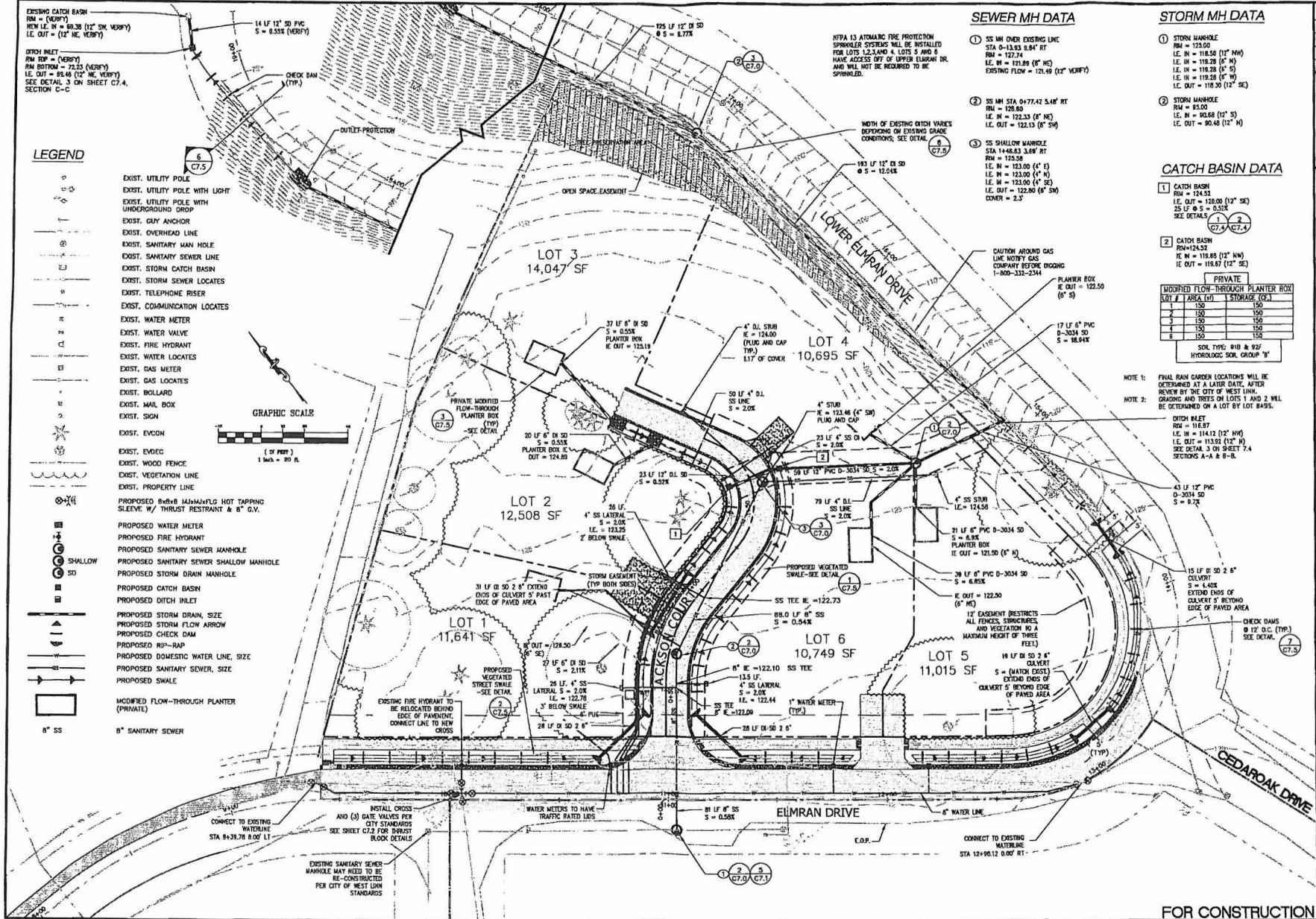
PRIVATE
 MODIFIED FLOW-THROUGH PLANTER BOX

BOX #	PLANTER BOX	STORAGE (CU FT)
1	150	150
2	150	150
3	150	150
4	150	150
5	150	150
6	150	150

SOL TYPE: #18 & #27
 HYDROLOGIC SOIL GROUP: "B"

NOTE 1: FINAL RAIN GARDEN LOCATIONS WILL BE DETERMINED AT A LATER DATE, AFTER REVIEW BY THE CITY OF WEST LINN. GRADING AND TREES ON LOTS 1 AND 2 WILL BE DETERMINED ON A LOT BY LOT BASIS.

NOTE 2: SEE DETAIL 3 ON SHEET C7.4, SECTION C-C



REVISIONS

4/1/10
 DON CUSHING
 REGISTERED PROFESSIONAL ENGINEER
 NO. 07-10000
 DIV. OF PROFESSIONAL ENGINEERING
 STATE OF OREGON
 RENEWAL DATE 12/31/11

BELLA FLATS SUBDIVISION
 4TH ELMRAN DRIVE
 WEST LINN, OREGON

UTILITY MASTER PLAN

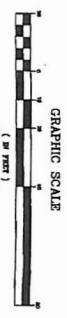
cushing
 Don Cushing Associates
 Civil Engineers
 4677 SE Piedmont Ave
 Suite 201
 Milwaukie, OR 97267
 Voice: (503) 287-5331
 Fax: (503) 286-2164

DATE: 8-1-2010
 SCALE: 1" = 20'
 DRAWN: JEG
 JOB: 08-011
 SHEET

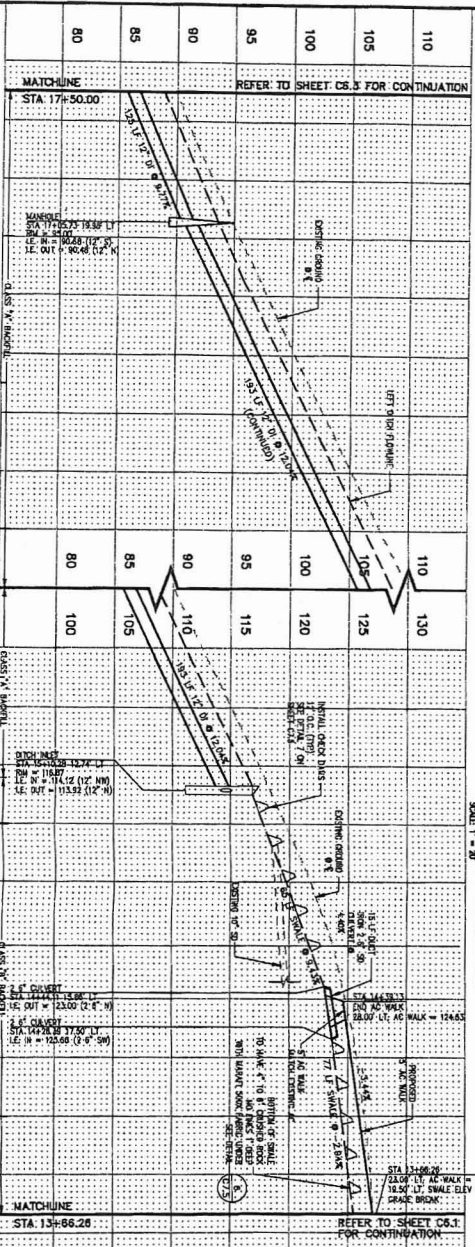
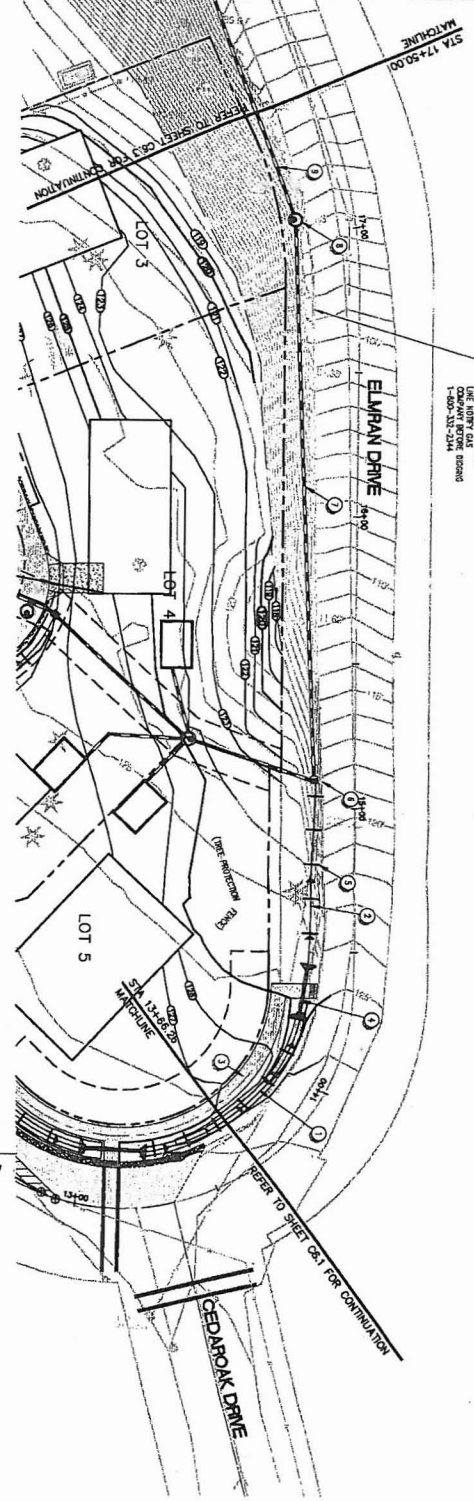
C5.0
 OF 19

