



## Agenda Bill 2019-04-08-02

Date: April 8, 2019

To: Russ Axelrod, Mayor  
Members, West Linn City Council

From: John J. Boyd AICP, Planning Manager *JJB*

Through: Eileen Stein, City Manager *ES*

Subject: Review of Condition of Approval No. 2 in the Final Decision and Order of File CUP-18-01/DR-18-04/VAR-18-02/03 for the Marylhurst School at 19915 and 19803 Old River Drive (Appeal File AP-19-02)

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### **Purpose**

For the City Council to hold a public hearing and consider a review of Condition of Approval 2 contained in the Planning Commission Final Decision and Order dated February 11, 2019. The review may consider a design and location of the proposed sidewalk along the property frontage and may consider a modification of the timing of the required street frontage improvements.

### **Question(s) for Council:**

1. Should the Council amend Condition of Approval 2 to allow an alternate design, phased improvements and location of the sidewalk?
2. Should the Council amend Condition of Approval 2, to modify the timing of required street improvements until Phase 2 of the Marylhurst School approval?
3. Should Council retain the Final Decision and Order signed by the Planning Commission on February 11, 2019?

### **Public Hearing Required:**

Yes

### **Background & Discussion:**

On February 6, 2019, the West Linn Planning Commission held a public hearing and unanimously approved a Conditional Use Permit, Design Review, and two Class II Variances for the proposed Marylhurst School on an existing church property at 19915 and 19803 Old River Drive on existing subdivision Lots 2, 3, and 4 of CedarOak Park Subdivision (CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03). The approval for Marylhurst School was approved in two phases.

At the February 19, 2019 City Council Work Session, Mayor Axelrod initiated a (call-up) review of this decision to consider Condition of Approval #2 in the Final Decision and Order for the Marylhurst School. Pursuant to Community Development Code (CDC) Chapter 99.240 Council has the authority to review a decision of the Planning Commission prior to final approval. The Marylhurst School Final Decision and Order signed February 11, 2019. The decision by Council on February 19<sup>th</sup> to approve the review was completed before the appeal period end on February 26, 2019. The Council review (call up) discussion

considered Condition of Approval #2 and related to the design and location of the sidewalk. This discussion considered oral and written material submitted into the public record.

Written testimony submitted to City Council on February 14, 2019 (and later placed in the record of AP-19-02), from Jim O'Toole was submitted into the record read in in part at the public workshop by Councilor Sakelik and outlined in the staff report for AP-19-02.

Public testimony from Dennis Pollmann, representing the Robinwood Neighborhood Association, and Christine Steele provided supporting review of the condition of approval. Both also support the Council consideration of an asphalt path as opposed to the concrete curb, gutter, and sidewalk as approved by the Planning Commission.

Mayor Axelrod moved to call up (CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03), under CDC Chapter 99.240, to evaluate and address Condition of Approval 2. Councilor Cummings seconded the motion. The motion was approved unanimously.

An appeal or review is a de novo hearing and not limited to the stated grounds for review and all relevant issues may be considered. All evidence presented to the lower approval authority shall be considered and given equal weight as evidence presented on appeal. The Criteria applicable to the Conditional Use Permit, Class II Design Review, and Class II Variances (required parking and required loading area ) approved by the Planning Commission and under review by City Council are in Chapters 11, 41, 42, 44, 46, 48, 54, 55, 60, 75, 92, 96, and 99 of the Community Development Code (CDC).

**Budget Impact:**

Future staff time to address appropriate improvements for the neighborhood (potential Comprehensive Plan/Community Development Code amendments).

**Sustainability Impact:**

None

**Council Options:**

1. Amend Condition of Approval 2 contained in the Planning Commission Final Decision and Order to allow an alternate sidewalk design (including in Phase One to provide revised plans showing alternate design of on-street drainage, alternate design and location of the sidewalk, construction of a temporary pedestrian path and location) and to delay offsite improvements until Phase Two (complete all half street improvements, including the alternate sidewalk design and location), as required for the Marylhurst School approval.
2. Amend Condition of Approval 2 contained in the Planning Commission Final Decision and Order to allow an alternate sidewalk design and location.
3. Retain Condition of Approval 2 as approved by the Planning Commission.

**Staff Recommendation:**

Staff supports City Council Option 1.

**Potential Motion:**

1. Move to approve an amendments to Condition of Approval #2 (in Phase One to provide revised plans showing an alternate design of on-street drainage, alternate design and location for the

sidewalk, construction of a temporary pedestrian path, and in Phase Two to complete all half street improvements) as shown in the staff recommendation provided in the staff report.

2. Move to retain the language found in the Planning Commission Final Decision and Order dated February 11, 2019.

**Attachments:**

1. Staff Report to City Council (April 8, 2019)
2. Staff Report to the Planning Commission (February 6, 2019)
3. Supplemental Findings Memorandum to the Planning Commission (February 6, 2019)

**Staff Recommendation from April 8, 2019 Staff Report for Appeal AP-19-02**

2. Engineering Standards. All public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite stormwater, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. These **onsite improvements** must be designed, constructed, and completed prior to the issuance of **framing inspection** for each phase of the project. **All offsite improvements, must be designed in Phase one and constructed as defined below.**

- a. Half Street Improvements - All offsite improvements including full half-street improvements as well as the undergrounding of electrical must be designed, constructed and completed prior to the issuance of framing inspection for phase two of the project.**
- b. Sidewalk Design. The applicant shall submit revised site plans showing the location and width of the asphalt sidewalk along the entire frontage of the property to accommodate pedestrian access. The applicant may submit a revised stormwater treatment plan using alternate treatment options for public right-of-way improvements in the revised site plans. The revised plans must be submitted before issuance of any building permits.**
- c. Temporary sidewalk. For Phase One the applicant shall construct a hard packed pedestrian path (the intent is the temporary pedestrian path becomes the base for the asphalt sidewalk completed in Phase Two) along the entire property frontage.**

**STAFF REPORT  
FOR THE CITY COUNCIL**

**FILE NUMBER:** AP-19-02

**HEARING DATE:** April 8, 2019

**REQUEST:** Council review of the Planning Commission approval for the tentatively approved Marylhurst School at 19915 and 19803 Old River Drive. The review is focused on Condition of Approval 2 contained in the Planning Commission Final Decision and Order.

