



CITY OF West Linn

REVISED STAFF REPORT FOR THE PLANNING COMMISSION (PC)

Deletions are identified with ~~striketrough~~ and additions are identified with **gray color shading, bold and underline text**

FILE NUMBER: CUP-25-02 / DR-25-02 / WAP-25-01

HEARING DATE: March 4, 2026 (Continued from 02/04/2026 and 2/18/2026 PC Hearing)

REQUEST: The applicant is requesting approval for a Conditional Use Permit, Class II Design Review, and a Water Resource Area Permit, to construct an operations complex serving the City of West Linn’s Public Works, Environmental Services, Parks and Recreation, and Water and Streets Departments. The complex includes landscaping, paved parking, fueling, vehicle wash, dewatering and decant functions at 23800 and 23834 Salamo Road.

APPROVAL CRITERIA: Community Development Code (CDC) Chapters 11, 32, 41, 42, 46, 48, 55, 60, 96, and 99

STAFF REPORT PREPARED BY: Aaron Gudelj, Associate Planner

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GENERAL INFORMATION

OWNER:	City of West Linn Attn: Erich Lais, Public Works Director and City Engineer 22500 Salamo Dr. West Linn, OR 97068
APPLICANT/ CONSULTANT:	Scott Edwards Architecture Attn: Jeff Dunn 2525 E. Burnside St. Portland, OR 97214
	Alex Simpson, PE Harper Houf Peterson Righellis, Inc. 205 SE Spokane St, Suite 200 Portland, OR 97202
SITE LOCATION:	23800 and 23834 Salamo Road.
SITE SIZE:	33.02 Acres
ZONING:	Residential, R-10
COMP PLAN DESIGNATION:	Low Density Residential
120-DAY PERIOD:	This application was deemed complete on December 17, 2025. The City must make a final decision, including all local appeals within 120 days, or by April 16, 2026.
PUBLIC NOTICE:	Public notice was mailed to the Barrington Heights neighborhood association and the Savanna Oaks neighborhood association and affected property owners on January 15, 2026. The property was posted with a notice sign on January 15, 2026. A notice was published in the West Linn Tidings on January 15, 2026. The notice requirements of CDC Chapter 99 have been met.

EXECUTIVE SUMMARY

Subsequent to the 02/04/2026 PC Hearing, per the Planning Commissions request, the Applicant's Submittal and associated technical reports have been individually extracted for ease of reference. Additionally, the Table of Contents of this Staff Report was revised to reflect the extracted technical reports.

The City received public comments on the subject application since the publication of the Staff Report on January 15, 2026, at the February 4, 2026 Planning Commission hearing, and after February 4, 2026 Planning Commission hearing and have added the public comments as Exhibits to this Staff Report. Additionally, the City has added recommended Conditions of Approval - **in gray color shading bold and underline text** - in response to the public testimony and comments received regarding concerns of geotechnical engineering, water quality, traffic impacts, noise impacts, and tree preservation/removal.

Project Summary:

The proposed project includes the construction of a new operations complex consisting of three new buildings totaling approximately 40,000 square feet, landscaping, parking, and paved lot fueling, truck wash, dewatering and decant functions. The future complex will serve the City of West Linn's Public Works, Environmental Services, Parks and Recreation, and Water and Streets Divisions. The project requires a Conditional Use Permit, Class II Design Review, and a Water Resource Area Permit.

The project site is a 33.02-acre vacant site located at 23800 and 23834 Salamo Road, on the east side of Salamo Road at the intersection of Salmo Road and Greene Street - directly south of the site is I-205 Freeway.

Site Conditions:

The project site is a vacant, 33.02-acre, R-10 zoned site with one stream and one wetland (See 'Water Resource Area Review' in Applicant Submittal). 'Wetland A' is approximately 28,555 square feet and is located at the southern portion of the site. Stream 1 is a total of 225 lineal feet flowing north to south and daylighting out of corrugated metal culverts downslope of Salamo Road, flows south through a ravine in the western portion of the project site and continues south out before draining under I-205, and ultimately the Willamette River.

Onsite vegetation is dominated by upland trees, shrubs, grasses, and forbs. An Arborist Report and Tree Preservation Plan detailing trees to be preserved, removed, and new trees to be planted has been provided as part of the Applicants Submittal.

The subject site contains areas of slopes greater than 25 percent and an area of mapped shallow landslide deposits stemming from a slope failure in the 1960's during I-205 construction and has been identified on the Landslide Inventory Map by the State of Oregon Department of Geology and Mineral Industries in 2009 (**Exhibit PC-J - Geotechnical Engineering Report**) See Applicants Submittal - Attachment PC-1; Pg 382) as having potential for a landslide. A series of terraces were excavated during the construction of I-205 in 1969 in an effort to improve

stability and remain undeveloped. A Geotechnical Engineering Service Report (**Exhibit PC-J Geotechnical Engineering Report**) Applicants Submittal – Attachment PC-1; Pg. 382) has been provided by the applicant detailing existing soil conditions and ultimate feasibility for development of the site for the proposed project.

Surrounding zoning is R-10, Residential and surrounding land uses include single-family residential homes to the north, east, and west, and the I-205 freeway to the south.



Project Description:

The proposed project includes construction of a new vehicular access drive from Salamo Road and construction of two primary buildings near the SW portion of the site consisting of the one-story 11,073 sq. ft. (Building A) that includes primarily fleet repair facilities, ancillary part storage, a sign shop, ancillary storage and general use spaces; a two-story 28,196 sq. ft. (Building B) that will serve as warehousing and fleet repair facilities with ancillary storage and general office use; and a covered carport (Building C) with nine parking spaces for fleet vehicles. An additional 33 uncovered parking spaces for fleet vehicles will be located next to Building A, B, and C as well as a transformer, emergency generator and trash facilities.

The new access drive also leads to another facility operations area near the center of the property consisting of a covered bulk storage building (Building D), a dewater/decant building (Building E), a truck wash (Building F), additional bulk open storage, fuel tank with a fueling pump, de-ice tank, and trash dumpsters.

The project proposes to permanently impact 15,120 sq. ft. of 'Water Resource Area' on the subject site and will subsequently re-vegetate 7,560 sq. ft. of Wetland A consistent with the requirements of Chapter 32 of the West Linn Community Development Code (CDC). The

applicant has provided a Water Resource Area report (**Exhibit PC-F Water Resources Area Review**) Applicants Submittal—PC 1—Pg.191) and a Wetland Delineation Report (**Exhibit PC-G Wetland Delineation Report**) Applicants Submittal—PC 1—Pg.218) Additional Water Resource Area analysis and findings are addressed under the findings for Chapter 32.

Public Comments:

Written public comments can be found in Exhibit PC-N.

RECOMMENDATION

Staff respectfully recommends the Planning Commission **to approve** CUP-25-02/ DR-25-02/WAP-25-01 based on: 1) the findings submitted by the applicant, which are incorporated by this reference, 2) supplementary staff findings included in the Addendum below, and 3) the addition of conditions of approval below. These findings and conditions demonstrate compliance with the applicable approval criteria.

RECOMMENDED CONDITIONS OF APPROVAL:

General:

- 1. The applicant shall construct the project in substantial conformance with the submitted application materials, except as modified by these conditions, including all Tentative Plan Sheets in Exhibit PC-1.**
- 2. The applicant shall design and construct all public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite stormwater, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. These must be designed, constructed, and completed prior to final building certificate of occupancy. The City may partner with the applicant to fund additional improvements as part of the project.**
- 3. The applicant shall ensure a minimum survival rate of 80% of trees and shrubs planted by the third year after the date at which all mitigation plantings were completed and submit a report at that time from the parties responsible for plantings that demonstrates the survival of plantings. The Applicant shall be responsible for monitoring and maintaining all plantings with the following practices:**
 - a. Plants that die must be replaced in kind.**
 - b. New plantings shall be mulched to a minimum of three inches in depth and 18 inches in diameter.**

- c. Plantings shall be watered one inch per week between June 15th to October 15th for the three years following planting.
 - d. Non-native and/or noxious vegetation shall be controlled or removed
 - e. Bare root trees shall be planted between December 1st and February 28th, and potted plants shall be planted between October 15th and April 30th.
 - f. Plant sleeves or fencing shall be used to protect trees and shrubs against wildlife browsing and damaging plants.
4. The applicant shall provide an annual monitoring report documenting the survival of the mitigation plantings. Plants that die shall be replaced in kind, as needed, to ensure 80% survival of the mitigation plantings. Plantings shall consist of plants on the Portland Plant list.
 5. The applicant will be responsible for ongoing maintenance of all buildings and associated site improvements.
 6. The operation of the Public Support Facility shall maintain compliance with Section 5.487 of the West Linn Municipal Code.

Prior to Issuance of a Building Permit:

7. The applicant shall provide a detailed Tree Plan outlining the dripline for significant trees to be retained.
8. Protection fencing around trees to be protected shall be installed prior to demolition, grubbing, grading, and/or construction activities begin. Signage shall be installed stating "WARNING: Tree Protection Zone" and remain on each protection fence at all times.
9. The Project Arborist shall attend a Preconstruction Conference to discuss methods of tree removal and any required tree protection. The Project Arborist shall be on site during tree removal.
10. The applicant shall provide a Final Stormwater Report including with downstream analysis and ensuring compliance with The Oregon Department of Environmental Quality standards Oregon Administrative Rules 340, Division 41.
11. The applicant shall either install the required improvements or agree to install required improvements, as provided in CDC Chapter 91.
12. The applicant shall provide a final geotechnical report.

Prior to Issuance of a Certificate of Occupancy:

- 13. The Project Arborist shall provide a final report to the City discussing concerns with trees negatively impacted during construction, and describe measures needed to maintain and protect the remaining trees for a minimum of two years after project completion.**
- 14. The applicant shall complete all site and public improvements per Condition of Approval 1 and satisfy the requirements of CDC Chapter 91.**
- 15. The applicant shall provide final approval from the Department of State Lands (DSL) and shall install all required mitigation plantings.**
- 16. The applicant has voluntarily agreed to provide a Traffic Impact Analysis consistent with the requirements of West Linn Community Development Code 85.170(B)(2)(c).**
- 17. The applicant has voluntarily agreed to provide a noise study prepared demonstrating that all applicable Department of Environmental Quality (DEQ) and City noise standards can be met.**

ADDENDUM
PLANNING COMMISSION STAFF REPORT
MEETING DATE: February 4, 2026

FINDINGS OF FACT AND APPLICABLE CRITERIA

Comprehensive Plan:

Goal 1: Citizen Involvement

Staff finding: The policies of Goal 1 are implemented through CDC Chapter 99. Compliance with CDC Chapter 99 is addressed by the findings for that chapter located below. This goal is met.

Goal 2: Land Use Planning

Staff finding: The policies of Goal 2 are implemented through CDC Chapter 11, CDC Chapter 32, CDC Chapter 46, CDC Chapter 48, CDC Chapter 55, and CDC Chapter 60. Compliance with CDC Chapter(s) 11, 32, 46, 48, 55, are addressed by the findings for that chapter located below. The goal is met.

Goal 5: Open Spaces, Scenic and Historic Areas, and Natural Resources

Staff Finding: The policies of Goal 5 are implemented through CDC Chapter 32. Compliance with CDC Chapter 32 is addressed by the findings located below. The goal is met.

Goal 6: Air, Water, and Land Resources Quality

Staff finding: The air quality policies of Goal 6 are implemented through CDC Chapter 55. The water quality policies of Goal 6 are implemented through CDC Chapter 41. The land resources (solid waste) and noise quality policies of Goal 6 are implemented through Chapter 55. Compliance with the respective chapters is addressed by the findings for each chapter located below. This is met.

Goal 7: Areas Subject to Natural Disasters and Hazards

Staff Finding: The policies of Goal 7 are implemented through CDC Chapter(s) 32, 55, and 60. Compliance with CDC Chapter(s) 32, 55, and 60 are addressed by the findings for that chapter located below. The goal is met.

[...]

Goal 11: Public Facilities and Services

Staff Finding: The policies of Goal 11 are implemented through CDC Chapters 11, 32, 46, 48, 55, and 60. Compliance with these CDC Chapters are addressed by the findings for that chapter located below. The goal is met.

Goal 12: Transportation

Staff Finding: The policies of Goal 12 are implemented though CDC Chapters 46, 48, 55, 60, and 96. Compliance with CDC Chapter(s) 46, 48, 55, 60, and 96 are addressed by the findings for that chapter located below. The goal is met.

Goal 13: Energy Conservation

Staff Finding: The policies of Goal 13 are implemented through CDC Chapter(s) 46 and 55. Compliance with CDC Chapter(s) 46 and 55 are addressed by the findings for that chapter located below. The goal is met.

[...]

Community Development Code:

I. CHAPTER 11, RESIDENTIAL, R-10

11.060 CONDITIONAL USES

The following are conditional uses which may be allowed in this zoning district subject to the provisions of Chapter 60 CDC, Conditional Uses.

....

4. Public support facilities.

....

Staff Finding 1: The proposed use, an operation facility with incidental storage and maintenance of necessary vehicles, is considered a ‘Public Support Facility’ per Chapter 2, Definitions, and may be permitted through approval of Conditional Use process within the R-10 Zone.

Findings of compliance with CDC Chapter 60, Conditional Uses, are addressed in Staff Findings 60 through 65. The criterion is met.

11.070 DIMENSIONAL REQUIREMENTS, USES PERMITTED OUTRIGHT AND USES PERMITTED UNDER PRESCRIBED CONDITIONS

Except as may be otherwise provided by the provisions of this code, the following requirements are the requirements for uses within this zone:

STANDARD	REQUIREMENT	ADDITIONAL NOTES
Minimum lot size Average minimum lot or parcel size for a townhouse project	10,000 sf 1,500 sf	For a single-family attached or detached unit
Minimum lot width at front lot line	35 ft	Does not apply to townhouses or cottage clusters
Average minimum lot width	50 ft	Does not apply to townhouses or cottage clusters
Minimum yard dimensions or minimum building setbacks		Except as specified in CDC <u>25.070(C)(1)</u> through (4) for the Willamette Historic District. Front, rear, and side yard setbacks in a cottage cluster project are 10 ft. There are no additional setbacks for individual structures on individual lots, but minimum distance between structures shall follow applicable building code requirements.
Front yard	20 ft	Except for steeply sloped lots where the provisions of CDC <u>41.010</u> shall apply
Interior side yard	7.5 ft	Townhouse common walls that are attached may have a 0-ft side setback.
Street side yard	15 ft	
Rear yard	20 ft	
Maximum building height	35 ft	Except for steeply sloped lots in which case the provisions of Chapter <u>41</u> CDC shall apply.
Maximum lot coverage	35%	Maximum lot coverage does not apply to cottage clusters. However, the maximum building footprint for a cottage cluster is less than 900 sf per dwelling unit. <ul style="list-style-type: none"> • This does not include detached garages, carports, or accessory structures. • A developer may deduct up to 200 sf for an attached garage or carport.
Minimum accessway width to a lot which does not abut a street or a flag lot	15 ft	

STANDARD	REQUIREMENT	ADDITIONAL NOTES
<i>Maximum floor area ratio</i>	<i>0.45</i>	<i>Maximum FAR does not apply to cottage clusters.</i>
<i>Duplex, triplex, and quadplex</i>	<i>0.60</i>	<i>Type I and II lands shall not be counted toward lot area when determining allowable floor area ratio, except that a minimum floor area ratio of 0.30 shall be allowed regardless of the classification of lands within the property. That 30 percent shall be based upon the entire property, including Type I and II lands. Existing residences in excess of this standard may be replaced to their prior dimensions when damaged without the requirement that the homeowner obtain a non-conforming structures permit under Chapter <u>66</u> CDC.</i>

1. *The sidewall provisions of Chapter 43 CDC shall apply.*

Staff Finding 2: The proposal complies with the requirements of Section 11.070, dimensional requirements, uses permitted outright and uses permitted under prescribed conditions, within the R-10 Zoning District. The subject site is 33 acres, in compliance with the minimum lot size of 10,000 square feet in the R-10 zone. The proposal complies with the minimum side yard requirement of 7.5 feet, as the proposed nearest structure is 55 feet from the southerly property line. Buildings D, E, and F near the center of the site are greater than 100 feet from the single-family homes to the north. The proposed facility exceeds the maximum building height of Chapter 11 (35 feet); however, the facility complies with the maximum building height requirement of 50 feet pursuant to CDC 41.040, addressed below. The building lot coverage is approximately 50,000 square feet and the lot size is approximate 1,400,000 square feet, well under the maximum Lot coverage (35 percent) and Floor Area Ratio (45). The criteria are met.

11.080 DIMENSIONAL REQUIREMENTS, CONDITIONAL USES

Except as may otherwise be established by this code, the appropriate lot or parcel size for a conditional use shall be determined by the approval authority at the time of consideration of the application based upon the criteria set forth in CDC 60.070(A) and (B).

Staff Finding 3: The criteria in CDC 60.070 (A) and (B) have been addressed in Finding 61 below. The criterion is met.

Chapter 32

WATER RESOURCE AREA PROTECTION

IV. CHAPTER 32, WATER RESOURCE AREA PROTECTION

32.060 APPROVAL CRITERIA (STANDARD PROCESS)

No application for development on property containing a WRA shall be approved unless the approval authority finds that the proposed development is consistent with the following approval criteria, or can satisfy the criteria by conditions of approval:

A. WRA protection/minimizing impacts.

1. Development shall be conducted in a manner that will avoid or, if avoidance is not possible, minimize adverse impact on WRAs.
2. Mitigation and re-vegetation of disturbed WRAs shall be completed per CDC 32.090 and 32.100, respectively.

Staff Finding 4: The City adopts the applicant findings.

"The proposed project has minimized impacts to WRAs to the extent practicable while maintaining a feasible project. Minimization measures include minimizing areas of vegetation disturbance outside of the WRAs to the greatest extent practicable, utilizing areas of previous disturbance to the maximum extent, and minimizing grading of the steep slopes above Stream 1. All temporarily disturbed WRAs will be permanently revegetated before completion of the project. The project proposes a total of 15,120 SF of permanent WRA impacts. A total of 7,560 SF of mitigation is proposed to offset the 15,120 SF of cumulative impacts to the various resource areas affected by the project. Responses to standards found in CDC 32.090 and 32.100 are provided below."

"Wetland and Waters of the U.S. and State Delineation Report: West Linn Public Works Facility" (Exhibit PC-G Wetland Delineation Report); ~~1 – PC 1 – Pg. 218~~) and "Water Resource Area Review" (Exhibit PC-F Water resources Area Review) ~~1 – PC 1 – Pg. 191~~) evaluated the existing resource conditions on the site. The study area of the two reports include the 14.3 acres of the subject 33.02 acre site, where the construction and building will occur, and identified one wetland (Wetland A) and one watercourse (Stream 1) within the site area.

Wetland A is located in the southern portion of the site, totals approximately 28,555 square feet and existing conditions consist of a broad, shallow depression, narrowing to two swales that flow to the south down the hillside toward I-205, where it is captured in a ditch a redirected to Stream 1 where it eventually drains to the Willamette River via Bernert Creek. Vegetation in the Wetland consists of broadleaf cattail, red fescue, meadow foxtail, and rice cut grass with areas of Himalayan Blackberry throughout and lesser amounts of teasel, tansy ragwort, duckweed, Oregon ash saplings and an unidentified willowherb.

Stream 1 is located in the SW corner of the project site and totals approximately 225 linear feet. Stream 1 flows north to south, daylight out of two corrugated metal culverts downslope of Salamo Road, flows south through a ravine before draining under I-205 and ultimately the Willamette Rive via Bernert Creek. The V-shaped channel consists of a bed substrate of gravels and pebbles with some silty material. Vegetation along Stream 1 includes a big leaf maple, Douglas fir, and black cottonwoods with an understory dominated by Himalayan blackberry, English ivy, and western swordfern.

Existing site constraints such as the steep slope, wetlands, and a stream naturally restrict developable area onsite. Figure 1 of the 'Water Resource Area Review' identifies 1,285 square feet of temporarily impacted areas onsite due to construction and 15,120 square feet of permanently impacted areas onsite due to placement of the SW corner of Building A as well

as the SW corner of the dewater/decant area (Building E) as proposed. Mitigation planting totaling 7,560 square feet will be planted to replace the 15,120 square feet of permanently impacted areas in compliance with CDC 32.090 and 32.100. Additional findings for CDC 32.090 and 32.100 can be found later in this report.

A planting schedule (Exhibit PC-F Water Resources Area Review) ~~Figure 2, Water Resource Area Review~~) has been included as part of the submittal. The criteria are met.

B. Storm water and storm water facilities.

1. Proposed developments shall be designed to maintain the existing WRAs and utilize them as the primary method of storm water conveyance through the project site unless:

a. The surface water management plan calls for alternate configurations (culverts, piping, etc.); or

b. Under CDC 32.070, the applicant demonstrates that the relocation of the water resource will not adversely impact the function of the WRA including, but not limited to, circumstances where the WRA is poorly defined or not clearly channelized.

Re-vegetation, enhancement and/or mitigation of the re-aligned water resource shall be required as applicable.