FOR CONSTRUCTION

CUSHING ASSOCIATES
 CONSULTING ENGINEERS
 1-800-321-2344



- KEYNOTES**
- ① 4" UT SHALE SET RETAIN
 - ② 4" UT SHALE SET RETAIN (27.5)
 - ③ 4" UT 5" WIDE ASPHALT SIDEWALK
 - ④ 12" UT CURB FOR 5' TO 6" TALLEST PER 8' 4" MIN
 - ⑤ REINFORCED CONCRETE DRIVE
 - ⑥ REINFORCED CONCRETE DRIVE
 - ⑦ REINFORCED CONCRETE DRIVE
 - ⑧ REINFORCED CONCRETE DRIVE
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FOR CONSTRUCTION

DATE: 6/1/2000
 SCALE: 1" = 20'
 DRAWN: JEC
 CHECKED: JEC
 DESIGNED: JEC
 PROJECT: BELL FLATS SUBDIVISION
 SHEET: C6.2 OF 19

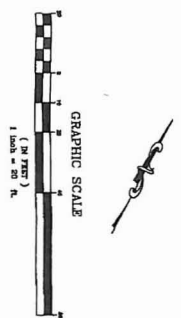
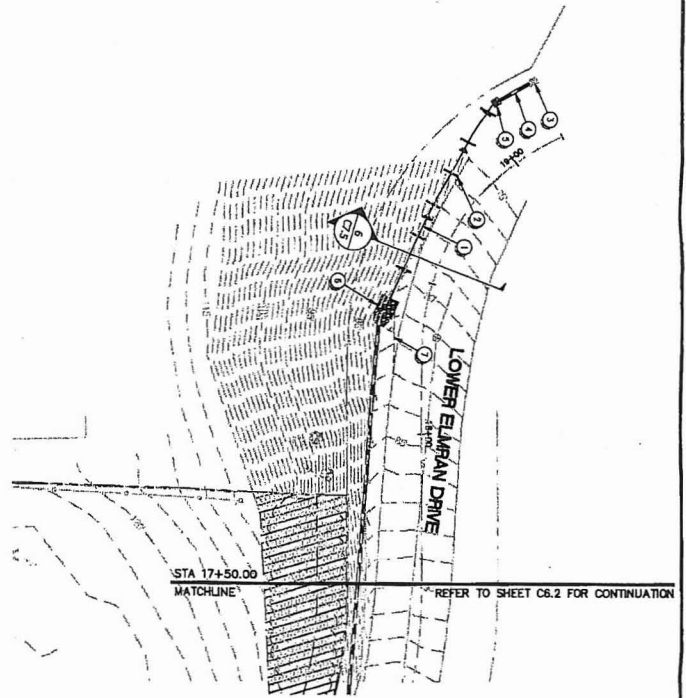
PLAN AND PROFILE
 ELMRAN DRIVE
 STA 13+66.26 TO STA 17+50.00

BELLA FLATS SUBDIVISION
 4111 ELMRAN DRIVE
 WEST LINN, OREGON

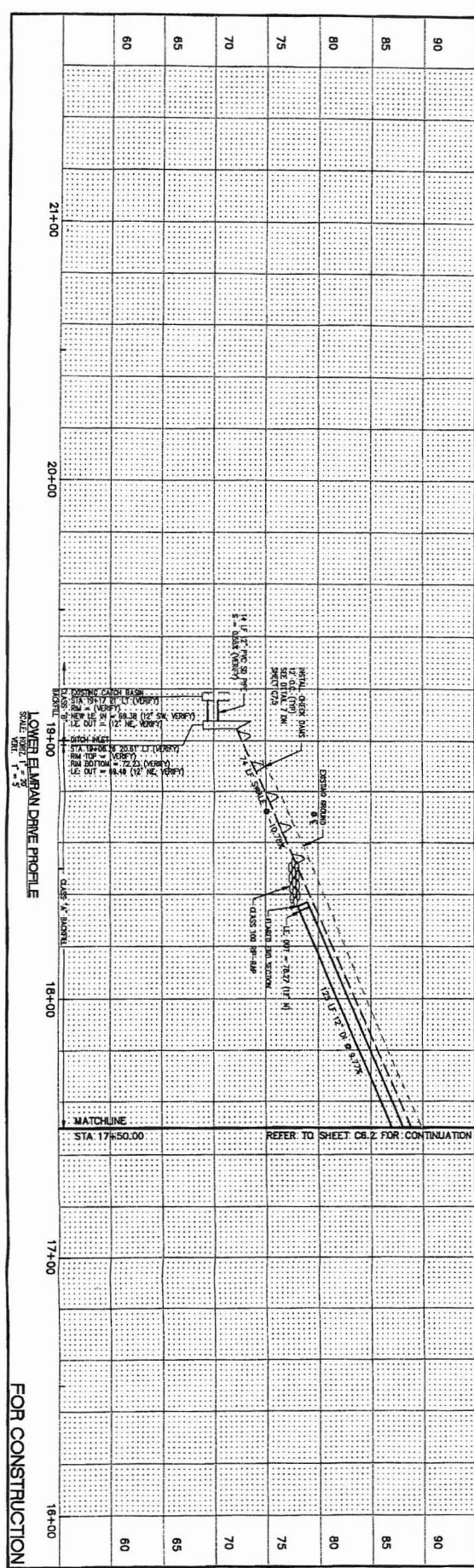
REVISIONS

NO.	DATE	DESCRIPTION

DATE: 6/1/2000
 SCALE: 1" = 20'
 DRAWN: JEC
 CHECKED: JEC
 DESIGNED: JEC
 PROJECT: BELL FLATS SUBDIVISION
 SHEET: C6.2 OF 19



- KEYNOTES**
- 1 74" U.P. SMALL SEE DETAIL (C7.1)
 - 2 12" U.P. SEE DETAIL (C7.2)
 - 3 12" U.P. SEE DETAIL (C7.3)
 - 4 12" U.P. SEE DETAIL (C7.4)
 - 5 12" U.P. SEE DETAIL (C7.5)
 - 6 12" U.P. SEE DETAIL (C7.6)
 - 7 12" U.P. SEE DETAIL (C7.7)



FOR CONSTRUCTION

C6.3
OF 19

cushing
Civil Engineering
4677 SE Peachtree Ave
Suite 201, OR 97207
Voice: (503) 387-5511
Fax: (503) 285-7104