**APPROVAL CRITERIA:** Community Development Code (CDC) Chapter 55; Chapter 60; Chapter 96; and Chapter 99.

**STAFF REPORT PREPARED BY:** John J. Boyd AICP, Planning Manager

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## GENERAL INFORMATION

**APPELLANT:** West Linn City Council  
22500 Salamo Road  
West Linn, OR 97068

**APPLICANT:** The Marylhurst School  
1232 Linn Avenue  
Oregon City, OR 97045  
Contact: Sheila Walker

**SITE LOCATION:** 19915 and 19803 Old River Drive

**LEGAL DESCRIPTION:** Clackamas County Assessor's Map 2S-1E-23AA, Taxlots 400 (0.47 acres) and 500 (0.99 acres)

**SITE SIZE:** 1.46 acres

**ZONING:** R-10, Single-Family Residential Detached

**COMP PLAN DESIGNATION:** Low-Density Residential

**120-DAY PERIOD:** This application became complete on December 5, 2018. The 120-day maximum application processing period ends on April 4, 2019. The applicant granted a 20-day extension, so the maximum application processing period now ends on April 24, 2019.

**PUBLIC NOTICE:** Notice was mailed to all affected government agencies, affected neighborhood associations, all persons with standing, and all property owners within 500 feet of the subject property on March 19, 2019. Notice was published in the West Linn Tidings on March 28, 2019. The property was posted with a notice sign on March 28, 2019. The notice and application were posted on the City's website March 19, 2019. Therefore, public notice requirements of CDC Chapter 99 have been met.

## EXECUTIVE SUMMARY

On February 6, 2019, the West Linn Planning Commission held a public hearing and unanimously approved a Conditional Use Permit, Design Review, and two Class II Variances for the relocation of the Marylhurst School to an existing church property at 19915 & 19803 Old River Drive (CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03). The approval is for redevelopment of the property in two phases. The Final Decision and Order was signed February 11, 2019 by the Planning Commission Chair. Without any appeal or review, the decision would have been final following the completion of the appeal period cited in the Final Decision and Order (on February 26, 2019.)

At the February 19, 2019 City Council Work Session, Mayor Axelrod discussed the review procedure and referred to CDC 99.170(G).2. This section allows council to consider during the appeal period, a review of the Planning Commission decision. The action to initiate a review, requires council a majority vote to approve a review of a decision pursuant to 99.240.

The Council discussed a review (call-up) of one condition of approval for the Marylhurst School approval as allowed by West Linn Community Development Code (CDC) Chapter 99.240. The Mayor suggested the review process for the Planning Commission decision related to the sidewalk placement and design, and cited the file number (CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03). Councilor Cummings clarified it was not a review of the entire land use decision and was limited review only related to the sidewalk improvement.

Testimony received from Jim O'Toole on February 14, 2019 (see Exhibit CC-2) was read into the public record for the workshop by Councilor Sakelik. He read from portions of the letter as follows: On Page 2 of 3 in February 14<sup>th</sup> letter he began reading:

“Our request to you is simple. We ask the City Council’s Reconsideration of Planning Commission Decision CUP 18-01, DR 18-04, VAR 18-02/03, Staff Recommendation No. 2.”

Councilor Sakelik then read the proposed amendment requested by Mr. O’Toole for Condition of Approval #2 be

“We would suggest the language be **modified** as follows:

“2. **Engineering Standards.** All public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite stormwater, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. These onsite improvements must be designed, constructed, and completed prior to the issuance of occupancy permits for Phase 1 of the project. All offsite improvements, including full half street improvements as well as the undergrounding of

electrical must be designed, constructed and completed prior to the issuance of occupancy permits for Phase 2 of the project.”

Dennis Pollmann, representing the Robinwood Neighborhood Association, and Christine Steele provided oral and written testimony supporting review of the condition of approval (see Exhibit CC-2). Both also support the Council consideration of an asphalt path as opposed to the concrete curb, gutter, and sidewalk as approved by the Planning Commission.

Mayor Axelrod moved, under CDC Chapter 99.240, to call-up for review Condition of Approval 2 in the Planning Commission Final Decision and Order for File CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03. Councilor Cummings seconded the motion. The motion was approved unanimously.

The Community Development Code (CDC) contains requirements for the installation of both public and private improvements, including water, sanitary sewer, stormwater, and transportation, in a number of chapters. The review proposed by Council, could modify the design and location of the sidewalk and potentially the timing of completion of specified offsite improvements. The timing of the transportation improvements are primarily addressed by three criteria that are applicable to the Marylhurst School application, as incorporated in the Final Decision and Order and are as follows:

1. CDC Chapter 55.100.I(1)
  - a. By reference 85.200(A)(3) & (16)
  - b. By reference 92.010.(H)
2. CDC Chapter 60.070.A(4)
3. CDC Chapter 96.020

The Council also has the option to consider an alternative placement and design of the sidewalk to allow an asphalt path as opposed to the approved concrete curb, gutter, and sidewalk as submitted by the applicant and approved by the Planning Commission. The CDC contains requirements for the installation of public right-of-way improvements, including width of travel lanes, bicycle lanes, sidewalks (alternate width allowances and recognition of connections to respond to site constraints such as avoiding existing trees), and landscape strips, as well as allowance for on-street parking. The CDC does not directly address the construction design standards, including the sidewalk construction material (concrete versus asphalt). This is addressed in the Public Works Design Standards, which are regulated in the Municipal Code.

However, an application for both a design review proposal and a conditional use permit provides the decision-maker a level of discretion. The type of sidewalk construction material could be addressed by the following criteria:

1. CDC 55.100.I(1)
2. CDC Chapter 60.070.A(3)

Staff has provided analysis and findings for the four criteria in the addendum that follows.

**Public comment:**

The public comments were submitted prior to the initiation of the review during the City Council meeting on February 19, 2019 were made part of this record. No other public were received by the mailing of this staff report. The comments can be found in Exhibit CC-2.

**RECOMMENDATION**

Staff supports City Council consider approval of two amendments to Condition of Approval 2: to allow for alternate sidewalk design and location, and to allow street improvements to be constructed during Phase Two of the Marylhurst School approval.