2. Public and private storm water detention, storm water treatment facilities and storm water outfall or energy dissipaters (e.g., rip rap) may encroach into the WRA if:

a. Accepted engineering practice requires it;

b. Encroachment on significant trees shall be avoided when possible, and any tree loss shall be consistent with the City's Tree Technical Manual and mitigated per CDC 32.090;

c. There shall be no direct outfall into the water resource, and any resulting outfall shall not have an erosive effect on the WRA or diminish the stability of slopes; and

d. There are no reasonable alternatives available.

A geotechnical report may be required to make the determination regarding slope stability.

3. Roadside storm water conveyance swales and ditches may be extended within rights-of-way located in a WRA. When possible, they shall be located along the side of the road furthest from the water resource. If the conveyance facility must be located along the side of the road closest to the water resource, it shall be located as close to the road/sidewalk as possible and include habitat friendly design features (treatment train, rain gardens, etc.).

4. Storm water detention and/or treatment facilities in the WRA shall be designed without permanent perimeter fencing and shall be landscaped with native vegetation.

5. Access to public storm water detention and/or treatment facilities shall be provided for maintenance purposes. Maintenance driveways shall be constructed to minimum width and use water permeable paving materials. Significant trees, including roots, shall not be disturbed to the degree possible. The encroachment and any tree loss shall be mitigated per CDC 32.090. There shall also be no adverse impacts upon the hydrologic conditions of the site.

6. Storm water detention and treatment and geologic hazards. Per the submittals required by CDC 32.050(F)(3) and 92.010(E), all proposed storm water detention and treatment facilities must comply with the standards for the improvement of public and private drainage systems located in the West Linn Public Works Design Standards, there will be no adverse off-site impacts caused by the development (including impacts from increased intensity of runoff downstream or

constrictions causing ponding upstream), and the applicant must provide sufficient factual data to support the conclusions of the submitted plan.

C. Repealed by Ord. 1647.

Staff Finding 5: A Geotechnical Engineering Services report prepared by Columbia West Engineering, Inc., dated April 2025 (Exhibit PC-J Geotechnical Engineering Report) ~~1-PC-1-Pg.382~~, for the proposed project does not recommend stormwater drainage to be infiltrated on the site due to site slopes and subsurface conditions. Subsequently, a Stormwater Management Report, dated November 2025, was prepared by Harper Hour Peterson Righellis Inc. (HHPR) (Exhibit PC-D Stormwater and Grading Plan) ~~1-PC-1-Pg.382~~ detailing the stormwater management for the proposed project. Based on the findings of the Geotechnical Engineering Services Report the stormwater management will generally maintain the flow characteristics (north à south) of the existing site by utilizing a network of underground storm pipes to continue to convey stormwater north à south. At the southwest portion of the site, near Building B, an underground chamber system is proposed for detention of stormwater runoff with a mechanical system providing pretreatment. The underground chamber system will capture the stormwater that has been collected by the network of underground storm pipes around project site (Exhibit PC-D, Appendix A, Stormwater and Grading Plan) ~~– Stormwater Management Report – Basin 1~~, pretreat the stormwater, and outfall the stormwater at the south end of the site towards the existing natural ravine.

Basin 2 (Exhibit PC-D, Appendix A, Stormwater and Grading Plan) ~~– Stormwater Management Report~~) consists of 1,232 sq. ft. of new impervious area and portions of pervious fill slope (0.49 acres) intended to catch the existing grade downslope of the project. Basin 2 will shed stormwater offsite since topographical constraints do not allow for stormwater to be captured and routed to the underground chamber at the SW portion of the site.

The existing northwest portion of the project site is steeply sloping and will remain undeveloped (Exhibit PC-D, Appendix A, Stormwater and Grading Plan) ~~See Appendix A – Stormwater Management Report – Basin 3~~). Stormwater from Basin 3 is proposed to convey to an inlet just west of Building E (De-water/Decant/Sandbag area) and discharge through a culvert to the wetland to the south, bypassing the project site.

An Arborist Report and Tree Preservation Plan, dated July 2025 (Exhibit PC-C Tree Preservation Plan and Arborist Report) ~~– PC-1 – Pg.121~~, was prepared by Todd Prager and Associates for the proposed project. Complete analysis of tree inventory, tree removal, and tree retention can be found in the report that was provided in the applicant’s submittal and additional tree removal and tree-related findings can be found later in this report.

Based on the findings of the Geotechnical Engineering Services Report and Draft Stormwater Management Report provided in the applicant’s submittal, the criteria are met.

D. WRA width. Except for the exemptions in CDC 32.040, applications that are using the alternate review process of CDC 32.070, or as authorized by the approval authority consistent with the provisions of this chapter, all development is prohibited in the WRA as established in Table 32-2 below:

Table 32-2. Required Width of WRA

Protected WRA Resource (see Chapter 2 CDC, Definitions)	Slope Adjacent to Protected Water Resource^{1, 3}	Starting Point for Measurements from Water Resource^{1, 3}	Width of WRA on Each Side of the Water Resource
A. Water Resource	0% - 25%	OHW or delineated edge of wetland	65 feet
B. Water Resource (Ravine)	over 25% to a distinct top of slope ²	OHW or delineated edge of wetland	From water resource to top of slope ² (30-foot minimum), plus an additional 50 feet ⁴
C. Water Resource	Over 25% for more than 30 feet, and no distinct top of slope for at least 150 feet	OHW or delineated edge of wetland	200 feet
D. Riparian Corridor	Any	OHW	100 feet
E. Formerly Closed Drainage Channel Reopened	Any	OHW	15 feet
F. Ephemeral Stream	Any	Stream thread or centerline	15 feet with treatment or vegetation (see CDC 32.050(G)(1))
G. Fish Bearing Streams per Oregon Department of Fish and Wildlife (ODFW) or 2003-2004 Survey	Applies to all that stream section where fish were inventoried and upstream to the first known barrier to fish passage.	OHW or delineated edge of wetland	100 feet when no greater than 25% slope. See B or C above for steeper slopes
H. Re-aligned Water Resource	See A, B, C, D, F, or G, above	OHW or delineated edge of wetland	See A, B, C, D, F, or G, above

- ¹ *The slope is the average slope in the first 50 feet as measured from bankfull stage or OHW.*
- ² *Where the protected water resource is confined by a ravine or gully, the top of slope is the location (30-foot minimum) where the slope breaks to less than 15 percent for at least 50 feet.*
- ³ *At least three slope measurements along the water resource, at no more than 100-foot increments, shall be made for each property for which development is proposed. Depending upon topography, the width of the protected corridor may vary.*
- ⁴ *The 50-foot distance may be reduced to 25 feet if a geotechnical study by a licensed engineer or similar accredited professional demonstrates that the slope is stable and not prone to erosion.*

Staff Finding 5: Staff adopts the applicant’s findings. The project site includes a wetland (Wetland A) that requires a 65-foot setback and a stream (Stream 1) with a ravine with slopes greater than 25% requiring to a distinct top of slope that extends from the water resource edge to the top of slope, plus an additional 50-feet. These buffer lengths were applied to the north side of Wetland A as there is a steep slope near wetland edge and along the ravine above Stream 1 within the project area. The project will permanently impact 15,120 sq. ft. of the water resource areas. In addition, 1,285 sq. ft. of temporary impacts will occur within the water resource areas. The permanently impacted water resource areas will be offset by 7,560 sq. ft. of mitigation plantings within the wetland, pursuant to West Linn CDC Section 32.090. The criteria are met.

E. Per the submittals required by CDC 32.050(F)(4), the applicant must demonstrate that the proposed methods of rendering known or potential hazard sites safe for development, including proposed geotechnical remediation, are feasible and adequate to prevent landslides or other damage to property and safety. The review authority may impose conditions, including limits on type or intensity of land use, which it determines are necessary to mitigate known risks of landslides or property damage.

Staff Finding 6: The Geotechnical Engineering Services Report prepared by Columbia West Engineering, Inc (Exhibit PC-J Geotechnical Engineering Report) ~~Exhibit xx, pages xx-yy~~) for the proposed project states within its Executive Summary that “Based on soil and groundwater conditions at the site, liquefaction and lateral spreading are not design considerations at this time.” The criterion is met.

F. Roads, driveways and utilities.

- 1. New roads, driveways, or utilities shall avoid WRAs unless the applicant demonstrates that no other practical alternative exists. In that case, road design and construction techniques shall minimize impacts and disturbance to the WRA by the following methods:*
 - a. New roads and utilities crossing riparian habitat areas or streams shall be aligned as close to perpendicular to the channel as possible.*

b. Roads and driveways traversing WRAs shall be of the minimum width possible to comply with applicable road standards and protect public safety. The footprint of grading and site clearing to accommodate the road shall be minimized.

c. Road and utility crossings shall avoid, where possible:

- 1) Salmonid spawning or rearing areas;*
- 2) Stands of mature conifer trees in riparian areas;*
- 3) Highly erodible soils;*
- 4) Landslide prone areas;*
- 5) Damage to, and fragmentation of, habitat; and*
- 6) Wetlands identified on the WRA Map.*

2. Crossing of fish bearing streams and riparian corridors shall use bridges or arch-bottomless culverts or the equivalent that provides comparable fish protection, to allow passage of wildlife and fish and to retain the natural stream bed.

3. New utilities spanning fish bearing stream sections, riparian corridors, and wetlands shall be located on existing roads/bridges, elevated walkways, conduit, or other existing structures or installed underground via tunneling or boring at a depth that avoids tree roots and does not alter the hydrology sustaining the water resource, unless the applicant demonstrates that it is not physically possible or it is cost prohibitive. Bore pits associated with the crossings shall be restored upon project completion. Dry, intermittent streams may be crossed with open cuts during a time period approved by the City and any agency with jurisdiction.

4. No fill or excavation is allowed within the ordinary high water mark of a water resource, unless all necessary permits are obtained from the City, U.S. Army Corps of Engineers and Oregon Department of State Lands (DSL).

5. Crossings of fish bearing streams shall be aligned, whenever possible, to serve multiple properties and be designed to accommodate conduit for utility lines. The applicant shall, to the extent legally permissible, work with the City to provide for a street layout and crossing location that will minimize the need for additional stream crossings in the future to serve surrounding properties.

Staff Finding 7: The proposed project includes portions of vehicular access lanes for service and emergency vehicles that are within the WRA. Due to the site's steep topography constraints, these portions of the vehicular access lanes are unable to be relocated outside of the WRA. Vehicular access is the minimum required width for emergency access. No impacts to fish bearing streams or below ordinary high-water mark are proposed. No new utilities are proposed within the impacts, except for the proposed stormwater outfall in the southwest portion of the site. The criteria are met.

1. The following habitat friendly development practices shall be incorporated into the design of any improvements or projects in the WRA to the degree possible:

- 1. Restore disturbed soils to original or higher level of porosity to regain infiltration and storm water storage capacity.*

2. *Apply a treatment train or series of storm water treatment measures to provide multiple opportunities for storm water treatment and reduce the possibility of system failure.*
3. *Incorporate storm water management in road rights-of-way.*
4. *Landscape with rain gardens to provide on-lot detention, filtering of rainwater, and groundwater recharge.*
5. *Use multi-functional open drainage systems in lieu of conventional curb-and-gutter systems.*
6. *Use green roofs for runoff reduction, energy savings, improved air quality, and enhanced aesthetics.*
7. *Retain rooftop runoff in a rain barrel for later on-lot use in lawn and garden watering.*
8. *Disconnect downspouts from roofs and direct the flow to vegetated infiltration/filtration areas such as rain gardens.*
9. *Use pervious paving materials for driveways, parking lots, sidewalks, patios, and walkways.*
10. *Reduce sidewalk width to a minimum four feet. Grade the sidewalk so it drains to the front yard of a residential lot or retention area instead of towards the street.*
11. *Use shared driveways.*
12. *Reduce width of residential streets and driveways, especially at WRA crossings.*
13. *Reduce street length, primarily in residential areas, by encouraging clustering.*
14. *Reduce cul-de-sac radii and use pervious and/or vegetated islands in center to minimize impervious surfaces.*
15. *Use previously developed areas (PDAs) when given an option of developing PDA versus non-PDA land.*
16. *Minimize the building, hardscape and disturbance footprint.*
17. *Consider multi-story construction over a bigger footprint. (Ord. [1623](#) § 1, 2014; Ord. [1635](#) § 19, 2014; Ord. [1647](#) § 5, 2016; Ord. [1662](#) § 7, 2017)*

Staff Finding 8: The proposed impacts to the WRA are the minimum impacts necessary for proper site operation and proper site circulation of emergency vehicles. As required by CDC 32.090, mitigation plantings totaling 7,560 square feet will be required inside Wetland A ([Exhibit PC-F Water resources Area Review](#)) ~~1—PC 1—Pg.192~~ due to as the proposed permanent impacts to the WRA. With recommended Condition of Approval 3, the criteria are met.

32.090 MITIGATION PLAN

A A mitigation plan shall only be required if development is proposed within a WRA (including development of a PDA). (Exempted activities of CDC 32.040 do not require mitigation unless specifically stated. Temporarily disturbed areas, including TDAs associated with exempted activities, do not require mitigation, just grade and soil restoration and re-vegetation.) The mitigation plan shall satisfy all applicable provisions of CDC 32.100, Re-Vegetation Plan Requirements.

B. Mitigation shall take place in the following locations, according to the following priorities (subsections (B)(1) through (4) of this section):

1. *On-site mitigation by restoring, creating or enhancing WRAs.*
2. *Off-site mitigation in the same sub-watershed will be allowed, but only if the applicant has demonstrated that:*
 - a. *It is not practicable to complete mitigation on-site, for example, there is not enough area on-site; and*
 - b. *The mitigation will provide equal or superior ecological function and value.*
3. *Off-site mitigation outside the sub-watershed will be allowed, but only if the applicant has demonstrated that:*
 - a. *It is not practicable to complete mitigation on-site, for example, there is not enough area on-site; and*
 - b. *The mitigation will provide equal or superior ecological function and value.*
4. *Purchasing mitigation credits through DSL or other acceptable mitigation bank.*
- C. *Amount of mitigation.*
 1. *The amount of mitigation shall be based on the square footage of the permanent disturbance area by the application. For every one square foot of non-PDA disturbed area, on-site mitigation shall require one square foot of WRA to be created, enhanced or restored.*
 2. *For every one square foot of PDA that is disturbed, on-site mitigation shall require one half a square foot of WRA vegetation to be created, enhanced or restored.*
 3. *For any off-site mitigation, including the use of DSL mitigation credits, the requirement shall be for every one square foot of WRA that is disturbed, two square feet of WRA shall be created, enhanced or restored. The DSL mitigation credits program or mitigation bank shall require a legitimate bid on the cost of on-site mitigation multiplied by two to arrive at the appropriate dollar amount.*

Staff Finding 9: The City adopts the following finding:

“Permanent impacts to WRA[] will occur in the form of grading and construction of roadway, parking lot, storage area, and the main building. Permanent impacts will be located in Previously Disturbed Areas (PDA) as the entire project area was altered or modified before January 1, 2006. The entire hillside was cut into two benched terraces as part of the I-205 construction during the late 1960’s and early 1970’s. This alteration removed native soils which were replaced with fill material. The area of total permanent impacts to WRAs is 15,120 sq. ft. The proposed impacts will be within PDAs. Therefore the required amount of mitigation is equal to $0.5 \times (15,120 \text{ sq. ft.}) = 7,560 \text{ sq. ft.}$ The applicant has proposed a total of 7,560 sq. ft. of mitigation planting in Wetland A.”

For a complete analysis of impacted areas and mitigation planting please see the Water Resource Area Review; June 2025 (Exhibit PC-F ~~1 – PC 1 – Pg.192~~). With Condition of Approval Number 3, the criteria are met.

E. A mitigation plan shall contain the following information:

1. A list of all responsible parties including, but not limited to, the owner, applicant, contractor, or other persons responsible for work on the development site.
2. A map showing where the specific adverse impacts will occur and where the mitigation activities will occur.
3. A re-vegetation plan for the area(s) to be mitigated that meets the standards of CDC 32.100.
4. An implementation schedule, including timeline for construction, mitigation, mitigation maintenance, monitoring, and reporting. All in-stream work in fish bearing streams shall be done in accordance with the Oregon Department of Fish and Wildlife.
5. Assurances shall be established to rectify any mitigation actions that are not successful within the first three years. This may include bonding or other surety. (Ord. 1623 § 1, 2014)

Staff Finding 10: The Water Resource Area Review; June 2025 (Exhibit PC-F) ~~1—PC-1—PG-192~~) shows impacted areas, mitigation areas, and the revegetation plan. Mitigation planting will be installed during or after construction and will be conducted as soon as practicable based on the construction schedule. Annual monitoring of the mitigation area will be performed in the first three years after planting to document survival of plant species, establish photo points, and vegetation assessment plots. Plants that die shall be replaced in kind as needed to ensure 80% of the required quantity of 76 trees and 378 shrubs survive during the monitoring period. No in-stream work is proposed. With recommended Condition of Approval Number 3, the criteria are met.

32.100 RE-VEGETATION PLAN REQUIREMENTS

A. In order to achieve the goal of re-establishing forested canopy, native shrub and ground cover and to meet the mitigation requirements of CDC 32.090 and vegetative enhancement of CDC 32.080, tree and vegetation plantings are required according to the following standards:

1. All trees, shrubs and ground cover to be planted must be native plants selected from the Portland Plant List.

Staff Finding 11: Only native species will be installed in the revegetation plantings. All species proposed for planting were selected from the Portland Plant List. A list of species to be planted is provided in Appendix A of the Water Resource Area Review; June 2025 (Exhibit-PC-F) ~~1—PC-1—Pg-192~~). The criteria are met.

2. Plant size. Replacement trees must be at least one-half inch in caliper, measured at six inches above the ground level for field grown trees or above the soil line for container grown trees (the one-half inch minimum size may be an average caliper measure, recognizing that trees are not uniformly round), unless they are oak or madrone which may be one gallon size. Shrubs must be in at least a one-gallon container or the equivalent in ball and burlap and must be at least 12 inches in height.

Staff Finding 12: All trees will be a minimum one-half inch caliper, and all shrubs will be at least one-gallon container or equivalent ball and burlap and at least 12 inches in height. A list of trees to be planted is provided in Appendix A of the 'Water Resource Area Review; June 2025' provided as part of the applicant's submittal. The criteria are met.

3. Plant coverage.

a. *Native trees and shrubs are required to be planted at a rate of five trees and 25 shrubs per every 500 square feet of disturbance area (calculated by dividing the number of square feet of disturbance area by 500, and then multiplying that result times five trees and 25 shrubs, and rounding all fractions to the nearest whole number of trees and shrubs; for example, if there will be 330 square feet of disturbance area, then 330 divided by 500 equals 0.66, and 0.66 times five equals 3.3, so three trees must be planted, and 0.66 times 25 equals 16.5, so 17 shrubs must be planted). Bare ground must be planted or seeded with native grasses or herbs. Non-native sterile wheat grass may also be planted or seeded, in equal or lesser proportion to the native grasses or herbs.*

b. *Trees shall be planted between eight and 12 feet on center and shrubs shall be planted between four and five feet on center, or clustered in single species groups of no more than four plants, with each cluster planted between eight and 10 feet on center. When planting near existing trees, the dripline of the existing tree shall be the starting point for plant spacing measurements.*

4. *Plant diversity. Shrubs must consist of at least two different species. If 10 trees or more are planted, then no more than 50 percent of the trees may be of the same genus.*

Staff Finding 13:

"A total of 7,560 sq. ft. of mitigation planting will occur. The total quantity of shrubs required is equal to $(7,560/500)*25= 378$, and the total required number of trees is equal to $(7,560/500)*5= 76$. The mitigation plan proposes to plant 76 trees and 378 shrubs."

A list of trees and shrubs to be planted is provided in Appendix A of the 'Water Resource Area Review; June 2025' provided as part of the applicant's submittal. The criteria are met.

5. Invasive vegetation. *Invasive non-native or noxious vegetation must be removed within the mitigation area prior to planting.*

Staff Finding 14: All invasive non-native or noxious weeds within mitigation areas will be removed prior to planting mitigation areas. The criteria are met.

6. Tree and shrub survival. *A minimum survival rate of 80 percent of the trees and shrubs planted is expected by the third anniversary of the date that the mitigation planting is completed.*

7. Monitoring and reporting. *Monitoring of the mitigation site is the ongoing responsibility of the property owner. Plants that die must be replaced in kind.*

Staff Finding 15: Annual monitoring of the mitigation area will be performed in the first three years after planting to document survival of plant species, establish photo points, and vegetation assessment plots. Plants that die shall be replaced in kind as needed to ensure 80% of the required quantity of 76 trees and 378 shrubs survive during the monitoring period. The criteria are met.

8. *To enhance survival of tree replacement and plantings, the following practices are required:*
 - a. Mulching. *Mulch new plantings a minimum of three inches in depth and 18 inches in diameter to retain moisture and discourage weed growth.*
 - b. Irrigation. *Water new plantings one inch per week between June 15th to October 15th, for the three years following planting.*
 - c. Weed control. *Remove, or control, non-native or noxious vegetation throughout maintenance period.*
 - d. Planting season. *Plant bare root trees between December 1st and February 28th, and potted plants between October 15th and April 30th.*
 - e. Wildlife protection. *Use plant sleeves or fencing to protect trees and shrubs against wildlife browsing and resulting damage to plants.*

Staff Finding 16: Mulch will be applied to new plantings within the mitigation area at the time of installation. New plantings will be irrigated to ensure survival beyond the monitoring period. Weeds will be monitored and controlled as needed through the maintenance period. Trees and shrubs will be planted between October 15th and April 30th and plant sleeves will be utilized on trees and shrubs to minimize damage from wildlife browsing. The criteria are met.