PLAN AND PROFILE
ELMRAN DRIVE
STA 17+50.00 TO STA 19+50.00

BELLA FLATS SUBDIVISION
4111 ELMRAN DRIVE
WEST LINN, OREGON

REVISIONS

9/1/10

RECORDED & RETURNED
COUNTY CLERK
CLATSOP COUNTY, OREGON
RECORD BOOK 100
PAGE 122

12/21/11

REVISIONS

6/3/10

PROPOSED PROFESSIONAL SEAL

RENEWAL DATE 12/31/11

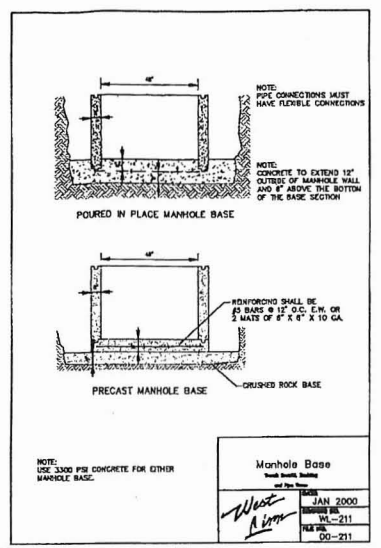
BELLA FLATS SUBDIVISION
4TH ELMFRAN DRIVE
WEST LINN, OREGON

SITE DETAILS
SANITARY

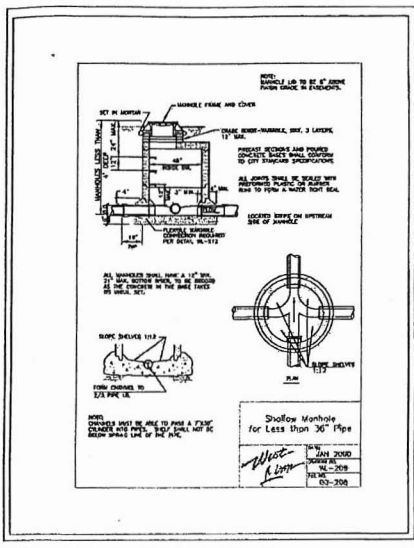
cushing
Don Cushing Associates
Civil Engineers
4677 SE Finhurst Ave
Suite 201
Milwaukie, OR 97267
Voice: (503) 387-5331
Fax: (503) 284-2164

DATE: 6/1/2010
SCALE: NONE
DRAWN: JEG
JOB: 06-011
SHEET

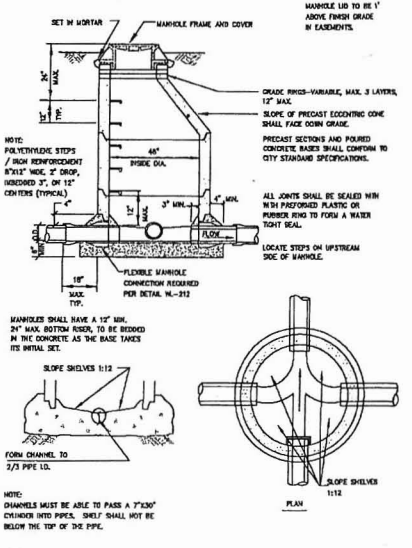
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OF 19



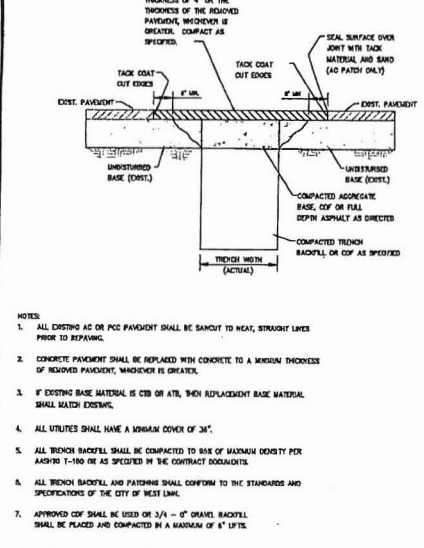
4 MANHOLE BASE WL-211
N.T.S.



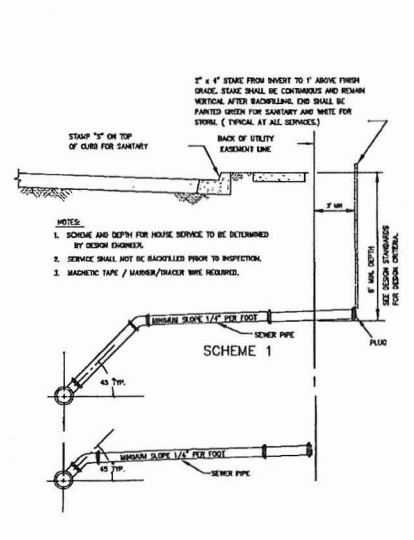
3 SHALLOW MANHOLE WL-208
N.T.S.



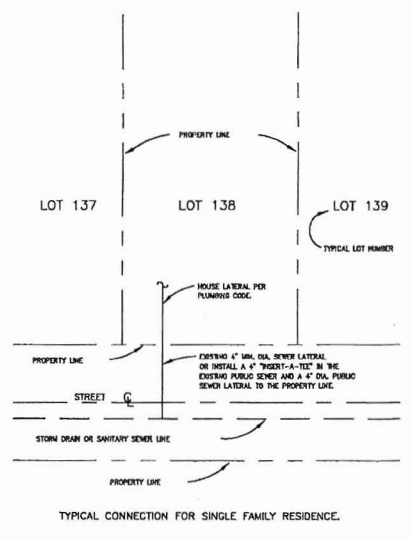
2 STANDARD MANHOLE WL-207
N.T.S.
FOR PIPES LESS THAN 36"



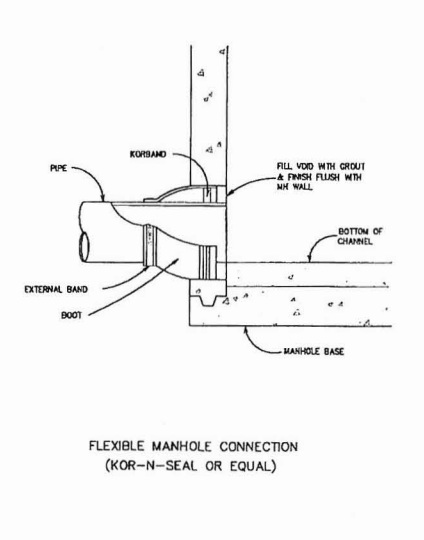
1 STREET T-CUT WL-203
N.T.S.



8 SERVICE BRANCH WL-218
N.T.S.

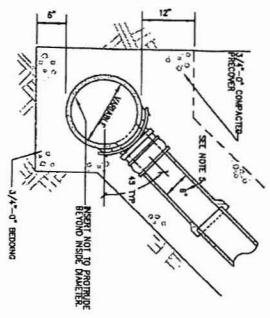


6 SEWER CONNECTION WL-216
N.T.S.
SINGLE FAMILY



5 FLEXIBLE MANHOLE CONNECTION WL-212
N.T.S.

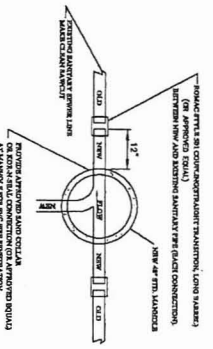
FOR CONSTRUCTION



- NOTES
1. A MINIMUM OF 24 HOURS NOTICE IS REQUIRED PRIOR TO A TAP CONNECTION. ALL TRENCHES SHALL BE SHOWN IN COMPLIANCE WITH OR-OSHA EXCAVATION RULES CHAPTER 337, SUBSECTION 37.1, ADAPTED SEPTEMBER 1, 1994. TRENCHES SHALL BE PROTECTED BY SHIELDING OR SHIELDING SHALL NOT EXCEED A 1/4\"/>
 - 2. THE TAP SHALL BE NOTICED BEFORE BEING ALLOWED AND BEFORE THE SITE SEWER CONNECTION IS MADE. THE CORE DILLED "SLURRY" IS TO BE SHOWN TO THE INSPECTOR TO INSURE IT WAS REMOVED FROM THE SEWER LINE.
 - 3. ALL SERVICE LINE CONNECTIONS SHALL BE MADE WITH AN APPROVED CONNECTOR MANUFACTURED AND DESIGNED TO CONNECT TO A CORE DILLED PIPE. FORMER 1/2\"/>
 - 4. THE EXTENSION OF TAP IS TO BE ABOVE THE SPRINGLINE.
 - 5. 4" MAXIMUM TAP FOR 8" MAIN (GUT-N-TEE) IS TO BE USED FOR 6" HORSE BRANCH ON 8" MAIN.
 - 6. 4" HORSE BRANCH MAY BE USED FOR SINGLE FAMILY LOTS ONLY.

