

April 24, 2026

Erica Smith, AICP, Senior Planner  
City of West Linn Planning Department  
22500 Salamo Road  
West Linn, OR

**RE: Request for Additional Information - Completeness of Application  
Expedited Land Division -- ELD-26-03**

Dear Erica,

This letter is submitted in response to your incompleteness letter dated April 16, 2026, requesting additional information in order to deem Case File ELD-26-03 complete. This letter and enclosed revised materials are submitted by the Applicant to update the application to be deemed complete. Each of the items in your letter is addressed as follows:

1. *Land Use Narrative*
  - a. *ORS 197A.142 Eligibility for expedited land division.*
  - b. *Describe how the proposed partition accommodates 80 percent or more of maximum net density permitted by the R-10 zoning designation.*

**RESPONSE: The narrative has been updated with responses to provisions of ORS 197A.142. As described in subsections 197A.142(1)(e)(A), both proposed parcels, at over 20,000 sq.ft. each, are of adequate size to allow building future residential units that meet or exceed the minimum 80 percent density of 3 for this property.**

2. *Preliminary Plat (Sheet C201)*
  - a. *An 8-foot wide PUE is required along the site's Cornwall Street Frontage.*

**RESPONSE: Sheet C201 of the Preliminary Engineering Plan Set, Exhibit B, has been updated to show the 8' PUE along Cornwall Street frontage of Parcel 1.**

3. *Site Plan (Sheet C300)*
  - a. *Curblin must align with curblin at 4190 Cornwall Street.*
  - b. *ADA ramps are required at all street corners and crossings.*

**RESPONSE: Sheet C300 of Exhibit B has been updated to illustrate the Cornwall frontage curblin aligned with existing curblines, and ADA ramps at street corners and crossings.**

4. *Composite Utility Plan (Sheet C500)*

- a. *If further land division is anticipated, extension of sewer main may be needed.*
- b. *A fire hydrant is needed to meet 400-foot spacing standard for residential areas (Design Standards 4.0032).*

**RESPONSE: The sanitary sewer main located within Landis Street has been extended as shown in the updated Sheet C500. As illustrated on C500, an existing fire hydrant on Landis south of the subject site meets the 400-foot spacing standard.**

With submittal of this letter and enclosed revised materials, it is our understanding that this land use application is complete. We look forward to your confirmation of completeness.

Sincerely,

Westlake Consultants, Inc.

A handwritten signature in blue ink, appearing to read "Kenneth L. Sandblast", with a long horizontal flourish extending to the right.

Kenneth L. Sandblast, AICP  
Director of Planning

# Expedited Land Division for 2-Parcel Partition

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**4110 Cornwall Street, West Linn  
Tax Map 21E36BA, Tax Lot 6100**

Prepared for:  
**The Portlock Company  
6105 W. A Street, Suite B  
West Linn, OR 97068**

Prepared by:  
**Westlake Consultants, Inc.  
15115 SW Sequoia Parkway, Suite 150  
Tigard, OR 97224**

**INDEX**

**List of Exhibits** ..... 1  
**Applicant and Subject Property Summary** ..... 2  
**Project Summary** ..... 3  
**Land Use Permit Request** ..... 4  
**Applicant Responses to Applicable Provisions** ..... 5  
**ORS 197A.142 Eligibility for Expedited Land Division**  
**Ch. 11: Residential R-10**  
**Ch. 48: Access, Egress and Circulation**  
**Ch. 85: Land Divisions – General Provisions**  
**Ch. 92: Required Improvements**  
**Ch. 96: Street Improvement Construction**  
**Ch. 99: Procedures**  
**Conclusion** ..... 25

**LIST OF EXHIBITS**

- A. Assessor’s Tax Map 2-1E-36BA**
- B. Preliminary Engineering Plan Set**
- C. Preliminary Stormwater Report**
- D. Service Provider Letter – TVFR**

**APPLICATION AND SUBJECT PROPERTY SUMMARY**

**SUBJECT PROPERTY:** Tax Map 21E36BA, Tax Lot 6100

**PROPERTY LOCATION:** 4110 Cornwall Street  
West Linn, OR 97068

**PROPOSAL:** Expedited Land Division (ELD) to partition one lot to create two parcels; dedication of right-of-way and construction of street extensions.

**LOT SIZE:** 1.46 AC

**ZONING DESIGNATION:** Residential, R-10

**APPLICANT / PROPERTY OWNER:** The Portlock Company  
6105 W. A Street, Suite B  
West Linn, OR 97068  
Phone: 425-829-1566  
Email: [jportlock@theportlockco.com](mailto:jportlock@theportlockco.com)

**APPLICANT'S REPRESENTATIVE:** Ken Sandblast, AICP  
Westlake Consultants, Inc.  
15115 SW Sequoia Parkway, Suite 150  
Tigard, OR 97224  
Phone: 503-684-0652  
Email: [ksandblast@westlakeconsultants.com](mailto:ksandblast@westlakeconsultants.com)

**Expedited 2-Parcel Partition  
4011 Cornwall Street, West Linn**

**PROJECT SUMMARY**

The subject property is located at 4110 Cornwall Street, West Linn, identified as Tax Lot 6100 of Tax Map 21E36BA. The site is zoned Residential R-10 and is approximately 1.46 acres in size, with frontage on Cornwall Street. The property is currently occupied by one existing single-family residential home and an accessory structure which are planned for removal. The site slopes gently from the northeast to the southwest; no significant natural features are located on the site. The site has access to public utilities located in both Cornwall and Landis Streets.

This application proposes a partition of the site into two parcels through the Expedited Land Division (ELD) process. An extension of Landis Street through the site is planned, as shown on the submitted Site Plan. In addition, half-street improvements, including 28' of pavement, are planned for a new right-of-way along the northern boundary of the site, as part of the future connection from Reed Street to Cornwall Street. Public Utilities will be extended through the proposed rights-of-way, as shown on the submitted Utilities Plan, included in the attached Exhibit B.

Figure 1. -- Vicinity Map

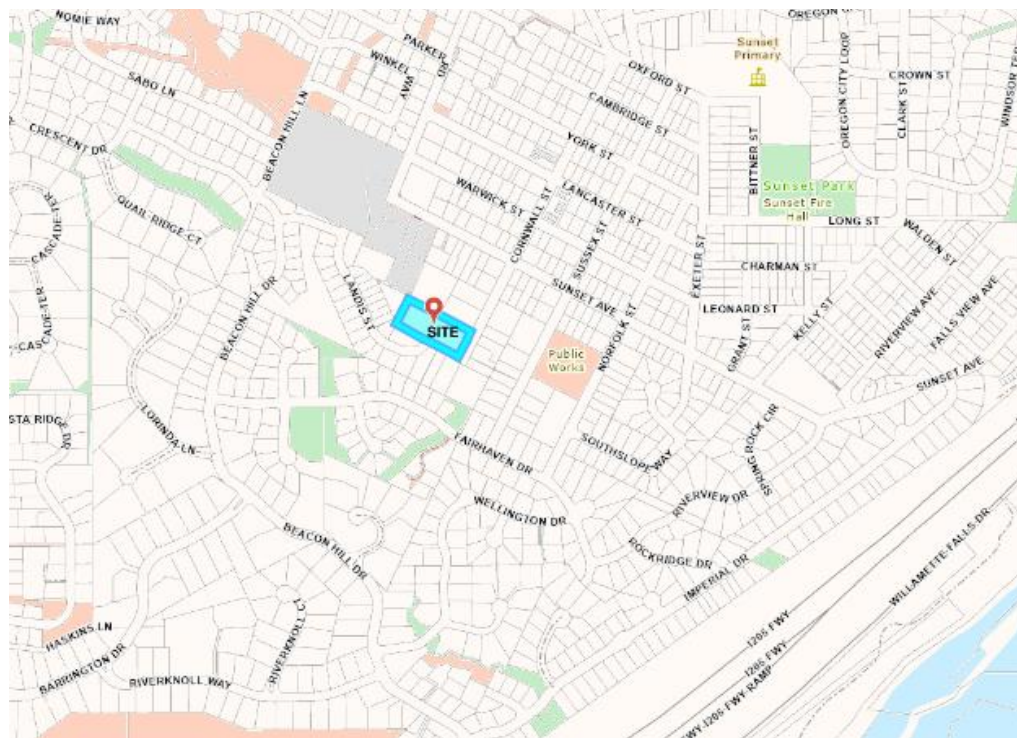
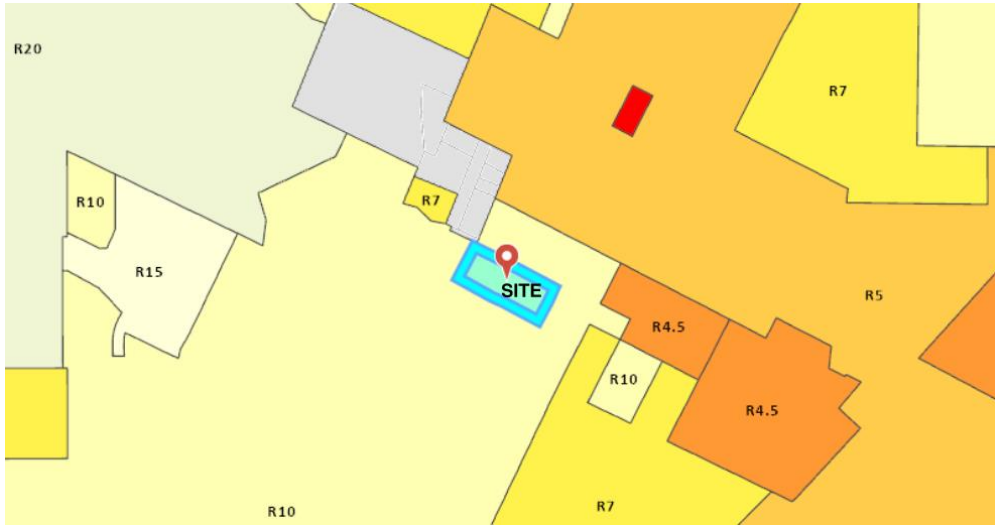


Figure 2. -- Zoning Map



**LAND USE PERMIT REQUEST:**

The Applicant proposes a partition of the subject site into two parcels through the Expedited Land Division process. Right-of-way dedication and street improvements for the extension of Landis Street, as well as half-street improvements along a portion of the site's northern boundary as a section of the Reed-Cornwall connector are also proposed through this application.

## **APPLICABLE STANDARDS FOR EXPEDITED LAND DIVISION**

After reviewing the West Linn *Community Development Code* and *ORS 197.360 through 197.380*, the following sections were found to be applicable to this partition application. The Applicant's responses follow each applicable provision.

### *ORS 197A.142 ELIGIBILITY FOR EXPEDITED LAND DIVISION*

*(1) If requested by the applicant, a local government shall approve a partition or subdivision made under ORS 92.010 to 92.192, 92.205 to 92.245 or 92.830 to 92.845 as an expedited land division under ORS 197A.140 if the division:*

*(a) Includes only land that is zoned for residential uses and is within an urban growth boundary.*

**RESPONSE: The subject site is zoned R-10 Residential and is located inside the urban growth boundary in the City of West Linn.**

*(b) Is solely for the purposes of residential use, including recreational or open space uses accessory to residential use.*

**RESPONSE: As demonstrated in the submitted plans, a partition of the site is proposed to support future residential development.**

*(c) Does not provide for dwellings or accessory buildings to be located on land that is specifically mapped and designated in the comprehensive plan and land use regulations for full or partial protection of natural features under the statewide planning goals that protect:*

*(A) Open spaces, scenic and historic areas and natural resources;*

*(B) The Willamette River Greenway;*

*(C) Estuarine resources;*

*(D) Coastal shorelands; and*

*(E) Beaches and dunes.*

**RESPONSE: There are no mapped natural features located on the site.**

*(d) Satisfies minimum street or other right-of-way connectivity standards established by acknowledged land use regulations or, if such standards are not contained in the applicable regulations, as required by statewide planning goals or rules.*

**RESPONSE: As demonstrated in the submitted plans, Exhibit B, the construction of improvements for two right-of-way segments is proposed as per the City's TSP, including the extension of Landis Street and the construction of a segment of the Reed-Cornwall connection. In addition, the Cornwall frontage of Parcel 1 will be improved as detailed on the submitted plans.**

*(e) Will result in development that either:*

*(A) Creates enough lots or parcels to allow building residential units at 80 percent or more of the maximum net density permitted by the zoning designation of the site; or*

*(B) Will be sold or rented to households with incomes below 120 percent of the median family income for the county in which the project is built.*

**RESPONSE: Following the proposed partition, the parcels will be 20,649 sq.ft. (Parcel 1) and 22,940 sq.ft. (Parcel 2) in size. Accordingly, both parcels are of adequate size to allow building future residential units that meet or exceed the minimum 80 percent density of 3 for this property.**

(2) ORS 197A.140 applies to all elements of a local government comprehensive plan and land use regulations applicable to a land division, including any planned unit development standards and any procedures designed to regulate:

- (a) The physical characteristics of permitted uses;
- (b) The dimensions of the lots or parcels to be created; or
- (b) Transportation, sewer, water, drainage and other facilities or services necessary for the proposed development, including but not limited to right-of-way standards, facility dimensions and on-site and off-site improvements.

**RESPONSE:** As demonstrated by the submitted preliminary engineering plan set, Exhibit B, and this narrative, the proposed partition has been designed to be in compliance with all dimensional, utility design and right-of-way improvement requirements for residential use in the R-10 zone.

(3) An application under this section must describe the manner in which the proposed division complies with each of the provisions of subsection (1) of this section. [Formerly 197.360]

**RESPONSE:** Per this subsection, the submitted narrative, plans and exhibits demonstrate compliance with the provisions of subsection (1), above.

#### WEST LINN COMMUNITY DEVELOPMENT CODE

#### CHAPTER 11: RESIDENTIAL, R-10

##### 11.030 PERMITTED USES

**RESPONSE:** The subject site is located within the R-10 zone. This application does not propose any residential development or change in use.

...

##### 11.070 DIMENSIONAL REQUIREMENTS

**RESPONSE:** Following the proposed land division, both parcels will meet all applicable dimensional requirements of the R-10 zone, as demonstrated in the submitted plan set attached as Exhibit B.

#### CHAPTER 48 ACCESS, EGRESS AND CIRCULATION

##### 48.020 APPLICABILITY AND GENERAL PROVISIONS

- A. The provisions of this chapter do not apply where the provisions of the Transportation System Plan or land division chapter are applicable and set forth differing standards.
- B. All lots shall have access from a public street or from a platted private street approved under the land division chapter.
- C. No building or other permit shall be issued until scaled plans are presented to the City and approved by the City as provided by this chapter, and show how the access, egress, and circulation requirements are to be

fulfilled. Access to State or County roads may require review, approval, and permits from the appropriate authority.