B. When weather or other conditions prohibit planting according to schedule, the applicant shall ensure that disturbed areas are correctly protected with erosion control measures and shall provide the City with funds in the amount of 125 percent of a bid from a recognized landscaper or nursery which will cover the cost of the plant materials, installation and any follow up maintenance. Once the planting conditions are favorable the applicant shall proceed with the plantings and receive the funds back from the City upon completion, or the City will complete the plantings using those funds. (Ord. 1623 § 1, 2014)

Staff Finding 17: The applicant will conduct required mitigation as soon as site conditions are conducive. In the event that mitigation areas cannot be installed on schedule, the applicant will provide revegetation funds as described above. The criteria are met.

*Chapter 41 BUILDING HEIGHT, STRUCTURES ON STEEP LOTS, EXCEPTIONS
41.040 PLACES OF WORSHIP OR GOVERNMENT BUILDINGS*

The height of a place of worship or governmental building may be built to a maximum height of 50 feet provided:

- A. The total floor area of the building does not exceed one and one-half times the area of the site;*
- B. The yard dimensions in each case are equal to at least two-thirds of the building height of the principal structure; and*
- C. The approval of this exception is a part of the approval of the conditional use allowed under Chapter 60 CDC. (Ord. 1604 § 45, 2011; Ord. 1745 § 1 (Exh. A), 2023)*

Staff Finding: 18: The total floor area of the building(s) is less than 1.5 times the area of the site; Building(s) = 47,600 sq. ft.; Total Site Size = 33.02 acres. The yard dimensions of the lot far exceed the building heights of the proposed height of the principal structure (50.17 ft). The application shows 'Building B' to be 50'2", which is rounded to the nearest whole number, thereby the proposed building height is equal to 50' tall. Findings for the Conditional Use Permit can be found in Findings 60 – 65. The criteria are met.

Chapter 42 CLEAR VISION AREAS

42.020 CLEAR VISION AREAS REQUIRED, USES PROHIBITED

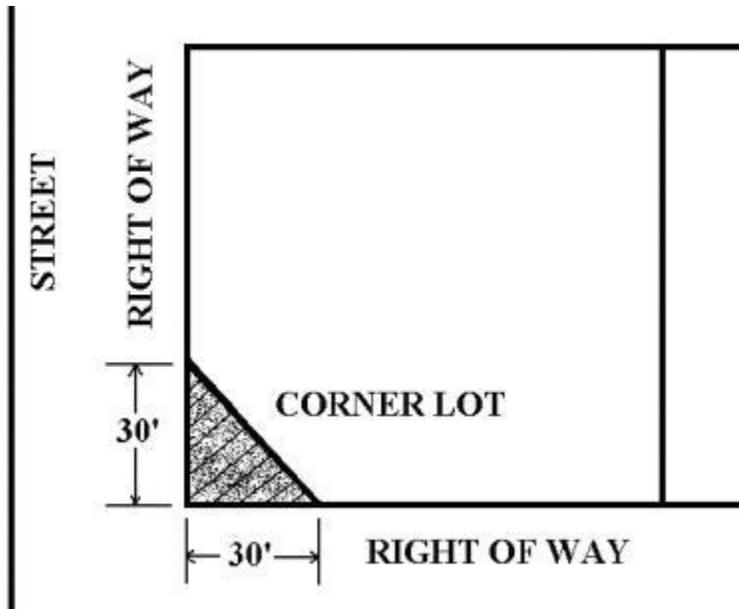
- A. A clear vision area shall be maintained on the corners of all property adjacent to an intersection as provided by CDC 42.040 and 42.050.*
- B. A clear vision area shall contain no planting, fence, wall, structure or temporary or permanent obstruction (except for an occasional utility pole or tree) exceeding three feet in height, measured from the top of the curb, or, where no curb exists, from the street centerline grade, except that trees exceeding this height may be located in this area, provided all branches below eight feet are removed. (Ord. 1192, 1987)*

.....

42.040 COMPUTATION; STREET AND ACCESSWAY 24 FEET OR MORE IN WIDTH

The clear vision area for all street intersections and street and accessway intersections (accessways having 24 feet or more in width) shall be that triangular area formed by the right-of-way or property lines along such lots and a straight line joining the right-of-way or property line at points which are 30 feet distant from the intersection of the right-of-way line and measured along such lines.

Clear vision area for corner lots and driveways 24 feet or more in width:



Staff Finding 18: The applicant’s submittal shows a 30’ x 30’ triangle corner vision area at the entrance along Salamo Road. The criteria are met.

Chapter 46 OFF-STREET PARKING, LOADING AND RESERVOIR AREAS

46.070 MAXIMUM DISTANCE ALLOWED BETWEEN PARKING AREA AND USE

.....

B. Parking areas must provide the following, wherever applicable:

- 1. Employee parking areas for carpools and vanpools shall be located closer to the entryway to the building than general employee parking.*
- 2. Stacked or valet parking is allowed if an attendant is present to move vehicles. If stacked parking is used for parking spaces, the applicant shall ensure that an attendant will always be present when the lot is in operation. The requirements for maximum spaces and all parking area development standards continue to apply for stacked parking.*
- 3. All disabled parking shall be placed closest to building entrances than all other parking. Appropriate ADA curb cuts and ramps to go from the parking lot to the ADA-accessible entrance shall be provided unless exempted by ADA code.*
- 4. Parking for cottage clusters may be further than 200 feet, if a pedestrian pathway is provided with the following conditions:*
 - a. Pedestrian walkways from the parking area to the main entrances of cottages is paved and meets standard ADA width and slopes to ensure the most direct path.*
 - b. Pedestrian scaled lighting is provided along the path. (Ord. 1547, 2007; Ord. 1736 § 1 (Exh. A), 2022; Ord. 1745 § 1 (Exh. A), 2023; Ord. 1754 § 1 (Exh. A), 2024)*

Staff Finding 19: ADA spaces are located closest to the building(s), followed by carpool spaces and then regular spaces. No stacked or valet parking is proposed. Pedestrian walkways and pathways will be ADA complaint with width and slope and a lighting plan included as part of the applicant’s submittal (Exhibit PC-H Lighting Plan and Service Provider Permit ~~1—PC-1—Pg.179~~) is provided and details lighting along the pathways. The criteria are met.

46.090 OFF-STREET PARKING SPACE REQUIREMENTS

A. Maximum parking. *Parking spaces shall not exceed the following amounts:*

<i>Land Use</i>	<i>Maximum Spaces (spaces per 1,000 sq. ft. of gross leasable area unless otherwise stated)</i>
<i>Multifamily studio unit (spaces per unit)</i>	<i>1.2</i>
<i>Multifamily non-studio (spaces per unit)</i>	<i>2.0</i>
<i>General office (includes office park, “flex-space,” government office and misc. services)</i>	<i>3.0</i>
<i>Warehouse (gross square feet; parking ratios apply to warehouses 150,000 gsf or greater)</i>	<i>0.4</i>
<i>Schools: college/university & high school (spaces/# of students and staff)</i>	<i>0.3</i>
<i>Tennis/racquetball court</i>	<i>1.3</i>
<i>Sports club/recreation facilities</i>	<i>4.8</i>
<i>Retail/commercial, including shopping centers</i>	<i>4.6</i>
<i>Bank with drive-in</i>	<i>3.0</i>
<i>Movie theater</i>	<i>0.4</i>
<i>Fast food with drive-through</i>	<i>11.0</i>
<i>Other restaurants</i>	<i>11.0</i>
<i>Place of worship (spaces/seats)</i>	<i>0.6</i>
<i>Medical/dental clinic</i>	<i>4.4</i>
<i>Other commercial uses</i>	<i>5.0</i>

- 1. For land uses with more than 65,000 square feet of floor area, surface parking may not consist of more area than the floor area of the building.*
- 2. Non-surface parking, such as tuck-under parking, underground and subsurface parking, and parking structures may be exempted from the calculations in this section.*

3. For land uses not listed in the table above, maximum parking will be calculated based on the most similar land use in the table.

B. For office, industrial, and public uses where there are more than 20 parking spaces for employees on the site, at least 10 percent of the employee parking spaces shall be reserved for carpool use before 9:00 a.m. on weekdays. The spaces will be the closest to the building entrance, except for any disabled parking and those signed for exclusive customer use. The carpool/vanpool spaces shall be clearly marked “Reserved – Carpool/Vanpool Before 9:00 a.m.”

C. Existing developments along transit streets or near transit stops may redevelop the existing parking spaces to provide transit-oriented facilities, including bus pullouts, bus stops and shelters, park and ride stations, and other similar facilities. (Ord. [1291](#), 1990; Ord. [1391](#), 1996; Ord. [1408](#), 1998; Ord. [1425](#), 1998; Ord. [1463](#), 2000; Ord. [1499](#), 2003; Ord. [1547](#), 2007; Ord. [1622](#) § 25, 2014; Ord. [1623](#) § 4, 2014; Ord. [1650](#) § 1 (Exh. A), 2016; Ord. [1675](#) § 38, 2018; Ord. [1736](#) § 1 (Exh. A), 2022; Ord. [1745](#) § 1 (Exh. A), 2023; Ord. [1754](#) § 1 (Exh. A), 2024)

Staff Finding 20: The two most similar land uses listed are “General Office” and “Warehouse”.

Building B 2nd Floor (Office): 14.09k sq. ft. x 3.0 = 42 spaces

Building B 1st Floor (Fleet Maintenance): 14.1k sq. ft. x 0.4 = 6 spaces

Building A (Warehouse): 11.07k sq. ft. x 0.4 = 4 spaces

Total maximum vehicle parking spaces is 52.

Proposed Parking

Employee/Visitor = 45 spaces

Fleet (Trucks Equipment) = 51 spaces

Total = 96 spaces

Employee/Visitor Lot Breakdown:

Standard Spaces: 38

ADA Spaces: 2

Carpool Spaces: 5

The 45 employee/visitor spaces provided will serve everyday staff and visitor needs and is less than the maximum 52 spaces allowed based on the above calculations. The 51 fleet-designated spaces are for the function of the public works facility and are intended for large vacuum trucks, dumpster trucks, heavy equipment, trailers and similar vehicles and are excluded from the maximum parking calculation. The criteria are met.

[...]

46.150 DESIGN AND IMPROVEMENT STANDARDS

The following standards apply to the design and improvement of areas used for vehicle parking, storage, loading, and circulation:

A. Design standards.

1. "One standard parking space" means a minimum for a parking stall of eight feet in width and 16 feet in length. These stalls shall be identified as "compact." To accommodate larger cars, 50 percent of the parking spaces shall have a minimum dimension of nine feet in width and 18 feet in length (nine feet by 18 feet). When multifamily parking stalls back onto a driveway, as opposed to a drive aisle within a parking lot, the stalls shall be nine feet by 20 feet. Parking for development in water resource areas may have 100 percent compact spaces.

2. Disabled parking and maneuvering spaces shall be consistent with current federal dimensional standards and subsection B of this section and placed nearest to accessible building entryways and ramps.

.....

4. Service drives for non-residential development shall be designed and constructed to facilitate the flow of traffic, provide maximum safety of traffic access and egress, and maximum safety of pedestrians and vehicular traffic on the site.

5. Each parking and/or loading space shall have clear access, whereby the relocation of other vehicles to utilize the parking space is not required, except where stacked parking is permitted pursuant to CDC 46.070(B)(4) or 46.090(A).

6. Except for single-family attached and detached residences, any area intended to be used for off-street parking as contained in this chapter shall have all parking spaces clearly marked using a permanent paint. All interior drives and access aisles shall be clearly marked and signed to show direction of flow and maintain vehicular and pedestrian safety. Permeable parking surface spaces may have an alternative delineation for parking spaces.

7. Except for residential parking, and parking for public parks and trailheads, at least 50 percent of all areas used for the parking and/or storage and/or maneuvering of any vehicle, boat and/or trailer shall be improved with asphalt or concrete surfaces according to the same standards required for the construction and acceptance of City streets. The remainder of the areas used for parking may use a permeable paving surface designed to reduce surface runoff. Parking for public parks or trailheads may use a permeable paving surface designed to reduce surface runoff for all parking areas. Where a parking lot contains both paved and unpaved areas, the paved areas shall be located closest to the use which they serve.

8. Off-street parking spaces for single-family attached and detached residences shall be improved with a paved surface. Other parking facilities for single-family homes that are to accommodate additional vehicles, boats, recreational vehicles, and trailers, etc., need not be paved. All parking for multifamily residential development shall be paved with concrete or asphalt. Driveways shall measure at least 20 feet from the back of sidewalk to garage or the end of the parking pad to accommodate cars and sport utility vehicles without the vehicles blocking the public sidewalk.

9. Access drives from the street to off-street parking or loading areas for non-residential development shall be designed and constructed to facilitate the flow of traffic and provide maximum safety for pedestrian and vehicular traffic on the site. The number of access drives shall be limited to the minimum that will allow the property to accommodate and service the

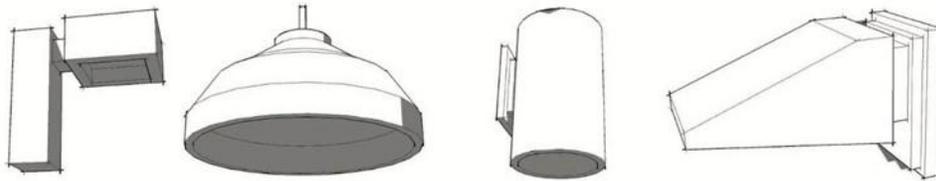
anticipated traffic. Access drives for all development shall be clearly and permanently marked and defined through use of rails, fences, walls, or other barriers or markers on frontage not occupied by service drives.

10. Access drives shall have a minimum vision clearance as provided in Chapter 42 CDC, Clear Vision Areas.

11. Parking spaces along the boundaries of a parking lot or adjacent to interior landscaped areas or sidewalks shall be provided with a wheel stop at least four inches high located two feet back from the front of the parking stall. Such parking spaces may be provided without wheel stops if the sidewalks or landscaped areas adjacent the parking stalls are two feet wider than the minimum width.

12. Off-street parking and loading areas shall be drained in accordance with City of West Linn Public Works Design Standards. Storm drainage at commercial sites may also have to be collected to treat oils and other residue.

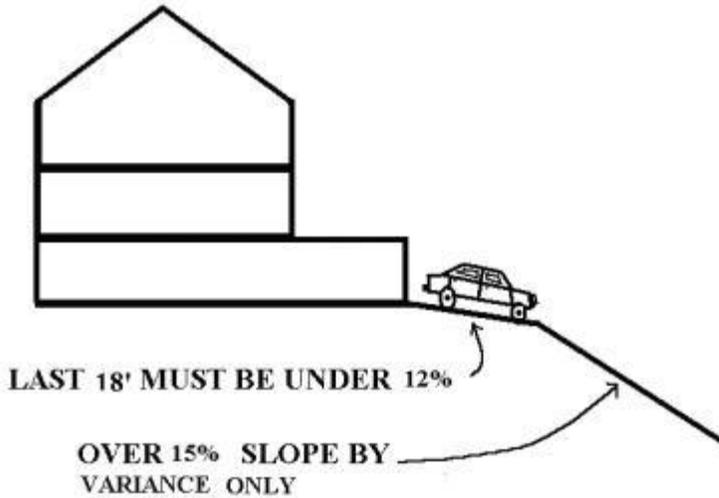
13. Artificial lighting on all off-street parking facilities shall be concealed or shielded with an Illuminating Engineering Society of North America (IESNA) full cut-off style fixture with an angle not exceeding 90 degrees to minimize the potential for glare and unnecessary diffusion on adjacent property and so as not to create a hazard to the public use of any road or street. Examples of shielded light fixtures are shown below.



Examples of shielded light fixtures

14. Directional arrows and traffic control devices which are placed on parking lots shall be identified.

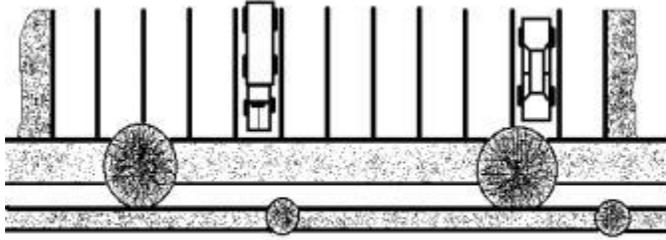
15. The maximum driveway grade for single-family housing shall be 15 percent. The 15 percent shall be measured along the centerline of the driveway only. Grades elsewhere along the driveway shall not apply. Variations require approval of a Class II variance by the Planning Commission pursuant to Chapter 75 CDC. Regardless, the last 18 feet in front of the garage must maintain a maximum grade of 12 percent as measured along the centerline of the driveway only. Grades elsewhere along the driveway shall not apply.



16. Visitor or guest parking must be identified by painted "GUEST" or "VISITOR."
17. Parking spaces shall have less than a five percent grade. No drainage across adjacent sidewalks or walkways is allowed.
18. Commercial, office, industrial, and public parking lots may not occupy more than 50 percent of the main lot frontage of a development site. The remaining frontage shall comprise buildings or landscaping. If over 50 percent of the lineal frontage comprises parking lot, the landscape strip between the right-of-way and parking lot shall be increased to 15 feet wide and shall include terrain variations (e.g., one-foot-high berm) plus landscaping. The defensible space of the parking lot should not be compromised.

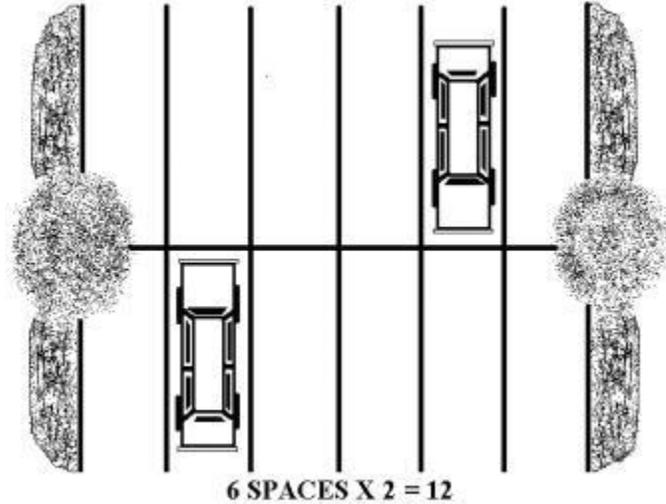
Staff Finding 22: The 45 parking spaces for employee/visitors are 9 ft. x 18 ft. and will be clearly marked with permanent paint. ADA spaces are located nearest to buildings. No stacked parking is proposed. The service drive from Salamo Road will be paved with non-pervious asphalt and includes a 30 ft. x 30 ft. vision clearance triangle at the access point on Salamo Road. The service roads throughout the site will be paved with non-pervious asphalt and are minimum of 24 feet wide to ensure proper maneuvering of vehicles onsite. Artificial lighting around parking the parking areas will be full cut fixtures not exceeding 90 degrees. Parking areas will be less than a 5% grade and no street frontage is proposed to be parking. The criteria are met.

19. Areas of the parking lot improved with asphalt or concrete surfaces shall be designed into areas of 12 or less spaces through the use of defined landscaped area. Groups of 12 or less spaces are defined as:
 - a. Twelve spaces in a row, provided there are no abutting parking spaces, as in the case when the spaces are abutting the perimeter of the lot; or

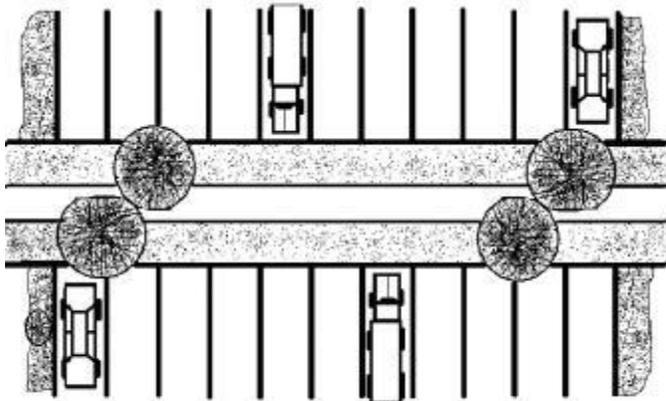


12 SPACES IN A ROW

b. Twelve spaces in a group with six spaces abutting together; or



c. Two groups of 12 spaces abutting each other, but separated by a 15-foot-wide landscape area including a six-foot-wide walkway.



12 SPACES X2 WITH LANDSCAPING

d. Parking areas improved with a permeable parking surface may be designed using the configurations shown in subsections (A)(19)(a), (b) and (c) of this section except that groups of up to 18 spaces are allowed.

e. The requirements of this chapter relating to total parking lot landscaping, landscaping buffers, perimeter landscaping, and landscaping the parking lot islands and interior may be waived or reduced pursuant to CDC 32.110(F) in a WRA application without a variance being required.