This recommendation for File AP-19-01 is based on: 1) supplementary staff findings included in the Addendum below, and 2) the record of CUP-18-01/DR-18-04/VAR-18-02/03. With these findings, the applicable approval criteria are met. If Council were to approve staff recommendation, the amended Condition of Approval 2 would read as follows:

2. Engineering Standards. All public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite stormwater, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. These **onsite improvements** must be designed, constructed, and completed prior to the issuance of **framing inspection** for each phase of the project. **All offsite improvements, must be designed in Phase one and constructed as defined below.**

- a. **Half Street Improvements - All offsite improvements including full half-street improvements as well as the undergrounding of electrical must be designed, constructed and completed prior to the issuance of framing inspection for phase two of the project.**
- b. **Sidewalk Design. The applicant shall submit revised site plans showing the location and width of the asphalt sidewalk along the entire frontage of the property to accommodate pedestrian access. The applicant may submit a revised stormwater treatment plan using alternate treatment options for public right-of-way improvements in the revised site plans. The revised plans must be submitted before issuance of any building permits.**
- c. **Temporary sidewalk. For Phase One the applicant shall construct a hard packed pedestrian path (the intent is the temporary pedestrian path becomes the base for the asphalt sidewalk completed in Phase Two) along the entire property frontage.**

**ALTERNATE RECOMMENDATION:** Alternately, if Council chose to remove the additional period of time to complete improvements, the modified condition of improvements would read as follows.



2. Engineering Standards. All public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite stormwater, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. These **onsite improvements and offside improvements** must be designed, constructed, and completed prior to the issuance of **framing inspection** for each phase of the project. **All offsite improvements, must be designed and constructed in Phase one.**

- a. **Sidewalk Design. The applicant shall submit revised site plans showing the location and width of the asphalt sidewalk along the entire frontage of the property to accommodate pedestrian access. The applicant may submit a revised stormwater treatment plan using alternate treatment options for public right-of-way improvements in the revised site plans. The revised plans must be submitted before issuance of any building permits.**

**ADDENDUM  
CITY COUNCIL STAFF REPORT  
April 8, 2019**

**STAFF EVALUATION OF THE PROPOSAL'S COMPLIANCE  
WITH APPLICABLE CODE CRITERIA**

The West Linn Community Development Code contains a number of different criteria that address required public and private infrastructure improvements. Findings for all of these criteria can be found in the Staff Report to the Planning Commission dated February 6, 2019 and the Supplemental Findings Memorandum to the Planning Commission dated February 6, 2019. Staff did not re-evaluate these findings, but instead focused on the criteria that directly address the timing of improvements per the City Council's direction to review Condition of Approval 2 and the delay of street improvements until Phase 2 of the Marylhurst School project.

*I. CHAPTER 55, DESIGN REVIEW*

*55.100 APPROVAL STANDARDS – CLASS II DESIGN REVIEW*

*1. Public facilities. An application may only be approved if adequate public facilities will be available to provide service to the property prior to occupancy.*

*(...)*

*1. Streets. Sufficient right-of-way and slope easement shall be dedicated to accommodate all abutting streets to be improved to the City's Improvement Standards and Specifications. The City Engineer shall determine the appropriate level of street and traffic control improvements to be required, including any off-site street and traffic control improvements, based upon the transportation analysis submitted. The City Engineer's determination of developer obligation, the extent of road improvement and City's share, if any, of improvements and the timing of improvements shall be made based upon the City's systems development charge ordinance and capital improvement program, and the rough proportionality between the impact of the development and the street improvement*

*In determining the appropriate sizing of the street in commercial, office, multi-family, and public settings, the street should be the minimum necessary to accommodate anticipated traffic load and needs and should provide substantial accommodations for pedestrians and bicyclists. Road and driveway alignment should consider and mitigate impacts on adjacent properties and in neighborhoods in terms of increased traffic loads, noise, vibrations, and glare.*

*The realignment or redesign of roads shall consider how the proposal meets accepted engineering standards, enhances public safety, and favorably relates to adjacent lands and land uses. Consideration should also be given to selecting an alignment or design that minimizes or avoids hazard areas and loss of significant natural features (drainageways, wetlands, heavily forested areas, etc.) unless site mitigation can clearly produce a superior landscape in terms of shape, grades, and reforestation, and is fully consistent with applicable code restrictions regarding resource areas.*

*Streets shall be installed per Chapter 85 CDC standards. The City Engineer has the authority to require that street widths match adjacent street widths. Sidewalks shall be installed per CDC*

*85.200(A)(3) for commercial and office projects, and CDC 85.200(A)(16) and 92.010(H) for residential projects, and applicable provisions of this chapter. Where streets bisect or traverse water resource areas (WRAs) the street width shall be reduced to the appropriate “constrained” cross-section width indicated in the TSP or alternate configurations which are appropriate to site conditions, minimize WRA disturbance or are consistent with an adopted transportation system plan. The street design shall also be consistent with habitat friendly provisions of CDC 32.060(I).*

*Based upon the City Manager’s or Manager’s designee’s determination, the applicant shall construct or cause to be constructed, or contribute a proportionate share of the costs, for all necessary off-site improvements identified by the transportation analysis commissioned to address CDC 55.125 that are required to mitigate impacts from the proposed development. Proportionate share of the costs shall be determined by the City Manager or Manager’s designee, who shall assume that the proposed development provides improvements in rough proportion to identified impacts of the development.*

**Reference in 55.100.I(1) to 85.200.(A)(16)**

*16. Sidewalks. Sidewalks shall be installed per CDC 92.010(H), Sidewalks. The residential sidewalk width is six feet plus planter strip as specified below. Sidewalks in commercial zones shall be constructed per subsection (A)(3) of this section. See also subsection C of this section. Sidewalk width may be reduced with City Engineer approval to the minimum amount (e.g., four feet wide) necessary to respond to site constraints such as grades, mature trees, rock outcroppings, etc., or to match existing sidewalks or right-of-way limitations.*

**Reference in 55.100.I(1) to 92.010(H)(3)**

3. The sidewalks shall measure at least six feet in width and be separated from the curb by a six-foot minimum width planter strip. Reductions in widths to preserve trees or other topographic features, inadequate right-of-way, or constraints, may be permitted if approved by the City Engineer in consultation with the Planning Director.