1 SEWER SERVICE TAP WL-303

N.T.S.
TO SHOWING DIMENSIONS FOR HOUSE LAYOUTS



NOTE: PER LOCAL ORDINANCE, ALL TAP CONNECTIONS SHALL BE MADE WITH AN APPROVED CONNECTOR MANUFACTURED AND DESIGNED TO CONNECT TO A CORE DILLED PIPE. FORMER 1/2\"/>

5 NEW MANHOLE OVER EXIST. LINE WL-210

N.T.S.

NOTE: PER LOCAL ORDINANCE, ALL TAP CONNECTIONS SHALL BE MADE WITH AN APPROVED CONNECTOR MANUFACTURED AND DESIGNED TO CONNECT TO A CORE DILLED PIPE. FORMER 1/2\"/>

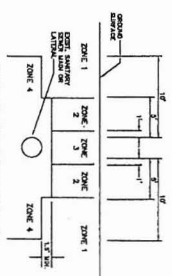
MEASUREMENTS TO PROPERTY LINE
NEW MANHOLE TO EXIST. TRENCH
EXIST. TRENCH TO PROPERTY LINE
EXIST. TRENCH TO EXIST. TRENCH
EXIST. TRENCH TO EXIST. TRENCH
EXIST. TRENCH TO EXIST. TRENCH

JAN 2000
9/1-110
04-210

2 SANITARY SEWER CONNECTION WL-304

N.T.S.

TO SHOWING DIMENSIONS FOR HOUSE LAYOUTS



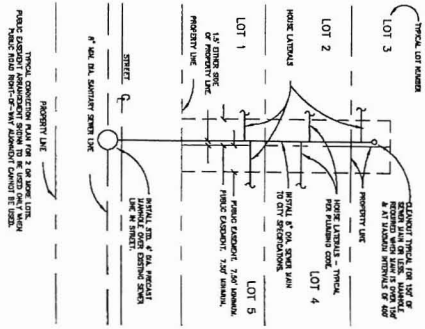
NOTE: PER LOCAL ORDINANCE, ALL TAP CONNECTIONS SHALL BE MADE WITH AN APPROVED CONNECTOR MANUFACTURED AND DESIGNED TO CONNECT TO A CORE DILLED PIPE. FORMER 1/2\"/>

6 STANDARD SANITARY CROSSING WL-409

N.T.S.

MEASUREMENTS TO PROPERTY LINE
NEW MANHOLE TO EXIST. TRENCH
EXIST. TRENCH TO PROPERTY LINE
EXIST. TRENCH TO EXIST. TRENCH
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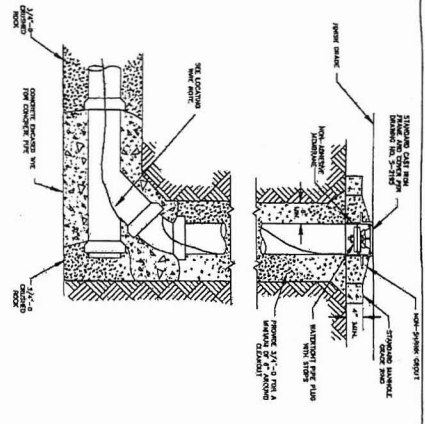
JAN 2000
9/1-110
04-210



NOTE: PER LOCAL ORDINANCE, ALL TAP CONNECTIONS SHALL BE MADE WITH AN APPROVED CONNECTOR MANUFACTURED AND DESIGNED TO CONNECT TO A CORE DILLED PIPE. FORMER 1/2\"/>

3 CLEANOUT DETAIL WL-206

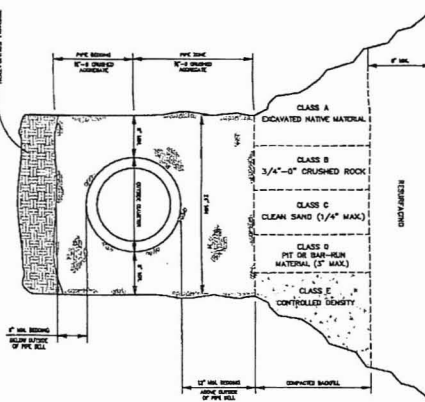
N.T.S.



NOTE: PER LOCAL ORDINANCE, ALL TAP CONNECTIONS SHALL BE MADE WITH AN APPROVED CONNECTOR MANUFACTURED AND DESIGNED TO CONNECT TO A CORE DILLED PIPE. FORMER 1/2\"/>

4 TRENCH BACKFILL, BEDDING, AND PIPE ZONE

N.T.S.

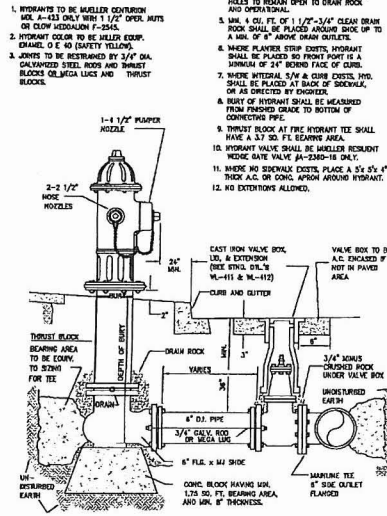


NOTE: PER LOCAL ORDINANCE, ALL TAP CONNECTIONS SHALL BE MADE WITH AN APPROVED CONNECTOR MANUFACTURED AND DESIGNED TO CONNECT TO A CORE DILLED PIPE. FORMER 1/2\"/>

FOR CONSTRUCTION

<p>REVISIONS</p> <p>DATE: 8/1/2010</p> <p>SCALE: NONE</p> <p>SHEET: 01</p> <p>JOB: 08-011</p> <p>BY: BLMK, JEG</p>	<p>CUSHING</p> <p>Don Cushing Associates Civil Engineers 4677 SE Hubbard Ave Milwaukie, OR 97267 Voice: (503) 347-5311 Fax: (503) 248-2144</p>	<p>SITE DETAILS SANITARY</p>	<p>BELLA FLATS SUBDIVISION 4111 ELMRAN DRIVE WEST LINN, OREGON</p>	<p>RECORDED INDEXED CLERK MAY 12/2011</p>

NOTES:



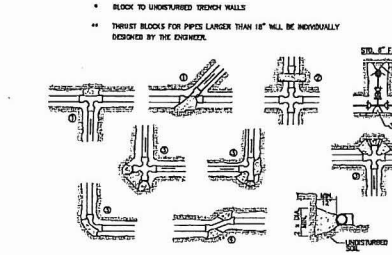
1 STANDARD FIRE HYDRANT ASSEMBLY WL-401
N.T.S.

1. FITTINGS TO BE Mueller CONFORMER NO. A-43 ONLY WITH 1 1/2" OPER. JOINTS OR CLOW HYDRANTION F-254S.
2. HYDRANT COLOR TO BE MILLER DEER. EXHAUST TO 40 (SAFETY YELLOW).
3. JOINTS TO BE RESTORED BY 3/4" DIA. GALVANIZED STEEL RODS AND WASHERS BLOCKS OR MICA LUGS AND THRUST BLOCKS.
4. ALL FITTINGS IN CONTACT W/CONCRETE SHALL BE WRAPPED IN PLASTIC. HYDRANT DRAIN HOSES TO REMAIN OPEN TO DRAIN ROCK AND OPERATIONAL.
5. MIN. 4' DIA. OF 1 1/2" 3/4" CLEAR DRAIN ROCK SHALL BE PLACED AROUND SHOKE UP TO A MIN. OF 6" ABOVE DRAIN CHUTE.
6. WAREE PLAYER STRIP EXITS, HYDRANT SHALL BE PLACED 90 FROM HOSE TO A JOURNAL OF 2" BORED FACE OF CURB.
7. WAREE INTERNAL 5/8" & OMBX EXITS, WHO. SHALL BE PLACED AT BACK OF SIDEWALK OR AS DIRECTED BY ENGINEER.
8. BUST OF HYDRANT SHALL BE WEARERD FROM FINISHED GRADE TO BOTTOM OF CONNECTING PIPE.
9. THRUST BLOCK AT FIRE HYDRANT TEE SHALL HAVE A 37 SQ. FT. BEARING AREA.
10. HYDRANT VALVE SHALL BE MILLER RECURRENT WAREE RATE VALVE W-240-18 ONLY.
11. WHERE NO SIDEWALK EXISTS PLACE A 36 X 24" THICK SLAB OF CONC. AROUND HYDRANT.
12. NO EXTORTIONS ALLOWED.