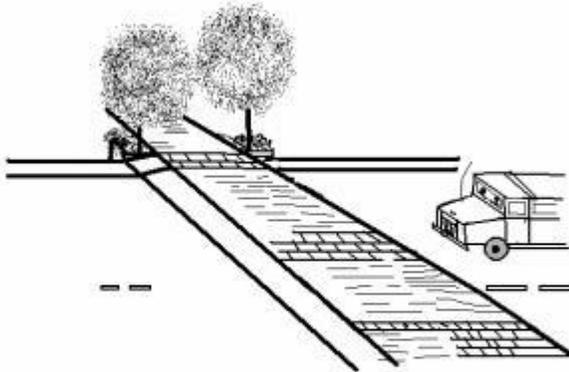
20. Pedestrian connections through parking areas.

a. Pedestrian walkways shall be provided in parking areas that are larger than one-half acre. The pedestrian walkways or sidewalks should provide access to building entrances, existing or planned pedestrian facilities in the public right-of-way, and to accessible parking spaces.

b. Walkways or sidewalks shall be constructed through parking lots where such parking lots lie between major buildings/activity areas (an example in multifamily housing: between recreation center, swimming pool, manager's office, park or open space areas, parking lots, etc.) within a development and adjacent streets or adjacent transit stops. The walkways or sidewalks should also provide access to building entrances, existing or planned pedestrian facilities in the public rights-of-way, and to accessible parking spaces.

c. Walkways through parking lots shall be constructed using a material that visually contrasts with the parking lot and driveway surface and that may have a contrasting surface texture (surface texture shall not interfere with safe use of wheelchairs, baby carriages, shopping carts, etc.). Walkways shall be physically separated from adjacent vehicle parking and parallel vehicle traffic through the use of grade separation, walls, curbs, and/or landscaping.

d. Walkways shall be a minimum of six feet wide.



**RAISED SIDEWALK/TEXTURED SURFACE
AUTOMOBILE BECOMES SUBSERVIENT
TO THE PEDESTRIAN**

21. Parking and circulation patterns shall be easily comprehended and defined. The patterns shall be clear to minimize traffic hazards and congestion and to facilitate emergency vehicles. Residential developments which are exempt from design review or subject to only clear and objective design review are not required to comply with this design standard.

22. Parking spaces for residential development subject to clear and objective review shall be provided on the same lot. Parking spaces for non-residential development or for residential development subject to discretionary review shall be close to the related use.

23. Permeable parking spaces shall be designed and built in compliance with the West Linn Public Works Design Standards.

B. Accessible parking standards for persons with disabilities. If any parking is provided for the public or visitors, or both, the needs of the people with disabilities shall be based upon the following standards or current applicable federal standards, whichever are more stringent:

1. Minimum number of accessible parking space requirements (see following table):

<u>NUMBER OF TOTAL PARKING SPACES</u>	<u>TOTAL NUMBER OF ACCESSIBLE SPACES</u>	<u>NUMBER OF VAN-ACCESSIBLE SPACES REQUIRED, OF TOTAL</u>	<u>SPACES SIGNED "WHEELCHAIR USE ONLY"</u>
<u>1 – 25</u>	<u>1</u>	<u>1</u>	<u>=</u>
<u>26 – 50</u>	<u>2</u>	<u>1</u>	<u>=</u>
<u>51 – 75</u>	<u>3</u>	<u>1</u>	<u>=</u>
<u>76 – 100</u>	<u>4</u>	<u>1</u>	<u>=</u>
<u>101 – 150</u>	<u>5</u>	<u>=</u>	<u>1</u>
<u>151 – 200</u>	<u>6</u>	<u>=</u>	<u>1</u>
<u>201 – 300</u>	<u>7</u>	<u>=</u>	<u>2</u>
<u>301 – 400</u>	<u>8</u>	<u>=</u>	<u>2</u>
<u>401 – 500</u>	<u>9</u>	<u>=</u>	<u>2</u>
<u>501 – 999</u>	<u>2 percent of total spaces</u>	<u>=</u>	<u>1 in every 6 accessible spaces or portion thereof</u>
<u>Over 1,000</u>	<u>20 spaces plus 1 for every 100 spaces, or fraction thereof, over 1,000</u>	<u>=</u>	<u>1 in every 6 spaces or portion thereof</u>

2. Location of parking spaces. Parking spaces for the individual with a disability that serve a particular building shall be located on the shortest possible accessible circulation route to an accessible entrance to a building. In separate parking structures or lots that do not serve a particular building, parking spaces for the persons with disabilities shall be located on the shortest possible circulation route to an accessible pedestrian entrance of the parking facility.

3. Accessible parking space and aisle shall meet ADA vertical and horizontal slope standards.

4. Where any differences exist between this section and current Federal standards, those Federal standards shall prevail over this code section.

5. One in every eight accessible spaces, but not less than one, shall be served by an access aisle 96 inches wide.

6. Van-accessible parking spaces shall have an additional sign marked "Van Accessible" mounted below the accessible parking sign. A van-accessible parking space reserved for wheelchair users shall have a sign that includes the words "Wheelchair Use Only." Van-accessible parking shall have an adjacent eight-foot-wide aisle. All other accessible stalls shall have a six-foot-wide aisle. Two vehicles may share the same aisle if it is between them. The vertical clearance of the van space shall be 96 inches.

C. Landscaping in parking areas. Reference Chapter 54 CDC, Landscaping.

D. Bicycle facilities and parking.

1. Provisions shall be made for pedestrian and bicycle ways if such facilities are identified on pages 24-26 of the West Linn Transportation System Plan or in the 2013 West Linn Trails Plan.

2. Bicycle parking improvements and location.

a. Bicycle parking facilities shall either be lockable enclosures in which the bicycle is stored, or secure stationary racks which accommodate bicyclists' locks securing the frame and both wheels.

b. The nearest bicycle parking space shall be no more than 50 feet from the entrance to the building and shall be visible from the building entrance.

c. Required parking shall be signed and reserved for bicycle parking only. If a bicycle parking area is not visible from the main building entrance, a directional sign shall be posted at the building entrance indicating the location of the bicycle parking area.

d. Required bicycle parking shall be lighted to a minimum three foot-candles to allow secure use at night.

3. Bicycle parking must be provided in the following amounts:

<u>LAND USE CATEGORY</u>	<u>MINIMUM REQUIRED BICYCLE PARKING SPACES</u>	<u>MINIMUM COVERED AMOUNT</u>
<u>Residential</u>	-	-
<u>Multifamily Residential</u>	<u>1 space per unit</u>	<u>50%</u>
<u>Institutional</u>	-	-
<u>Schools – Elementary</u>	<u>2 spaces per classroom</u>	<u>50%</u>
<u>Schools – Jr. High or Middle Schools</u>	<u>4 spaces per classroom</u>	<u>50%</u>
<u>Schools – Sr. High</u>	<u>2 spaces per classroom</u>	<u>50%</u>
<u>College</u>	<u>1 space per 4 students</u>	<u>50%</u>

<u>LAND USE CATEGORY</u>	<u>MINIMUM REQUIRED BICYCLE PARKING SPACES</u>	<u>MINIMUM COVERED AMOUNT</u>
<u>Transit Centers/Park & Ride Lots</u>	<u>5% of auto spaces, or 100% of demand, depending on location/accessibility to bicyclists</u>	<u>100%</u>
<u>Religious Institutions</u>	<u>1 space per 40-seat capacity</u>	<u>25%</u>
<u>Hospitals</u>	<u>1 space per 5 beds</u>	<u>50%</u>
<u>Doctor, Dentist Offices</u>	<u>2, or 0.5 spaces per 1,000 gross sq. ft., whichever is greater</u>	<u>25%</u>
<u>Libraries, Museums, Government Offices, etc.</u>	<u>2, or 1.5 spaces per 1,000 gross sq. ft., whichever is greater</u>	<u>25%</u>
<u>Commercial</u>	-	-
<u>Retail Sales</u>	<u>0.33 spaces per 1,000 gross sq. ft.</u>	<u>50%</u>
<u>Auto-oriented Services (including 7-11s)</u>	<u>2, or 0.33 spaces per 1,000 gross sq. ft., whichever is greater</u>	<u>10%</u>
<u>Groceries/Supermarkets</u>	<u>0.33 spaces per 1,000 gross sq. ft./bldg.</u>	<u>10%</u>
<u>Office</u>	<u>2, or 0.5 spaces per 1,000 gross sq. ft., whichever is greater</u>	<u>10%</u>
<u>Quality Restaurant</u>	<u>1 space per 1,000 gross sq. ft.</u>	<u>25%</u>
<u>Drive-in Restaurant</u>	<u>2 spaces per 1,000 gross sq. ft.</u>	<u>25%</u>
<u>Shopping Center (by size)</u>	<u>0.33 spaces per 1,000 gross sq. ft./bldg.</u>	<u>50%</u>
<u>Financial Institutions</u>	<u>2, or 0.33 spaces per 1,000 gross sq. ft.</u>	<u>25%</u>
<u>Theaters, Auditoriums, etc.</u>	<u>1 space per 30 seats</u>	<u>25%</u>
<u>Industrial</u>	-	-
<u>Industrial Park</u>	<u>2, or 0.5 spaces per 1,000 gross sq. ft.</u>	<u>50%</u>
<u>Warehouse</u>	<u>2, or 0.1 spaces per 1,000 gross sq. ft.</u>	<u>50%</u>
<u>Manufacturing, etc.</u>	<u>2, or 0.15 spaces per 1,000 gross sq. ft.</u>	<u>50%</u>

E. (See Figures 1 and 2 below.)

Figure 1. MINIMUM STANDARDS FOR PARKING LOT LAYOUT

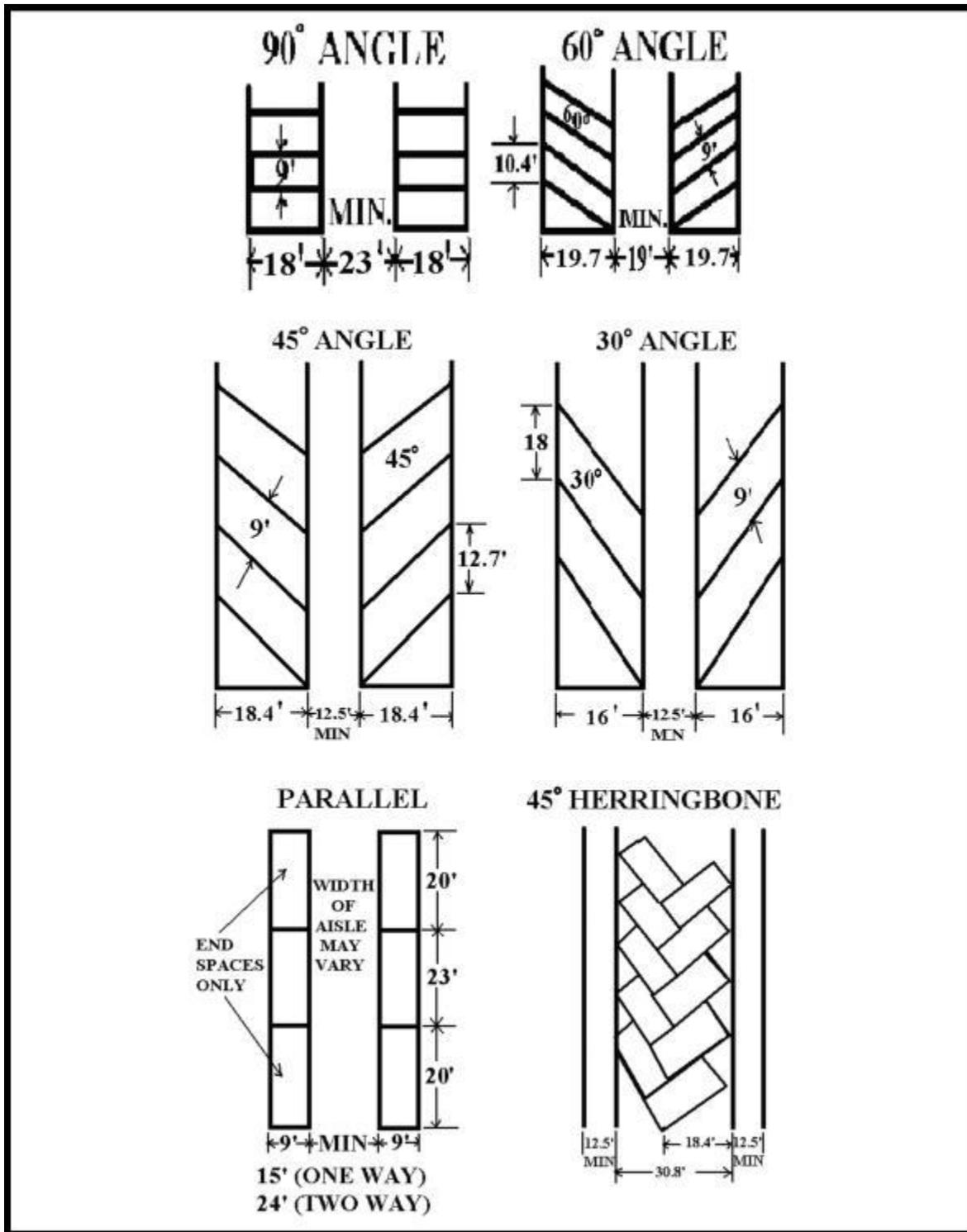
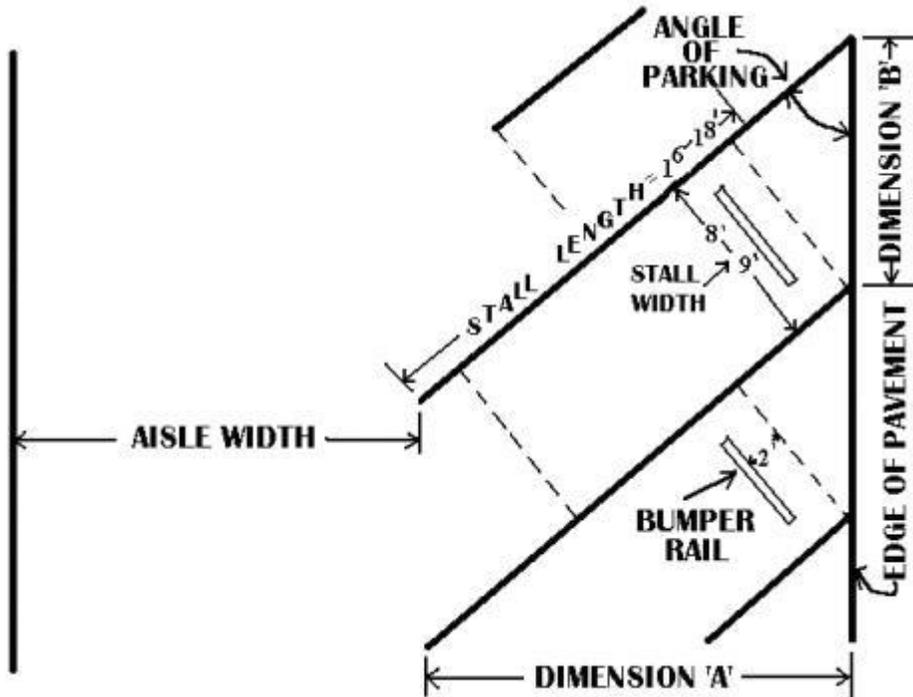


Figure 2. MINIMUM DISTANCE FOR PARKING STALLS



<u>ANGLE OF PARKING</u>	<u>DIRECTION OF PARKING</u>	<u>AISLE WIDTH</u>		<u>DIMENSION 'A'</u>		<u>DIMENSION 'B'</u>	
		<u>STALL WIDTH</u>		<u>STALL WIDTH</u>		<u>STALL WIDTH</u>	
		<u>9.0'</u>	<u>8.0'</u>	<u>9.0'</u>	<u>8.0'</u>	<u>9.0'</u>	<u>8.0'</u>
<u>30°</u>	<u>DRIVE-IN</u>	<u>12.5'</u>	<u>12.5'</u>	<u>16.8'</u>	<u>13.8'</u>	<u>18.0'</u>	<u>16.0'</u>
<u>45°</u>	<u>DRIVE-IN</u>	<u>12.5'</u>	<u>12.5'</u>	<u>19.1'</u>	<u>17.0'</u>	<u>12.7'</u>	<u>11.3'</u>
<u>60°</u>	<u>DRIVE-IN</u>	<u>19.0'</u>	<u>18.0'</u>	<u>20.1'</u>	<u>17.8'</u>	<u>10.4'</u>	<u>9.2'</u>
<u>60°</u>	<u>BACK-IN</u>	<u>17.0'</u>	<u>17.0'</u>	<u>20.1'</u>	<u>17.8'</u>	<u>10.4'</u>	<u>9.2'</u>
<u>90°</u>	<u>DRIVE-IN</u>	<u>23.0'</u>	<u>23.0'</u>	<u>18.0'</u>	<u>16.0'</u>	<u>9.0'</u>	<u>8.0'</u>
<u>90°</u>	<u>BACK-IN</u>	<u>22.0'</u>	<u>22.0'</u>	<u>18.0'</u>	<u>16.0'</u>	<u>9.0'</u>	<u>8.0'</u>

Staff Finding 23: The lowest paved area (surrounding Building A) shows striping for a number of surface parking spaces. However, this entire lower area will be used as storage, laydown, and fleet maneuvering spaces. Parking striping is shown to outline occasional use for city vehicle, trailer, and/or truck parking; the flexibility of this area – including large vehicle turning movement, staging, and other associated activities - is an essential component to the function of the public works facility. Therefore, landscape strips, planting, or trees are not proposed in this area.

For the upper lot, existing trees are preserved on the edges of the lot to provide tree canopy and shading in-lieu of a planting strip.

Required bicycle parking is equal to:

Building A (warehouse): 11,073 sq. ft.

$11,073/1,000 \times 0.1 = 1$ bike space

Building B 1st floor (warehouse): 14,106 sq. ft.

$14,106/1,000 \times 0.1 = 1$ bike space

Building B 2nd Floor (office use): 14,900 sq. ft.

$14,090/1,000 \times 0.5^* = 7$ bike spaces

*The office portion of the building operates at a less dense staff expectation than the above “libraries, museums, government office” designation, therefore the ‘Office’ category has been applied.

Bike spaces total = 9

The proposed drive aisle width for the 90-degree parking spaces is 24-ft and is compliant with the 23-ft minimum standard outlined above in the West Linn CDC.

The criteria are met.

F. The following requirements apply to any new development that includes more than one-half acre of new off-street surface parking on a lot or parcel. The new surface parking area shall be measured based on the perimeter of all new off-street parking spaces, maneuvering lanes, and maneuvering areas, including driveways and drive aisles:

1. Tree canopy covering at least 40 percent of the new parking lot area at maturity but no more than 15 years after planting. Trees planted or preserved to meet parking area landscaping requirements in CDC 54.020 may count toward the tree canopy cover percentage.

2. At a minimum, trees planted to meet canopy cover requirements must be planted at the standards and specifications no lower than the 2021 American National Standards Institute A300 standards.

G. New construction or redevelopment for the following uses must include provision of electric service capacity, as defined in ORS 455.417 and in this Code, for a minimum percentage of vehicle parking spaces:

- | | |
|--|------------|
| <i>1. Commercial buildings under private ownership</i> | <i>20%</i> |
| <i>2. Multifamily buildings (5 or more units)</i> | <i>40%</i> |
| <i>3. Mixed-use buildings consisting of privately owned commercial space and five or more residential dwelling units</i> | <i>40%</i> |

(Ord. 1425, 1998; Ord. 1463, 2000; Ord. 1513, 2005; Ord. 1547, 2007; Ord. 1590 § 1, 2009; Ord. 1604 § 46, 2011; Ord. 1622 § 25, 2014; Ord. 1623 § 4, 2014; Ord. 1635 § 24, 2014; Ord. 1736 § 1 (Exh. A), 2022; Ord. 1745 § 1 (Exh. A), 2023; Ord. 1754 § 1 (Exh. A), 2024)

Staff Finding 24: The upper-level parking areas are less than one half acre in size. The intent of these provisions was to satisfy Oregon’s Climate Friendly and Equitable Communities (CFEC) legislation, and to more specifically address greenhouse gas emissions caused by vehicular transportation. Because the purpose of the lower parking area is to accommodate vehicles, whose purpose is maintenance of City of West Linn public facilities, and do not serve a transportation function, staff finds that the lower-level parking area is not subject to these requirements. Therefore, the criteria are met.

Chapter 48 ACCESS, EGRESS AND CIRCULATION

A. Purpose. *The following access control standards apply to public, industrial, commercial and residential developments including land divisions. Access shall be managed to maintain an adequate level of service and to maintain the functional classification of roadways as required by the West Linn Transportation System Plan.*

B. Access control standards.

1. Traffic impact analysis requirements. *A traffic analysis prepared by a qualified professional may be required to determine access, circulation and other transportation requirements. The purpose, applicability and standards of this analysis are found in CDC 85.170(B)(2).*

85.170 SUPPLEMENTAL SUBMITTAL REQUIREMENTS FOR TENTATIVE SUBDIVISION OR PARTITION PLAN

.....