D. Should the owner or occupant of a lot, parcel or building enlarge or change the use to which the lot, parcel or building is put, resulting in increasing any of the requirements of this chapter, it shall be unlawful and a violation of this code to begin or maintain such altered use until the provisions of this chapter have been met, and, if required, until the appropriate approval authority under Chapter 99 CDC has approved the change.

E. Owners of two or more uses, structures, lots, parcels, or units of land may agree to utilize jointly the same access and egress when the combined access and egress of both uses, structures, or parcels of land satisfies the requirements as designated in this code; provided, that satisfactory legal evidence is presented to the City Attorney in the form of deeds, easements, leases, or contracts to establish joint use. Copies of said instrument shall be placed on permanent file with the City Recorder.

F. Property owners with access to their property via platted stems of flag lots may request alternate access as part of a discretionary review if other driveways and easements are available and approved by the City Engineer.

**RESPONSE: The extension of Landis Street and a portion of the Reed-Cornwall connection are proposed through this application, as listed in the City's TSP. The Applicant proposes that each parcel will have separate access from the Reed-Cornwall connection along the north portion of the site. No residential development is proposed through this application.**

#### 48.025 ACCESS CONTROL

##### B. Access control standards.

1. *Traffic impact analysis requirements.* A traffic analysis prepared by a qualified professional may be required to determine access, circulation and other transportation requirements. The purpose, applicability and standards of this analysis are found in CDC 85.170(B)(2).

**RESPONSE: This application proposes a two-parcel partition of the subject site. No residential development is proposed at this time and no traffic will be generated by the land partition. As such, the proposed partition does not meet the threshold of CDC 85.170(B)(2), and a traffic impact analysis is not needed at this time.**

2. *In order to comply with the access standards in this chapter, the City or other agency with access permit jurisdiction may require the closing or consolidation of existing curb cuts or other vehicle access points, recording of reciprocal access easements (i.e., for shared driveways), development of a frontage street, installation of traffic control devices, and/or other mitigation as a condition of granting an access permit. Access to and from off-street parking areas shall not permit backing onto a public street.*

**RESPONSE: Access to each parcel is proposed to be provided from the proposed segment of the Reed-Cornwall connection to be constructed as a public street along the north boundary of the site.**

3. *Access options.* When vehicle access is required for development (i.e., for off-street parking, delivery, service, drive-through facilities, etc.), access shall be provided from a public street adjacent to the development lot or parcel. Street accesses shall comply with access spacing standards in subsection (B)(6) of this section, the West Linn Public Works Design Standards, and TSP. As an alternative, the applicant may request alternative access provisions listed below as Option 1 and Option 2, subject to approval by the City Engineer through a discretionary process.

...

**RESPONSE:** The proposed lots will have direct access to a public street, as shown in the attached plans, Exhibit B. Driveway access from both parcels will be from the segment of the Reed-Cornwall connector along the north of the site.

4. *Subdivisions fronting onto an arterial street.* New residential land divisions fronting onto an arterial street shall be required to provide alleys or secondary (local or collector) streets for access to individual lots. When alleys or secondary streets cannot be constructed due to topographic or other physical constraints, access may be provided by consolidating driveways for clusters of two or more lots.

**RESPONSE:** All existing and proposed streets through or adjacent to the subject site are categorized as public local streets. This provision is not applicable.

5. *Double-frontage lots.* When a lot or parcel has frontage onto two or more streets, access shall be provided first from the street with the lowest classification. For example, access shall be provided from a local street before a collector or arterial street.

**RESPONSE:** As noted, all streets through (proposed) or adjacent to the site are categorized as local streets. Access to the proposed parcels will be provided from the new Reed-Cornwall connector. No direct access from Parcel 1 onto Cornwall Street is proposed.

6. *Access spacing.*

a. *The access spacing standards found in Tables 14 and 15 of the TSP and in CDC [48.060](#) shall be applicable to all newly established public street intersections, non-traversable medians, and curb cuts. Deviation from the access spacing standards may be granted by the City Engineer as part of a discretionary review if the applicant demonstrates that the deviation will not compromise the safe and efficient operation of the street and highway system.*

b. *Private drives and other access ways are subject to the requirements of CDC [48.060](#).*

**RESPONSE:** The Applicant acknowledges the applicable provisions of this section. Site access to each parcel will be provided from the Reed-Cornwall connector proposed along the site's northern boundary. Final curb cut width and spacing will be determined through future development plans.

7. *Number of access points.* For single-family (detached and attached) housing types, one street access point is permitted per lot or parcel when alley access cannot otherwise be provided; except that two access points may be permitted corner lots (i.e., no more than one access per street), subject to the access spacing standards in CDC [48.060](#). The number of street access points for multiple family development is subject to the access spacing standards in CDC [48.060](#). The number of street access points for commercial, industrial, and public/institutional developments shall be minimized to protect the function, safety and operation of the street(s) and sidewalk(s) for all users. Shared access may be required, in conformance with subsection (C)(8) of this section, in order to maintain the required access spacing, and minimize the number of access points.

**RESPONSE:** The Applicant acknowledges the applicable provisions of this section. Parcels 1 and 2 will each have one access point from the proposed segment of the Reed-Cornwall connector along the northern portion of the site.

8. *Shared driveways.* For residential development, shared driveways may be required in order to meet the access spacing standards in subsection (C)(6) of this section. For non-residential development, the number of driveway and private street intersections with public streets shall be minimized by the use of shared driveways

with adjoining lots where feasible. The City shall require shared driveways as a condition of land division or site design review, as applicable, for traffic safety and access management purposes in accordance with the following standards:

**RESPONSE: No residential development is proposed through this application. Driveway location will be determined at the time of future development of the site.**

C. Street connectivity and formation of blocks required. In order to promote efficient vehicular and pedestrian circulation throughout the City, land divisions and site developments shall produce complete blocks bounded by a connecting network of public and/or private streets, in accordance with the following standards:

1. Block length and perimeter. The maximum block length shall not exceed 800 feet along a collector, neighborhood route, or local street, or 1,800 feet along an arterial, unless a smaller block length is required pursuant to CDC [85.200\(B\)\(2\)](#).

**RESPONSE: The proposed public street improvements are consistent with the City's TSP. The alignment of the Reed-Cornwall connector creates a block length of approximately 430' from the connector to Sunset Ave.**

2. Street standards. Public and private streets shall also conform to Chapter [92](#) CDC, Required Improvements, and to any other applicable sections of the West Linn Community Development Code and approved TSP.

**RESPONSE: Please refer to the responses to applicable sections of Chapter 92, below. As noted above, the proposed right-of-way dedications and improvements are consistent with the City's TSP local street projects list.**

3. Exception. Exceptions to the above standards may be granted as part of a discretionary review when blocks are divided by one or more pathway(s), in conformance with the provisions of CDC [85.200\(C\)](#), Pedestrian and bicycle trails, or cases where extreme topographic (e.g., slope, creek, wetlands, etc.) conditions or compelling functional limitations preclude implementation, not just inconveniences or design challenges.

**RESPONSE: No exceptions are requested through this application.**

#### 48.060 WIDTH AND LOCATION OF CURB CUTS AND ACCESS SEPARATION REQUIREMENTS

A. Minimum curb cut width shall be 16 feet.

B. Maximum curb cut width shall be 36 feet, except along Highway 43 in which case the maximum curb cut shall be 40 feet. For emergency service providers, including fire stations, the maximum shall be 50 feet.

C. No curb cuts shall be allowed any closer to an intersecting street right-of-way line than the following:

1. On an arterial when intersected by another arterial, 150 feet.
2. On an arterial when intersected by a collector, 100 feet.
3. On an arterial when intersected by a local street, 100 feet.
4. On a collector when intersecting an arterial street, 100 feet.
5. On a collector when intersected by another collector or local street, 35 feet.
6. On a local street when intersecting any other street, 35 feet.

D. There shall be a minimum distance between any two adjacent curb cuts on the same side of a public street, except for one-way entrances and exits, as follows:

1. *On an arterial street, 150 feet.*
2. *On a collector street, 75 feet.*
3. *Between any two curb cuts on the same lot or parcel on a local street, 30 feet.*

**RESPONSE: This application acknowledges these provisions. No residential development is proposed through this application. At the time of future development, curb cuts will be located in compliance with the applicable provisions of this section.**

*E. A rolled curb may be installed in lieu of curb cuts and access separation requirements.*

**RESPONSE: Standard curbs are proposed through this application.**

*F. For non-residential development, curb cuts shall be kept to the minimum, particularly on Highway 43. Consolidation of driveways is preferred. The standard on Highway 43 is one curb cut per business if consolidation of driveways is not possible.*

**RESPONSE: The subject site is located in the R-10 zone and, as such, future development will be residential. This provision is not applicable.**

*G. Clear vision areas shall be maintained, pursuant to Chapter [42](#) CDC, and required line of sight shall be provided at each driveway or accessway, pursuant to the West Linn Public Works Design Standards.*

**RESPONSE: The Applicant acknowledges this provision. Intersections are designed to provide clear line of sight.**

## CHAPTER 85 LAND DIVISIONS – GENERAL PROVISIONS

### 85.050 APPROVAL REQUIRED BEFORE CREATING STREET OR ROAD TO PARTITION LAND

*A. No person shall create a street or road for the purpose of partitioning an area or tract of land without approval by the approval authority under the provisions of CDC [99.060](#)(A) and (B).*

**RESPONSE: The Applicant acknowledges this provision and is seeking approval of the proposed partition and right-of-way improvements through the Expedited Land Division process under authority of the Planning Director or designee.**

*B. No instrument dedicating land to public use shall be accepted for recording unless such instrument bears the approval of the Planning Director or City Engineer, as applicable, under the provisions of CDC [99.060](#)(A) and (B), procedures for decision-making.*

**RESPONSE: The Applicant acknowledges this provision.**

### 85.140 PRE-APPLICATION CONFERENCE REQUIRED

*A. An applicant shall participate in a pre-application conference with staff prior to the submission of a complete tentative plan.*

- B. *The Planning staff shall explain the applicable plan policies, ordinance provisions, opportunities, and constraints which may be applicable to the site and type of proposed land division.*
- C. *The City Engineering staff shall explain the public improvement requirements which may be applicable to the site and type of proposed land division, including potential for the applicant to apply for a waiver of street improvements.*

**RESPONSE: The Applicant attended a pre-application conference with City staff on March 5, 2026, and received meeting notes on March 16, 2026.**

#### 85.150 APPLICATION – TENTATIVE PLAN

- A. *The applicant shall submit a completed application which shall include:*
1. *The completed application form(s).*
  2. *Copies of the tentative plan and supplemental drawings shall include one copy at the original scale plus one copy reduced in paper size not greater than 11 inches by 17 inches. The applicant shall also submit one copy of the complete application in a digital format acceptable to the City. When the application submittal is determined to be complete, additional copies may be required as determined by the Community Development Department.*
  3. *A narrative explaining all aspects of land division per CDC [85.200](#).*
- B. *The applicant shall pay the requisite fee.*

**RESPONSE: The submitted application includes all required materials, including a completed application form, a preliminary engineering plan set with a tentative partition plan, and this narrative, which addresses applicable code sections of CDC 85.200. The Applicant has submitted the requisite fee.**

#### 85.160 SUBMITTAL REQUIREMENTS FOR TENTATIVE PLAN

*(Text omitted for brevity.)*

**RESPONSE: The submitted tentative plan conforms to the requirements of this section. Please see attached plan set, Exhibit B.**

#### 85.170 SUPPLEMENTAL SUBMITTAL REQUIREMENTS FOR TENTATIVE SUBDIVISION OR PARTITION PLAN

**RESPONSE: The attached Preliminary Engineering Plan set, Exhibit B, has been prepared in accordance with the requirements of this section. Please refer to the plan set for details.**

#### 85.200 APPROVAL CRITERIA

*No tentative subdivision or partition plan shall be approved unless adequate public facilities will be available to provide service to the partition or subdivision area prior to final plat approval and the Planning Commission or Planning Director, as applicable, finds that the following standards have been satisfied, or can be satisfied by conditions of approval:*

A. Streets.

1. Purpose and guiding principles. *The purpose of these standards is to promote safe, efficient, and convenient options for walking, bicycling, and driving while accommodating access to individual properties, as needed, and access to transit. The following principles shall guide land division applications:*
  - a. *The location, width and grade of streets shall be considered in their relation to existing and planned streets, to the generalized or reasonable layout of streets on adjacent undeveloped lots or parcels, to topographical conditions, to public convenience and safety, to accommodate various types of transportation (automobile, bus, pedestrian, bicycle), and to the proposed use of land to be served by the streets.*
  - b. *The functional class of a street aids in defining the primary function and associated design standards for the facility. The hierarchy of the facilities within the network in regard to the type of traffic served (through or local trips), balance of function (providing access and/or capacity), and the level of use (generally measured in vehicles per day) are generally dictated by the functional class.*
  - c. *The street system shall assure an adequate traffic or circulation system with intersection angles, grades, tangents, and curves appropriate for the traffic to be carried.*
  - d. *Streets should provide for the continuation, or the appropriate projection, of existing principal streets in surrounding areas and should not impede or adversely affect development of adjoining lands or access thereto.*
  - e. *To accomplish this, the emphasis should be upon a connected continuous pattern of local, collector, and arterial streets rather than discontinuous curvilinear streets and cul-de-sacs. Deviation from this pattern of connected streets should only be permitted in cases of extreme topographical challenges including excessive slopes (35 percent plus), hazard areas, steep drainageways, wetlands, etc. In such cases, deviations may be allowed but the connected continuous pattern must be reestablished once the topographic challenge is passed.*

**RESPONSE:** This application proposes a two-parcel partition and right-of-way dedication/construction of two local street segments per the West Linn TSP local streets project list. The extension of Landis Street from the existing stub at the site's south boundary, and the proposed segment of the Reed-Cornwall connection are consistent with the pattern of local streets in the area and will provide access to the proposed parcels, as well as promote safe, efficient, and convenient options for all modes of travel. The proposed segment of the Reed-Cornwall connection will support the future completion by others of that TSP project and further connectivity.

2. *In situations where the level-of-service or volume-to-capacity performance standard for an affected City or State roadway is currently failing or projected to fail to meet the standard at a date determined within a traffic impact analysis, and an improvement project is not programmed, the development shall avoid further degradation of the affected transportation facility. Mitigation must be provided to bring the facility performance standard to existing conditions at the time of occupancy.*

**RESPONSE:** This application proposes a partition of one lot into two residential parcels. No residential development is currently proposed, and therefore, no impact to level of service or traffic volume is expected from the proposed land division.