***The application reviewed by the Planning Commission considered a two phased development Proposal as follows:***

***Phase One is adaptive reuse of the existing buildings on-site with a maximum of 115 primary/middle school students. The main building (4,500 sq. ft.) would be used for the main office and school assemblies, activities, etc., while the annex building (4,000 sq. ft.) would be used for classrooms and administrative offices. A portable building (1,800 sq. ft.) would be located in the parking lot and contain two temporary classrooms. The portable would be removed after the completion of Phase Two.***

Phase Two would include the construction of a new, 16,300 square foot, two-story building to house classrooms and a library. Maximum enrollment after completion of Phase Two is 194 pre-school/primary/middle school students. Redesigned parking, open space areas, outdoor recreation areas, pedestrian pathways, and new landscaping will also be included.

**Finding 1: City Council finds that adequate public facilities are available to provide service to the property prior to occupancy of Phase One of the Marylhurst School approval at 19915 & 19803 Old River Drive. The previous occupancy of the existing buildings on site was a church**

and pre-school. City Council finds that the maximum enrollment of 115 students for Phase One of the Marylhurst School project is consistent with the prior occupancy and with one exception (sidewalks) the existing transportation infrastructure is adequate to consider deferring public street improvements until Phase Two. In addition, the Marylhurst School submitted a Traffic Impact Study and On Site Queuing Memorandum showing the proposed parking lot and 120-foot drop-off lane will provide sufficient on-site drop-off/pick-up storage to prevent stacking in the right-of-way.

**FINDING 2:** City Council finds that pursuant to Chapter 55, 60, 92 and 96 this neighborhood school serves the local area for Kindergarten to 8<sup>th</sup> Grade. For the first phase, revised Condition of Approval two requires a temporary sidewalk path toward a connection to the existing sidewalk network (to the north, on Old River Drive). When completed, this connection would benefit the children that were able to walk to school and benefit the local pedestrians in the adjacent community. The location of this path will be the location approved by council for the alternate design (reduced width and surface type) and location within the right of way would not be a standard linear design for the sidewalk. It would make a connection to existing sidewalks adjacent to the property.

This temporary path, to be built in Phase One would be the hard packed base to serve as a pedestrian trail for the eventual asphalt sidewalk location completed in Phase Two.

#### *IX. CHAPTER 60, CONDITIONAL USES*

##### *60.070 APPROVAL STANDARDS AND CONDITIONS*

*A. The Planning Commission shall approve, approve with conditions, or deny an application for a conditional use, except for a manufactured home subdivision in which case the approval standards and conditions shall be those specified in CDC 36.030, or to enlarge or alter a conditional use based on findings of fact with respect to each of the following criteria:*

*(...)*

*4. Adequate public facilities will be available to provide service to the property at the time of occupancy.*

**Finding 3:** City Council finds that subject to the modified conditions of approval, adequate public facilities are available to provide service to the property prior to occupancy of Phase One of the Marylhurst School redevelopment approval at 19915 & 19803 Old River Drive. The previous occupancy of the existing buildings on site was a church and pre-school.

**Finding 4:** City Council finds that the maximum enrollment of 115 students for Phase One of the Marylhurst School project is consistent with the prior occupancy and the existing transportation infrastructure is adequate so public street improvements can safely be delayed until Phase Two. In addition, the Marylhurst School submitted a Traffic Impact Study and On Site Queuing Memorandum showing the proposed parking lot and 120-foot drop-off lane will provide sufficient on-site drop-off/pick-up storage to prevent stacking in the right-of-way.

## *XII. CHAPTER 96, STREET IMPROVEMENT CONSTRUCTION*

### *96.020 STANDARDS*

*Street improvements shall be installed according to the City standards and shall be completed prior to the issuance of any occupancy permit for the new or remodeled structure or building. In unimproved areas of the City, the City Engineer may grant a time extension of the provisions of this section; provided, that the applicant provides sufficient security in amount and quantity satisfactory to the City Attorney to assure payment of such improvement costs.*

**Finding 5: City Council finds that subject to the completion of the conditions of approval, adequate public facilities are available to provide service to the property prior to occupancy of Phase One of the Marylhurst School redevelopment approval at 19915 & 19803 Old River Drive. The previous occupancy of the existing buildings on site was a church and pre-school. City Council finds that the maximum enrollment of 115 students for Phase One of the Marylhurst School project is consistent with the prior occupancy and the existing transportation infrastructure is adequate so public street improvements can safely be delayed until Phase Two. In addition, the Marylhurst School submitted a Traffic Impact Study and On Site Queuing Memorandum showing the proposed parking lot and 120-foot drop-off lane will provide sufficient on-site drop-off/pick-up storage to prevent stacking in the right-of-way.**

**Finding 6: City Council also finds that Old River Drive, not currently constructed to City street standards, is considered an unimproved area of the City. This makes street improvements eligible for a time extension, which is consistent with delaying street improvements until Phase Two of the Marylhurst School project.**

The Council may also choose to consider an alternative placement and design of the sidewalk to allow an asphalt path as opposed to the approved concrete curb, gutter, and sidewalk. The West Linn Community Development Code contains a number of different criteria that address the installation of required right-of-way improvements. Findings for all of these criteria can be found in the Staff Report to the Planning Commission dated February 6, 2019 and the Supplemental Findings Memorandum to the Planning Commission dated February 6, 2019. Staff did not re-evaluate these findings. However, the CDC does not directly address the construction design standards, including the sidewalk construction material (concrete versus asphalt). This is addressed in the Public Works Design Standards, which are regulated in the Municipal Code. An application for a conditional use permit provides the decision-maker a level of discretion, including the installation of public improvements could be addressed by the following criteria:

## *IX. CHAPTER 60, CONDITIONAL USES*

### *60.070 APPROVAL STANDARDS AND CONDITIONS*

*A. The Planning Commission shall approve, approve with conditions, or deny an application for a conditional use, except for a manufactured home subdivision in which case the approval standards and conditions shall be those specified in CDC 36.030, or to enlarge or alter a conditional use based on findings of fact with respect to each of the following criteria:*

(...)