B. Transportation.

1. *Centerline profiles with extensions shall be provided beyond the limits of the proposed subdivision to the point where grades meet, showing the finished grade of streets and the nature and extent of street construction. Where street connections are not proposed within or beyond the limits of the proposed subdivision on blocks exceeding 330 feet, or for cul-de-sacs, the tentative plat or partition shall indicate the location of easements that provide connectivity for bicycle and pedestrian use to accessible public rights-of-way.*

2. Traffic impact analysis (TIA).

a. Purpose. *The purpose of this section is to implement Section 660-012-0045(2)(e) of the State Transportation Planning Rule that requires the City to adopt a process to apply conditions to development proposals in order to minimize adverse impacts to and protect transportation facilities. This section establishes the standards for when a proposal must be reviewed for potential traffic impacts; when a Traffic Impact Analysis must be submitted with a development application in order to determine whether conditions are needed to minimize impacts to and protect transportation facilities; what must be in a traffic impact analysis; and who is qualified to prepare the study.*

b. Typical average daily trips. The latest edition of the Trip Generation Manual, published by the Institute of Transportation Engineers (ITE) shall be used as the standards by which to gauge average daily vehicle trips.

c. Traffic impact analysis (no dwellings). For development applications that do not propose any new dwelling units, a traffic impact analysis may be required to be submitted to the City with a land use application, when the following conditions apply:

1) The development application involves one or more of the following actions:

(A) A change in zoning or a plan amendment designation; or

(B) Any proposed development or land use action that ODOT states may have operational or safety concerns along a State highway; and

(C) The development shall cause one or more of the following effects, which can be determined by field counts, site observation, traffic impact analysis or study, field measurements, crash history, Institute of Transportation Engineers Trip Generation manual, and information and studies provided by the local reviewing jurisdiction and/or ODOT:

(1) An increase in site traffic volume generation by 250 average daily trips (ADT) or more (or as required by the City Engineer); or

(2) An increase in use of adjacent streets by vehicles exceeding the 20,000 pound gross vehicle weights by 10 vehicles or more per day; or

(3) The location of the access driveway does not meet minimum intersection sight distance requirements, or is located where vehicles entering or leaving the property are restricted, or such vehicles queue or hesitate on the State highway, creating a safety hazard; or

(4) The location of the access driveway does not meet the access spacing standard of the roadway on which the driveway is located; or

(5) A change in internal traffic patterns that may cause safety problems, such as backup onto the highway or traffic crashes in the approach area.

Staff Finding 25: A ‘Trip Generation Report’ was prepared by Kittelson and Associates for the proposed project on October 15, 2025, and included in the Applicant’s submittal (Exhibit PC-E Transportation Assessment). Traffic counts at the existing public works facility on Norfolk Ft/Sussex St were collected over three days in September 2025 and the trips associated with the existing facility did not exceed the 250 daily trip thresholds on two of the three days of observation. Additionally, the applicant has indicated that the proposed facility will not generate 10 or more heavy vehicles (20,000 pounds) trips per day. Based on the findings of the trip count data not exceeding the threshold on two of the three observation days, the location of the proposed facility on a higher road classification – Salamo Road = Minor Arterial street; Norfolk St/Sussex St. = Local street, and less than 10 heavy vehicles using the site daily, Kittelson and Associates does not recommend the need for a formal Traffic Impact Analysis (TIA) pursuant to CDC 85.170(B)(2). Further, because Salamo Road is a fully developed facility with a turn pocket to accommodate southbound left

turns into the site and additional intersection control is not identified in the Transportation System Plan, no further improvements would be warranted. Therefore, the results of a full TIA would not change the recommended outcome. The criteria are met.

3. Access options. When vehicle access is required for development (i.e., for off-street parking, delivery, service, drive-through facilities, etc.), access shall be provided from a public street adjacent to the development lot or parcel. Street accesses shall comply with access spacing standards in subsection (B)(6) of this section, the West Linn Public Works Design Standards, and TSP. As an alternative, the applicant may request alternative access provisions listed below as Option 1 and Option 2, subject to approval by the City Engineer through a discretionary process.

Staff Finding 26: The project will be accessed from Salamo Road, a public street adjacent to the development. The criteria are met.

6. Access spacing.

a. The access spacing standards found in Tables 14 and 15 of the TSP and in CDC 48.060 shall be applicable to all newly established public street intersections, non-traversable medians, and curb cuts. Deviation from the access spacing standards may be granted by the City Engineer as part of a discretionary review if the applicant demonstrates that the deviation will not compromise the safe and efficient operation of the street and highway system.

b. Private drives and other access ways are subject to the requirements of CDC 48.060.

7. Number of access points. For single-family (detached and attached) housing types, one street access point is permitted per lot or parcel when alley access cannot otherwise be provided; except that two access points may be permitted corner lots (i.e., no more than one access per street), subject to the access spacing standards in CDC 48.060. The number of street access points for multiple family development is subject to the access spacing standards in CDC 48.060. The number of street access points for commercial, industrial, and public/institutional developments shall be minimized to protect the function, safety and operation of the street(s) and sidewalk(s) for all users. Shared access may be required, in conformance with subsection (C)(8) of this section, in order to maintain the required access spacing, and minimize the number of access points.

8. Shared driveways. For residential development, shared driveways may be required in order to meet the access spacing standards in subsection (C)(6) of this section. For non-residential development, the number of driveway and private street intersections with public streets shall be minimized by the use of shared driveways with adjoining lots where feasible. The City shall require shared driveways as a condition of land division or site design review, as applicable, for traffic safety and access management purposes in accordance with the following standards:

a. When necessary pursuant to this subsection (C)(8), shared driveways and/or frontage streets shall be required to consolidate access onto a collector or arterial street. When shared driveways or frontage streets are required, they shall be stubbed to adjacent developable parcels to indicate future extension. "Stub" means that a driveway or street temporarily ends at

the property line, but may be extended in the future as the adjacent lot or parcel develops. “Developable” means that a lot or parcel is either vacant or it is likely to receive additional development (i.e., due to infill or redevelopment potential).

b. Access easements (i.e., for the benefit of affected properties) shall be recorded for all shared driveways, including pathways, at the time of final plat approval or as a condition of site development approval.

c. Exception. Exceptions to the shared driveway or frontage street requirements may be granted as part of a discretionary review if the City determines that existing development patterns or physical constraints (e.g., topography, lot or parcel configuration, and similar conditions) prevent extending the street/driveway in the future.

C. Street connectivity and formation of blocks required. In order to promote efficient vehicular and pedestrian circulation throughout the City, land divisions and site developments shall produce complete blocks bounded by a connecting network of public and/or private streets, in accordance with the following standards:

1. Block length and perimeter. The maximum block length shall not exceed 800 feet along a collector, neighborhood route, or local street, or 1,800 feet along an arterial, unless a smaller block length is required pursuant to CDC 85.200(B)(2).
2. Street standards. Public and private streets shall also conform to Chapter 92 CDC, Required Improvements, and to any other applicable sections of the West Linn Community Development Code and approved TSP.
3. Exception. Exceptions to the above standards may be granted as part of a discretionary review when blocks are divided by one or more pathway(s), in conformance with the provisions of CDC 85.200(C), Pedestrian and bicycle trails, or cases where extreme topographic (e.g., slope, creek, wetlands, etc.) conditions or compelling functional limitations preclude implementation, not just inconveniences or design challenges. (Ord. 1635 § 25, 2014; Ord. 1636 § 33, 2014; Ord. 1650 § 1 (Exh. A), 2016; Ord. 1675 § 40, 2018; Ord. 1745 § 1 (Exh. A), 2023)

Staff Finding 27: The minimum separation distance between curb-cuts along an arterial street (Salamo Road) is 150 feet. The sole proposed access to the project site on the east side of Salamo Road is approximately 500 ft from the nearest access along Salamo Road to the north. No shared driveways or access easements are proposed. The criteria are met.

48.040 MINIMUM VEHICLE REQUIREMENTS FOR NON-RESIDENTIAL USES

Access, egress, and circulation system for all non-residential uses shall not be less than the following:

- A. Service drives for non-residential uses shall be fully improved with hard surface pavement:
1. With a minimum of 24-foot width when accommodating two-way traffic; or
 2. With a minimum of 15-foot width when accommodating one-way traffic. Horizontal clearance shall be two and one-half feet wide on either side of the driveway.
 3. Meet the requirements of CDC 48.030(E)(3) through (6).

4. *Pickup window driveways may be 12 feet wide unless the Fire Chief determines additional width is required.*
- B. *All non-residential uses shall be served by one or more service drives as determined necessary to provide convenient and safe access to the property and designed according to CDC 48.030(A). In no case shall the design of the service drive or drives require or facilitate the backward movement or other maneuvering of a vehicle within a street, other than an alley.*
- C. *All on-site maneuvering and/or access drives shall be maintained pursuant to CDC 46.130.*
- D. *Gated accessways to non-residential uses are prohibited unless required for public safety or security. (Ord. 1408, 1998, Ord. 1463, 2000; Ord. 1745 § 1 (Exh. A), 2023)*

Staff Finding 28: The access drive from Salamo Road is designed to be 24 in width and improved with a hard surface pavement. No backup movements are required on a public street. A security gate at the Salamo road access point will be installed for after hour security of the site and will be open during regular business hours. The criteria are met.

48.060 WIDTH AND LOCATION OF CURB CUTS AND ACCESS SEPARATION REQUIREMENTS

- A. *Minimum curb cut width shall be 16 feet.*
- B. *Maximum curb cut width shall be 36 feet, except along Highway 43 in which case the maximum curb cut shall be 40 feet. For emergency service providers, including fire stations, the maximum shall be 50 feet.*
- C. *No curb cuts shall be allowed any closer to an intersecting street right-of-way line than the following:*
 1. *On an arterial when intersected by another arterial, 150 feet.*
 2. *On an arterial when intersected by a collector, 100 feet.*
 3. *On an arterial when intersected by a local street, 100 feet.*
 4. *On a collector when intersecting an arterial street, 100 feet.*
 5. *On a collector when intersected by another collector or local street, 35 feet.*
 6. *On a local street when intersecting any other street, 35 feet.*
- D. *There shall be a minimum distance between any two adjacent curb cuts on the same side of a public street, except for one-way entrances and exits, as follows:*
 1. *On an arterial street, 150 feet.*
 2. *On a collector street, 75 feet.*
 3. *Between any two curb cuts on the same lot or parcel on a local street, 30 feet.*
- E. *A rolled curb may be installed in lieu of curb cuts and access separation requirements.*
- F. *For non-residential development, curb cuts shall be kept to the minimum, particularly on Highway 43. Consolidation of driveways is preferred. The standard on Highway 43 is one curb cut per business if consolidation of driveways is not possible.*
- G. *Clear vision areas shall be maintained, pursuant to Chapter 42 CDC, and required line of sight shall be provided at each driveway or accessway, pursuant to the West Linn Public Works Design Standards. (Ord. 1270, 1990; Ord. 1584, 2008; Ord. 1636 § 35, 2014; Ord. 1745 § 1 (Exh. A), 2023)*

Staff Finding 29: The proposed curb cut along Salamo Road is 24 feet wide and is greater than 500 feet to the nearest curb cut along the eastside of Salamo Road. A clear vision area of 30 ft. by 30 ft. is proposed on both sides of the curb cut to ensure proper sightlines are provided for ingress and egress. The criteria are met.

48.080 BICYCLE AND PEDESTRIAN CIRCULATION

A. Within all multifamily developments, each residential dwelling shall be connected to vehicular parking stalls, common open space, and recreation facilities by a pedestrian pathway system having a minimum width of six feet and constructed of concrete, asphalt, brick or masonry pavers, or other hard surface. The pathway material shall be of a different color or composition from the driveway. (Bicycle routes adjacent to the travel lanes do not have to be of different color or composition.)

B. Bicycle and pedestrian ways within a subdivision shall be constructed according to the provisions in CDC 85.200(C).

C. Bicycle and pedestrian ways at commercial or industrial sites shall be provided according to the provisions of Chapter 55 CDC, Design Review. (Ord. 1745 § 1 (Exh. A), 2023)

Staff Finding 30: Design Review findings for CDC Chapter 55 are found later in this report under Findings 32 – 59. The criteria are met.

Chapter 54 LANDSCAPING

54.020 APPROVAL CRITERIA

A. Tree preservation.

1. Intent and guidance. In designing the new project, every reasonable attempt should be made to preserve and protect existing trees and to incorporate them into the new landscape plan. Similarly, significant landscaping (e.g., bushes, shrubs) should be integrated. The rationale is that saving a 30-foot-tall mature tree helps maintain the continuity of the site, they are qualitatively superior to two or three two-inch caliper street trees, they provide immediate microclimate benefits (e.g., shade), they soften views of the street, and they can increase the attractiveness, marketability, and value of the development.

2. Inventory required. Every development proposal shall submit an inventory of existing site conditions, which includes significant trees and heritage trees.

3. Repealed by Ord. 1754.

4. Developers must also comply with the municipal code chapter on tree protection and, if applicable, with CDC 55.100(B) or 55.105(B).

5. Heritage trees. Heritage trees are trees which, because of their age, type, notability, or historical association, are of special importance. Heritage trees are trees designated by the City Council following review of a nomination. A heritage tree may not be removed without a public hearing at least 30 days prior to the proposed date of removal. Development proposals

involving land with heritage tree(s) shall be required to protect and save the tree(s). Further discussion of heritage trees is found in the municipal code.

B. Landscaping – By type, location and amount.

1. Residential uses (non-single-family). A minimum of 25 percent of the gross site area including parking, loading and service areas shall be landscaped, and may include the open space and recreation area requirements under CDC 55.100 or 55.105, as applicable. Parking lot landscaping may be counted in the percentage.

2. Non-residential uses. A minimum of 20 percent of the gross site area shall be landscaped. Parking lot landscaping may be counted in the percentage.

3. Parking area landscaping – All uses (residential uses (non-single-family) and non-residential uses):

a. There shall be one shade tree planted for every eight parking spaces, except as required under subsection (B)(3)(d) of this section. Shade trees are defined as medium to large trees with a canopy, at maturity, at least 40 feet in diameter. These trees shall be evenly distributed throughout the parking lot to provide shade.

b. Parking lots with over 20 spaces shall have a minimum 10 percent of the interior of the parking lot devoted to landscaping. Pedestrian walkways in the landscaped areas are not to be counted in the percentage. The perimeter landscaping, as provided in subsection (B)(3)(h) of this section, shall not be included in the 10 percent figure.

c. Parking lots with 10 to 20 spaces shall have a minimum five percent of the interior of the parking lot devoted to landscaping. The perimeter landscaping, as provided in subsection (B)(3)(h) of this section, shall not be included in the five percent.

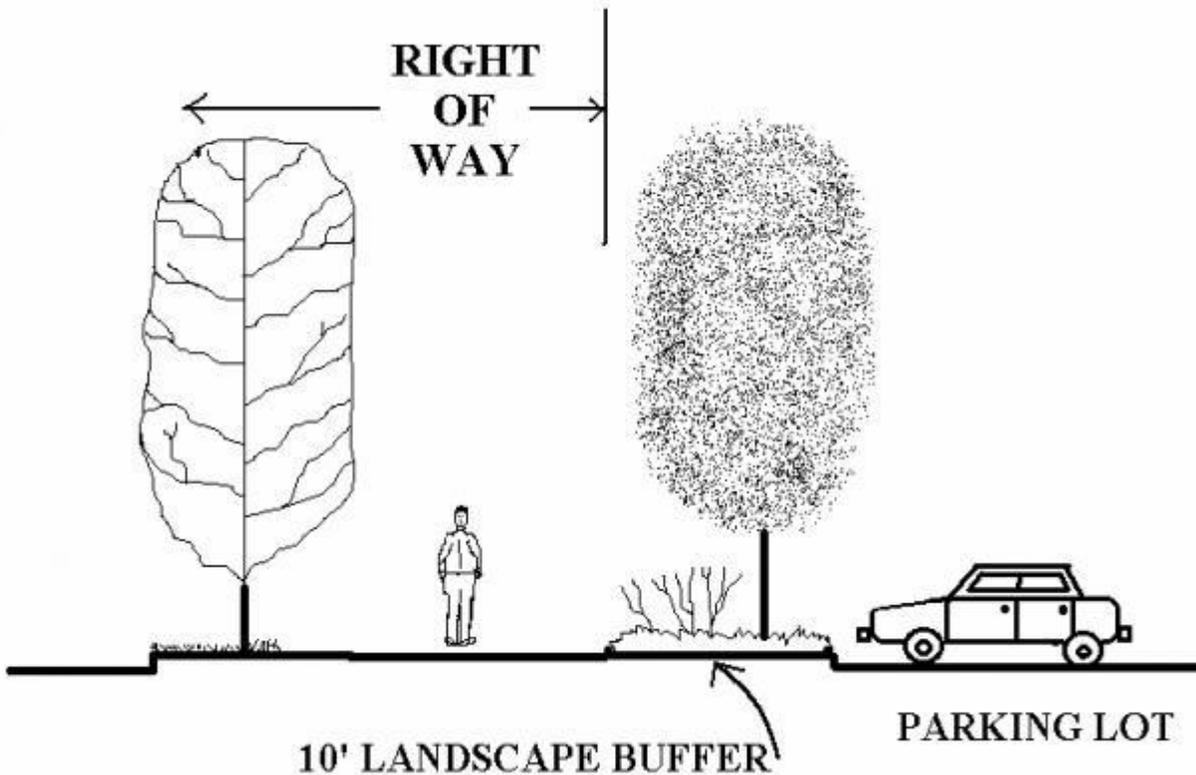
d. Parking lots with fewer than 10 spaces shall have perimeter landscaping, if required under subsection (B)(3)(h) of this section, and at least two shade trees.

e. Non-residential parking areas paved with a permeable parking surface may reduce the required minimum interior landscaping by one-third for the area with the permeable parking surface only.

f. The landscaped areas shall not have a width of less than five feet and shall be uniformly distributed throughout the parking or loading area.

g. The soils, site, proposed soil amendments, and proposed irrigation system shall be appropriate for the healthy and long-term maintenance of the proposed plant species.

h. A parking, loading, or service area which abuts a street shall be set back from the right-of-way line by perimeter landscaping in the form of a landscaped strip at least 10 feet in width. When a parking, loading, or service area or driveway is contiguous to an adjoining lot or parcel, there shall be an intervening five-foot-wide landscape strip. The landscaped area shall contain:



- 1) Trees spaced as appropriate to the species, not to exceed 30 feet apart on center, on the average;
- 2) Shrubs, not to reach a height greater than three feet, six inches, spaced no more than five feet apart on the average; and
- 3) Vegetative ground cover such as grass, wildflowers, or other landscape material to cover 100 percent of the exposed ground within two growing seasons. No bark mulch shall be allowed except under the canopy of low level shrubs.
 - i. If over 50 percent of the lineal frontage of the main street or arterial adjacent to the development site comprises parking lot, the landscape strip between the right-of-way and parking lot shall be increased to 15 feet in width and shall include terrain variations (e.g., one-foot-high berm) plus landscaping. This extra requirement only applies to one street frontage.
 - j. All areas in a parking lot not used for parking, maneuvering, or circulation shall be landscaped.
 - k. The landscaping in parking areas shall maintain minimum vision clearance as provided in Chapter 42 CDC, Clear Vision Areas.
4. Other landscaping standards – All uses (residential uses (non-single-family) and non-residential uses):
 - a. Outdoor storage areas, service areas (loading docks, refuse deposits, and delivery areas), and above-ground utility facilities shall be buffered and screened to obscure their view from adjoining properties and to reduce noise levels to acceptable levels at the property line. The

adequacy of the buffer and screening shall be determined by the criteria set forth in CDC [55.100\(C\)\(1\)](#) or [55.105\(G\)](#), as applicable.

b. For non-residential development, crime prevention shall be considered and plant materials shall not be located in a manner which prohibits surveillance of public and semi-public areas (shared or common areas).

c. Irrigation facilities shall be located so that landscaped areas can be properly maintained and so that the facilities do not interfere with vehicular or pedestrian circulation.

d. For commercial, office, and other non-residential sites, the developer shall select trees that possess the following characteristics:

- 1) Provide generous “spreading” canopy for shade.
- 2) Roots do not break up adjacent paving.
- 3) Tree canopy spread starts at least six feet up from grade in, or adjacent to, parking lots, roads, or sidewalks unless the tree is columnar in nature.
- 4) No sticky leaves or sap-dripping trees (no honey-dew excretion).
- 5) No seed pods or fruit-bearing trees (flowering trees are acceptable).
- 6) Disease-resistant.
- 7) Compatible with planter size.
- 8) Drought-tolerant unless irrigation is provided.
- 9) Attractive foliage or form all seasons.

e. Plant materials (shrubs, ground cover, etc.) shall be selected for their appropriateness to the site, drought tolerance, year-round greenery and coverage, staggered flowering periods, and avoidance of nuisance plants (Scotch broom, etc.).

.....