3. *Tree protection.* Streets shall be laid out to avoid and protect significant trees and significant tree clusters, but not to the extent that it would compromise connectivity requirements per this subsection A, or bring the achievable density below 70 percent of the maximum density for the developable net area as calculated per CDC [05.025](#).

**RESPONSE:** The proposed extension of Landis Street from the existing stub at the site's southern boundary conforms to the City's connectivity requirements and is listed in the TSP. Similarly, the proposed segment of the connection between Reed and Cornwall Streets, which is listed in the City's TSP, conforms to the connectivity requirements. The alignments of these rights-of-way are determined by the TSP and existing street network. As shown on the Existing Conditions Plan, Exhibit B, three trees are proposed for removal to accommodate the alignment of the proposed Reed-Cornwall connector.

4. *Street connections.* The developer shall make all local street connections identified in the Transportation System Plan, Table 17 and Figure 12, that are within the boundaries of the project.

**RESPONSE:** As noted above, the proposed Landis Street extension and the proposed right-of-way along the northern portion of the site are listed in the TSP. The Applicant proposes to complete the full street improvements for the Landis Street extension through the site, and to complete half-street improvements, including 28' of pavement, along the south side of the segment of Reed-Cornwall connection, as illustrated in the attached Preliminary Plans, Exhibit B. This provision is satisfied.

5. *Street improvements.*

- a. *Streets that are internal to the land division site are the responsibility of the developer. All streets bordering the development site are to be developed by the developer with, typically, half-street improvements to the City of West Linn Public Works Design Standards. Additional travel lanes may be required to be consistent with adjacent road widths or to be consistent with the adopted Transportation System Plan (TSP), Tables 26 through 30 and Exhibits 6 through 9.*
- b. *Waiver of required street improvements and in-lieu fee.* An applicant may submit a written request for a waiver of abutting street improvements if the improvement would be prohibited by the TSP. When a requested waiver is granted, the applicant shall pay an in-lieu fee in accordance with the City's adopted fee structure. The in-lieu fee shall be used for in-kind or related improvements.
- c. *Right-of-way widths shall depend upon which classification of street is proposed. The right-of-way widths are established in the adopted TSP, Exhibits 6 through 9.*
- d. *Public Works Design Standards.* Street design shall conform to the standards of the applicable roadway authority; for City streets that is the West Linn Public Works Design Standards manual. Where a conflict occurs between this code and the Public Works Design Standards manual, the provisions of this code shall govern.

**RESPONSE:** As demonstrated in the submitted Site Plan, included in Exhibit B, the proposed street improvements have been designed in conformance with the West Linn Public Works Design Standards manual. The Applicant proposes to construct the extension of Landis Street through the site. In addition, construction of half street improvements and a total of 28' of pavement for a portion of the connection from Reed to Cornwall Street is proposed through this application, as shown on the Site Plan. Improvements will also be constructed along the existing Cornwall Street frontage, as shown on the plans.

6. *Street widths.* Street widths shall depend upon the classification of street proposed. The classifications and required cross sections are established in the adopted TSP, Tables 26 through 30 and Exhibits 6 through 9.

Table 85-1 identifies street width standards (curb to curb) in feet for various street classifications. The standard width shall be required unless the applicant or their engineer can demonstrate that site conditions, topography, or site design require the reduced minimum width through a discretionary review.

**RESPONSE:** The proposed street improvements have been designed to conform to the City's established street-width standards. The Applicant proposes a Landis Street extension right-of-way width of 52', as shown on the Site Plan in Exhibit B. The proposed segment of the Reed-Cornwall connection is planned to provide half-street improvements, including a 6' sidewalk and 28' pavement width, to allow for parking on one side.

7. *The decision-making body shall consider the City Engineer's recommendations on the desired right-of-way width, pavement width and geometry for streets within or adjacent to the subdivision. To approve a street design less than the width in Table 85-1, the applicant shall demonstrate with proper documentation that one of the following applies:*

- a. *The street design will help protect a water resource area and complies with the submittal requirements and approval standards found in Chapter 32 CDC.*
- b. *The street design will help protect a flood management area and complies with the submittal requirements and approval standards found in Chapter 27 CDC.*
- c. *The street design will help protect the Willamette River Greenway, Tualatin River Greenway, or a habitat conservation area and complies with the submittal requirements and approval standards found in Chapter 28 CDC.*
- d. *The street design will help protect steep slopes and complies with the submittal requirements found in CDC 85.170(C) and approval standards found in subsection E of this section.*
- e. *The street design will help protect a significant tree cluster and complies with subsection (J)(9) of this section.*

**RESPONSE:** As demonstrated in the submitted plan set, attached as Exhibit B, the proposed right-of-way improvements within and adjacent to the subject site have been designed in compliance with the applicable width standards.

8. *Reserve strips.* Reserve strips or street plugs controlling the access to streets are not permitted unless owned by the City.

**RESPONSE:** This application does not propose a reserve strip.

9. *Alignment.* All streets other than local streets or cul-de-sacs shall be in alignment with existing streets by continuations of the centerlines thereof. The staggering of street alignments resulting in "T" intersections shall leave a minimum distance of 200 feet between the centerlines of streets having approximately the same direction and otherwise shall not be less than 100 feet. Exceptions to these requirements shall only be approved if the applicant demonstrates that compliance is not practical through a discretionary review.

**RESPONSE:** The proposed extension of Landis Street is in alignment with the centerline of the existing portion of Landis Street that stubs into the site's southern boundary. Similarly, the Cornwall frontage improvements are designed to be in alignment with the existing improvements on Cornwall. The proposed segment of the Reed-Cornwall connection has been designed to accommodate future alignment with the remaining portion of the future connection to the northwest. The intersection of the connector and Cornwall is over 450' from the centerline of the Cornwall/Sunset intersection.

10. Future extension of streets. The street system of a proposed development shall be designed to connect to existing, proposed, and planned streets adjacent to the development. Wherever a proposed development abuts unplatted land or a future development phase of an existing development, street stubs shall be provided to allow access to future abutting subdivisions and to logically extend the street system into the surrounding area. Where the stubbed street is over 100 feet long, street ends shall contain temporary turnarounds built to Oregon Fire Code standards and shall be designed to facilitate future extension in terms of grading, width, and temporary barricades.

**RESPONSE:** The Applicant proposes to construct a portion of the new Reed-Cornwall connection to provide the proposed parcels with access to/from Cornwall Street. The new street segment will be stubbed at its western end near the northwest corner of the subject site. As shown in the submitted plans, Exhibit B, the proposed street design allows for fire access. A Service Provider Letter from Tualatin Valley Fire & Rescue is attached as Exhibit D.

11. Intersection angles.

a. Except as specified in subsection (A)(11)(c) of this section, street intersections shall be located and designed as follows:

- 1) Streets shall be located and designed to intersect at, or close to, right angles (i.e., 90 degrees or within three degrees of 90 degrees).
- 2) All legs of an intersection shall meet the above standard for at least 100 feet back from the point of intersection.
- 3) No more than two streets shall intersect, i.e., creating a four-legged intersection, at any one point.
- 4) Street jogs and intersection offsets of less than 125 feet are not permitted.

**RESPONSE:** As shown on the submitted plans, Exhibit B, proposed streets are designed to intersect with other proposed and existing streets at right angles. The on-site alignment of the Reed-Cornwall connection that extends to the northwest of the site is curvilinear, to allow for future alignment that connects with Reed Street.

b. Curb radii.

- 1) Intersections which are not at right angles shall have minimum corner radii of 15 feet along right-of-way lines which form acute angles.
- 2) Right-of-way lines at intersections with arterial streets shall have minimum curb radii of not less than 35 feet.
- 3) Other street intersections shall have curb radii of not less than 25 feet.
- 4) All radii shall maintain a uniform width between the roadway and the right-of-way lines.

**RESPONSE:** As shown on the submitted Site Plan, proposed curb radii of proposed intersections are 25 feet.

c. Through a discretionary review, applicants may request the City consider modifications of the standards in subsections (A)(11)(a) and (b) of this section; provided, that the following are met:

- 1) *Where an intersection is constrained by topography, the applicant may propose lesser intersection angles. However, intersection angles of less than 60 degrees are not allowed unless a special intersection design is requested and approved.*
- 2) *The intersection of more than two streets at any one point or a street jogs or intersection offset of less than 125 feet is necessary because no alternative design exists.*

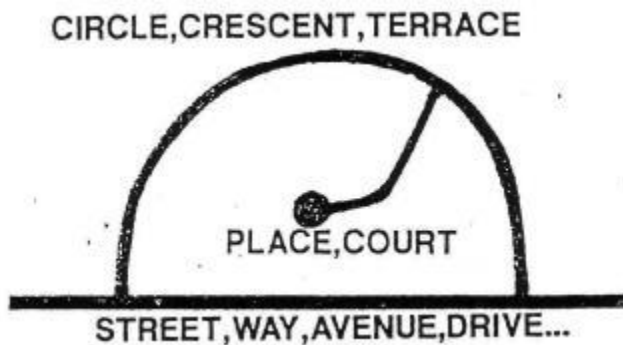
**RESPONSE: The Applicant does not request modification to any intersection standards through this application.**

*12. Additional right-of-way for existing streets. Wherever existing street rights-of-way adjacent to or within a tract are of inadequate widths based upon the standards of this chapter, additional right-of-way shall be dedicated at the time of subdivision or partition.*

**RESPONSE: The existing Cornwall Street right-of-way meets the City’s width requirement. No additional dedication of right-of-way is required.**

...

*14. Street names. No street names shall be used which will duplicate or be confused with the names of existing streets within the City. Street names that involve difficult or unusual spellings are discouraged. Street names shall be subject to the approval of the Planning Commission or Planning Director, as applicable. Continuations of existing streets shall have the name of the existing street. Streets, drives, avenues, ways, boulevards, and lanes shall describe through streets. Place and court shall describe cul-de-sacs. Crescent, terrace, and circle shall describe loop or arcing roads.*



**RESPONSE: The on-site extension of Landis Street will retain that name.**

*15. Grades and curves. Grades and horizontal/vertical curves shall meet the West Linn Public Works Design Standards.*

**RESPONSE: All proposed grades and curves meet the West Linn Public Works standards, as demonstrated in the attached Preliminary Plans, Exhibit B.**

16. Access to local streets.

- a. *Except as provided in subsection (A)(16)(c) of this section, intersection of a local residential street with an arterial street shall be prohibited by the decision-making authority if one or more alternatives exist for providing interconnection of proposed local residential streets with other local streets.*
- b. *Where a residential subdivision or partition abuts or contains an existing or proposed major arterial street, the design shall incorporate at least three of the following measures to protect residential properties from incompatible land uses, and to ensure separation of through traffic and local traffic: marginal access streets, reverse-frontage lots with lot depth of at least 100 feet, visual barriers, noise barriers, berms, no-access reservations along side and rear property lines, and/or other similar measures proposed by the applicant.*
- c. *At the applicant's request, the City may consider design alternatives to subsections (A)(16)(a) and (b) of this section through a discretionary review.*

**RESPONSE: All streets adjacent to or in the vicinity of the subject site are public local streets. This provision is not applicable.**

...

18. Sidewalks. *Sidewalks shall be installed per CDC [92.010\(H\)](#), Sidewalks. The residential sidewalk width is six feet plus planter strip as specified below. Sidewalks in commercial zones shall be constructed per subsection (A)(6) of this section. See also subsection C of this section. If part of a discretionary review, sidewalk width may be reduced with City Engineer approval to the minimum amount (e.g., four feet wide) necessary to respond to site constraints such as grades, mature trees, rock outcroppings, etc., or to match existing sidewalks or right-of-way limitations.*

**RESPONSE: As shown on the submitted plans, attached as Exhibit B, this application proposes 6'-wide sidewalks along both parcel frontages on Landis and the connection to Cornwall, and a 6' sidewalk along Parcel 1's frontage on Cornwall.**

19. Planter strip. *The planter strip is between the curb and sidewalk providing space for a grassed or landscaped area and street trees. The planter strip shall be at least six feet wide to accommodate a fully matured tree without the boughs interfering with pedestrians on the sidewalk or vehicles along the curbline. If part of a discretionary review, planter strip width may be reduced or eliminated, with City Engineer approval, when it cannot be corrected by site plan, to the minimum amount necessary to respond to site constraints such as grades, mature trees, rock outcroppings, etc., or in response to right-of-way limitations.*

**RESPONSE: As shown in the submitted Preliminary Plans, attached as Exhibit B, planter strips along both sides of Landis are proposed to be 5.5 feet wide. A 5.5-foot wide landscape strip and vegetated stormwater planters are proposed along Parcel 1's frontage on Cornwall Street. As part of the half-street improvements along the northern portion of the site, a 5.5'-wide landscape strip is proposed along the north frontage of Parcel 2.**

20. Streets and roads shall be dedicated without any reservations or restrictions.

**RESPONSE: The Applicant proposes public dedication of right-of-way to accommodate the proposed Landis and Reed-Cornwall Connector improvements. This provision is satisfied.**

21. All lots in a subdivision shall have access to a public street. Lots created by partition may have access to a public street via an access easement pursuant to the standards and limitations set forth for such accessways in Chapter 48 CDC.

**RESPONSE:** As shown in the submitted plans, Exhibit B, both parcels will have access to a public street, with future driveway access along the proposed segment of the Reed-Cornwall connection.

B. Blocks and lots.

1. Purpose. The length, width, and shape of blocks shall be designed with due regard for the provision of adequate building sites for the use contemplated; consideration of the need for traffic safety, convenience, access, circulation, and control; and recognition of limitations and opportunities of topography and solar access.

**RESPONSE:** As demonstrated in the submitted plan set, Exhibit B, the parcel and street layout has been designed to provide connections to the surrounding street system. Both proposed street extensions are included in the City's TSP. The Landis stub from the south will be extended through the site and connect to the proposed street along the site's north boundary (Reed-Cornwall connection). The proposed street improvements will also provide for efficient use of the subject site for future residential development, as well as convenient and safe access to and from the proposed parcels. This provision is satisfied.

2. Sizes.

- a. Except as required under subsection (B)(2)(c) of this section, block lengths shall not exceed 800 feet, except for blocks adjacent to arterial streets or unless topographical conditions or the layout of adjacent streets justifies a variation as part of a discretionary review.
- b. Designs of proposed intersections shall demonstrate sight distances consistent with the West Linn Public Works Design Standards.

...