3. The granting of the proposal will produce a facility that provides an overall benefit to the City.

(...)

7. The use will comply with the applicable policies of the Comprehensive Plan.

**Finding 7: City Council finds that the approval of the modified conditions of approval for Marylhurst School (File AP-19-01) provides an overall benefit to the City. City Council also finds that the minimal sidewalks and the use of an alternate stormwater infrastructure in the right of way has complies with the CDC requirement addressed in this report. .**

**Finding 8: If approved by City Council, in Phase One the temporary path location and final asphalt surface design will provide safe pedestrian access and provide a buffer to traffic for children choosing to walk to school and thus an overall benefit to the community than standard concrete curb, gutter, and sidewalk approved for the Marylhurst school site. The revised condition of approval requires Marylhurst School to submit revised site plans and a revised stormwater treatment plan for the public right-of-way improvements as outlined in modified Condition of Approval 2.**

**EXHIBIT CC-1: PLANNING COMMISSION FINAL DECISION AND ORDER**

**WEST LINN PLANNING COMMISSION**  
**FINAL DECISION AND ORDER**  
**CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03**

**IN THE MATTER OF A PROPOSAL TO DEVELOP THE MARYLHURST  
SCHOOL AT 19915/19803 OLD RIVER DRIVE**

**I. Overview**

At its meeting on February 6, 2019, the West Linn Planning Commission ("Commission") held a public hearing to consider the request by Sheila Walker, Director of the Marylhurst School, to approve a proposal to develop the school in a two-phase process. The approval criteria for a conditional use permit, design review, and variances are found in Chapters 11, 41, 42, 44, 46, 48, 54, 55, 60, 75, 92, 96, and 99 of the Community Development Code (CDC). The hearing was conducted pursuant to the provisions of CDC Chapter 99.

The hearing commenced with a staff report presented by Darren Wyss, Associate Planner. Sheila Walker, Beth Cantrell (DECA Architecture), Mark Wharry (KPF Engineering), Debra Pearson (OTAK Consulting), and Todd Mobley (Lancaster Engineering) presented on behalf of the applicant. Jasmine Fullman and Susan Zettergren testified in support of the application. Stan Christiansen lives down the street and testified he has no concerns with the issues raised and supports the application. Elizabeth Hayes lives adjacent to the subject property and testified she has no concerns, appreciated the willingness of the applicants to work with the neighborhood on the design, and supports the application. Eleanora Larson provided written testimony in support of the application.

The Commission discussed several topics with the applicant, including timing of traffic counts, the impact to the intersection of Cedaroak Drive and Highway 43, staggering of school start times to minimize traffic impacts, the need for a loading dock, Highway 43 improvements, impact of the new building on the neighboring property, asphalt versus concrete sidewalk, potential for infiltration of parking lot stormwater run-off, potential for solar panels on the new building, location/height/sewer connection for the modular classrooms, stormwater planters as a minor utility, and the timing of removal of the modular classrooms and completion of Phase Two. Commissioner Mathews requested feedback from City staff on options for conditioning the length of time the modular building could remain in use. Staff noted that the Conditional Use approval would be good for three years. Commissioner Mathews requested clarification on whether the Phase Two construction would need to be initiated or completed within the three years. Planning Manager John Boyd suggested a check-in at year two of the three-year process for the applicant to provide an update on the fundraising effort for Phase Two, address the length of time the modular classrooms will remain on site, and update the time needed to complete Phase Two. If Phase Two will not be completed within three years, the applicant



could then submit an application for an extension of the approval. The Commission asked the applicant if this action was acceptable and the applicant volunteered to meet this condition.

The public hearing was closed and a motion was made by Commissioner Mathews and seconded by Commissioner Farrell to approve the application with ten existing conditions of approval set forth by the Planning Department of West Linn in the 2/6/2019 Staff Report and one additional condition of approval. The motion was passed unanimously 6-0.

## **II. The Record**

The record was finalized at the February 6, 2019, hearing. The record includes the entire file from CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03, including the material (with video) provided at the hearing.

## **III. Findings of Fact**

- 1) The Overview set forth above is true and correct.
- 2) The applicant is Sheila Walker.
- 3) The Commission finds that it has received all information necessary to make a decision based on the Staff Report and attached findings; Supplemental Findings Memo dated February 6, 2019; public comment, if any; and the evidence in the whole record, including any exhibits received at the hearing.

## **IV. Findings**

The Commission adopts the Staff Report for February 6, 2019, with attachments, and the Supplemental Findings Memo dated February 6, 2019 as its findings, which are incorporated by this reference. The Commission added one Condition of Approval identified as Condition 11, supported by the following finding:

Supplement Finding. The Commission was concerned about the length of time the modular building could remain in use. The Commission acknowledges the Conditional Use approval is in effect for three years, but wanted to clarify whether the Phase Two construction would need to be initiated or completed within the three years. The Commission received information from the applicant that their fundraising goal is to complete Phase Two in two years. The Commission finds it reasonable that if Phase Two is not complete in two years, then the applicant will check-in with the Planning Department at year two of the three-year-long Conditional Use approval to provide an update on the fundraising effort for Phase Two, address the length of time the modular classrooms will remain on site, update the time needed to complete Phase Two, and, if more time is needed, submit an application for an extension of the Conditional Use approval. The Commission asked the applicant if this action was acceptable and the applicant volunteered to meet this condition.