D. Landscaping requirements in water resource areas (WRAs). Pursuant to CDC [32.110\(E\)\(3\)](#) the requirements of this chapter relating to total site landscaping, landscaping buffers, landscaping around parking lots, and landscaping the parking lot interior may be waived or reduced in a WRA application without a variance being required. (Ord. [1408](#), 1998; Ord. [1463](#), 2000; Ord. [1623](#) § 5, 2014; Ord. [1636](#) § 36, 2014; Ord. [1745](#) § 1 (Exh. A), 2023; Ord. [1754](#) § 1 (Exh. A), 2024)

Staff Finding 31: An Arborist Report was prepared by Todd Prager and Associates for the proposed project. The arborist report inventoried 212 existing trees onsite and proposes to retain 58 of the 212 trees (58/212 = 27 %). The landscape plan indicates 64,726 square feet of landscaping on the site consisting of native plants, shrubs, grass, and trees which is greater than 20% of the site. Parking lot landscaping was designed to protect existing trees and landscaping adjacent to the site in order to provide shade for the parking areas. As previously discussed in Findings 23 and 24, landscaping within the parking areas is not feasible due to interference with proper maneuvering of vehicles onsite. The applicant proposes planting of an additional 6 trees in close proximity of the upper lot to provide shade canopy around the parking area in lieu of providing landscaping strips within the parking areas. Loading, refuse and delivery areas will be shielded from the public street view by large

trees and the down sloping grade conditions of the site from the Salamo Road access point. Proposed Landscaping in the Water Resource Areas is discussed in Findings 4 – 17. The criteria are met.

Chapter 55 DESIGN REVIEW

55.020 CLASSES OF DESIGN REVIEW

A. *Class I Design Review. The following are subject to Class I Design Review:*

.....

B. *Class II Design Review. Class II design review applies to all uses/activities except those uses/activities listed under Class I design review, and the exemptions of CDC 55.025. Class II design review applies to the proposed improvements listed in this section when the proposed improvement (e.g., new sidewalk) is part of a major commercial, office, industrial, public, or multifamily construction project (e.g., a new shopping center). (Ord. 1547, 2007; Ord. 1604 § 50, 2011; Ord. 1622 § 20, 2014; Ord. 1745 § 1 (Exh. A), 2023)*

Staff Finding 32: The proposed project does not meet the requirements of a Class I Design Review of CDC 55.020 or exemptions listed in CDC 55.025. A Class II Design Review is required. Findings for the Class II Design Review can be found later in this report under Findings 33 – 59. The criteria are met.

55.030 ADMINISTRATION AND APPROVAL PROCESS

A. *A pre-application conference is required before submitting a development plan application for design review as provided by CDC 99.030(B).*

B. *The application shall be submitted by the record owner(s) of the property, authorized agent, or condemnor.*

C. *Action on the development plan application shall be as provided by Chapter 99 CDC, Procedures for Decision-Making: Quasi-Judicial, and the following:*

1. *The Planning Director for Class I design review applications, or Planning Commission for Class II design review applications, shall approve, approve with conditions, or deny the application based on findings related to the applicable criteria set forth in CDC 99.110 and this chapter.*

Staff Finding 33: A pre-application conference for the proposal was held on March 6, 2025. The application will be reviewed by the Planning Commission at their February 4, 2026, public hearing. The criteria are met.

55.100 APPROVAL STANDARDS – CLASS II DESIGN REVIEW – GENERAL/DISCRETIONARY

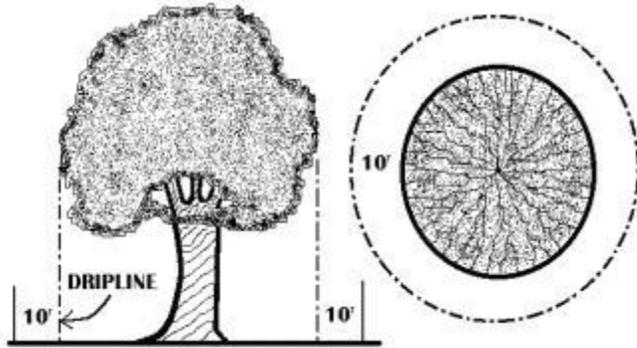
Except for applications subject to CDC 55.105, the approval authority shall make findings with respect to the following criteria when approving, approving with conditions, or denying a Class II design review application:

A. *The provisions of the following chapters shall be met:*

1. Chapter 34 CDC, Accessory Structures, Accessory Dwelling Units, and Accessory Uses.
 2. Chapter 38 CDC, Additional Yard Area Required; Exceptions to Yard Requirements; Storage in Yards; Projections into Yards.
 3. Chapter 41 CDC, Building Height, Structures on Steep Lots, Exceptions.
 4. Chapter 42 CDC, Clear Vision Areas.
 5. Chapter 44 CDC, Fences.
 6. Chapter 46 CDC, Off-Street Parking, Loading and Reservoir Areas.
 7. Chapter 48 CDC, Access, Egress and Circulation.
 8. Chapter 52 CDC, Signs.
 9. Chapter 54 CDC, Landscaping.
- B. Relationship to the natural and physical environment.*
1. *The buildings and other site elements shall be designed and located so that all heritage trees, as defined in the municipal code, shall be saved. Diseased heritage trees, as determined by the City Arborist, may be removed at their direction.*

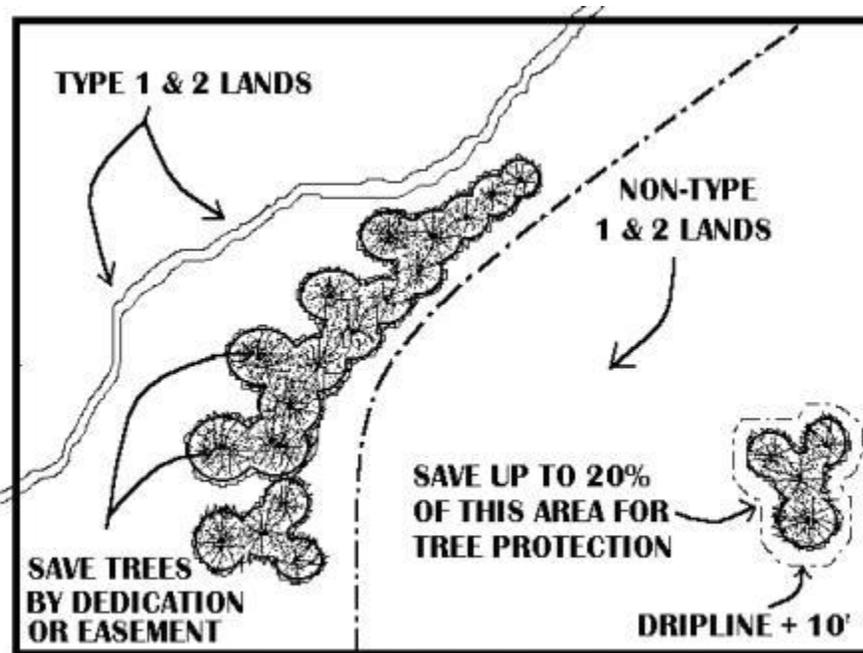
Staff Finding 34: The Arborist Report prepared for the proposed project prepared by Todd Prager and Associates (Exhibit PC-C Tree Preservation Plan and Arborist Report) ~~1—PC-1—Pg.121~~ did not identify any heritage trees onsite. The criteria is not applicable.

2. *All heritage trees, as defined in the municipal code, all trees and clusters of trees (“cluster” is defined as three or more trees with overlapping driplines; however, native oaks need not have an overlapping dripline) that are considered significant by the City Arborist, either individually or in consultation with certified arborists or similarly qualified professionals, based on accepted arboricultural standards including consideration of their size, type, location, health, long term survivability, and/or numbers, shall be protected pursuant to the criteria of subsections (B)(2)(a) through (f) of this section. In cases where there is a difference of opinion on the significance of a tree or tree cluster, the City Arborist’s findings shall prevail. It is important to acknowledge that all trees are not significant and, further, that this code section will not necessarily protect all trees deemed significant.*
 - a. *Non-residential and residential projects on Type I and II lands shall protect all heritage trees and all significant trees and tree clusters by limiting development in the protected area. The protected area includes the protected tree, its dripline, and an additional 10 feet beyond the dripline, as depicted in the figure below. Development of Type I and II lands shall require the careful layout of streets, driveways, building pads, lots, and utilities to avoid heritage trees and significant trees and tree clusters, and other natural resources pursuant to this code. The method for delineating the protected trees or tree clusters (“dripline plus 10 feet”) is explained in subsection (B)(2)(b) of this section. Exemptions of subsections (B)(2)(c), (e), and (f) of this section shall apply.*



PROTECTED AREA = DRIPLINE + 10 FEET

b. Non-residential and residential projects on non-Type I and II lands shall set aside up to 20 percent of the protected areas for significant trees and tree clusters, plus any heritage trees. Therefore, in the event that the City Arborist determines that a significant tree cluster exists at a development site, then up to 20 percent of the non-Type I and II lands shall be devoted to the protection of those trees by limiting development in the protected areas. The exact percentage is determined by establishing the driplines of the trees or tree clusters that are to be protected. In order to protect the roots which typically extend further, an additional 10-foot measurement beyond the dripline shall be added. The square footage of the area inside this “dripline plus 10 feet” measurement shall be the basis for calculating the percentage (see figure below). The City Arborist will identify which tree(s) are to be protected. Development of non-Type I and II lands shall also require the careful layout of streets, driveways, building pads, lots, and utilities to avoid significant trees, tree clusters, heritage trees, and other natural resources pursuant to this code. Exemptions of subsections (B)(2)(c), (e), and (f) of this section shall apply. Please note that in the event that more than 20 percent of the non-Type I and II lands comprise significant trees or tree clusters, the developer shall not be required to save the excess trees, but is encouraged to do so.



METHOD OF PERCENTAGE CALCULATION

E.G., DRIPLINE + 10 FT. AREA = 2,500 SQ. FT. OR 18% OF TOTAL NON-TYPE I AND II LAND DENSITY CALCULATIONS FOR THIS PARCEL WILL BE BASED ON REMAINING NET SQ. FOOTAGE OF SITE (EXCLUDING THE 2,500 SQ. FT.)

- c. Where stubouts of streets occur on abutting properties, and the extension of those streets will mean the loss of significant trees, tree clusters, or heritage trees, it is understood that tree loss may be inevitable. In these cases, the objective shall be to minimize tree loss. These provisions shall also apply in those cases where access, per construction code standards, to a lot or parcel is blocked by a row or screen of significant trees or tree clusters.
- d. For both non-residential and residential development, the layout shall achieve at least 70 percent of maximum density for the developable net area. The developable net area excludes all Type I and II lands and up to 20 percent of the remainder of the site for the purpose of protection of stands or clusters of trees as defined in subsection (B)(2) of this section.
- e. For arterial and collector street projects, including Oregon Department of Transportation street improvements, the roads and graded areas shall avoid tree clusters where possible. Significant trees, tree clusters, and heritage tree loss may occur, however, but shall be minimized.
- f. If the protection of significant tree(s) or tree clusters is to occur in an area of grading that is necessary for the development of street grades, per City construction codes, which will result in an adjustment in the grade of over or under two feet, which will then threaten the health of the tree(s), the applicant will submit evidence to the Planning Director that all reasonable alternative grading plans have been considered and cannot work. The applicant will then submit a mitigation plan to the City Arborist to compensate for the removal of the tree(s) on an "inch by

inch” basis (e.g., a 48-inch Douglas fir could be replaced by 12 trees, each four-inch). The mix of tree sizes and types shall be approved by the City Arborist.

Staff Finding 35: The project will retain 23% of existing significant trees, thereby exceeding the 20% minimum threshold. The Arborist Report provided as part of the applicant’s submittal details the trees to remain and the trees to be removed. Project grading does not affect the grading of adjacent streets. The criteria are met.

3. The topography and natural drainage shall be preserved to the greatest degree possible.

Staff Finding 36: A detailed stormwater and grading plan is provided as part of the applicants submittal. The project proposes significant grading to allow for the construction of a new access road and building/vehicular pads necessary for the functioning of the proposed facility, with every effort made to maintain natural drainage and topography. The criterion is met

4. The structures shall not be located in areas subject to slumping and sliding. The Comprehensive Plan Background Report’s Hazard Map, or updated material as available and as deemed acceptable by the Planning Director, shall be the basis for preliminary determination.

Staff Finding 37: Per the Geotechnical Engineering Services Report prepared by Columbia West Engineering (Exhibit PC-J), “the site background (including stabilization of the landslide triggered during the original construction of Interstate 2025 (1969) proposed development plans including proposed cut and fill grading plans, proposed fill slopes with gradients of approximately 2horizontal: 1vertical or flatter, proposed buildings and walls layout and estimated foundation loads, the composition of soil and rock and groundwater conditions encountered in our subsurface explorations, laboratory testing, and engineering and slope stability analyses, it is Columbia West’s opinion that the proposed development can be safely constructed at the site, provided the geotechnical recommendations presented in Columbia West’s geotechnical report are incorporated into the design and construction of the project. The criterion is met.

5. There shall be adequate distance between on-site buildings and on-site and off-site buildings on adjoining properties to provide for adequate light and air circulation and for fire protection.

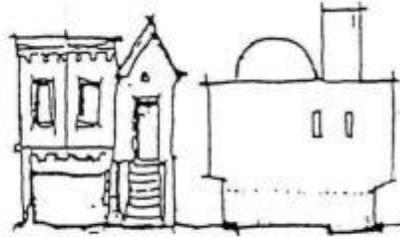
Staff Finding 37: The nearest buildings to the north are greater than 100 feet from the nearest building onsite. Proposed building separations are substantial – greater than 15 feet – and will provide adequate light and air circulation onsite and to surrounding properties. The criterion is met.

6. Architecture.

a. The proposed structure(s) scale shall be compatible with the existing structure(s) on site and on adjoining sites. Contextual design is required. Contextual design means respecting and incorporating prominent architectural styles, building lines, roof forms, rhythm of windows, building scale and massing of surrounding buildings in the proposed structure. The materials and colors shall be complementary to the surrounding buildings.



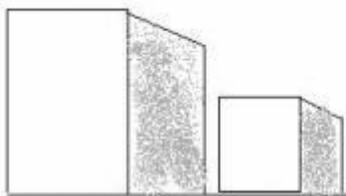
contextual design



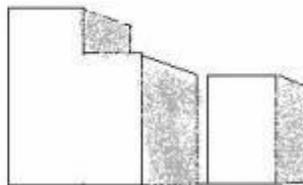
contrasting design

Staff Finding 38: The applicants submittal provides elevations and renderings of the project. The site is steeply, heavily wooded, and very little (if any) adjacency with the nearest buildings to the north. The design utilizes building materials consisting of dark gray metal panels, wood, concrete with low gabled roof lines to relate contextually to the residential scale and formal qualities of buildings nearby.

b. While there has been discussion in Chapter [24](#) CDC about transition, it is appropriate that new buildings should architecturally transition in terms of bulk and mass to work with, or fit, adjacent existing buildings. This transition can be accomplished by selecting designs that “step down” or “step up” from small to big structures and vice versa (see figure below). Transitions may also take the form of carrying building patterns and lines (e.g., parapets, windows, etc.) from the existing building to the new one.



mass/bulk overwhelms smaller building



mass/bulk effectively transitions

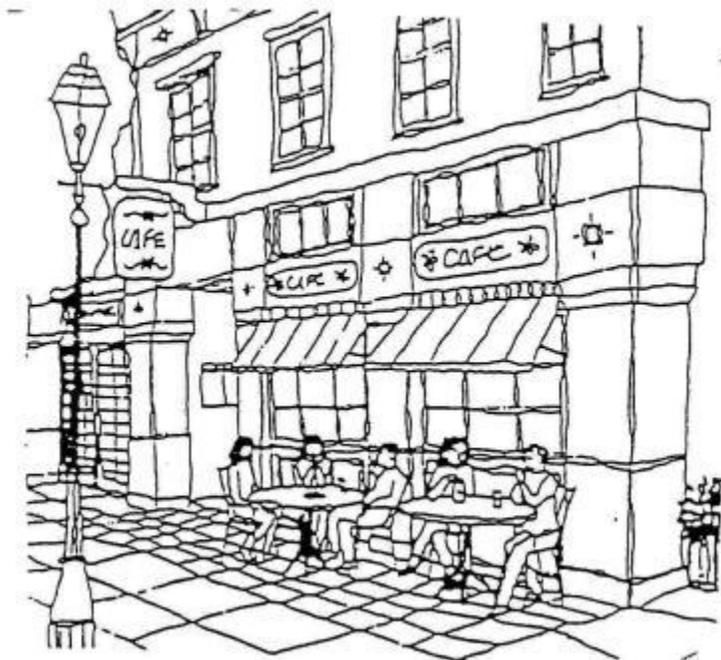


Staff Finding 39: The project site is heavily wooded, steeply graded, and has little (if any) adjacency to the nearest buildings to the north. The design of the buildings is sensitive to the neighboring residential development to the north with a one and two-story buildings that are within the height limit (50-ft tall) and placed a substantial distance (greater than 100 feet) from the developments to the north. The criterion is met.

c. Contrasting architecture shall only be permitted when the design is manifestly superior to adjacent architecture in terms of creativity, design, and workmanship, and/or it is adequately separated from other buildings by distance, screening, grade variations, or is part of a development site that is large enough to set its own style of architecture.

Staff Finding 40: The proposed buildings are separated from the nearest roadways and buildings to the north by greater than 100 feet and steep slopes that are almost 100 feet lower elevation than the buildings to the north. The design of the two main buildings Building A and Building B, incorporated design elements similar to the buildings to the north with gable roofs, clerestory windows, wood columns and beams, and standing seam metal roofs. The design is proposed to blend into the natural hillside setting while also relating to nearby architecture to the north. The criterion is met.

d. Human scale is a term that seeks to accommodate the users of the building and the notion that buildings should be designed around the human scale (i.e., their size and the average range of their perception). Human scale shall be accommodated in all designs by, for example, multi-light windows that are broken up into numerous panes, intimately scaled entryways, and visual breaks (exaggerated eaves, indentations, ledges, parapets, awnings, engaged columns, etc.) in the facades of buildings, both vertically and horizontally. The human scale is enhanced by bringing the building and its main entrance up to the edge of the sidewalk. It creates a more dramatic and interesting streetscape and improves the “height and width” ratio referenced in this section.



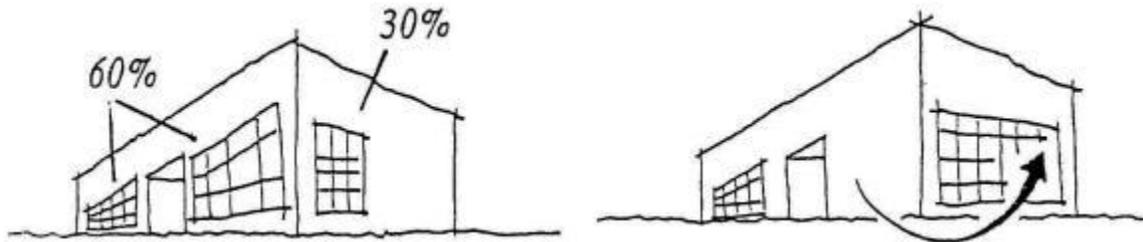
human scale is captured in this example

Staff Finding 41: The project site is steeply graded on the western edge and prevents buildings from being located on or near Salamo Road. The proposed access road will proceed from Salamo Road down the hillside to the new buildings which will be roughly 60 feet lower than the Salamo Road access point and also more than 200 feet from the road, thereby there is no sidewalk presence.

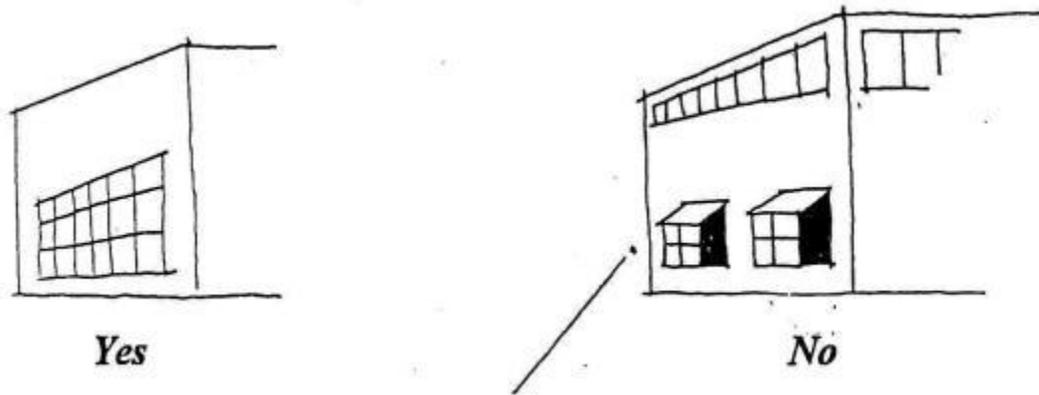
The office building is designed with human scale in mind. The pedestrian bridge provides users of the site with access to the main building (Building B) from the upper parking lot via a 10-foot deep covered deck area. The gabled roof has a clerestory portion which breaks down the scale and mass of the structure to allow natural daylight into the spaces within. Windows are scaled and composed to relate to human scale, with a non-repeating pattern. The criteria are met.

e. The main front elevation of commercial and office buildings shall provide at least 60 percent windows or transparency at the pedestrian level to create more interesting streetscape and window shopping opportunities. One side elevation shall provide at least 30 percent transparency. Any additional side or rear elevation, which is visible from a collector road or greater classification, shall also have at least 30 percent transparency. Transparency on other elevations is optional. The transparency is measured in lineal fashion. For example, a 100-foot-long building elevation shall have at least 60 feet (60 percent of 100 feet) in length of windows. The window height shall be, at minimum, three feet tall. The exception to transparency would be cases where demonstrated functional constraints or topography restrict that elevation from

being used. When this exemption is applied to the main front elevation, the square footage of transparency that would ordinarily be required by the above formula shall be installed on the remaining elevations at pedestrian level in addition to any transparency required by a side elevation, and vice versa. The rear of the building is not required to include transparency. The transparency must be flush with the building elevation.