- d. If block lengths are greater than 530 feet, accessways on public easements or right-of-way for pedestrians and cyclists shall be provided not more than 330 feet apart.
- e. If streets must cross water features protected pursuant to UGMFP Title 3, a crossing must be provided every 800 to 1,200 feet unless habitat quality or the length of the crossing prevents a full street connection.

**RESPONSE:** The layout of existing streets and the designation of the Landis Street extension and the Reed-Cornwall connection as planned TSP projects have guided the design of the proposed street improvements. Intersections designed with right angles allow for sight distance that meets the West Linn Public Works Design Standards. With the construction of the Reed-to-Cornwall connector, the block length from the connector to Sunset Avenue is approximately 475 feet, as shown on the submitted plans.

3. Lot size. Lot or parcel sizes and dimensions shall conform to the minimum standards of the CDC, unless as allowed by planned unit development (PUD). No lot or parcel shall be dimensioned to contain part of an existing or proposed street. All lots or parcels shall be buildable. "Buildable" describes lots that are free of constraints such as wetlands, drainageways, etc., that would make home construction impossible.

*Depth and width of properties zoned for commercial and industrial purposes shall be adequate to provide for the off-street parking and service facilities required by the type of use proposed.*

**RESPONSE:** This application proposes a partition of one lot into two parcels, with resulting parcel sizes of 20,649 sq.ft. for Parcel 1 and 22,940 sq.ft. for Parcel 2, as shown in the submitted Site Plan included in Exhibit B. No residential development is proposed through this application; however, the proposed parcels will be of adequate size to accommodate future residential development to greater than minimum density.

4. *Access.* Access to subdivisions, partitions, and lots shall conform to the provisions of Chapter [48](#) CDC, Access, Egress and Circulation.

**RESPONSE:** The proposed partition is designed to comply with the applicable provisions of Chapter 48. As noted throughout this narrative, the extension of Landis Street through the site and the construction of half-street improvements and 28' of pavement along the north portion of the site as part of the Reed-Cornwall connection, provide access and egress to the proposed parcels and will enhance the circulation in the vicinity of the site.

5. *Through lots and parcels.* Through lots and parcels have frontage on a street at the front and rear property lines. Through lots and parcels shall be avoided except where they are necessary to avoid residential lots with frontage on arterial streets. Additional exceptions may be granted as part of a discretionary review if an applicant proposes through lots to provide separation from adjacent non-residential activities, or to overcome specific disadvantages of topography and orientation. As part of the discretionary review, a planting screen or impact mitigation easement at least 10 feet wide, and across which there shall be no right of access, may be required along the line of building sites abutting such a traffic artery or other incompatible use.

**RESPONSE:** Neither of the proposed parcels is a through lot. The front lot line of both parcels will be to the north, along the Reed-Cornwall connector.

6. *Lot shape.* Each lot or parcel must be rectilinear in shape with straight side lot lines at right angles to front lot lines, and straight rear lot lines parallel to front lot lines, except where not practicable due to location along a street radius or because of existing natural features or lot lines. Side and rear lot lines that are segmented must not contain cumulative lateral changes in direction that exceed 10 percent of the distance between opposing lot corners, as measured using the process of CDC [05.025](#).

**RESPONSE:** Following the proposed partition, the resulting parcels will be of generally rectangular shape. Parcel 2 will accommodate the curvilinear alignment of the Reed-Cornwall connection in the NE corner of the parcel.

8. *Large lots or parcels.* In dividing tracts into large lots or parcels that are more than double the minimum area designated by the zoning district:

- a. *Those lots must be arranged so as to allow further subdivision, and must contain such easements and site restrictions as will provide for extension and opening of future streets where it would be necessary to serve potential lots; or*

- b. *Alternately, in order to prevent further subdivision or partition of oversized and constrained lots or parcels, restrictions may be imposed on the subdivision or partition plat.*

**RESPONSE:** This application proposes a partition of one lot into two parcels, with resulting parcel sizes of 20,649 sq.ft. for Parcel 1 and 22,940 sq.ft. for Parcel 2, as shown in the submitted Site Plan included in Exhibit B. No residential development is proposed through this application; however, the proposed parcels will be of adequate size to accommodate future residential development to greater than minimum density, as allowed in the R-10 zone.

...

E. Grading. *Grading of building sites shall conform to the following standards unless physical conditions demonstrate the propriety of other standards:*

1. *All cuts and fills shall comply with the excavation and grading provisions of the Building Code and the following:*
  - a. *Cut slopes shall not exceed one and one-half feet horizontally to one foot vertically (i.e., 67 percent grade).*
  - b. *Fill slopes shall not exceed two feet horizontally to one foot vertically (i.e., 50 percent grade) Please see the following illustration. [ILLUSTRATION OMITTED FOR BREVITY]*
2. *If areas are to be graded, compliance with CDC 85.170(C) is required.*
3. *The proposed grading shall be the minimum grading necessary to meet roadway standards, pursuant to the West Linn Public Works Design Standards, and to create buildable sites, considering maximum allowed driveway grades.*

**RESPONSE:** As demonstrated in the preliminary grading plan included as Sheet C400 of Exhibit B, proposed grading complies with the provisions of this section.

...

F. Water.

1. *A plan for domestic water supply lines or related water service facilities shall be prepared consistent with the adopted Comprehensive Water System Master Plan, updated in 2008, and subsequent superseding revisions or updates. The plan shall include:*
  - a. *Location and sizing of the water lines consistent with the Water System Master Plan and West Linn Public Works Design Standards.*
  - b. *For all non-single-family developments, there shall be a demonstration of adequate fire flow to serve the site, as demonstrated by consistency with West Linn Public Works Design Standards.*
  - c. *A written statement, signed by the City Engineer, that water service can be made available to the site by the construction of on-site and off-site improvements and that such water service has sufficient volume and pressure to serve the proposed development's domestic, commercial, industrial, and fire flows.*

**RESPONSE:** Per the City's Engineering Department pre-app comments, the existing 8" DI water main in Landis Street has adequate capacity to serve the proposed partition. As required, the existing main will be extended with the proposed Landis Street section to serve both parcels. A water main will be provided in the Reed-Cornwall connector, as well. Please refer to the Composite Utility Plan, Sheet C500 of Exhibit B.

G. Sewer.

1. A plan prepared by a licensed engineer shall show how the proposal is consistent with the current Sanitary Sewer Master Plan and subsequent updates and amendments applicable at the time the proposal is submitted. Agreement with that plan must demonstrate how the sanitary sewer proposal will be accomplished and how it is gravity-efficient. The sewer system must be in the correct basin and allow for full gravity service.
2. Sanitary sewer information will include plan view of the sanitary sewer lines, including manhole locations and depth or invert elevations.
3. Sanitary sewer lines shall be located in the public right-of-way, particularly the street, unless the applicant can demonstrate as part of a discretionary review why the alternative location is necessary and meets accepted engineering standards.
4. Sanitary sewer line shall be at a depth that can facilitate connection with down-system properties in an efficient manner.
5. For non-residential development, the sanitary sewer line should be designed to minimize the amount of lineal feet in the system.
6. The sanitary sewer line shall avoid disturbance of wetland and drainageways. In those cases where that is unavoidable, disturbance shall be mitigated pursuant to Chapter 32 CDC, Water Resource Area Protection, all trees replaced, and proper permits obtained. Dual sewer lines may be required so the drainageway is not disturbed.
7. Sanitary sewer shall be extended or stubbed out to adjacent undeveloped land or a point in the street that allows for connection with adjacent or nearby properties.
8. The sanitary sewer system shall be built pursuant to DEQ, City, and Tri-City Service District sewer standards. The design of the sewer system shall be prepared by a licensed engineer, and the applicant must be able to demonstrate the ability to satisfy these submittal requirements or standards at the pre-construction phase.
9. A written statement, signed by the City Engineer, that sanitary sewers with sufficient capacity to serve the proposed development and that adequate sewage treatment plant capacity is available to the City to serve the proposed development.

**RESPONSE:** Per the City's Engineering Department pre-app comments, the existing 8" PVC main in Landis Street has adequate capacity to serve the proposed partition. As required, the existing main will be extended into the site within the proposed Landis Street section to serve Parcels 1 and 2. Additionally, a new sewer line will be installed in the Reed-Cornwall connector proposed along the northern boundary of the site to serve development to the north. Please refer to the Composite Utility Plan, Sheet C500 of Exhibit B.

H. Storm detention and treatment. All proposed storm detention and treatment facilities comply with the standards for the improvement of public and private drainage systems located in the West Linn Public Works Design Standards, as demonstrated by stormwater plan and report stamped by a professional engineer.

**RESPONSE:** As demonstrated by the submitted plans, Exhibit B, on-site stormwater will be captured and treated in vegetated planters along both sides of Landis Street, the south side of the Reed-Cornwall connection along Parcel 2's frontage, and along Parcel 1's Cornwall frontage before conveyance to the public system. As detailed in the submitted Composite Utility Plan, Exhibit B, and the Stormwater Memo, Exhibit C, the stormwater facilities have been designed to comply with West Linn Public Works Design Standards. Please see the stormwater memo for additional information.

I. Utility easements. Subdivisions and partitions shall establish utility easements to accommodate the required service providers as specified in the West Linn Public Works Design Standards.

**RESPONSE:** As shown on the submitted plans, an 8' public utility easement is provided along the Landis and Reed-Cornwall frontages of each parcel, and along the Cornwall frontage of Parcel 1.

J. Supplemental Provisions

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3. Street trees. Street trees are required as identified in Section [8.720](#) of the municipal code and Chapter [54](#) CDC.

**RESPONSE:** Street trees will be planted in the planting strips in accordance with requirements.

...

5. Dedications and exactions. The City may require an applicant to dedicate land and/or construct a public improvement that provides a benefit to property or persons outside the property that is the subject of the application when the exaction is roughly proportional. No exaction shall be imposed unless supported by a determination that the exaction is roughly proportional to the impact of development.

**RESPONSE:** As shown in the submitted plan set, Exhibit B, dedication of right-of-way and construction of Landis Street and a section of the Reed-Cornwall connection are proposed through this application. Both right-of-way improvements are listed in the City's TSP.

6. Underground utilities. All utilities, such as electrical, telephone, and television cable, that may at times be above ground or overhead shall be buried underground in the case of new development. Exceptions shall be permitted in those cases where adjacent properties have above-ground utilities and where the development site's frontage is under 200 feet and the site is less than one acre. High voltage transmission lines, as classified by Portland General Electric or electric service provider, are also exempted. Where adjacent future development is planned or proposed, conduits may be required at the direction of the City Engineer. All services shall be underground with the exception of standard above-grade equipment such as some meters, etc.

**RESPONSE:** All utilities on the site will be underground. Please refer to the submitted plans, Exhibit B, for additional information.

7. Density requirement. Density shall occur at 70 percent or more of the maximum density allowed by the underlying zoning as calculated per CDC [05.025](#).

a. These provisions do not apply to planned unit developments where density is calculated per Chapter [24](#) CDC.

b. Minimum density calculations per CDC [05.025](#) that result in three parcels or less are exempt and can divide into two parcels.

**RESPONSE:** This application proposes a partition of one lot into two parcels, with resulting parcel sizes of 20,649 for Parcel 1 and 22,940 for Parcel 2, as shown in the submitted Site Plan included in Exhibit B. No residential development is proposed through this application; however, the proposed parcels will be of adequate size to accommodate future residential development to greater than minimum density as allowed in the R-10 zone.

### 85.220 EXPEDITED LAND DIVISIONS

An expedited land division, as defined by ORS [197.360](#), is an alternative process for the review of preliminary partition or subdivision plats. An expedited land division compresses review timelines, provides for different noticing requirements, and mandates a specific appeal process.

#### A. Submittal requirements.

1. Written responses to ORS [197.360](#)(1) describing how the proposal qualifies for an expedited land division.
2. Confirmation of participation in a pre-application conference per CDC [85.140](#).
3. Required neighborhood meeting materials per CDC [99.038](#) for subdivision proposals.
4. Materials identified in CDC [85.150](#) through [85.190](#).

#### B. Approval criteria.

1. ORS [197.360](#) through [197.380](#).
2. CDC [85.200](#).

**RESPONSE:** This application includes the required materials listed above, including this narrative addressing applicable approval criteria, application form, preliminary engineering plan set, supporting exhibits and a Tualatin Valley Fire & Rescue SPL. As requested by Planning staff, we have included responses to ORS [197A.142](#) in place of ORS [197.360](#).

## CHAPTER 92 REQUIRED IMPROVEMENTS

### 92.010 PUBLIC IMPROVEMENTS FOR LAND DIVISIONS

The following improvements shall be installed at the expense of the developer and meet all City codes and standards:

#### A. Streets within subdivisions.

...

**RESPONSE:** *Subsections 92.010.A - Q* refer to subdivisions and are not applicable to partitions.

### 92.020 IMPROVEMENTS IN PARTITIONS

The same improvements shall be installed to serve each parcel of a partition as are required of a subdivision, as specified in CDC [92.010](#). However, if the approval authority finds that the nature of development in the vicinity of the partition makes installation of some improvements unreasonable, at the written request of the applicant those improvements may be waived. If the street improvement requirements are waived, the applicant shall pay an in-lieu fee for off-site street improvements, pursuant to the provisions of CDC [85.200](#)(A)(1).

In lieu of accepting an improvement, the Planning Director may recommend to the City Council that the improvement be installed in the area under special assessment financing or other facility extension policies of the City.

**RESPONSE:** As discussed in responses to *Section 85.200*, above, and depicted on the submitted Preliminary Engineering Plans, attached as Exhibit B, the Applicant proposes to design and construct the extension of Landis Street and a portion of the connection between Reed Street and Cornwall Street streets to meet the City's design and construction standards. In addition, storm, water and sanitary sewer utilities will be provided to serve the parcels as shown in the plans.

#### *92.030 IMPROVEMENT PROCEDURES*

*In addition to other requirements, public improvements installed by the developer, either as a requirement of these regulations or at the developer's own option, shall conform to the requirements of this title and permanent improvement standards and specifications adopted by the City and shall be installed in accordance with the following procedure:*

- A. Improvement work shall not be commenced until plans have been checked for adequacy and approved by the City. To the extent necessary for evaluation of the proposal, the improvement plans may be required before approval of the tentative plan of a subdivision or partition. Plans shall be prepared in accordance with the requirements of the City.*
- B. Improvement work shall not be commenced until a preconstruction meeting has been held.*
- C. Improvements shall be constructed under the City Engineer's supervision and authorization. The City may require changes in typical sections and details in the public interest if unusual conditions arise during construction to warrant the change.*
- D. All underground utilities, sanitary sewers, and storm drains installed in streets by the subdivider or by any utility company shall be constructed prior to the surfacing of the streets. Stubs for service connections for underground utilities and sanitary sewers shall be placed to a length obviating the necessity for disturbing the street improvements when service connections are made.*
- E. A digital map showing all public improvements as built shall be filed with the City Engineer upon completion of the improvements.*

**RESPONSE:** The Applicant acknowledges the applicability of *Section 92.030*, above. Improvements will be preceded by approval of plans and a preconstruction meeting and will be installed under the supervision/authorization of the City Engineer. Installation of underground utilities will be done in timely order and using methods that allow for future service connections. As-builts will be filed upon completion of the public improvements.