The Commission concludes that all of the required approval criteria are met subject to the following conditions of approval:

1. **Site Plans.** With the exception of modifications required by these conditions, the project shall substantially conform to all Tentative Plan Sheets.
2. **Engineering Standards.** All public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite stormwater, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. These must be designed, constructed, and completed prior to the issuance of occupancy permits for each phase of the project.
3. **Retaining Wall Fences.** The proposed three-foot fence surrounding the stormwater facility shall maintain a two-foot setback from top of retaining wall for any portion of the wall that exceeds five and one-half feet per Staff Finding 9.
4. **Parking Space Size.** The site plan shall be modified to reconfigure the parking spaces so that 50 percent are nine feet by eighteen feet per Staff Finding 19.
5. **Landscaping Installation.** All required landscaping shall be installed prior to final building certificate of occupancy for each of the two phases per Staff Finding 52.
6. **Property Line Adjustment.** The applicant shall consolidate Lots 2, 3, and 4 of the Cedaroak Park Subdivision prior to the issuance of building permits for purposes of meeting building code requirements per Staff Finding 100.
7. **15 Foot Sanitary Sewer Line.** The applicant shall vacate the existing sanitary sewer line easement on the south end of the property and record a new 15 foot easement centered on the existing sanitary sewer line prior to issuance of occupancy permits per Staff Finding 86.
8. **15 Foot Water Line Easement.** The applicant shall dedicate a 15 foot wide easement, centered over the water line on the north side of the property, to the City for on-going operation and maintenance of the existing water line. The easement shall be dedicated to the City, on a form provided by the City, prior to issuance of occupancy permits per Staff Finding 84.
9. **23 Foot Drive Aisle.** Prior to issuance of building permits, the applicant shall modify the site plan to redesign the curb bulb at the end of the student drop-off zone to create a minimum 23 foot drive aisle per Staff Finding 33.

10. **One-Way Drive Aisle.** The site plan shall be modified to show treatments that reduce conflict associated with wrong way movements from motor vehicles in the one-way drive aisles.

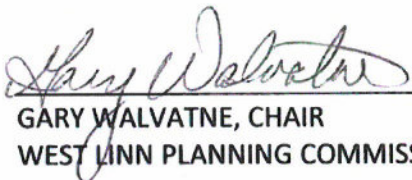
11. **2-Year Check-In on Phase Two Progress.** Within two years of the date of approval, (February 2021) the applicant shall submit a progress report to the Planning Department. The Planning Department shall schedule an agenda item before the next available Planning Commission meeting, to allow the applicant to provide an update on the project funding and how that funding status impacts the completion of Phase Two of the project.

a. This update should include a date when the modular classrooms will be removed.

b. If the applicant estimates the Phase Two timeline will extend beyond the two year period, the progress report should be submitted with a city application form (with appropriate fees) to request for extension of time to the Condition Use approval, which will also require review and approval by the Planning Commission.

**V. Order**

The Commission concludes that CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03 is approved based on the Record, Findings of Fact, and Findings above.

  
GARY WALVATNE, CHAIR  
WEST LINN PLANNING COMMISSION

February 11, 2019  
DATE

This decision may be appealed to the City Council pursuant to the provisions of Chapter 99 of the Community Development Code and any other applicable rules and statutes. This decision will become effective 14 days from the date of mailing of this final decision as identified below.

Mailed this 12<sup>th</sup> day of February, 2019.

Therefore, this decision becomes effective at 5 p.m., February 26, 2019.

**EXHIBIT CC-2: PUBLIC COMMENT**

## Shroyer, Shauna

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**From:** Axelrod, Russell  
**Sent:** Tuesday, February 19, 2019 9:55 AM  
**To:** Cummings, Teri; Stein, Eileen  
**Subject:** Re: Letter from the Robinwood Neighborhood Association

I'm very interested in this as well and have discussed the matter briefly at end of last week with RNA rep's. I'm not sure of the process, but believe we are under tight time constraint for any council action. I'm copying Tim here so he can help with defining the process steps for us.

Thanks,  
Russ

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**From:** Cummings, Teri  
**Sent:** Tuesday, February 19, 2019 12:18 AM  
**To:** Axelrod, Russell; Stein, Eileen  
**Subject:** Fw: Letter from the Robinwood Neighborhood Association

Mayor Axelrod and Ms. Stein, could we please have information about the CDC remand procedure available tomorrow since I assume time is of the essence in this matter? I am willing to hear the one issue on remand.  
Teri Cummings

---

**From:** Jim OTOOLE <jandeotoole@comcast.net>  
**Sent:** Thursday, February 14, 2019 10:15 PM  
**To:** Axelrod, Russell  
**Cc:** Cummings, Teri; Sakelik, Richard; Walters, Julianna; Relyea, William  
**Subject:** Letter from the Robinwood Neighborhood Association

Dear Mr. Mayor

On behalf of Mr. Dennis Pollmann, President of the RNA, I have attached a letter regarding a request for a revision on a "condition of approval", from a final decision issued by the City Planning Commission relative to The Marylhurst School Project.

Thank you in advance for your consideration of this request.

JO'T

Jim O'Toole  
West Linn, Oregon

## ROBINWOOD NEIGHBORHOOD ASSOCIATION

The Honorable Russ Axelrod  
Mayor  
West Linn, Oregon  
22500 Salamo Road  
West Linn, Oregon 97068

February 14, 2019

RE: Request for City Council Reconsideration of Planning Commission Decision  
CUP 18-01, DR 18-04, VAR 18-02/03, Staff Recommendation No. 2

Dear Mr. Mayor,

During this weeks monthly meeting of the Association the neighbors were made aware of the progress and campaign by The Marylhurst School through the City Planning Commission to relocate their facilities within the boundaries of the neighborhood. We welcome them whole heartedly into our midst.

While the cause for celebration is real, there is a deep underlying concern surrounding the requirement to place a "new concrete sidewalk and curb to nowhere" along the property on Old River Road.

The applicant, The Marylhurst School, originally proposed an asphalt type of pathway similar in nature and design as three other existing walkways within the Neighborhood to conform and blend in with the more rural setting of the streetscape; as well as, to address concerns of the Neighborhood Association, as outlined in the letter to you, dated June 19, 2018 (attachment).

The applicant's proposal was denied. Subsequently, the applicant recommended the decision of the sidewalk, street and curb structure be deferred until the Marylhurst School reached phase two of their construction plan which would be approximately two years into the future. This would also serve to provide the Association the time needed to address the street and sidewalk issues during the revision process of the Neighborhood Plan according to the applicant.

This proposal was also denied; notwithstanding very little construction and installation of new structures or facilities will take place until financing, plans and programs for phase two have been finalized. Unfortunately, there appears to be no flexibility in the interpretation of the City Code to allow for what we consider a very reasonable request.