FITTING SIZE (NOMINAL)	TEE, WYE, HYDRANTS	BRANDADE BLOCK	10" BORED PAVED DRIVE	48" BORED	22 1/2" BORED	11 1/2" BORED
2	*	*	*	*	*	*
4	1.7	2.1	3.6	1.3	7	1
6	3.7	4.9	8.3	2.9	1.5	1
8	6.7	8.7	15.3	5.1	2.7	1.3
10	12.5	13.8	24.8	8	4.1	1
12	18.1	18.8	31.3	11.8	5.0	1.8
14						
16	28.9	31.8	37.9	20.5	10.4	5.2
18	33.9	40	47.8	28.1	12.8	6.7
LARGER	**	**	**	**	**	**

BEARING AREA OF THRUST BLOCKS (SQ. FT.)

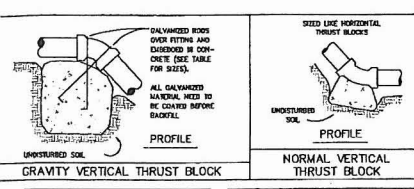
1. ALL VALUES ARE BASED ON THE FOLLOWING ASSUMPTIONS:
A. WIND PRESSURE = 100 PSF ± 2 (WINDY WINDS)
B. 1000 PPF SOIL BEARING CAPACITY
C. NORMAL DISTRIBUTION WIND VELOCITY NOT TO EXCEED 8 1/2 MPH
2. ALL FITTINGS SHALL BE WRAPPED IN PLASTIC PRIOR TO PLACEMENT OF CONCRETE.
3. BEARING SURFACE OF THRUST BLOCKING SHALL BE AGAINST UNDISTURBED SOIL.
4. ALL CONCRETE SHALL HAVE A MIN. 28 DAY STRENGTH OF 3000 PSI.
5. ALL PIPE ZONES SHALL BE GRAVEL FILLED AND COMPACTED.
6. THRUST BLOCKS FOR PAVED DRIVE AND PAVED TEE SHALL HAVE A MIN. 48" LIFTING LOOPS INSTALLED AS SHOWN.
7. VERTICAL THRUST DETAILS-SEE DWG. WL-402.
8. STRIKABLE BLOCK DETAILS-SEE DWG. WL-403.



2 HORIZONTAL THRUST BLOCKING WL-406
N.T.S.

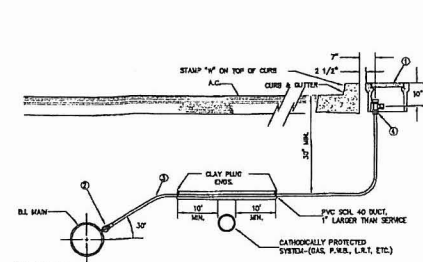
NOTES:

1. GRAVITY VERTICAL THRUST BLOCKS SHALL BE DESIGNED BY THE ENGINEER.
2. KEEP CONCRETE CLEAR OF JOINT AND JOINT ACCESSORIES. FITTINGS SHALL BE WRAPPED IN PLASTIC PRIOR TO PLACEMENT OF CONCRETE.
3. CONCRETE THRUST BLOCKING SHALL BE POURED AGAINST UNDISTURBED EARTH.
4. CONCRETE LAB SHALL HAVE A MIN. 28 DAY STRENGTH OF 3000 P.S.I.
5. THRUST BLOCK VOLUMES FOR VERTICAL, SLOPE BEARING UPWARD RESISTANT THRUSTS ARE BASED ON TEST PRESSURE OF 150 P.S.I. AND THE MOCHT OF CONCRETE = 4000 LBS./CU.YD.
6. VERTICAL BLOCKS THAT REQUIRE A THRUST BLOCK VOLUME EXCEEDING 5 CU.YD. YARDS REQUIRE SPECIAL BLOCKING DETAILS. SEE PLANS FOR VOLUMES SHOWN IN THIS HEAVY LINE IN TABLE.
7. PAYMENT SHALL BE THE SAME AS FOR HORIZONTAL THRUST BLOCKS.
8. ALL REBAR SHALL BE GALVANIZED BY ACCORDANCE WITH ASTM-113 (MIN. 2.4 MIL). REBAR SHALL BE BENT BEFORE GALVANIZING, AND LAST 4" OF BAR SHALL BE BENT 90 DEGREES WITH A 1/2" RADIUS BEND. REBAR SHALL BE WORKY FIT TO RESTRAINED FITTING.
9. FOR HORIZONTAL THRUST BLOCK DETAILS SEE DWG. WL-404.



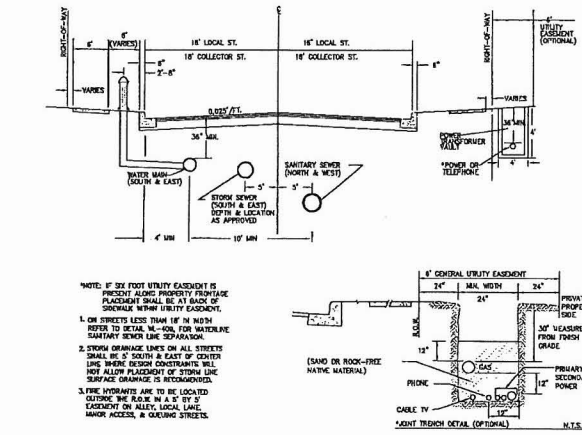
FITTING SIZE	VOLUME OF THRUST BLOCK IN CU.YD. YARDS (VERTICAL BENDS)		
	45°	22 1/2°	11 1/4°
4	1.1	0.6	0.3
6	2.7	1.0	0.4
8	4.0	1.6	0.6
10	6.0	2.3	0.9
12	8.5	3.2	1.3
14	11.5	4.3	1.8
16	14.8	5.8	2.3

3 VERTICAL THRUST BLOCKING WL-407
N.T.S.

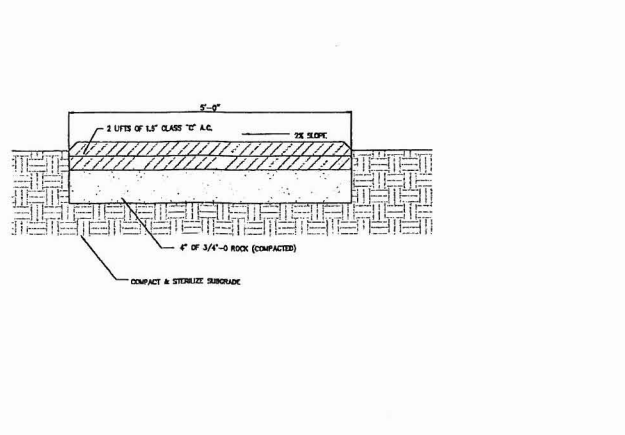


- MATERIALS:
1. BROOKS METER BOX BODY NO. 37, UD. AND COVER NO. 37-S.
 2. MILLER COMP. STOP NO. H-1500A OR FORD 1500-40 SET CORR. STOP WITH OPERATING NUT AT J ON B O'LOCK.
 3. 1" SOFT TEEPER, TYPE "K" COPPER TUBING COMPLIANT WITH ASTM B-88.
 4. MILLER ANGLE METER STOP NO. H-1525A (FORM NO. H-1525-0).
- NOTES:
1. MACHINER DRILLED AND TAPPED ONLY. NO HAND DRILLING IS ALLOWED.
 1. SUBSTITUTES FOR ANY MATERIALS SHOWN SHALL BE APPROVED BY THE CITY ENGINEER.
 2. ALL PIPE AND STRUCTURE ZONES SHALL BE BACKFILLED USING 3/4" MINUS CRUSHED AGG. AND COMPACTED TO 95% MAX DENSITY AS DETERMINED BY ASTM D-155.
 3. WHEN AN ACTIVE CATHODIC PROTECTED SYSTEM IS DISCONTINUED, SOIL 40 PVC SHALL BE INSTALLED AS SHOWN ABOVE WITH CLAY PLUG.
 4. METER BOX SHALL BE COVERED OVER THE COMPLETED METER ASSEMBLY.
 5. FOR VACANT RESIDENTIAL LOTS, LOCATE SINGLE SERVICE 18" INSIDE SIDE LOT LINE.
 6. TAPS INTO MAIN TO BE AT 18" CENTERLINE MIN.
 7. ANGLE METER STOPS TO BE 18" FROM PROPERTY LINE AND NOT IN SIDEWALK APPROACH.