#### *96.010 CONSTRUCTION REQUIRED*

- A. Street improvements for residential construction are required when:*
  - 1. Construction of a new single-family attached or detached structure (replacement of an existing structure is exempt); or*
  - 2. Replacement of a single-family home increases the square feet by 50 percent or greater; or*
  - 3. Construction of a new multifamily structure; or*
  - 4. Increase in dwelling unit density on site (accessory dwelling units are exempt).*
- B. Street improvements for commercial construction are required when:*
  - 1. Construction of a new commercial structure; or*
  - 2. Remodel of an existing commercial structure with an increase in floor area; or*

3. Construction that increases the dwelling unit density on site; or
4. Construction which requires a change in type, number, or location of accessways; or
5. Replacement of an existing structure with an increase in floor area.

**RESPONSE:** This application proposes a two-parcel partition. Right-of-way improvements are proposed for two local street projects listed in the City's TSP, as well as frontage improvements along Cornwall.

#### 96.020 FEE-IN-LIEU

A. An applicant may apply for a waiver of street improvements and the option to pay a fee-in-lieu (in accordance with the City's adopted fee structure) of constructing street improvements if one of the following are met:

1. Located on a cul-de-sac with no existing curb and/or no existing sidewalk; or
2. Located on a street less than 1,320 linear feet in length and not planned as a through street; or
3. Located more than 1,320 linear feet from nearest street improvements on the same street or connecting street.

**RESPONSE:** The Applicant proposes construction of right-of-way improvements as shown on the submitted plans. No waiver/fee-in-lieu is requested through this application.

#### 96.030 STANDARDS

Street improvements shall be installed according to the City standards and shall be completed prior to the issuance of any occupancy permit for the new or remodeled structure or building. In unimproved areas of the City, the City Engineer may grant a time extension of the provisions of this section; provided the applicant submit sufficient security in an amount and quantity satisfactory to the City Attorney to assure payment of such improvement costs.

**RESPONSE:** The Applicant acknowledges this provision.

#### Chapter 99 PROCEDURES FOR DECISION MAKING: QUASI-JUDICIAL

**RESPONSE:** The Applicant is seeking approval of the proposed partition and right-of-way improvements through the Expedited Land Division process under authority of the Planning Director or designee.

#### Conclusion

This application provides substantial evidence demonstrating that the proposed two-parcel Expedited Land Division satisfies all applicable standards of the *Oregon Revised Statutes Chapter 197* and the *West Linn Community Development Code*. The Applicant respectfully requests approval of this two-parcel Expedited Land Division application.

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WEST LINN

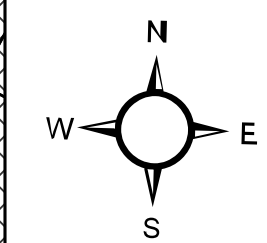
N.E. 1/4 N.W. 1/4 SEC. 36 T.2S. R.1E. W. M.  
CLACKAMAS COUNTY

1" = 100'

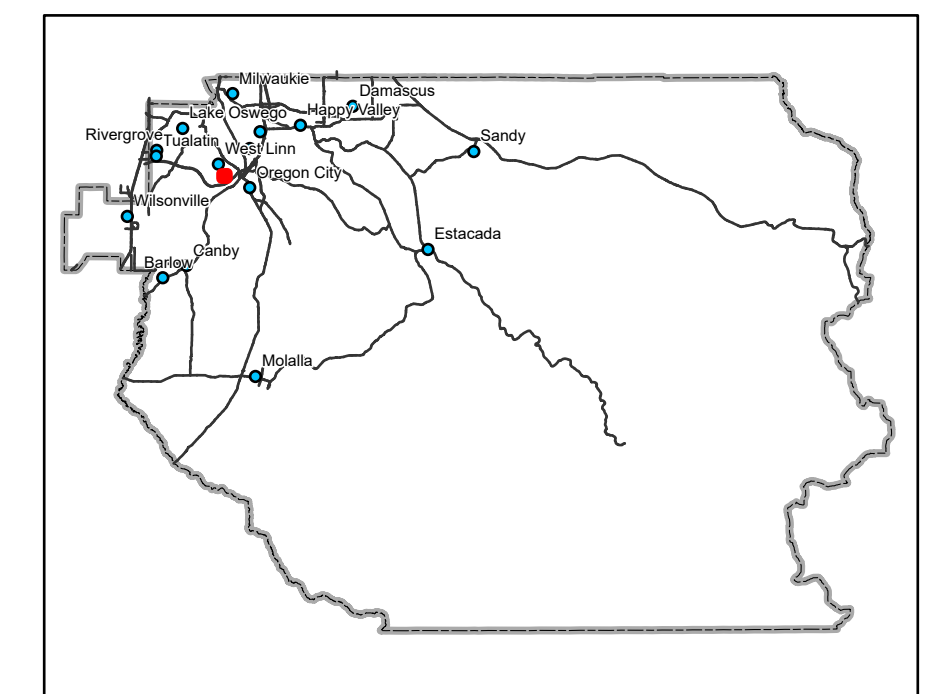
D. L. C.  
JULIA ANN LEWIS NO. 54

Cancelled Numbers

- 5700
- 7200
- 6200
- 5400
- 5800
- 3800
- 4600
- 4200
- 5990
- 300
- 5600
- 3000
- 6300



- Parcel Boundary
- Private Road ROW
- Historical Boundary
- Railroad Centerline
- Tax Code Lines
- Water Lines
- Land Use Zoning
- Plats
- Water
- Corner
- Section Corner
- 1/16th Line
- Govt Lot Line
- DLC Line
- Meander Line
- PLSS Section Line
- Historic Corridor 40'
- Historic Corridor 20'



THIS MAP IS FOR ASSESSMENT  
PURPOSES ONLY

Print Date: 7/14/2025

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WEST LINN



# PRELIMINARY PLANS FOR CORNWALL PARTITION WEST LINN, OREGON

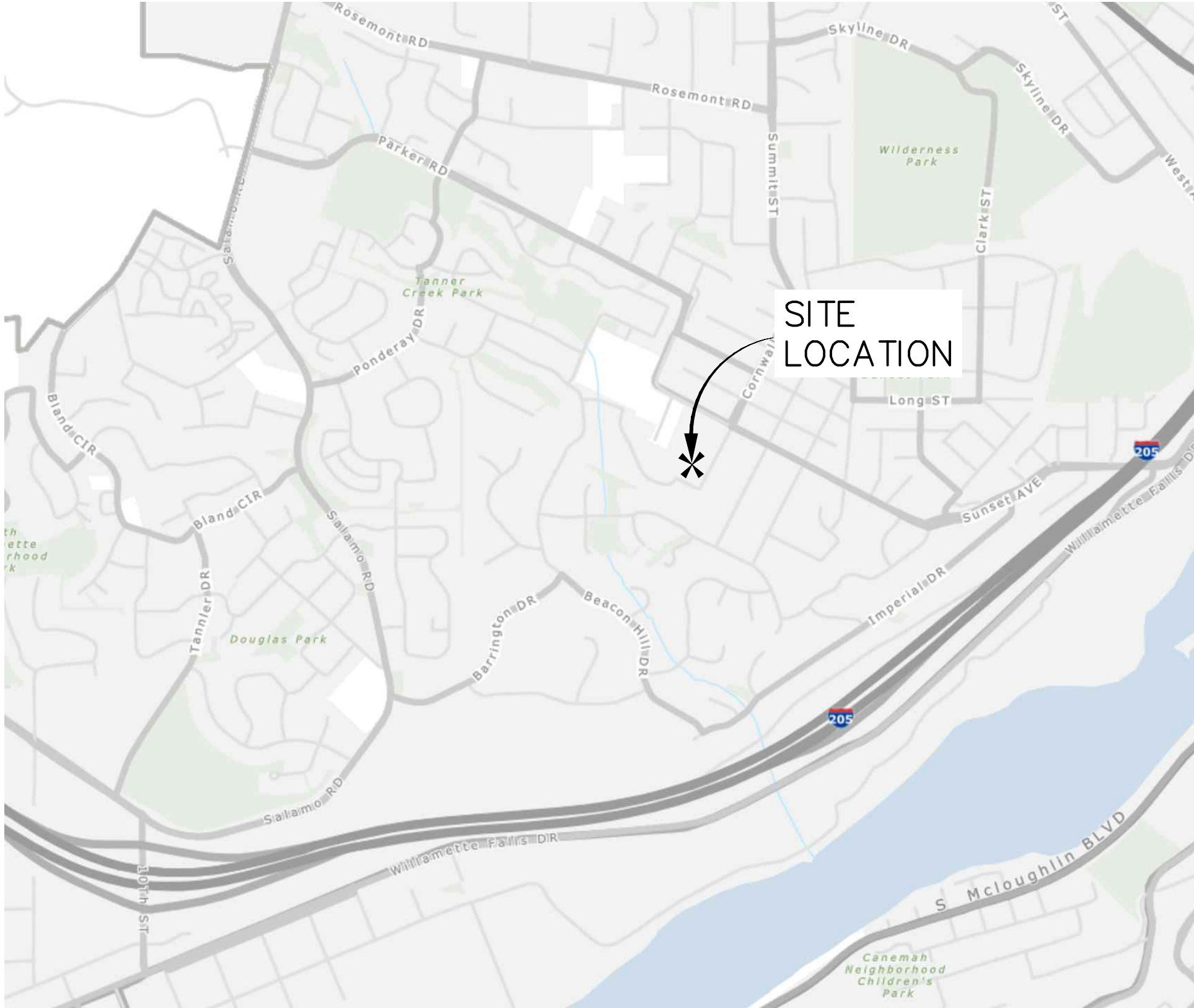
**PRELIMINARY**

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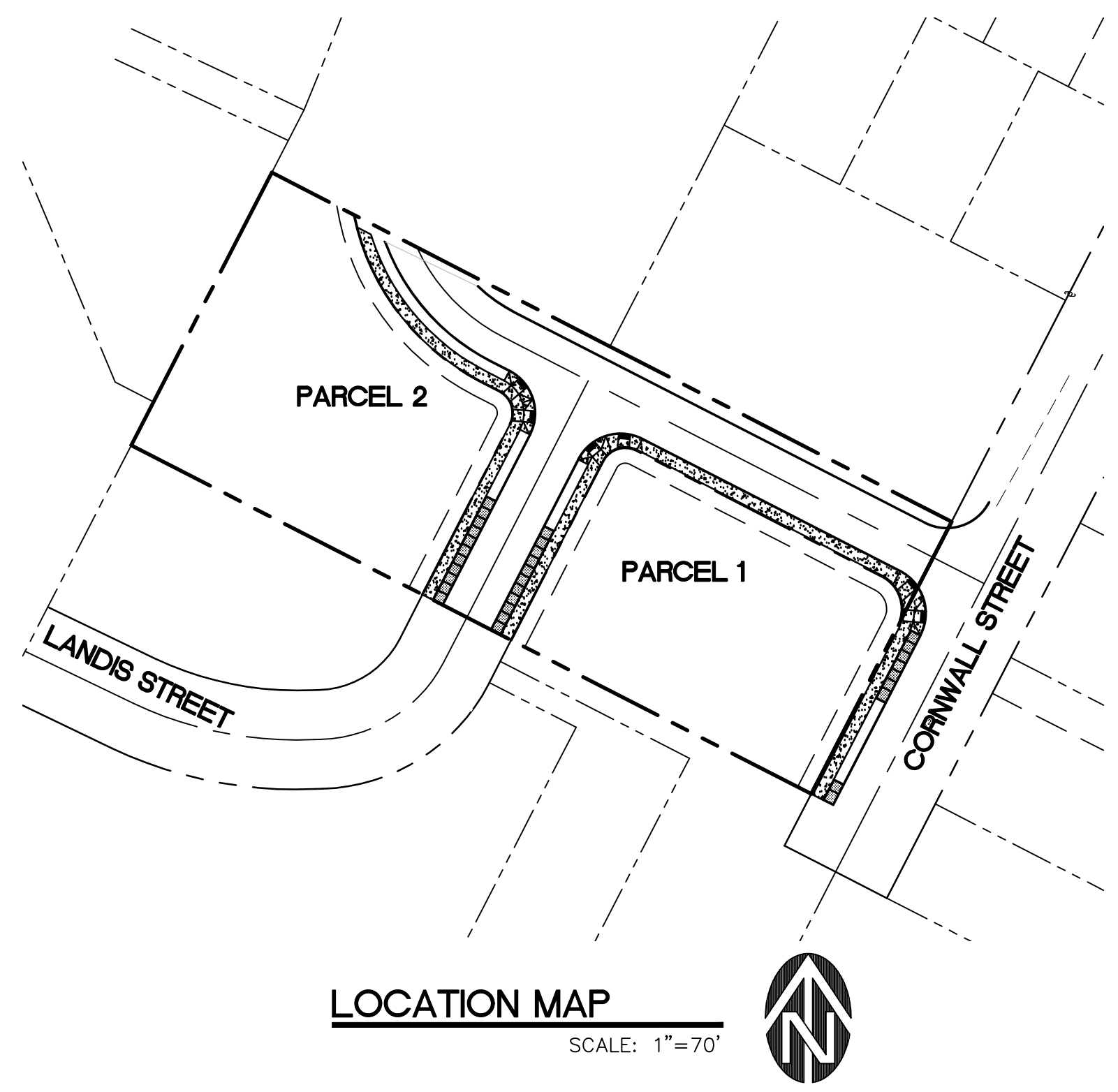
NO.	DATE	DESCRIPTION	DRAFT BY:	CHECK BY:

SHEET  
**C100**  
JOB NO.  
3077-004

1ST LAND USE SUBMITTAL



**VICINITY MAP**  
NOT TO SCALE



**LOCATION MAP**  
SCALE: 1"=70'

**SHEET INDEX**

C100	COVER SHEET
C200	EXISTING CONDITIONS & DEMOLITION PLAN
C201	PRELIMINARY PLAT
C300	SITE PLAN
C400	GRADING PLAN
C500	COMPOSITE UTILITY PLAN

**PROPERTY DESCRIPTION**

**TAX MAP & TL:**  
TAX MAP 21E36BA, TAX LOT 4100

**SITE SIZE:**  
1.46 ACRES

**PROPOSAL:**  
2-LOT PARTITION

**STREET ADDRESS**

4110 CORNWALL STREET  
WEST LINN, OR 97068

**BENCHMARK**

THE BASIS OF BEARINGS WAS SURVEY NO. 2014-039, CLACKAMAS COUNTY RECORDS.