60 percent of lineal street facing or main elevation is windows. 30 percent of one side elevation is windows. You may transfer windows from the side to front, or vice versa.



(Windows not at eye level and/or not flush with building.)

f. Variations in depth and roof line are encouraged for all elevations.

To vary the otherwise blank wall of most rear elevations, continuous flat elevations of over 100 feet in length should be avoided by indents or variations in the wall. The use of decorative brick, masonry, or stone insets and/or designs is encouraged. Another way to vary or soften this elevation is through terrain variations such as an undulating grass area with trees to provide vertical relief.

Staff Finding 42: The project is setback form the street by more than 200 feet and a drop in elevation of roughly 60 feet. None of the buildings front onto a public street and there is no “streetscape” or “window shopping opportunity” given the proposed use as a Public Support Facility. The buildings contain a large amount of glazing for the building users with a transparency percentage of 40% on north elevation of Building B (office level) and 20% for the west elevation of Building B (office level). The main office structure (Building B) is designed with a clerestory roof that allows daylight into the center of the building that interrupts the gable roof form. The materiality of the building employs a standing seam

metal roof and metal wall panels with reveal lines, which further provide texture and relief to the exterior surfaces. The criteria are met.

g. Consideration of the micro-climate (e.g., sensitivity to wind, sun angles, shade, etc.) shall be made for building users, pedestrians, and transit users, including features like awnings.

Staff Finding 43: The bridge and entry covered deck allow users to have shelter from weather elements before entering the building. The large roll-up garage doors on the lower fleet maintenance level also allow users to open them and experience breezes and daylight during their daily work while being shaded from the elements.

h. The vision statement identified a strong commitment to developing safe and attractive pedestrian environments with broad sidewalks, canopied with trees and awnings.



trees, awnings, and building orientation enhance micro-climate

Staff Finding 44: Due to the site steep grade, the buildings are located deeply internal to the site and there is no building frontage on a public right-of-way. The buildings are over 200 feet from Salamo Road. Internal sidewalks connecting the buildings are provided for site users once the user has parked onsite.

The existing access point at Salamo Road will maintain most of the existing trees along the road, shading the existing sidewalk and limiting view into the maintenance yard below. A 30-ft x 30-ft vision clear vision triangle is provided at the Salamo Road access point to ensure proper sight distance for vehicles entering and existing the site. The criteria are met

i. Sidewalk cafes, kiosks, vendors, and street furniture are encouraged. However, at least a four-foot-wide pedestrian accessway must be maintained per Chapter [53](#) CDC, Sidewalk Use.

Staff Finding 45: The proposed buildings are located greater than 200 feet from Salamo Road due to the steep grade of the project site, thereby limiting a ‘pedestrian experience’ at the site. The proposed use of a Public Support Facility is not intended for a commercial pedestrian experience at the site, thereby no sidewalk cafes, kiosks, or street furniture is proposed. The criteria are met.

7. Transportation. *The automobile shall be shifted from a dominant role, relative to other modes of transportation, by the following means:*

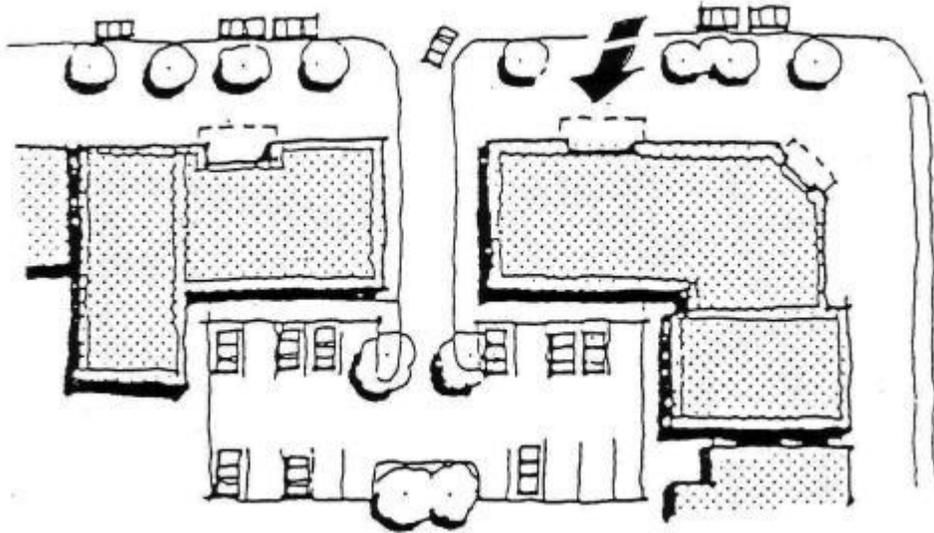
a. *Commercial and office development shall be oriented to the street. At least one public entrance shall be located facing an arterial street; or, if the project does not front on an arterial, facing a collector street; or, if the project does not front on a collector, facing the local street with highest traffic levels. Parking lots shall be placed behind or to the side of commercial and office development. When a large and/or multi-building development is occurring on a large undeveloped tract (three plus acres), it is acceptable to focus internally; however, at least 20 percent of the main adjacent right-of-way shall have buildings contiguous to it unless waived per subsection (B)(7)(c) of this section. These buildings shall be oriented to the adjacent street and include pedestrian-oriented transparencies on those elevations.*

For individual buildings on smaller individual lots, at least 30 lineal feet or 50 percent of the building must be adjacent to the right-of-way unless waived per subsection (B)(7)(c) of this section. The elevations oriented to the right-of-way must incorporate pedestrian-oriented transparency.

.....

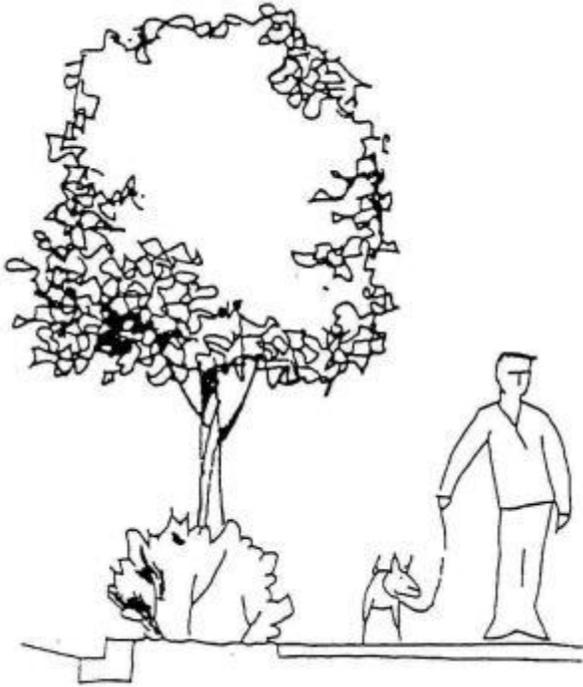
c. *Commercial, office, and multifamily projects shall be built as close to the adjacent main right-of-way as practical to facilitate safe pedestrian and transit access. Reduced frontages by buildings on public rights-of-way may be allowed due to extreme topographic (e.g., slope, creek, wetlands, etc.) conditions or compelling functional limitations, not just inconveniences or design challenges.*

entrance from right-of-way



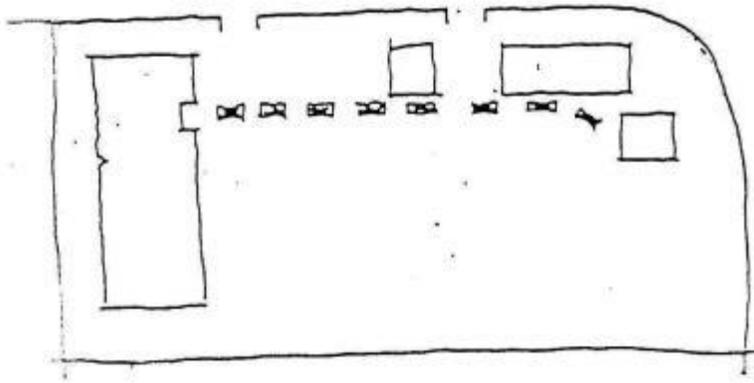
Staff Finding 46: Due to the steep grade at the project site the proposed buildings and parking areas are positioned greater than 200 feet from Salamo Road and do not have street frontage, per subsection (B)(7)(c).

d. Accessways, parking lots, and internal driveways shall accommodate pedestrian circulation and access by specially textured, colored, or clearly defined footpaths at least six feet wide. Paths shall be eight feet wide when abutting parking areas or travel lanes. Paths shall be separated from parking or travel lanes by either landscaping, planters, curbs, bollards, or raised surfaces. Sidewalks in front of storefronts on the arterials and main store entrances on the arterials identified in CDC [85.200\(A\)\(3\)](#) shall be 12 feet wide to accommodate pedestrians, sidewalk sales, sidewalk cafes, etc. Sidewalks in front of storefronts and main store entrances in commercial/OBC zone development on local streets and collectors shall be eight feet wide.



landscaping

e. Paths shall provide direct routes that pedestrians will use between buildings, adjacent rights-of-way, and adjacent commercial developments. They shall be clearly identified. They shall be laid out to attract use and to discourage people from cutting through parking lots and impacting environmentally sensitive areas.



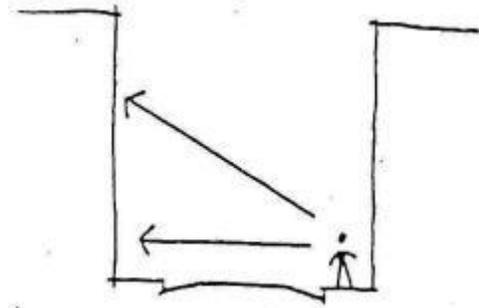
direct pedestrian route required (- -)

Staff Finding 47: Sidewalks are provided between the upper parking lot and Building B to accommodate pedestrian movement. The criteria is met.

g. Where transit service exists, or is expected to exist, there shall be a main entrance within a safe and reasonable distance of the transit stop. A pathway shall be provided to facilitate a direct connection.

Staff Finding 48: An existing transit stop is located on the eastside of Salamo Road roughly 400 feet from the Salamo Road access point. The criteria is met.

h. Projects shall bring at least part of the project adjacent to or near the main street right-of-way in order to enhance the height-to-width ratio along that particular street. (The “height-to-width ratio” is an architectural term that emphasizes height or vertical dimension of buildings adjacent to streets. The higher and closer the building is, and the narrower the width of the street, the more attractive and intimate the streetscape becomes.) For every one foot in street width, the adjacent building ideally should be one to two feet higher. This ratio is considered ideal in framing and defining the streetscape.



*1:1 height to width ratio is ideal
(example only)*

i. These architectural standards shall apply to public facilities such as reservoirs, water towers, treatment plants, fire stations, pump stations, power transmission facilities, etc. It is recognized that many of these facilities, due to their functional requirements, cannot readily be configured to meet these architectural standards. However, attempts shall be made to make the design sympathetic to surrounding properties by landscaping, setbacks, buffers, and all reasonable architectural means.

Staff Finding 49: Due to the steeply graded site and topographical constraints like a ravine and wetland the proposed buildings will be located at an elevation approximately 60 feet below Salamo Road and over 200 feet from Salamo Road. The height and building related to the width of Salamo Road will not have any bearing on the streetscape experience. The criteria are met. Design considerations sympathetic to the surrounding properties were incorporated including gabled roofs, glazing, metal panels, and wood columns. The criteria are met.

C. Compatibility between adjoining uses, buffering, and screening.

1. In addition to the compatibility requirements contained in Chapter 24 CDC, buffering shall be provided between different types of land uses; for example, buffering between single-family homes and apartment blocks. However, no buffering is required between single-family homes and duplexes or single-family attached units. The following factors shall be considered in determining the adequacy of the type and extent of the buffer:

- a. *The purpose of the buffer, for example to decrease noise levels, absorb air pollution, filter dust, or to provide a visual barrier.*
 - b. *The size of the buffer required to achieve the purpose in terms of width and height.*
 - c. *The direction(s) from which buffering is needed.*
 - d. *The required density of the buffering.*
 - e. *Whether the viewer is stationary or mobile.*
2. *On-site screening from view from adjoining properties of such things as service areas, storage areas, and parking lots shall be provided and the following factors will be considered in determining the adequacy of the type and extent of the screening:*
- a. *What needs to be screened?*
 - b. *The direction from which it is needed.*
 - c. *How dense the screen needs to be.*
 - d. *Whether the viewer is stationary or mobile.*
 - e. *Whether the screening needs to be year-round.*
3. *Rooftop air cooling and heating systems and other mechanical equipment shall be screened from view from adjoining properties.*

Staff Finding 50: There are multiple preexisting natural buffers between the project building site and the buildings to the north and roadway to the west.

There is a roughly 60-foot drop in grade between the closest road (Salamo Road) and the building pad, which is greater than the 50-foot building height. Additionally, the proposed buildings are greater than 200 feet from Salamo Road and over 350 feet from the buildings to the north. Large, mature trees between the proposed buildings and Salamo Road and the buildings to the north provide additional buffering and screening. The criteria are met.

D. Privacy and noise.

[...]

3. *Structures or on-site activity areas which generate noise, lights, or glare shall be buffered from adjoining residential uses in accordance with the standards in subsection C of this section where applicable.*

4. *Businesses or activities that can reasonably be expected to generate noise in excess of the noise standards contained in West Linn Municipal Code Section 5.487 shall undertake and submit appropriate noise studies and mitigate as necessary to comply with the code. (See CDC 55.110(B)(11) and 55.120(M).)*

If the decision-making authority reasonably believes a proposed use may generate noise exceeding the standards specified in the municipal code, then the authority may require the applicant to supply professional noise studies from time to time during the user's first year of operation to monitor compliance with City standards and permit requirements.

Staff Finding 51: The proposed structures and on-site activity areas which may generate noise, lights, or glare will be buffered from adjoining residential uses in accordance with the

standards in subsection C, which are addressed above at Finding 50. There is no evidence in the record to conclude that the proposed operations facility would generate noise in excess of the standards contained in West Linn Municipal Code Section 5.487. Section 5.487 prohibits the creation of “unreasonable” noise, and provides a “reasonable person” standard, subject to several additional qualifications, for typical activities, but exempts noise from regular vehicular traffic upon premises open to the public such as the subject facility, and applies to all land uses in West Linn on an ongoing basis. Therefore, staff recommends Condition of Approval Number 4, which would require compliance with Section 5.487. With recommended Condition of Approval Number 4, the criteria are met.

G. Demarcation of public, semi-public, and private spaces. The structures and site improvements shall be designed so that public areas such as streets or public gathering places, semi-public areas, and private outdoor areas are clearly defined in order to establish persons having a right to be in the space, to provide for crime prevention, and to establish maintenance responsibility. These areas may be defined by:

- 1. A deck, patio, fence, low wall, hedge, or draping vine;*
- 2. A trellis or arbor;*
- 3. A change in level;*
- 4. A change in the texture of the path material;*
- 5. Sign; or*
- 6. Landscaping.*

Use of gates to demarcate the boundary between a public street and a private access driveway is prohibited.

Staff Finding 52: Signage will be used to mark the separation between the public street and private zone of the project. Compliance with size/dimensions of proposed signage is subject to compliance with West Linn CDC Chapter 52 and subject to review at the time of building-related permit submittal and review. The criteria are met.

H. Public transit.

1. Provisions for public transit may be required where the site abuts an existing or planned public transit route. The required facilities shall be based on the following:

- a. The location of other transit facilities in the area.*
- b. The size and type of the proposed development.*
- c. The rough proportionality between the impacts from the development and the required facility.*

2. The required facilities shall be limited to such facilities as the following:

- a. A waiting shelter with a bench surrounded by a three-sided covered structure, with transparency to allow easy surveillance of approaching buses.*
- b. A turnout area for loading and unloading designed per regional transit agency standards.*
- c. Hard-surface paths connecting the development to the waiting and boarding areas.*

- d. *Regional transit agency standards shall, however, prevail if they supersede these standards.*
3. *The transit stop shall be located as close as possible to the main entrance to the shopping center, public or office building, or multifamily project. The entrance shall not be more than 200 feet from the transit stop with a clearly identified pedestrian link.*
4. *All commercial business centers (over three acres) and multifamily projects (over 40 units) may be required to provide for the relocation of transit stops to the front of the site if the existing stop is within 200 to 400 yards of the site and the exaction is roughly proportional to the impact of the development. The commercial or multifamily project may be required to provide new facilities in those cases where the nearest stop is over 400 yards away. The transit stop shall be built per subsection (H)(2) of this section.*

Staff Finding 53: The proposed project is not a commercial business or multifamily project. An existing transit stop located on the eastside of Salamo Road approximately 400 feet from the sites entrance on Salamo Road will not be affected. The criteria are met.

1. *Public facilities. An application may only be approved if adequate public facilities, as defined in CDC [2.030](#), will be available to provide service to the property prior to occupancy.*

.....

3. *Storm detention and treatment and geologic hazards. Per the submittals required by CDC [55.130](#) and [92.010\(E\)](#), all proposed storm detention and treatment facilities must comply with the standards for the improvement of public and private drainage systems located in the West Linn Public Works Design Standards, there will be no adverse off-site impacts caused by the development (including impacts from increased intensity of runoff downstream or constrictions causing ponding upstream), and the applicant must provide sufficient factual data to support the conclusions of the submitted plan.*

Staff Finding 54: Stormwater for the proposed development will manage all new impervious areas including a new underground filter vault and detention system at the south end of the site. A stormwater management report was included as part of the Applicants submittal and provides detailed exhibits and analysis. The criteria are met.

4. *Municipal water. A registered civil engineer shall prepare a plan for the provision of water which demonstrates to the City Engineer's satisfaction the availability of sufficient volume, capacity, and pressure to serve the proposed development's domestic, commercial, and industrial fire flows. All plans will then be reviewed by the City Engineer.*

5. *Sanitary sewers. A registered civil engineer shall prepare a sewerage collection system plan which demonstrates sufficient on-site capacity to serve the proposed development. The City Engineer shall determine whether the existing City system has sufficient capacity to serve the development.*

6. *Solid waste and recycling storage areas. Appropriately sized and located solid waste and recycling storage areas shall be provided. Metro standards shall be used.*

Staff Finding 55: Proposed water and fire service will serve the site for the existing water main in Salamo Road. Sanitary sewer for the proposed development will be collected onsite and pumped through a privately owned and maintained sewer lift station into the existing public sewer main in Salamo Road. Solid waste and recycling storage areas are located on the upper lot and lower lot. The criteria are met.

J. Crime prevention and safety/defensible space.

- 1. Windows shall be located so that areas vulnerable to crime can be surveyed by the occupants.*
- 2. Interior laundry and service areas shall be located in a way that they can be observed by others.*
- 3. Mailboxes, recycling, and solid waste facilities shall be located in lighted areas having vehicular or pedestrian traffic.*
- 4. The exterior lighting levels shall be selected and the angles shall be oriented towards areas vulnerable to crime.*
- 5. Light fixtures shall be provided in areas having heavy pedestrian or vehicular traffic and in potentially dangerous areas such as parking lots, stairs, ramps, and abrupt grade changes.*
- 6. Fixtures shall be placed at a height so that light patterns overlap at a height of seven feet which is sufficient to illuminate a person. All commercial, industrial, residential, and public facility projects undergoing design review shall use low or high pressure sodium bulbs and be able to demonstrate effective shielding so that the light is directed downwards rather than omni-directional. Omni-directional lights of an ornamental nature may be used in general commercial districts only.*
- 7. Lines of sight shall be reasonably established so that the development site is visible to police and residents.*
- 8. Security fences for utilities (e.g., power transformers, pump stations, pipeline control equipment, etc.) or wireless communication facilities may be up to eight feet tall in order to protect public safety. No variances are required regardless of location.*

Staff Finding 56: Windows are located on all sides of the upper floor of Building B. An interior laundry room is located within a locked building and not accessible to the general public.

Mailboxes are within the interior of Building B.

Trash and recycling facilities are on the upper lot and lower lot and with ample site/safety lighting. Exterior lighting is proposed around the project site – along roadways, pedestrian walkways, storage areas – to provide protection from areas vulnerable to crime. Exterior lighting will be directed so that they overlap 7 feet and fixtures will have full cut-off so that all light is directed downward and does not create light pollution to outward/upward.

The project will be not be directly visible from Salamo Road but will be readily visible form the street by way of building signage. A six-foot tall chain link fence surrounding the development is proposed in order to protect public safety. The criteria are met.