Considering the time which could be allowed and with no adverse effect present, it makes little sense to stifle the opportunity to explore different and new available pathway construction options and alternatives which may be more compatible, and in sync, with the surroundings through the revision process afforded to us by our upcoming scheduled work on the existing Neighborhood Plan.

Our request to you is simple. We ask the City Council's Reconsideration of Planning Commission Decision CUP 18-01, DR 18-04, VAR 18-02/03, Staff Recommendation No. 2

For your convenience below, please find a copy of the condition of approval from the final decision, followed by our proposed language for the condition of approval.

The condition of approval is as follows:

2. **Engineering Standards**. All public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite stormwater, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. These must be designed, constructed, and completed prior to the issuance of occupancy permits for each phase of the project.

We would suggest the language be **modified** as follows:

2. **Engineering Standards**. All public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite stormwater, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. These onsite improvements must be designed, constructed, and completed prior to the issuance of occupancy permits for Phase 1 of the project. All offsite improvements, including full half street improvements as well as the undergrounding of electrical must be designed, constructed and completed prior to the issuance of occupancy permits for Phase 2 of the project.

We request an expedited remedy to this issue. Time is extremely limited for us to exercise any other options short of an appeal. We want to seriously avoid any situation which might turn out to be an encumbrance to The Marylhurst School and their plans.

Should you require any additional information, justifications, or my appearance before the City Council to further elaborate on this request, I will be more than pleased to make myself available to your calendar.

Page3 of 3  
The Honorable Russ Axelrod  
February 14, 2019

The active members of the Robinwood Association stand ready to assist you in this endeavor in any way that will prove helpful. We look forward to your kind response and thank you for your continued support for our neighbors.

Sincerely yours,

*/s/ Jim*

James T. O'Toole  
Special Projects

*/s/ Dennis A. Pollmann*

President  
Robinwood Neighborhood Association

*/s/ Christine Steel*

Vice-president  
Robinwood Neighborhood Association

*Attachment: June 19, 2018 Letter to The Honorable Russ Axelrod*

cc: The Honorable Teri Cummings  
Member of the West Linn City Council

The Honorable Richard Sakelik  
Member of the West Linn City Council

The Honorable Jules Walters  
Member of the West Linn City Council

The Honorable William Relyea  
Member of the West Linn City Council

**Robinwood Neighborhood Association Board Members**

Dennis Pollmann, President  
Kevin Bryck, Treasurer

Christine Steel, Vice-president  
Kazi Ahmed, Ambassador



*(Attachment: June 19, 2018 Letter to The Honorable Russ Axelrod)*

## ROBINWOOD NEIGHBORHOOD ASSOCIATION

The Honorable Russ Axelrod  
Mayor  
West Linn, Oregon  
22500 Salamo Road  
West Linn, Oregon 97068

June 19, 2018

Dear Mr. Mayor,

During this month's meeting of the Robinwood Neighborhood Association the topic of improvements to "sidewalks and streets" was discussed at length. The Association voted, unanimously, to seek your assistance in order to finally resolve this dilemma.

Over the years the sections of Chapter 96 of the Community Development Code have been inconsistently applied to remodel and building projects within the boundaries of the Robinwood Neighborhood. Specifically, those provisions and conditions affecting the installation of concrete curbs and sidewalks, along with street improvements, have been most controversial and problematic.

As you are aware, the majority of residents within the developed sections of the Robinwood Neighborhood have long had a strong preference to maintain the current rural fabric. They enjoy the narrow shared roadways which maximizes the natural setting and minimizes hardscape.

The current thinking of the City planners regarding the piece-meal installation of curbs and sidewalks, seems to be that some day, in the far off future, all of the individual curbs and sidewalks will miraculously all "meet-up" in our life time, is illusory. In the interim, the rationale for the installation of a series of "sidewalks to no-where" contributes little to the current aesthetics and/or individual property values within the neighborhood. Given the time it will take to complete this fancy, no account is given for the value of "undeveloped" lands. Nor, does the program take into account that residents will defer upgrades and improvements to their individual properties in light of the significant added expenses which would be incurred. As a result, the neighborhood will not benefit from the improvements, including remodeled and/or new housing normally witnessed within the growth of a typical neighborhood.

There are provisions within the code which allow for a determination to be made which allows the resident to pay for the curbs, sidewalks and street improvements elsewhere in the City; in lieu of placing same at their residence. These provisions are just onerous. The concept, usually reserved for construction of commercial properties is, in most cases, considered the cost of doing business and limited to improvements within and around the exterior of the property to be developed, not properties outside the control of the developer at other locations. Other property owners in neighborhoods within the city, who make improvements to their property, are not subject to the same financial burdens, which are not inexpensive by the way, as neighbors who do not already enjoy the amenities your planning division now find necessary. In short, the application of this development criteria and extra costs to residential units seems by many, to be ambiguous, discriminatory, overreaching, and a punitive tax on certain citizens.

The "in-lieu" program was last presented to the Neighborhood Association meeting by the former City Manager, It was not received well then, and such sentiments have produced little improvement to the muddle since.

The Neighbors are very aware there are others within the neighborhood who would find the addition of sidewalks, and improved streets desirable, particularly those residents in the newer additions, along the periphery of the existing Neighborhood Boundaries; and, there are some who would advocate viable alternatives such as an asphalt pathway along one side of some streets within the older sections of the Robinwood Neighborhood itself.

The purported thinking surrounding safety and remedies can be found on both sides of the street as equal good and bad conclusions have been advanced by both advocates of developed and undeveloped camps. In addition, we have been advised the Tualatin Valley Fire and Rescue officials may have expressed some interest in this program. Certainly their views need to be considered in any new development and accommodated to the extent practical in existing developments.

We would ask you to consider addressing these issues through the City Council and/or Planning Commission, and initiate a comprehensive and narrowly focused study; produce a neighborhood plan based upon a wide-array of Robinwood resident participation, and consider subsequent code revisions which might arise from the foregoing to address future developments and improvements to the neighborhood infrastructure (i.e. curb, sidewalks and street improvements).

Most importantly, we request you and the City Council cause a moratorium be placed into effect on those policies and procedures as described in Chapter 96 of the Community Development Code concerning the consideration of curbs, sidewalks and street improvements in the application for permits from the City, for remodeling and/or the construction of residences, within the Robinwood Neighborhood until the study, plan and subsequent code revisions are finalized.