4 STANDARD 1' WATER SERVICE WL-402
N.T.S.



5 TYPICAL UTILITY PLACEMENT WL-500
N.T.S.



6 PEDESTRIAN PATH / BIKEWAY WL-510
N.T.S.

REVISIONS

APPROVED PROFESSIONAL ENGINEER
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Civil Engineers
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Suite 211
Milwaukie, OR 97127
Voice: (503) 287-5331
Fax: (503) 286-2164

REVISION DATE 12/31/11

BELLA FLATS SUBDIVISION
4111 ELMIRAN DRIVE
WEST LINN, OREGON

SITE DETAILS
WATER AND STREET

DATE: 6/1/2010
SCALE: NONE
DRAWN: JEC
JOB: 06-011
SHEET

C7.2
OF 19
FOR CONSTRUCTION

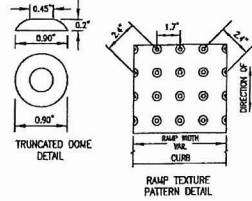


CROSSWALK - SPACE TWO WHITE LINES AS SHOWN ON PLANS. CROSSWALK SHALL BE HOT-LAID THERMOPLASTIC PAVEMENT MARKING MATERIAL.

NOTES:

1. LOCATE STOP BARS 10' BACK OF THE EXTENDED FOG LINE, EDGE OF PAVEMENT, OR CURB FACE. VERIFY SIGHT DISTANCE.
2. LOCATE CROSSWALKS AS PER WHEELCHAIR RAMP LOCATIONS OR 5' BACK OF EXTENDED FOGLINE, EDGE OF PAVEMENT OR CURB FACE.
3. REMOVAL OF EXISTING STRIPING IS TO BE DETERMINED IN THE FIELD. STRIPING SHALL BE GROUND OFF AS DIRECTED BY THE ENGINEER.
4. ALL THERMOPLASTIC PAVEMENT MARKING MATERIAL SHALL BE INSTALLED AS PER SECTION 0055 OF THE OREGON STANDARD SPECIFICATIONS FOR HWY. CONSTRUCTION.
5. ALL PAVEMENT MARKING SHALL CONFORM TO THE MOST CURRENT ODOT SPECIFICATION. COPIES OF THE MATERIALS SPECIFICATIONS ARE AVAILABLE FROM:
ENGINEER OF MATERIALS AND RESEARCH
HWY. MATERIALS LABORATORY
600 AIRPORT ROAD S.E.
SALEM, OR, 97310
(503) 966-3100
6. THE ENGINEER MUST OBTAIN AUTHORIZATION FOR PLACEMENT OF "RAILROAD CROSSING" MARKINGS PRIOR TO PLAN APPROVAL. AUTHORIZATION SHALL BE OBTAINED FROM THE ODOT RAIL SAFETY SECTION.

* LANE MARKING DIMENSION LOCATION AT ϵ OF STRIPING UNLESS NOTED OTHERWISE.



NOTE:
PLACE TRUNCATED DOME DETECTABLE WARNING TEXTURE IN THE LOWER 2 1/4" OF THE BROAD OF THE RAMP ONLY. AVERAGE DOMES LOCATED IN LINE PATTERN ONLY AS SHOWN IN DETAIL. COLOR OF TEXTURE TO BE SAFETY YELLOW. FOR CONSTRUCTION OF SIDEWALK BARRIERS OUTSIDE OF PUBLIC RIGHT-OF-WAY, CHECK WITH STATE BUILDING CODES.

1 STRIPING DETAIL
N.T.S.

2 RAMP TEXTURE DETAIL
N.T.S.

REVISIONS

6/13/10

RENEWAL DATE 12/31/11

BELLA FLATS SUBDIVISION
4111 ELMFRAN DRIVE
WEST LINN, OREGON

SITE DETAILS
WATER AND STREET

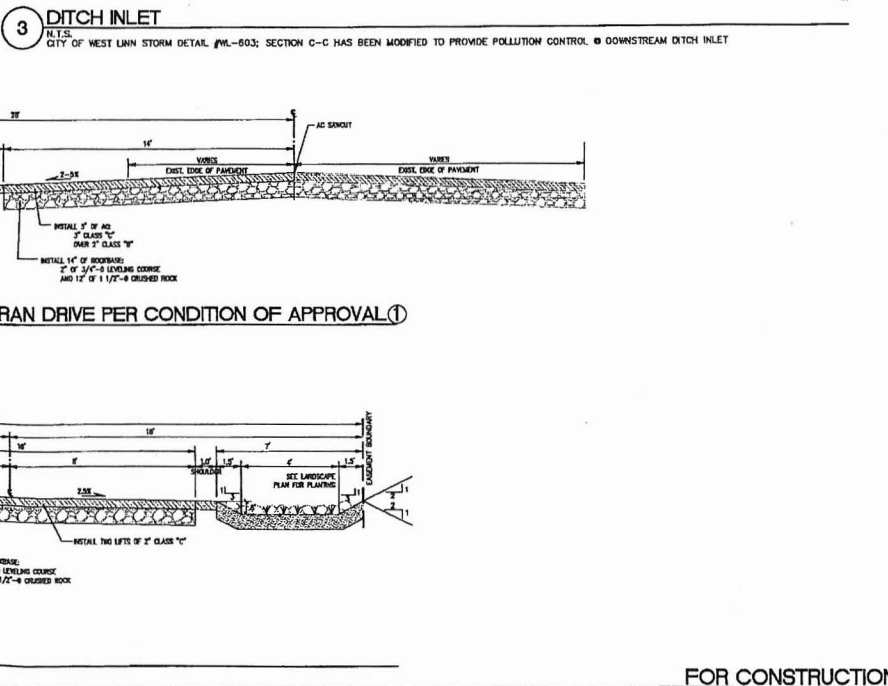
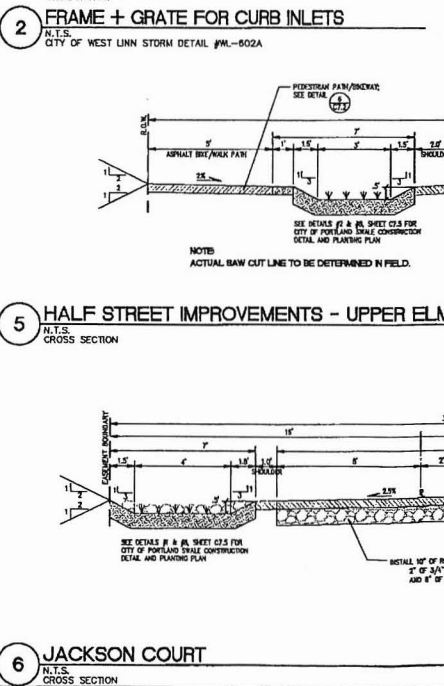
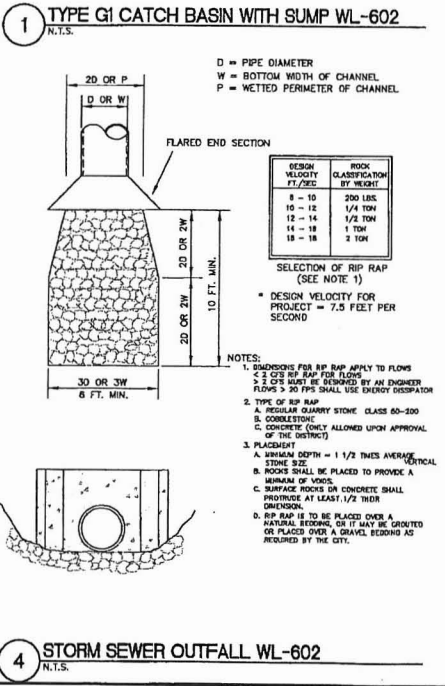
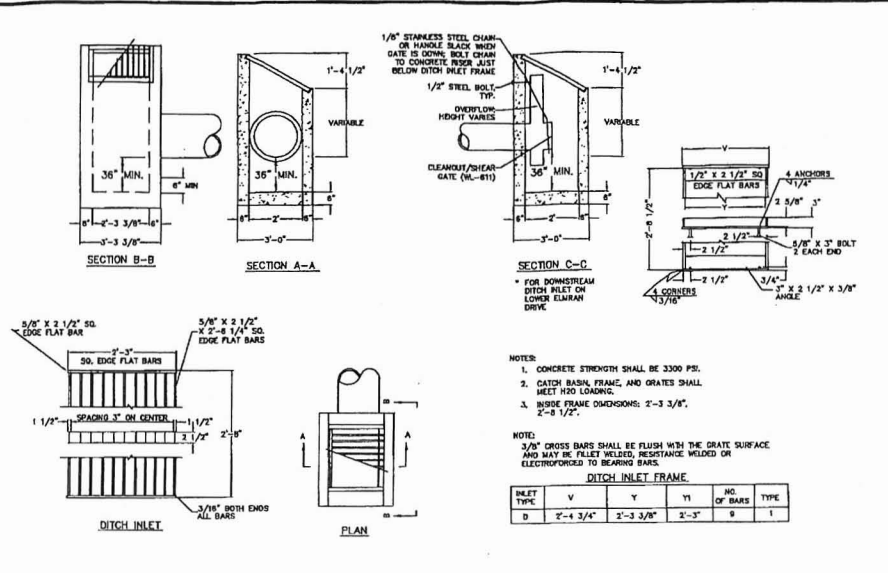
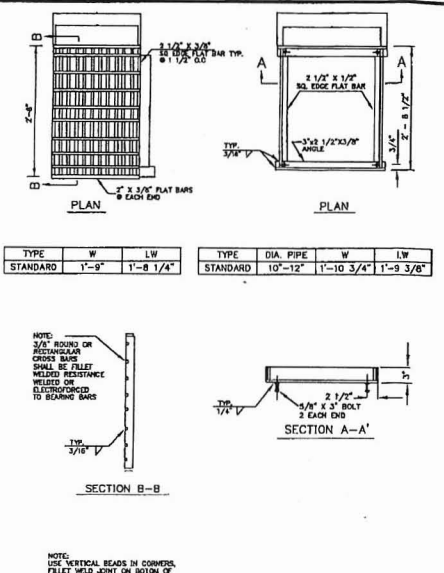
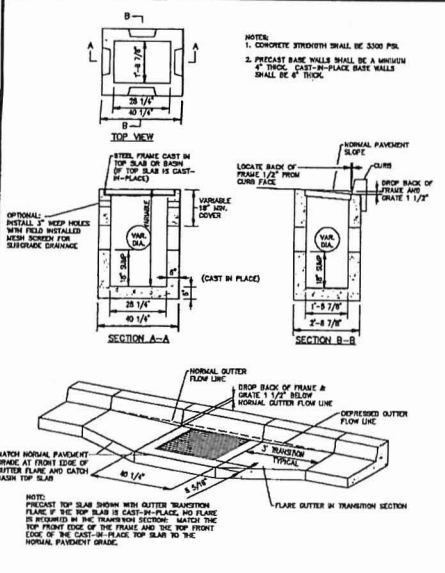
cushing
Don Cushing Associates
Civil Engineers
4677 SE Pinchurst Ave
Suite 201
Milwaukie, OR 97267
Voice: (503) 387-5331
Fax: (503) 286-2164