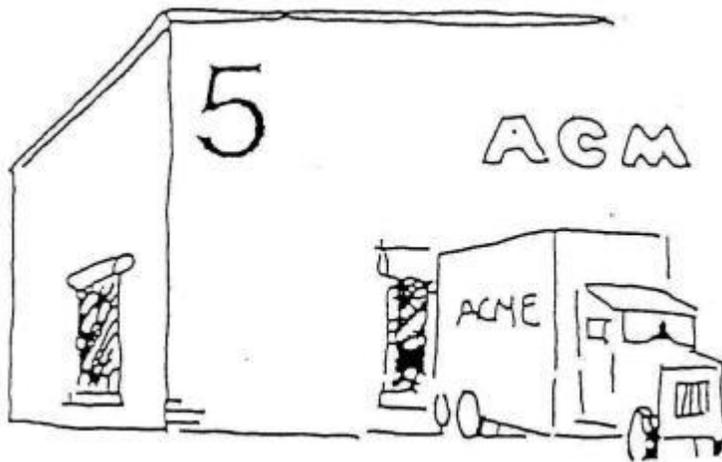
K. Provisions for persons with disabilities.

1. The needs of a person with a disability shall be provided for. Accessible routes shall be provided between all buildings and accessible site facilities. The accessible route shall be the most practical direct route between accessible building entries, accessible site facilities, and the accessible entry to the site. An accessible route shall connect to the public right-of-way and to at least one on-site or adjacent transit stop (if the area is served by transit). All facilities shall conform to, or exceed, the Americans with Disabilities Act (ADA) standards, including those included in the Uniform Building Code.

Staff Finding 57: An ADA accessible route from the upper lot parking area and down to the lower buildings and paved area is provided. The criteria are met.

L. Signs.

1. Based on considerations of crime prevention and the needs of emergency vehicles, a system of signs for identifying the location of each residential unit, store, or industry shall be established.



buildings shall be numbered for emergency identification

2. The signs, graphics, and letter styles shall be designed to be compatible with surrounding development, to contribute to a sense of project identity, or, when appropriate, to reflect a sense of the history of the area and the architectural style.

3. The sign graphics and letter styles shall announce, inform, and designate particular areas or uses as simply and clearly as possible.

4. The signs shall not obscure vehicle driver's sight distance.

5. Signs indicating future use shall be installed on land dedicated for public facilities (e.g., parks, water reservoir, fire halls, etc.).

6. Signs and appropriate traffic control devices and markings shall be installed or painted in the driveway and parking lot areas to identify bicycle and pedestrian routes.

Staff Finding 58: Signage will be used to identify the project from Salamo Road and all parking/loading areas. The criteria are met.

O. Refuse and recycling standards.

1. All commercial, industrial and multifamily developments over five units requiring Class II design review shall comply with the standards set forth in these provisions. Modifications to these provisions may be permitted if the Planning Commission determines that the changes are consistent with the purpose of these provisions and the City receives written evidence from the local franchised solid waste and recycling firm that they are in agreement with the proposed modifications.

2. Compactors, containers, and drop boxes shall be located on a level Portland cement concrete pad, a minimum of four inches thick, at ground elevation or other location compatible with the local franchise collection firm's equipment at the time of construction. The pad shall be designed to discharge surface water runoff to avoid ponding.

3. Recycling and solid waste service areas.

a. Recycling receptacles shall be designed and located to serve the collection requirements for the specific type of material.

b. The recycling area shall be located in close proximity to the garbage container areas and be accessible to the local franchised collection firm's equipment.

c. Recycling receptacles or shelters located outside a structure shall have lids and be covered by a roof constructed of water and insect-resistive material. The maintenance of enclosures, receptacles and shelters is the responsibility of the property owner.

d. The location of the recycling area and method of storage shall be approved by the local fire marshal.

e. Recycling and solid waste service areas shall be at ground level and/or otherwise accessible to the franchised solid waste and recycling collection firm.

f. Recycling and solid waste service areas shall be used only for purposes of storing solid waste and recyclable materials and shall not be a general storage area to store personal belongings of tenants, lessees, property management or owners of the development or premises.

g. Recyclable material service areas shall be maintained in a clean and safe condition.

4. Special wastes or recyclable materials.

a. Environmentally hazardous wastes defined in ORS 466.005 shall be located, prepared, stored, maintained, collected, transported, and disposed in a manner acceptable to the Oregon Department of Environmental Quality.

b. Containers used to store cooking oils, grease or animal renderings for recycling or disposal shall not be located in the principal recyclable materials or solid waste storage areas. These materials shall be stored in a separate storage area designed for such purpose.

5. Screening and buffering.

a. Enclosures shall include a curbed landscape area at least three feet in width on the sides and rear. Landscaping shall include, at a minimum, a continuous hedge maintained at a height of 36 inches.

b. Placement of enclosures adjacent to residentially zoned property and along street frontages is strongly discouraged. They shall be located so as to conceal them from public view to the maximum extent possible.

c. All dumpsters and other trash containers shall be completely screened on all four sides with an enclosure that is comprised of a durable material such as masonry with a finish that is architecturally compatible with the project. Chain link fencing, with or without slats, will not be allowed.

6. Litter receptacles.

a. Location. Litter receptacles may not encroach upon the minimum required walkway widths.

b. Litter receptacles may not be located within public rights-of-way except as permitted through an agreement with the City in a manner acceptable to the City Attorney or their designee.

c. Number. The number and location of proposed litter receptacles shall be based on the type and size of the proposed uses. However, at a minimum, for non-residential uses, at least one external litter receptacle shall be provided for every 25 parking spaces for first 100 spaces, plus one receptacle for every additional 100 spaces. (Ord. 1547, 2007; Ord. 1604 § 52, 2011; Ord. 1613 § 12, 2013; amended during July 2014 supplement; Ord. 1623 § 6, 2014; Ord. 1635 § 26, 2014; Ord. 1636 § 37, 2014; Ord. 1647 § 6, 2016; Ord. 1650 § 1 (Exh. A), 2016; Ord. 1662 § 8, 2017; Ord. 1675 § 45, 2018; Ord. 1745 § 1 (Exh. A), 2023)

Staff Finding 59: The project will have (3) 40-yard containers for solid waste and/or recycling locate don a flat, drained, level asphalt pad. The criteria are met.

Chapter 60 *CONDITIONAL USES*

60.030 *ADMINISTRATION AND APPROVAL PROCESS*

A. Conditional use applications shall be decided by the Planning Commission in the manner set forth in CDC 99.060(B). A petition for review by the Council may be filed as provided by CDC 99.240(B).

B. All approved conditional use applications in new buildings, or buildings with a major modification, shall be subject to design review under the provisions of Chapter 55 CDC, and in the manner set forth in CDC 99.060(B).

1. Except where stated otherwise in the base zone requirements of other applicable development standards.

C. All approved conditional use applications within existing buildings shall not be subject to design review. (Ord. 1635 § 28, 2014; Ord. 1736 § 1 (Exh. A), 2022; Ord. 1745 § 1 (Exh. A), 2023)

Staff Finding 60: The subject Conditional Use Permit, Design Review, and Water Resource Area Permit application will be reviewed by the Planning Commission at a public hearing on February 4, 2026. The criteria are met.

60.070 *APPROVAL STANDARDS AND CONDITIONS*

- A. *The Planning Commission shall approve, approve with conditions, or deny an application for a conditional use, except for a manufactured home subdivision in which case the approval standards and conditions shall be those specified in CDC 36.030, or to enlarge or alter a conditional use based on findings of fact with respect to each of the following criteria:*
1. *The site size and dimensions provide:*
 - a. *Adequate area for the needs of the proposed use; and*
 - b. *Adequate area for aesthetic design treatment to mitigate any possible adverse effect from the use on surrounding properties and uses.*
 2. *The characteristics of the site are suitable for the proposed use considering size, shape, location, topography, and natural features.*
 3. *The granting of the proposal will produce a facility that provides an overall benefit to the City.*
 4. *Adequate public facilities will be available to provide service to the property at the time of occupancy. In situations where the level-of-service or volume-to-capacity performance standard for an affected City or State roadway is currently failing or projected to fail to meet the standard at a date determined within a traffic impact analysis, and an improvement project is not programmed, the development shall avoid further degradation of the affected transportation facility. Mitigation must be provided to bring the facility performance standard to existing conditions at the time of occupancy.*
 5. *The applicable requirements of the zone are met, except as modified by this chapter.*
 6. *The supplementary requirements set forth in Chapters 52 to 55 CDC and CDC 92.010(E) are met, if applicable.*
 7. *The use will comply with the applicable policies of the Comprehensive Plan.*

Staff Finding 61: The subject site is 33.02 acres and provides ample space for the proposed operations facility. The proposed buildings and operations are setback from the nearest properties to the north by more than 300 linear feet and are at a substantially lower elevation than the buildings to the north due to the steep grade – approximately 60 feet. Views to the subject buildings/property are mitigated by substantial preexisting trees around the site. The design of the buildings at the is comprised of one and two-story buildings constructed with mass timber and gray metal siding that will blend with the natural surroundings. The 33.02 acre site provides ample space for necessary vehicle access, movements, and building configurations necessary to support the proposed Public Support Facility. The benefits of the building are clear and substantial; a modern, fully functional facility supporting public works, fleet maintenance, parks and recreation, water, and environmental services. Adequate public facilities are or will be available to provide service to the site at the time of occupancy. The supplementary requirements of Chapters 52 to 55 and 92.010(E) are met and discussed in this report. The Comprehensive Plan defines the property as low-density residential and the proposed Conditional Use will comply with the policies with the policies of the General Plan.

B. An approved conditional use or enlargement or alteration of an existing conditional use shall be subject to the development review provisions set forth in Chapter [55](#) CDC.

Staff Finding 62: The subject application has applied the development review provisions of Chapter 55 in this report (Findings 32-59). The criteria are met.

C. The Planning Commission may impose conditions on its approval of a conditional use which it finds are necessary to assure the use is compatible with other uses in the vicinity. These conditions may include, but are not limited to, the following:

- 1. Limiting the hours, days, place, and manner of operation.*
- 2. Requiring design features which minimize environmental impacts such as noise, vibration, air pollution, glare, odor, and dust.*
- 3. Requiring additional setback areas, lot area, or lot depth, or width.*
- 4. Limiting the building height, size or lot coverage, or location on the site.*
- 5. Designating the size, number, location and design of vehicle access points.*
- 6. Requiring street right-of-way to be dedicated and the street to be improved including all steps necessary to address future street improvements identified in the adopted Transportation System Plan.*
- 7. Requiring participation in making the intersection improvement or improvements identified in the Transportation System Plan when a traffic analysis (compiled as an element of a conditional use application for the property) indicates the application should contribute toward.*
- 8. Requiring landscaping, screening, drainage, and surfacing of parking and loading areas.*
- 9. Limiting the number, size, location, height, and lighting of signs.*
- 10. Limiting or setting standards for the location and intensity of outdoor lighting.*
- 11. Requiring berming, screening, or landscaping and the establishment of standards for their installation and maintenance.*
- 12. Requiring and designating the size, height, location, and materials for fences.*
- 13. Requiring the protection and preservation of existing trees, soils, vegetation, watercourses, habitat areas, and drainage areas.*

Staff Finding 63: The applicant understands the authority of the Planning Commission and will work to ensure any potential conditions that may be deemed necessary or beneficial are met. The criteria are met.

D. Aggregate extraction uses shall also be subject to the provisions of ORS [541.605](#).

E. The Historic Review Board shall review an application for a conditional use, or to enlarge a conditional use on a property designated as a historic resource, based on findings of fact that the use will:

- 1. Preserve or improve a historic resource which would probably not be preserved or improved otherwise; and*

2. Utilize existing structures rather than new structures. (Ord. [1291](#), 1987; Ord. [1408](#), 1998; Ord. [1544](#), 2007; Ord. [1614](#) § 13, 2013; Ord. [1655](#) § 7, 2016; Ord. [1662](#) § 14, 2017; Ord. [1745](#) § 1 (Exh. A), 2023)

Staff Finding 64: The applicant does not propose any aggregate extraction use. There are no identified historic resources on the subject vacant site. The criteria are met.

60.100 ADDITIONAL CRITERIA FOR SCHOOLS AND OTHER GOVERNMENT FACILITIES

Schools and other government facilities that attract a regular and significant volume of users shall, to the greatest extent possible, be centrally located relative to the majority of the population that they will serve and be serviceable by sidewalks and bike routes/lanes. Police and fire stations shall meet these standards to the greatest extent possible but it is acknowledged that access to arterials remains a key locational determinant for those uses. (Ord. [1590](#) § 1, 2009; Ord. [1745](#) § 1 (Exh. A), 2023)

Staff Finding 65: The proposed Public Works Operations Facility is located on Salamo Road, a centrally located minor arterial near I-205 Exit 6, providing north to south travel through the City of West Linn. Although the proposed facility is not intended to be directly used by the public on a daily basis, the project site is accessible to the majority of the population of the City via serviceable sidewalks and bike routes/lanes along Salamo Road and surrounding streets. The criteria are met.

96.010 CONSTRUCTION REQUIRED

A. Street improvements for residential construction are required when:

1. Construction of a new single-family attached or detached structure (replacement of an existing structure is exempt); or
2. Replacement of a single-family home increases the square feet by 50 percent or greater; or
3. Construction of a new multifamily structure; or
4. Increase in dwelling unit density on site (accessory dwelling units are exempt).

B. Street improvements for commercial construction are required when:

1. Construction of a new commercial structure; or
2. Remodel of an existing commercial structure with an increase in floor area; or
3. Construction that increases the dwelling unit density on site; or
4. Construction which requires a change in type, number, or location of accessways; or
5. Replacement of an existing structure with an increase in floor area. (Ord. [1314](#), 1992; Ord. [1442](#), 1999; Ord. [1544](#), 2007; Ord. [1547](#), 2007; Ord. [1590](#) § 1, 2009; Ord. [1613](#) § 21, 2013; Ord. [1739](#) § 2 (Exh. B), 2022; Ord. [1754](#) § 1 (Exh. A), 2024)

96.020 FEE-IN-LIEU

A. An applicant may apply for a waiver of street improvements and the option to pay a fee-in-lieu (in accordance with the City's adopted fee structure) of constructing street improvements if one of the following are met:

1. Located on a cul-de-sac with no existing curb and/or no existing sidewalk; or
2. Located on a street less than 1,320 linear feet in length and not planned as a through street; or
3. Located more than 1,320 linear feet from nearest street improvements on the same street or connecting street. (Ord. 1739 § 2 (Exh. B), 2022)

Staff Finding 66: The applicant does not intend to provide improvements to Salamo Road frontage since it is not feasible due to existing steep slopes and environmental buffers along the west SE side of Salamo Road. The criteria is not applicable.

99.030 APPLICATION PROCESS: WHO MAY APPLY, PRE-APPLICATION CONFERENCE, REQUIREMENTS, REFUSAL OF APPLICATION, FEES

.....

B. Pre-application conferences.

1. Subject to subsection (B)(4) of this section, a pre-application conference is required for, but not limited to, each of the following applications:

a. Boundary changes, per Chapter 81 CDC;

.....

d. Conditional uses;

e. Design review (Class I and Class II);

.....

p. Development subject to Chapter 32 CDC, Water Resource Area Protection;

.....

Staff Finding 67: A Pre-application meeting for the project took place on March 6, 2025. The criteria are met.

99.038 NEIGHBORHOOD CONTACT REQUIRED FOR CERTAIN APPLICATIONS

Prior to submittal of an application for any subdivision, conditional use permit, multifamily project, planned unit development of four or more lots, non-residential buildings over 1,500 square feet, or a zone change that requires a Comprehensive Plan amendment, the applicant shall contact and discuss the proposed development with any affected neighborhood as provided in this section. Although not required for other or smaller projects, contact with neighbors is highly recommended. The Planning Director may require neighborhood contact pursuant to this section prior to the filing of an application for any other development permit if the Director deems neighborhood contact to be beneficial.

A. Purpose. The purpose of neighborhood contact is to identify potential issues or conflicts regarding a proposed application so that they may be addressed prior to filing. This contact is intended to result in a better application and to expedite and lessen the expense of the review process by avoiding needless delays, appeals, remands, or denials. The City expects an applicant to take the reasonable concerns and recommendations of the neighborhood into consideration

when preparing an application. The City expects the neighborhood association to work with the applicant to provide such input.

B. The applicant shall contact by letter all recognized neighborhood associations whose boundaries contain all or part of the site of the proposed development and all property owners within 500 feet of the site.

C. The letter shall be sent to the president of the neighborhood association, and to one designee as submitted to the City by the neighborhood association, and shall be sent by regular mail to the other officers of the association and the property owners within 500 feet. If another neighborhood association boundary is located within the 500-foot notice radius, the letter shall be sent to that association's president, and to one designee as submitted to the City by the neighborhood association as well. The letter shall briefly describe the nature and location of the proposed development, and invite the association and interested persons to a meeting to discuss the proposal in more detail. The meeting shall be scheduled at the association's regularly scheduled monthly meeting, or at another time at the discretion of the association, and not less than 20 days from the date of mailing of the notice. If the meeting is scheduled as part of the association's regular monthly meeting, the letter shall explain that the proposal may not be the only topic of discussion on the meeting agenda. The letter shall encourage concerned citizens to contact their association president, or their association designee, with any questions that they may want to relay to the applicant.

Neighborhood contact shall be initiated by the applicant by mailing the association president, and to one designee as submitted to the City by the neighborhood association, a letter, return receipt requested, formally requesting, within 60 days, a date and location to have their required neighborhood meeting. The 60 days shall be calculated from the date that the applicant mails this letter to the association. If the neighborhood association does not want to meet within the 60-day timeframe, or if there is no neighborhood association, the applicant shall hold a public meeting during the evening after 6:00 p.m., or on the weekend no less than 20 days from the date of mailing of the notice. All meetings shall be held at a location open to the public within the boundaries of the association or at a public facility within the City of West Linn. If the meeting is held at a business, it shall be posted at the time of the meeting as the meeting place and shall note that the meeting is open to the public and all interested persons may attend.

D. On the same date the letters described in subsections A through C of this section are mailed, the applicant shall provide and post notice on the property subject to the proposed application. The notice shall be posted at a location visible from the public right-of-way. If the site is not located adjacent to a through street, then an additional sign shall be posted on the nearest through street. The sign notice shall be at least 11 inches by 17 inches in size on durable material and in clear, legible writing. The notice shall state that the site may be subject to a proposed development (e.g., subdivision, variance, conditional use) and shall set forth the name of the applicant and a telephone number where the applicant can be reached for additional information. The site shall remain posted until the conclusion of the meeting.

E. An application shall not be accepted as complete unless and until the applicant demonstrates compliance with this section by including with the application:

1. A copy of the certified letter to the neighborhood association with a copy of return receipt;
2. A copy of the letter to officers of the association and to property owners within 500 feet, including an affidavit of mailing and a copy of the mailing list containing the names and addresses of such owners and residents;
3. A copy of the required posted notice, along with an affidavit of posting;
4. A copy of the minutes of the meetings, produced by the neighborhood association, which shall include a record of any verbal comments received, and copies of any written comments from property owners, residents, and neighborhood association members. If there are no minutes, the applicant may provide a summary of the meeting comments. The applicant shall also send a copy of the summary to the chair of the neighborhood association. The chair shall be allowed to supplement the summary with any additional comments regarding the content of the meeting, as long as such comments are filed before the record is closed;
5. An audiotape of the meeting; and
6. In the event that it is discovered by staff that the aforementioned procedures of this section were not followed, or that a review of the audio tape and meeting minutes show the applicant has made a material misrepresentation of the project at the neighborhood meeting, the application shall be deemed incomplete until the applicant demonstrates compliance with this section. (Ord. 1425, 1998; Ord. 1474, 2001; Ord. 1568, 2008; Ord. 1590 § 1, 2009; Ord. 1613 § 23, 2013; Ord. 1635 § 37, 2014; Ord. 1745 § 1 (Exh. A), 2023)

Staff Finding 68: A Neighborhood Association meeting was held at City Hall on June 9, 2025 and members from Savannah Oaks, Barrington Heights, and Willamette neighborhood Associations were invited. Documentation of the meeting is provided in the applicant's submittal (Exhibit PC-I Neighborhood Meeting Outreach) ~~1—PC 1—Pg.185~~. The criteria are met.

99.060 APPROVAL AUTHORITY

This section explains the authority of the Planning Director, Planning Commission, City Council, and Historic Review Board as it relates to quasi-judicial and legislative action.

A. Planning Director authority. The Planning Director, or designee, shall have the authority to:

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B. Planning Commission authority. The Planning Commission shall have the authority to:

1. Make a recommendation to approve, deny, or approve with conditions to the Council:

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2. Approve, deny, or approve with conditions the following applications:

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b. A conditional use (Chapter 60 CDC).

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h. Design review, Class II (Chapter 55 CDC).

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Staff Finding 69: The subject Conditional Use Permit, Class II Design Review, and Water Resource Area Permit are scheduled to be reviewed at the February 4th, 2026 Planning Commission public hearing. The criteria are met.

99.080 NOTICE

Notice shall be given in the following ways:

A. Class A Notice. Notice of proposed action or a development application pursuant to CDC 99.060 shall be given by the Director in the following manner:

1. At least 20 days prior to the scheduled hearing date notice shall be sent by mail to:

Staff Finding 70: The subject application has been properly noticed by the City. Please see Exhibit PC-L Affidavit and Notice Packet ~~PC-3~~. The criteria are met.