LOCAL DATUM WAS ESTABLISHED BY THE CITY OF WEST LINN MANHOLE RIM DATA ON SANITARY SEWER MANHOLES.

**APPLICANT**

THE PORTLOCK COMPANY  
6105 W A STREET, SUITE B  
WEST LINN, OR 97068  
PHONE: (425) 829-1566  
CONTACT: JJ PORTLOCK

**ENGINEER**

WESTLAKE CONSULTANTS, INC.  
PACIFIC CORPORATE CENTER  
15115 S.W. SEQUOIA PARKWAY,  
SUITE 150 TIGARD, OREGON 97224  
PHONE: (503) 684-0652  
CONTACT: DANIEL BAILEY, PE  
EMAIL: DBAILEY@WESTLAKECONSULTANTS.COM

**ATTENTION:**  
OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES BY CALLING THE CENTER. (NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION CENTER IS (503) 232-1987).

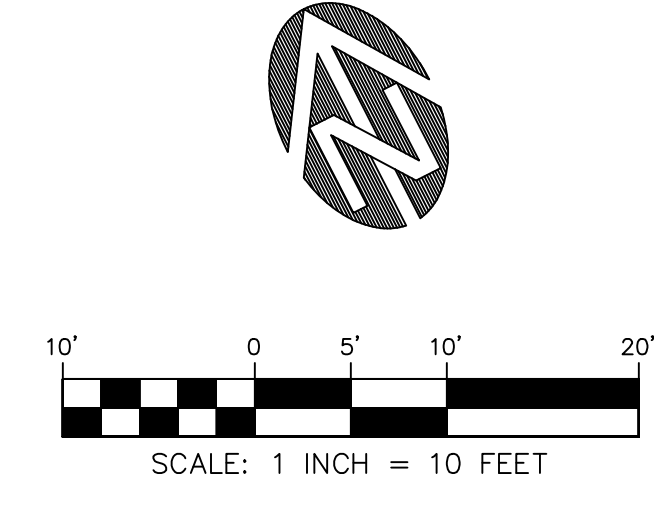
**UTILITY STATEMENT:**  
THE UNDERGROUND UTILITIES SHOWN ARE PER FIELD MARKINGS AND RECORD DRAWINGS PROVIDED BY THE RESPECTIVE UTILITY AGENCIES. LOCATION OF NON-OBSERVABLE AND/OR UNDERGROUND UTILITIES ARE SHOWN FOR INFORMATION ONLY AND ARE NOT GUARANTEED TO BE COMPLETE OR ACCURATE.

**UTILITY VERIFICATION:**  
CONTRACTOR SHALL POTHOLE TO VERIFY LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO COMMENCING CONSTRUCTION AND SHALL PROVIDE WESTLAKE CONSULTANTS, INC. 72-HOURS NOTICE OF ANY POTENTIAL CONFLICTS.

**ORS UTILITY STATEMENT**  
THIS DESIGN COMPLIES WITH ORS 92.044 (7) IN THAT NO UTILITY INFRASTRUCTURE IS DESIGNED TO BE WITHIN ONE (1) FOOT OF A SURVEY MONUMENT LOCATION SHOWN ON A SUBDIVISION OR PARTITION PLAT. NO DESIGN EXCEPTIONS NOR FINAL FIELD LOCATION CHANGES SHALL BE PERMITTED IF THAT CHANGE WOULD CAUSE ANY UTILITY INFRASTRUCTURE TO BE PLACED WITHIN THE PROHIBITED AREA.



DRAWING NAME: J:\3077-004-26-6-ENGINEERING\3-CAD\PRODUCTION\1-LU\3077-004-C200.DWG 2026/04/24 - 03:17PM - JDW



**LEGEND**

- EXISTING DECIDUOUS TREE W/ TRUNK DIAMETER (INCHES)(CL=CLUSTER)
- EXISTING CONIFEROUS TREE W/ TRUNK DIAMETER (INCHES)(CL=CLUSTER)
- EXISTING ELECTRIC METER
- EXISTING ELECTRIC RISER
- EXISTING ELECTRIC VAULT
- EXISTING POWER POLE
- EXISTING GUY ANCHOR
- EXISTING OVERHEAD POWER LINES
- EXISTING WATER METER
- EXISTING UNDERGROUND GAS LINE
- EXISTING UNDERGROUND STORM LINE
- EXISTING UNDERGROUND SANITARY SEWER LINE
- EXISTING SANITARY MANHOLE
- EXISTING STORM MANHOLE
- EXISTING MAILBOX
- EXISTING FENCE
- FOUND MONUMENTS
- EXISTING FIRE HYDRANT

**DEMOLITION KEYNOTES**

- ① EXISTING STRUCTURE TO BE DEMOLISHED. DEBRIS AND REFUSE TO BE DISPOSED OF OFF-SITE AT AN APPROVED LOCATION. CONTRACTOR TO OBTAIN DEMO PERMIT FROM THE CITY OF WEST LINN PRIOR TO DEMOLISHING THE HOUSE.
- ② REMOVE EXISTING RETAINING WALL.
- ③ RELOCATE OR UNDERGROUND EXISTING POWER POLE.
- ④ REMOVE EXISTING DRIVEWAY.
- ⑤ REMOVE EXISTING TREE FOR ROADWAY IMPROVEMENTS.

**NOTES**

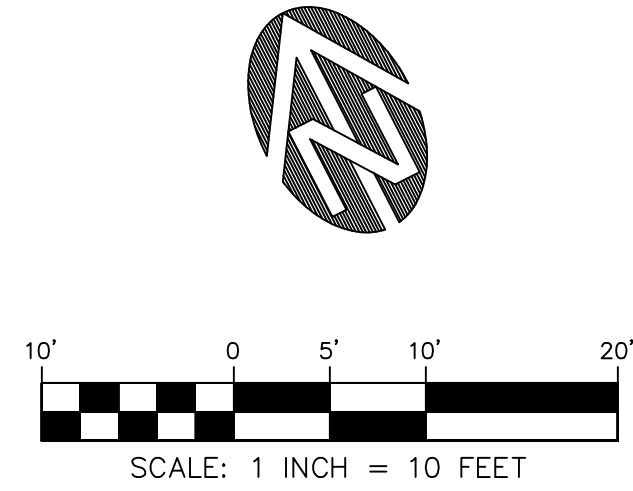
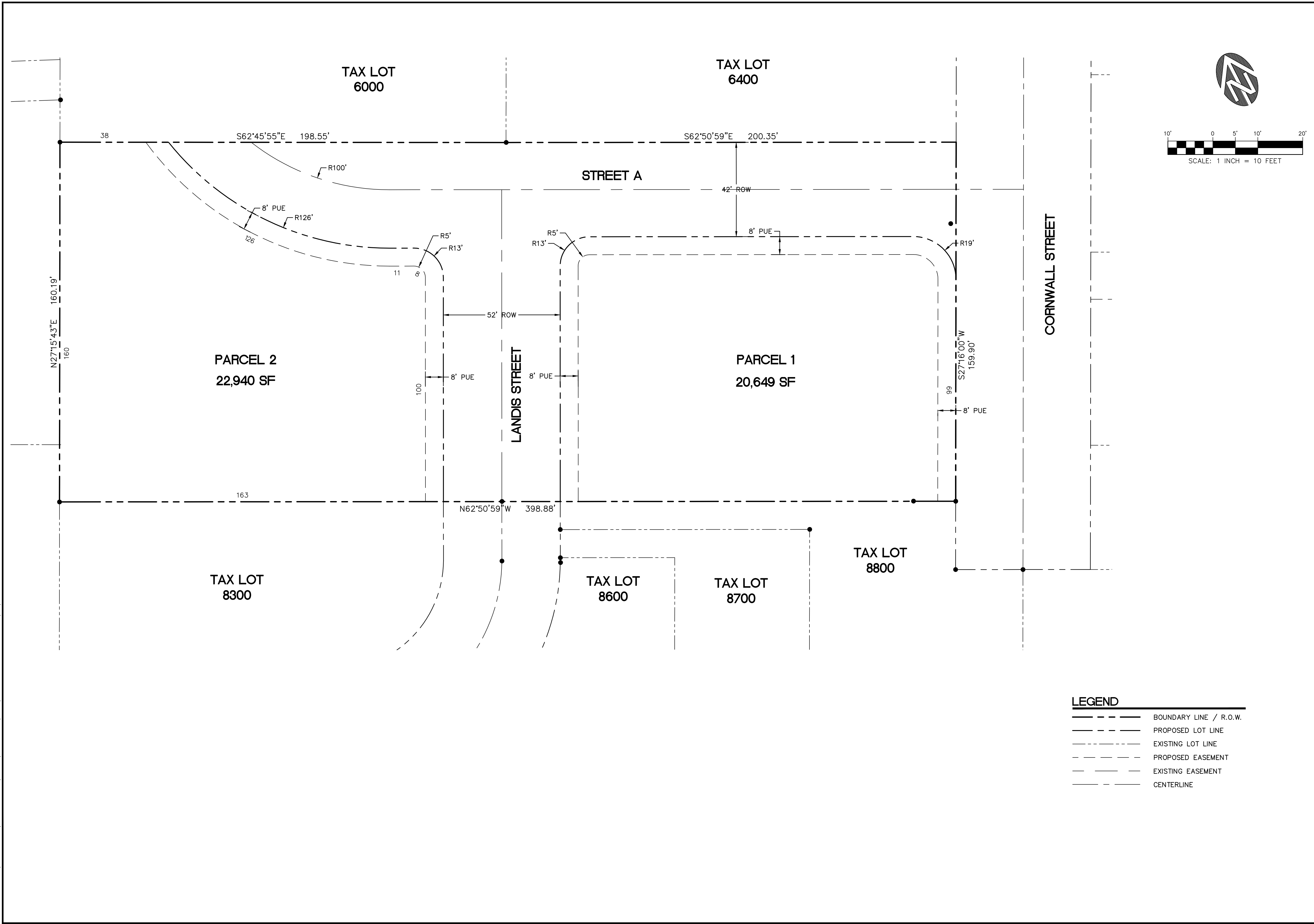
- 1. THE PURPOSE OF THIS MAP WAS TO SHOW THE EXISTING CONDITIONS FOR 4110 CORNWALL STREET. BUILDING FOOTPRINTS ARE MEASURED TO SIDING UNLESS OTHERWISE NOTED.
- 2. THE BASIS OF BEARINGS WAS SURVEY NO. 2014-039, CLACKAMAS COUNTY RECORDS.
- 3. LOCAL DATUM WAS ESTABLISHED BY THE CITY OF WEST LINN MANHOLE RIM DATA ON SANITARY SEWER MANHOLES.
- 4. THIS MAP WAS PREPARED FOR THE EXCLUSIVE USE OF THE PORTLOCK COMPANY.
- 5. THIS MAP WAS PREPARED BY PLAT RECORDS, CALCULATED DATA, AND FIELD MEASUREMENTS. PROPERTY LINES SHOULD BE CONSIDERED APPROXIMATE. A RECORDED PARTITION PLAT WILL BE FILED AT A DATE TO BE DETERMINED.
- 6. ALL UTILITY LOCATIONS ARE SHOWN BY ABOVE GROUND FEATURES, LOCATION OF PAINT MARKS SUPPLIED BY THE LOCAL UTILITY COMPANIES, OR AVAILABLE ONLINE DATA. CMT TAKES NO RESPONSIBILITY OF UNDERGROUND LOCATION AND MAKES NO GUARANTEE THIS MAP REPRESENTS ALL UTILITIES IN THE AREA. PLEASE NOTIFY THE UTILITY NOTIFICATION CENTER BEFORE ANY DIGGING 1-800-332-2344.
- 7. EXISTING CONDITIONS SHOWN ON THIS SHEET ARE BASED ON A SURVEY BY CMT SURVEYING AND CONSULTING, DATED 11/10/2025. WESTLAKE DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THIS SURVEY.

**PRELIMINARY**

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NO.	DATE	DESCRIPTION	DRAFT BY:	CHECK BY:

DRAWING NAME: J:\3077-004\26\6. ENGINEERING\3. CAD\PRODUCTION\1. LU\3077-004. C201.DWG - 2026/04/24 - 03:18PM - JDM



**LEGEND**

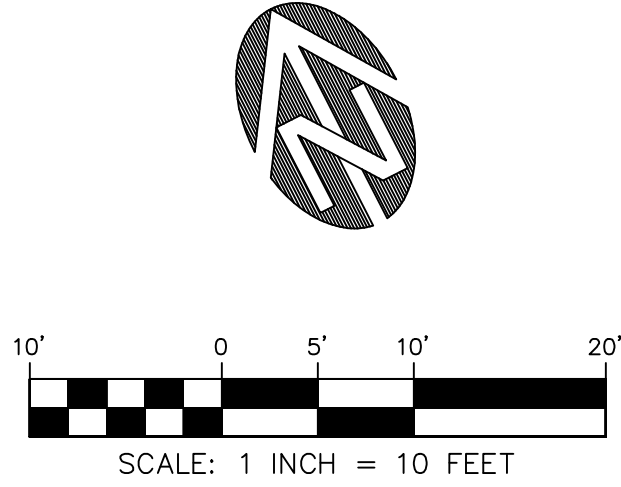
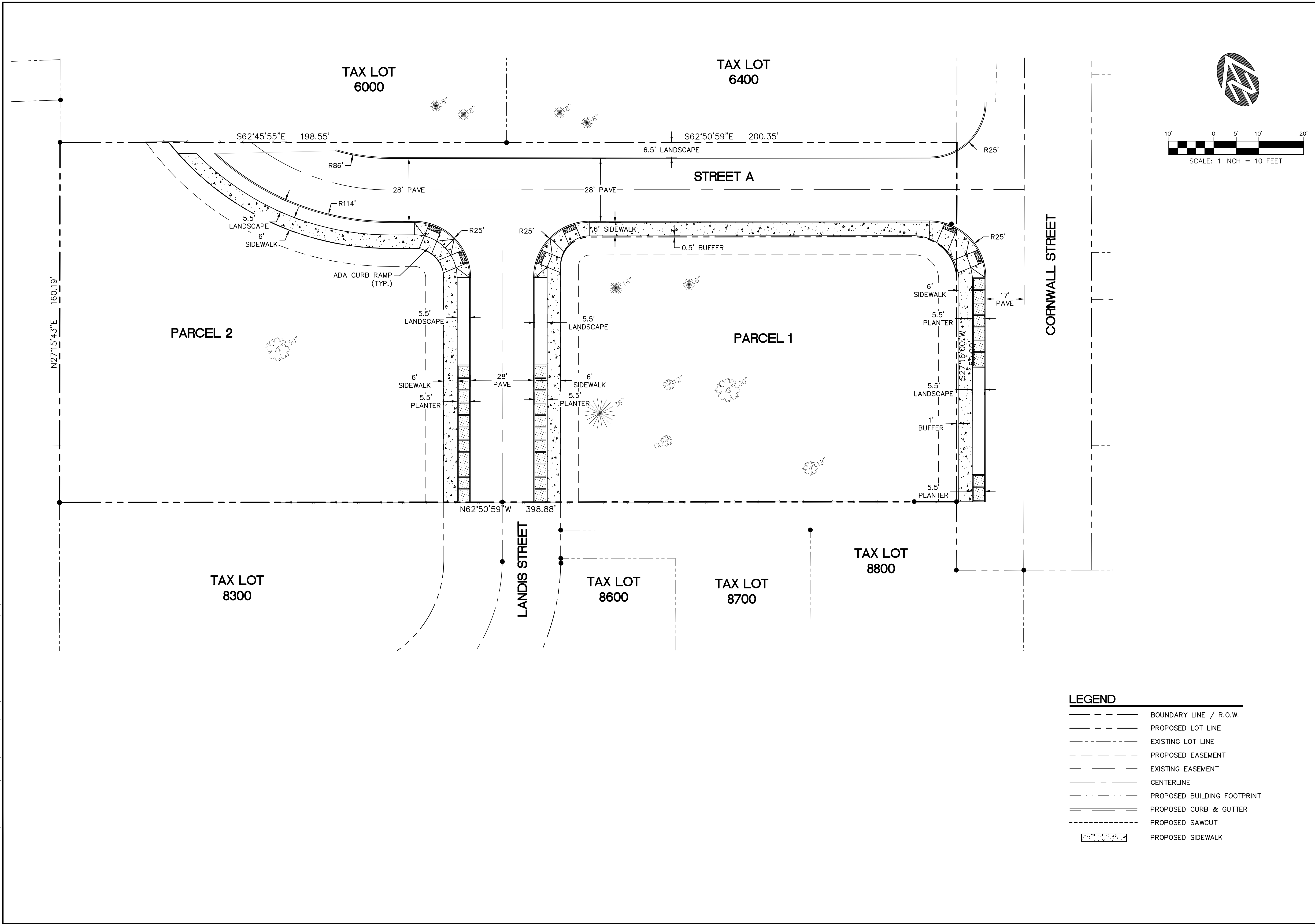
	BOUNDARY LINE / R.O.W.
	PROPOSED LOT LINE
	EXISTING LOT LINE
	PROPOSED EASEMENT
	EXISTING EASEMENT
	CENTERLINE

**PRELIMINARY**

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DRAWING NAME: J:\3077-004\26\6. ENGINEERING\3. CAD\PRODUCTION\1. LU\3077-004\_C300.DWG 2026/04/24 - 03:18PM - JDM



**LEGEND**

	BOUNDARY LINE / R.O.W.
	PROPOSED LOT LINE
	EXISTING LOT LINE
	PROPOSED EASEMENT
	EXISTING EASEMENT
	CENTERLINE
	PROPOSED BUILDING FOOTPRINT
	PROPOSED CURB & GUTTER
	PROPOSED SAWCUT
	PROPOSED SIDEWALK

**CORNWALL PARTITION**  
 WEST LINN, OR  
 SITE PLAN

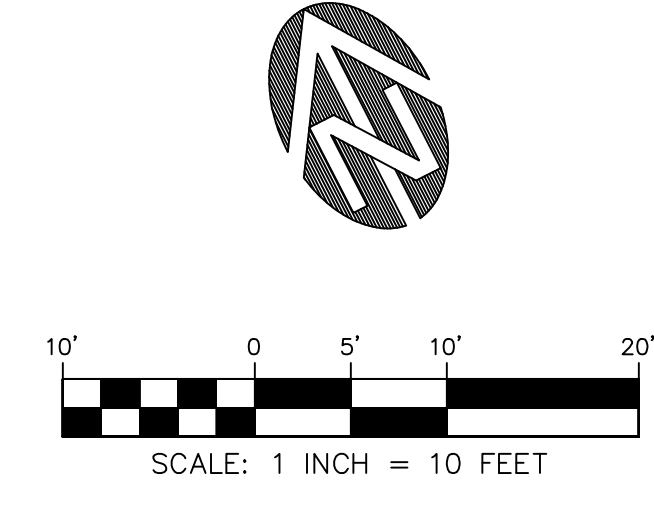
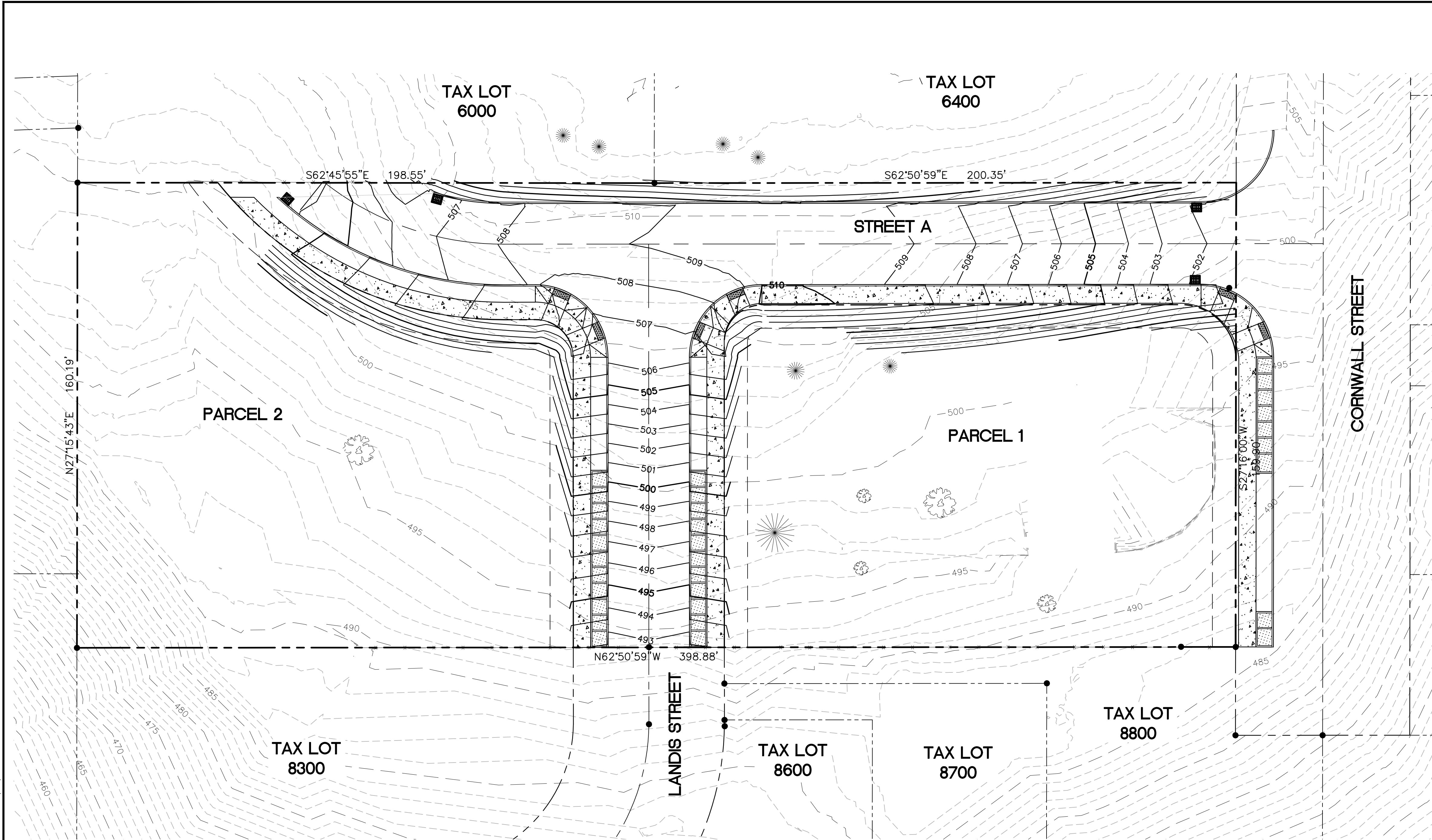
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NO.	DATE	DESCRIPTION	DRAFT BY:	CHECK BY:

SHEET  
**C300**  
 JOB NO.  
 3077-004

DRAWING NAME: J:\3077-004\26\6. ENGINEERING\3. CAD\PRODUCTION\1. LU\3077-004. C400.DWG 2026/04/24 - 03:18PM - JDW



**LEGEND**

---	221	---	EXISTING CONTOUR - 1' INTERVAL
---	220	---	EXISTING CONTOUR - 5' INTERVAL
---	221	---	PROPOSED CONTOUR - 1' INTERVAL
---	220	---	PROPOSED CONTOUR - 5' INTERVAL

**CORNWALL PARTITION**  
 WEST LINN, OR  
 GRADING PLAN

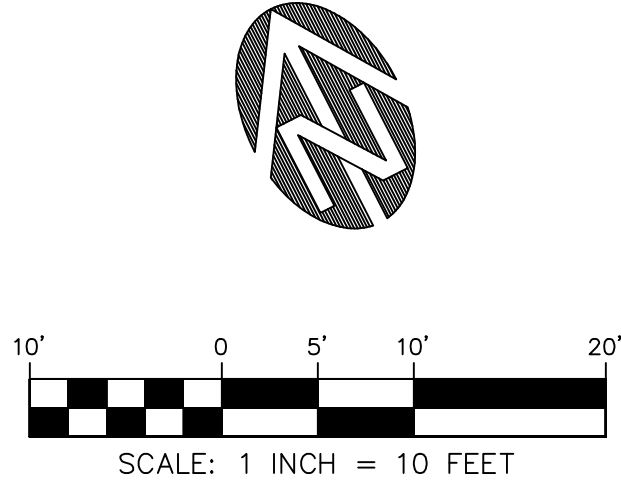
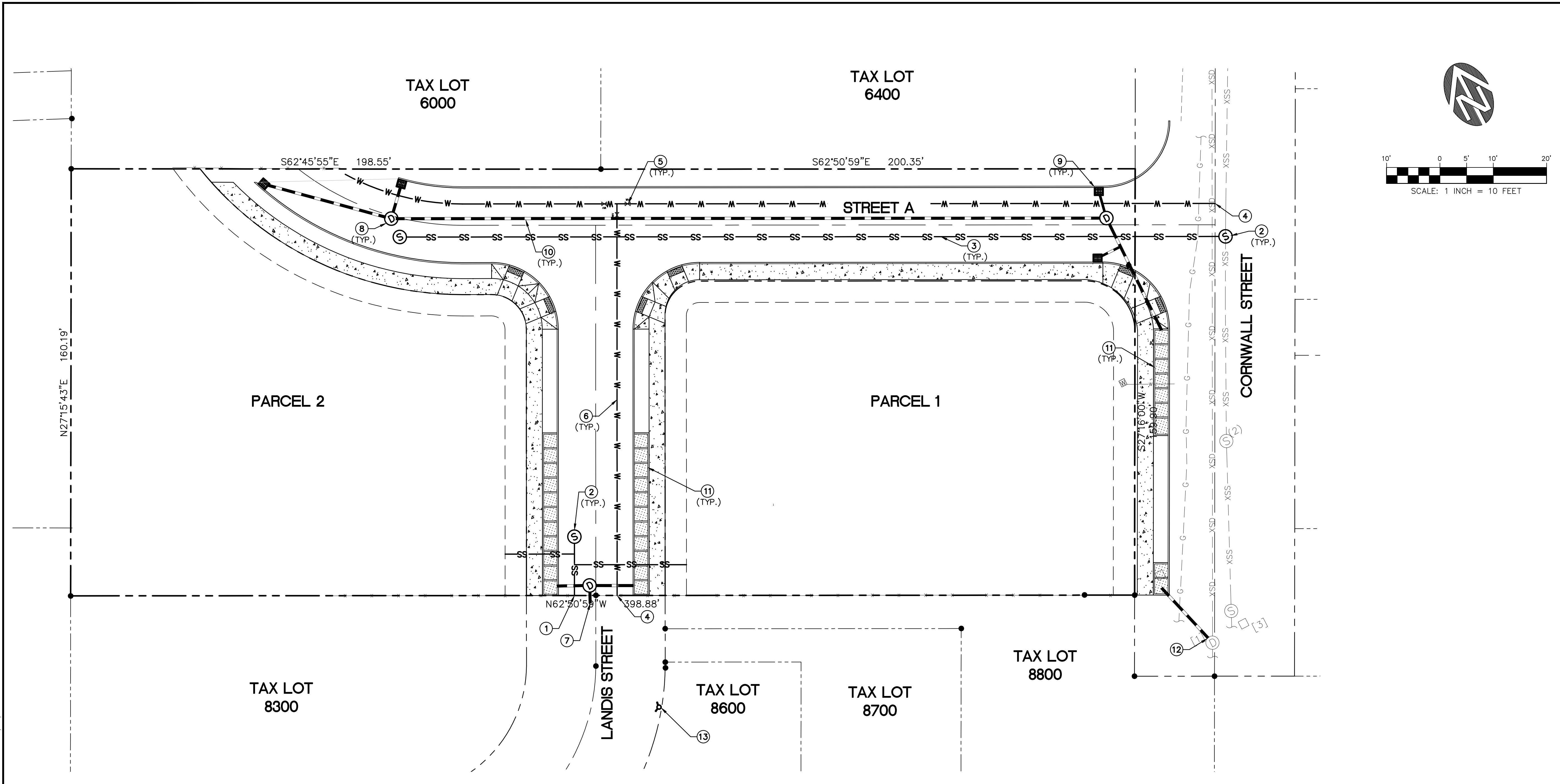
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NO.	DATE	DESCRIPTION	DRAFT BY:	CHECK BY:

SHEET  
**C400**  
 JOB NO.  
 3077-004

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- UTILITY KEYNOTES**
- ① CONNECT TO EXISTING SANITARY SEWER.
  - ② PROPOSED SANITARY SEWER MANHOLE.
  - ③ PROPOSED SANITARY SEWER LINE.
  - ④ CONNECT TO EXISTING WATER LINE.
  - ⑤ PROPOSED WATER VALVE.
  - ⑥ PROPOSED WATER LINE.
  - ⑦ CONNECT TO EXISTING STORM LINE.
  - ⑧ PROPOSED STORM MANHOLE.
  - ⑨ PROPOSED STORM GUTTER INLETS.
  - ⑩ PROPOSED STORM MAIN LINE.
  - ⑪ PROPOSED STORM PLANTER.
  - ⑫ CONNECT TO EXISTING STORM MANHOLE.
  - ⑬ EXISTING FIRE HYDRANT.