DATE: 6/1/2010
SCALE: NONE
DRAWN: JFG
JOB: 06-011
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SHEET

C7.3
OF 19

FOR CONSTRUCTION

FOR CONSTRUCTION



REVISIONS

4/1/10

CUSHING
REGISTERED PROFESSIONAL
LANDSCAPE ARCHITECT
OR. 0110008
LON CUSHING

RENEWAL DATE 12/31/11

BELLA FLATS SUBDIVISION
4TH ELMRAN DRIVE
WEST LINN, OREGON

SITE DETAILS
MISC.

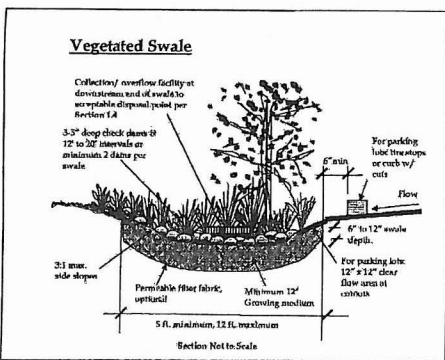
121

cushing
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4677 SE Finckhurst Ave
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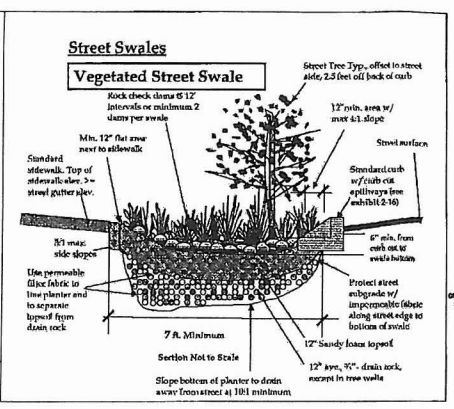
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PROJECT: 08-011-001

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OF 19

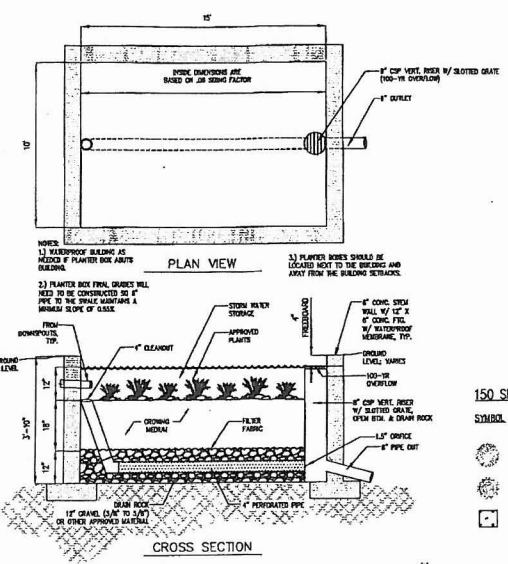
FOR CONSTRUCTION



1 VEGETATED SWALE (JACKSON COURT)
N.T.S.



2 VEGETATED STREET SWALE (ELMRAN)
N.T.S.

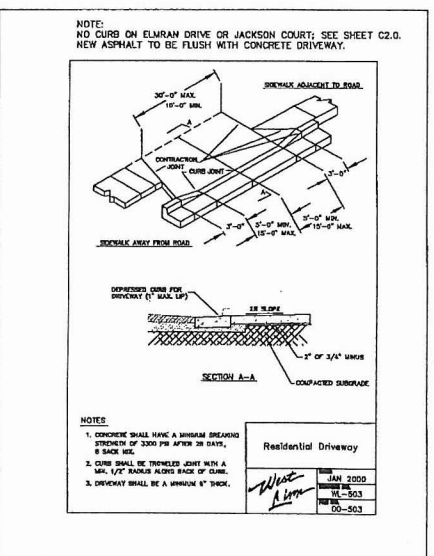


3 MODIFIED FLOW-THROUGH PLANTER BOX
N.T.S.

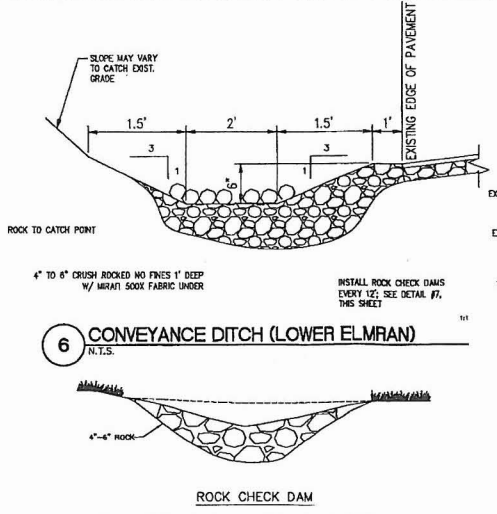
150 SF OF PLANTER BOX:

SYMBOL	QUANTITY	PLANTING TYPE	SIZE
⊙	6	SITKA WILLOW / "SALIX SITCHENSIS"	3-GALLON
⊙	9	REDTNG DOGWOOD / "CORNUS SERICEA"	1-GALLON
⊞		SEED AMERICAN SLOUGH GRASS / "BECKMANNIA SYZYGACHNE"	SEED

4 PLANTER BOX PLANTING PLAN
N.T.S.

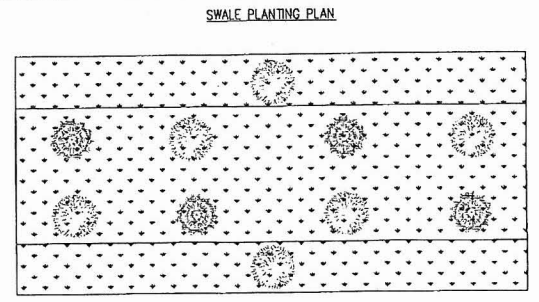


5 DRIVEWAY DETAIL
N.T.S.



6 CONVEYANCE DITCH (LOWER ELMRAN)
N.T.S.

7 CHECK DAM DETAIL
N.T.S.



PER 100 SF OF SWALE:

SYMBOL	QUANTITY	PLANTING TYPE	SIZE
⊙	4	VINE MAPLE / "ACER CIRCINATUM"	3-GALLON
⊙	6	REDTNG DOGWOOD / "CORNUS SERICEA"	1-GALLON
⊞		SEED HOBBS AND HOBBS'S COMPANION MIX	SEED

8 SWALE PLANTING PLAN
N.T.S.

NOTE: CHECK DAMS SHALL BE CONSTRUCTED OF ROCKS BETWEEN 4" AND 6" IN SIZE. CHECK DAMS SHALL BE 12 INCHES IN LENGTH, BY THE WIDTH OF THE SWALE OR DITCH, BY 3 TO 6 INCHES IN HEIGHT. LOW POINT OF CHECK DAM SHALL BE IN THE MIDDLE OF SWALE.

TOPOGRAPHIC SURVEY
 OF
 TAX LOTS 3100 AND 3201, MAP 2-1E-13CD
 CLACKAMAS COUNTY, OREGON
 DATE: 9/19/2006

bluedot GROUP
 Land Surveying & Mapping
 8720 Franklin St, Suite B
 Tigard, OR 97138
 P: 503.628.0254
 F: 503.628.2771

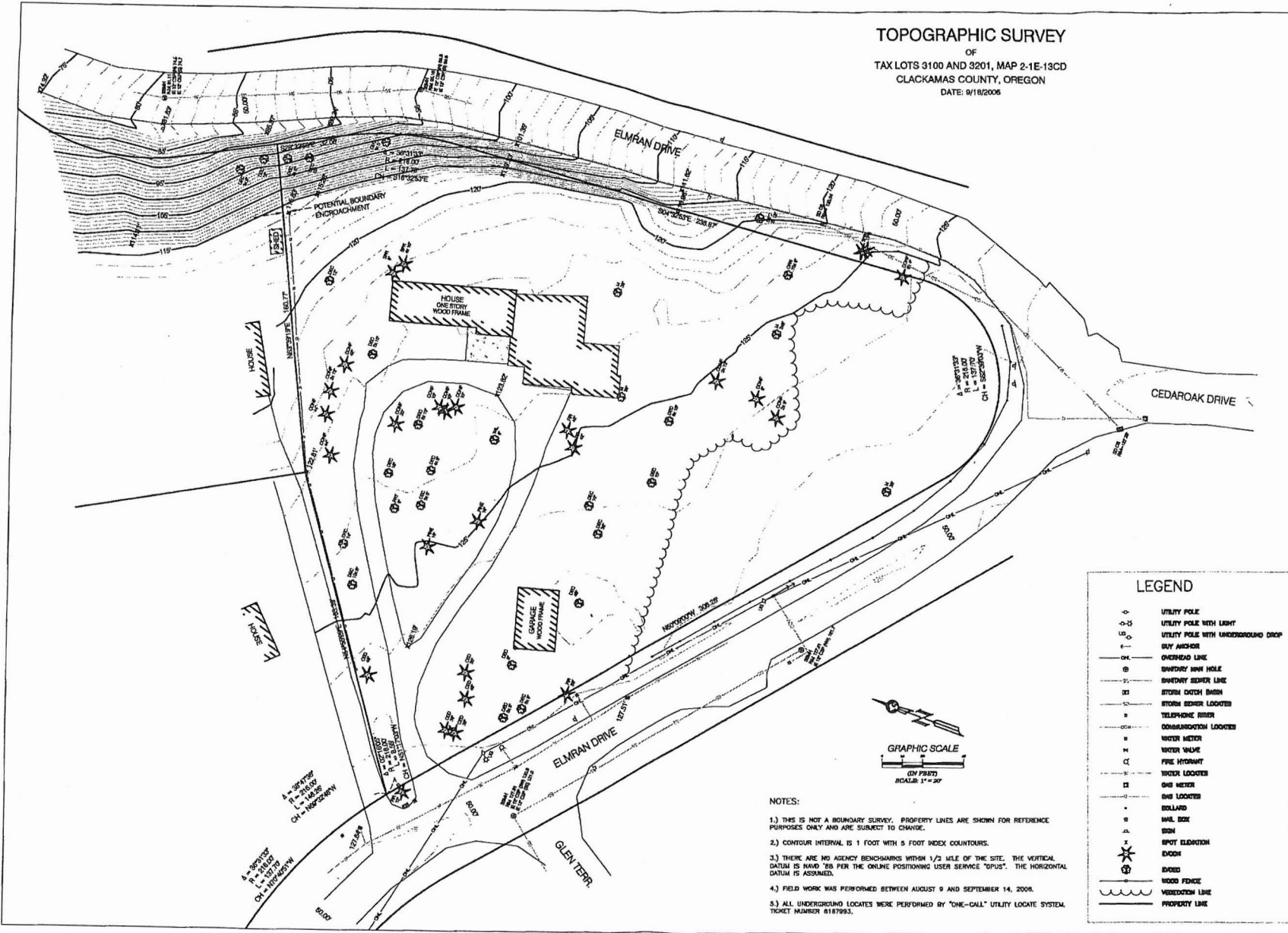
TOPOGRAPHIC SURVEY
 PROPOSED 6LOT SUBDIVISION
 4411 ELMRAV DR. WEST
 CLACKAMAS CO., OREGON 97138

REVISIONS

NO.	DATE	DESCRIPTION
1	09/19/06	
2	2006-02	

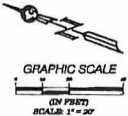
V1

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LEGEND

- ○ ○ ○ ○ UTILITY POLE
- ○ ○ ○ ○ UTILITY POLE WITH LIGHT
- ○ ○ ○ ○ UTILITY POLE WITH UNDERGROUND DROP
- ○ ○ ○ ○ GUY ANCHOR
- OH — OVERHEAD LINE
- S — SEWER LINE
- ST — STORM DRAIN
- S — SEWER LOCATOR
- S — STORM LOCATOR
- T — TELEPHONE RIVER
- T — TELEPHONE LOCATOR
- W — WATER METER
- W — WATER VALVE
- F — FIRE HYDRANT
- W — WATER LOCATOR
- S — SEWER LOCATOR
- B — BOLLARD
- B — BENCH MARK
- S — SPOT ELEVATION
- D — DITCH
- B — BOUNDARY
- V — VEGETATION LINE
- P — PROPERTY LINE



- NOTES:**
- 1.) THIS IS NOT A BOUNDARY SURVEY. PROPERTY LINES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE.
 - 2.) CONTOUR INTERVAL IS 1 FOOT WITH 5 FOOT INDEX CONTOURS.
 - 3.) THERE ARE NO AGENCY BENCHMARKS WITHIN 1/2 MILE OF THIS SITE. THE VERTICAL DATUM IS NAVD '88 PER THE ONLINE POSITIONING USER SERVICE 'TOPUS'. THE HORIZONTAL DATUM IS ASSUMED.
 - 4.) FIELD WORK WAS PERFORMED BETWEEN AUGUST 9 AND SEPTEMBER 14, 2006.
 - 5.) ALL UNDERGROUND LOCATES WERE PERFORMED BY "ONE-CALL" UTILITY LOCATE SYSTEM. TICKET NUMBER 6167993.