- LEGEND**
- SS—SS—SS— PROPOSED SANITARY LINE
  - ⊙ PROPOSED SANITARY MANHOLE
  - S—S—S— PROPOSED STORM LINE
  - ⊙ PROPOSED STORM MANHOLE
  - PROPOSED CURB INLET
  - PROPOSED DITCH INLET
  - W—W—W— PROPOSED WATER LINE
  - ⊙ PROPOSED FIRE HYDRANT
  - ⊙ PROPOSED WATER VALVE
  - ⊙ PROPOSED BLOWOFF
  - ⊙ PROPOSED WATER METER
  - ⊙ PROPOSED COMBINATION AIR RELEASE VALVE

**CORNWALL PARTITION**  
 WEST LINN, OR  
 COMPOSITE UTILITY PLAN

**PRELIMINARY**

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NO.	DATE	DESCRIPTION	DRAFT BY:	CHECK BY:

SHEET  
**C500**  
 JOB NO.  
 3077-004

March 25, 2026

City of West Linn

Building Division

RE: Preliminary Stormwater Management Strategy for 4110 Cornwall Street  
Westlake Project No.: 3077-004

The purpose of this stormwater management memo is to support the development of a two-lot partition at 4110 Cornwall Street, with associated public roads and utilities. The site is a 1.47 acre lot with an existing home on site. Grades slope to the southwest and generally range from 10% to 20%. On-site infiltration testing was conducted by Rapid Soil Solutions on 2/25/2026. Measured infiltration rates ranged from 8 in/hour to 240 in/hour. A factor of safety of 3 was applied when determining the design infiltration rates.

The client proposes to divide the lot into two parcels and construct public streets to serve the development. Stormwater runoff from the public streets will be managed in four street-side infiltration planters constructed per West Linn standard detail WL-STM006. These planters are sized in accordance with the Engineered Sizing Method outlined in the West Linn Stormwater Management Manual, and will fully infiltrate the 25-year storm event. Table 1 below outlines the preliminary sizing of these planters. More detailed information on sizing methodology will be provided during the Permitting phase of this project. In the event that the infiltration facilities fail, stormwater will overflow into the public storm system in Landis Street.

Table 1: Preliminary Planter Sizing

Location	Basin Area (SF) (assume 100% impervious)	Design Infiltration Rate (in/hour)	Planter Area Required (SF)	Planter Area Provided (SF)
Basin 1	3,985	2.7	275	275
Basin 2	3,921	2.7	275	275
Basin 3	15,335	80	180	180
Basin 4	1,246	80	54	54

Stormwater runoff from the private lots will be managed in private infiltration facilities located on each lot. Sizing, location and design of these facilities will be provided during the Permitting phase of this project.

This memo demonstrates that this development will comply with the requirements of the City of West Linn Stormwater Management Manual.

Sincerely,

*Daniel Bailey*

Daniel Bailey, PE

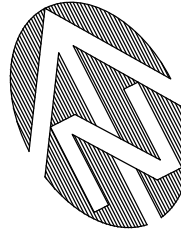
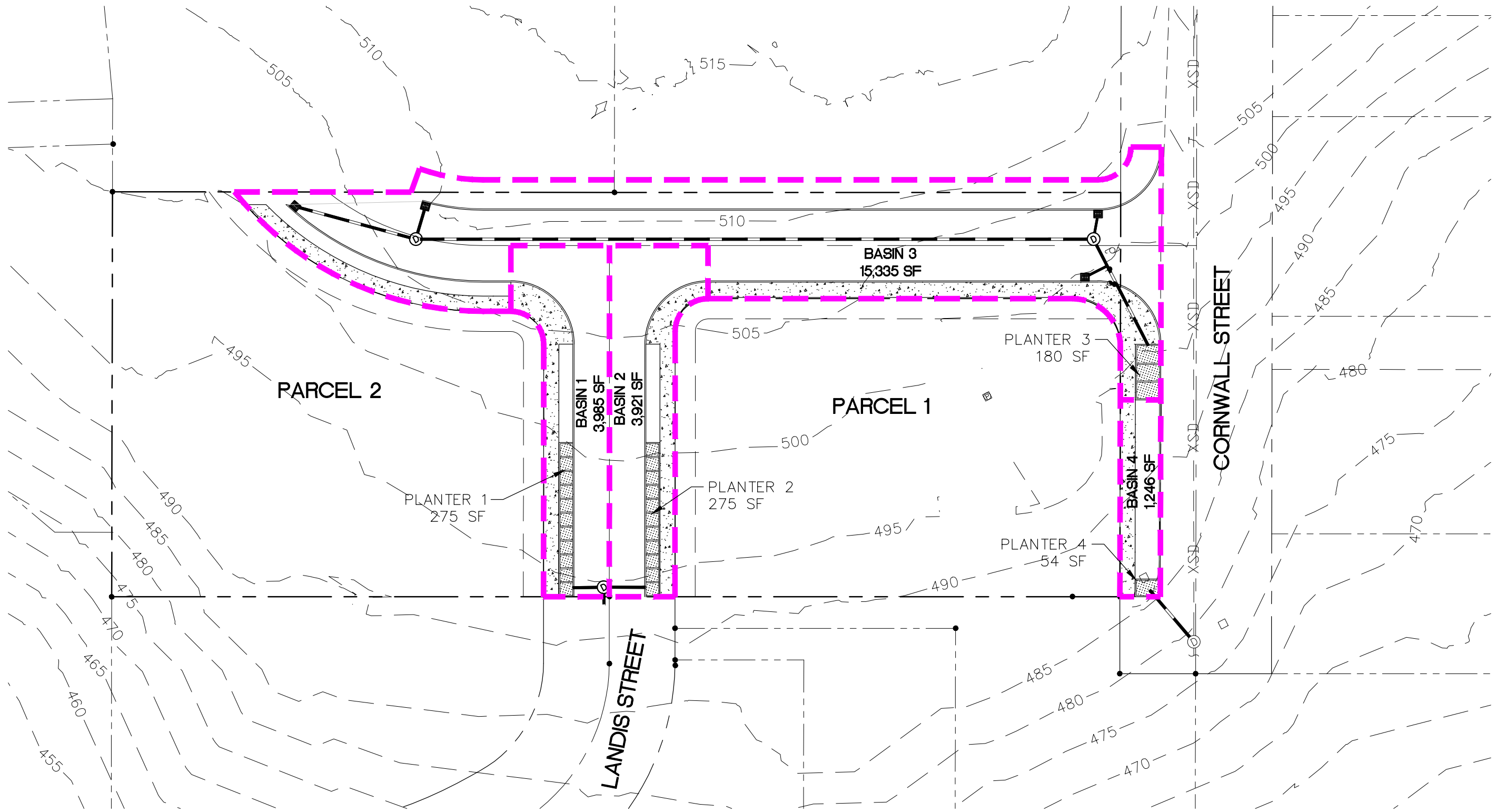
Project Engineer

Westlake Consultants, Inc.

List of Attachments:

1. Proposed Site Plan
2. Infiltration Testing Report

DRAWING NAME: J:\3077-004\26\6-ENGINEERING\4-REPORTS (CALCS)\2025-02-02-LU STORM REPORT\3077-004 SITE PLANDWG 2025/03/25 - 08:20AM - JPB



0 20' 40'  
SCALE: 1" = 40'

DATE	3/2026
REVISION	0
DRAWN BY	JPB
CHECKED BY	---
JOB NO.	3077-004

**CORNWALL PARTITION**  
WEST LINN, OR  
  
SITE PLAN



**WESTLAKE  
CONSULTANTS**  
Planning | Engineering | Surveying

JJ Portlock  
425-829-1566  
jportlock@theportlockco.com

25 February 2026

Re: Infiltration testing for 4110 Cornwall St, West Linn, OR

Dear Mr. Portlock,

**Field Investigation:**

Rapid Soil Solutions (RSS) has attempted to performed one (1) infiltration tests. Figure 1 below shows the project site location. Soils found on site match those in by DOGMI. RSS found stiff fine grained flood deposits, then coarse grained rocks.

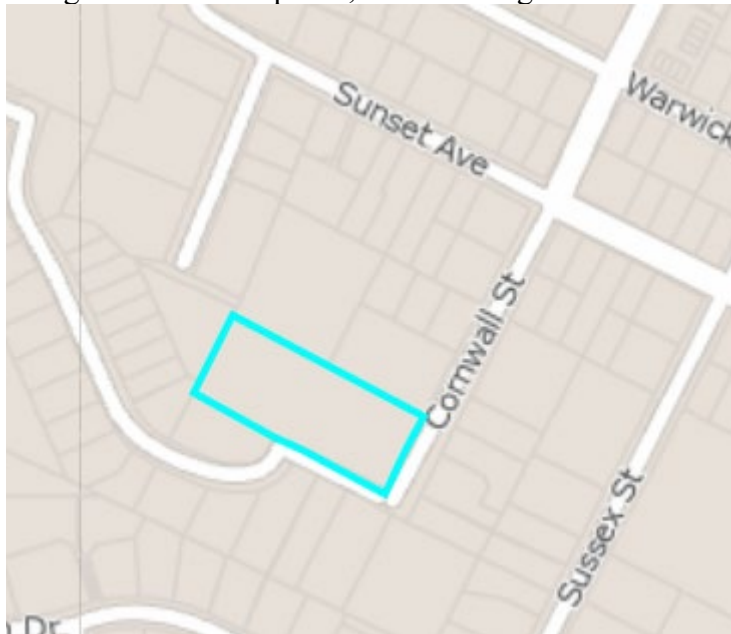


Figure 1

**Infiltration Testing:**

RSS performed three (3) infiltration test per the West Linn Storm water manual. Please see attached testing sheets for location, soil found. A summary of the rates is shown in the below table. A pre-soak was performed on all the holes. That drain instantly, so testing was started.

Location	Depth (ft)	Rate (in/hr.)	Soils found
TP#1	5	240	0-1 Gravels, 1-3ft gravely silt, 3-5ft decomposing rock
TP#2	5	160	0-1 Gravels, 1-3ft gravely silt, 3-5ft decomposing rock
TP#3	7	8	0-1 Gravels, 1-5ft gravely silt, 5-7ft decomposing rock

Site plan with testing location is shown below.



**Groundwater**

Ground water is estimated to be 80ft below grade.



USGS 5059

The analysis, conclusions and recommendations contained in this report are based on site conditions as they existed at the time of explorations. Any questions regarding this report please contact me at the below number or email.

Sincerely,



EXPIRES: 12/31/26

Mia Mahedy, PE GE.

**FIRE CODE / LAND USE / BUILDING REVIEW  
APPLICATION**



**North Operating Center**  
11945 SW 70<sup>th</sup> Avenue  
Tigard, OR 97223  
Phone: 503-649-8577

**South Operating Center**  
8445 SW Elligsen Rd  
Wilsonville, OR 97070  
Phone: 503-649-8577

REV 6-30-20

**Project Information**

Applicant Name: Ken Sandblast / Westlake Consultants, Inc.  
Address: 15115 SW Sequoia Pkwy, #150, Tigard OR 97224\_  
Phone: 503-684-0652  
Email: ksandblast@westlakeconsultants.com  
Site Address: 4110 Cornwall Street  
City: West Linn  
Map & Tax Lot #: 2-1E-36BA 6100  
Business Name: \_\_\_\_\_  
Land Use/Building Jurisdiction: West Linn  
Land Use/ Building Permit # \_\_\_\_\_

Choose from: Beaverton, Tigard, Newberg, Tualatin, North Plains, West Linn, Wilsonville, Sherwood, Rivergrove, Durham, King City, Washington County, Clackamas County, Multnomah County, Yamhill County

**Project Description**  
**Partition of Tax Lot 6100 to create two parcels; Extension of Landis St. through the site; construction of segment of the Reed-Cornwall connection.**

**Permit/Review Type (check one):**

- Land Use / Building Review - Service Provider Permit
- Emergency Radio Responder Coverage Install/Test
- LPG Tank (Greater than 2,000 gallons)
- Flammable or Combustible Liquid Tank Installation (Greater than 1,000 gallons)
  - \* Exception: Underground Storage Tanks (UST) are deferred to DEQ for regulation.
- Explosives Blasting (Blasting plan is required)
- Exterior Toxic, Pyrophoric or Corrosive Gas Installation (in excess of 810 cu.ft.)
- Tents or Temporary Membrane Structures (in excess of 10,000 square feet)
- Temporary Haunted House or similar
- OLCC Cannabis Extraction License Review
- Ceremonial Fire or Bonfire (For gathering, ceremony or other assembly)

**For Fire Marshal's Office Use Only**

TVFR Permit # 2026-0032  
Permit Type: SPP - West Linn  
Submittal Date: 3-9-26  
Assigned To: DFM Arn  
Due Date: NA  
Fees Due: Ø  
Fees Paid: Ø

**Approval/Inspection Conditions**  
(For Fire Marshal's Office Use Only)

**This section is for application approval only**

[Signature] 9-9-26  
Fire Marshal or Designee Date  
Conditions: See approved fire service plan. No fire access approved partition only.  
See Attached Conditions:  Yes  No  
Site Inspection Required:  Yes  No

**This section used when site inspection is required**

Inspection Comments:

\_\_\_\_\_  
Final TVFR Approval Signature & Emp ID Date



APPROVED PLANS

APPROVAL OF PLANS IS NOT AN APPROVAL OF OMISSIONS OR OVERSIGHTS.

*Jason Am...*  
Deputy Fire Marshal II

TVF&R Permit# 2026-0032

ELD Partition Site Plan  
4110 Cornwall St Tax Lot 6100 2-1E-36BA

FS-1

Mr. JJ Portlock

**WESTLAKE**  
CONSULTANTS inc.

ENGINEERING ♦ SURVEYING ♦ PLANNING

PACIFIC CORPORATE CENTER  
15115 S.W. SEQUOIA PARKWAY, SUITE 150 (503) 684-0652  
TIGARD, OREGON 97224 FAX (503) 624-0187