In the past, for one reason or another, residents of Robinwood have considered these issues resolved, only to find the controversy rise once again. We believe it is time to put this dispute to a final resolution.

The active members of the Robinwood Association stand ready to assist you in this endeavor in any way that will prove helpful.

In this regard, we cordially invite you and/or your designee to address these issues at our next Robinwood Meeting on Tuesday, July 10, 2018 at 7pm. Given the importance of the points of contention we will defer all other neighborhood business to allow an appropriate amount of time for discussion and questions on these issues.

Page3 of 3  
The Honorable Russ Axelrod  
June 19, 2018

We look forward to your kind response and thank you for your continued support for our neighbors.

Sincerely yours,

/s/ Jim

James T. O'Toole  
President

Cc: The Honorable Brenda Perry  
Council President, West Linn City Council

The Honorable Robert Martin  
Member of the West Linn City Council

The Honorable Teri Cummings  
Member of the West Linn City Council

The Honorable Richard Sakelik  
Member of the West Linn City Council

Bcc: Robinwood Neighborhood Association Members

Robinwood Neighborhood Association Board Members

Jim O'Toole, President      Kazi Ahmed, Vice-president  
Jenne Henderson, Secretary      Christine Steel, Treasurer  
Sharon Pollmann, Ambassador

## Cover Sheet

Dennis Pollmann  
President - Robinwood Neighborhood Association  
Email: robinwoodna@westlinnoregon.gov

West Linn Special City Council Meeting Tuesday February 19, 2019  
Public Comment - Supporting documentation

## Robinwood Neighborhood Plan – Excerpt

### Vision Statement - Page 6, paragraph 7

*Residents have sidewalks where they want to have them and where they aid in keeping pedestrians and our children safe. Most of the neighborhood does not have sidewalks but does have safe walking/biking shoulders on the roadways. Shoulders and bike paths are important to the safety of all.*

### ROBINWOOD NEIGHBORHOOD PLAN - Goals and Policies – Page 8, Goal 3.3 & 3.4

3.3 Provide appropriate pedestrian facilities along residential streets.

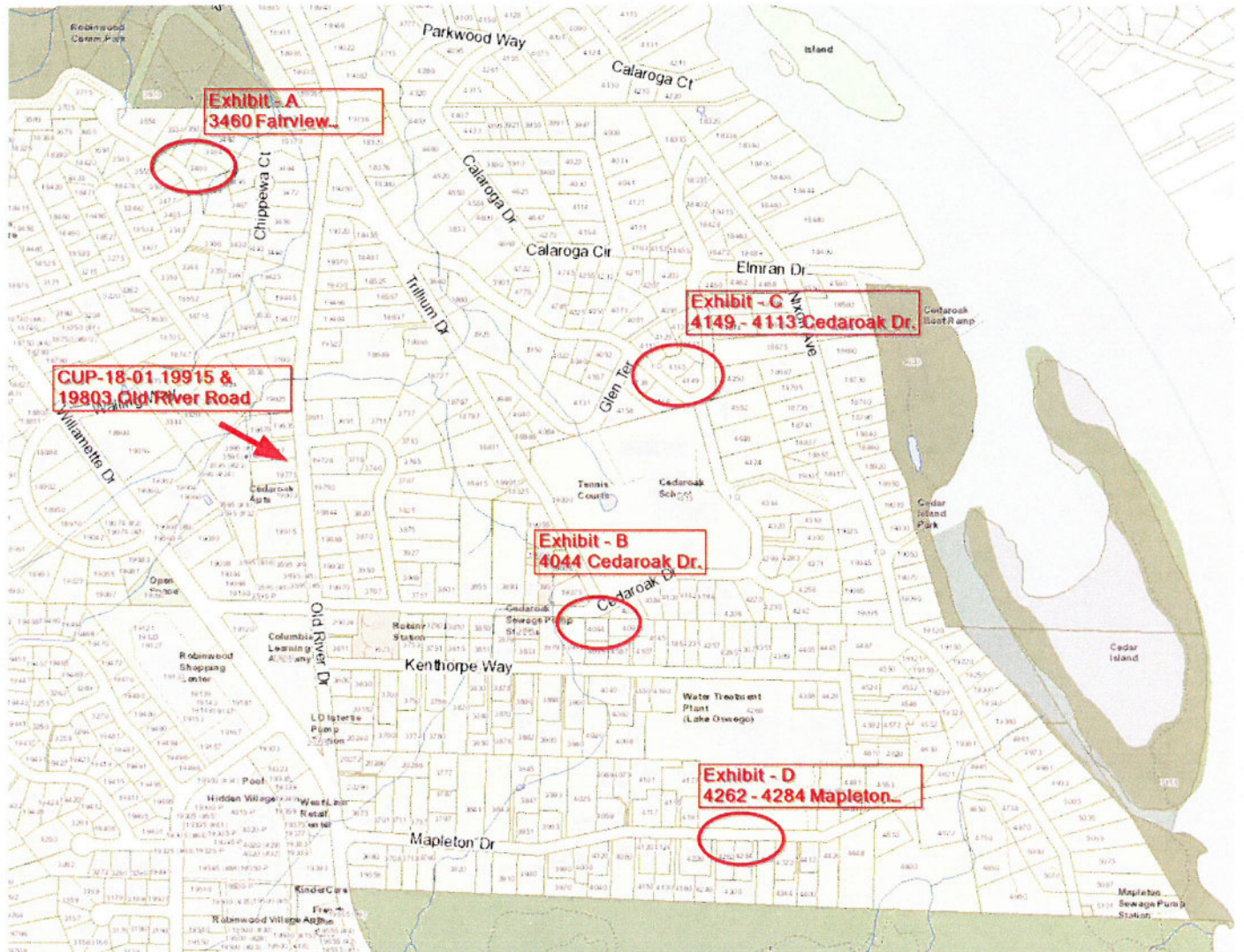
3.4 Implement “green street” concepts for residential streets.

### ROBINWOOD NEIGHBORHOOD PLAN - Action Measures – Page 15, section 3.3 & 3.4

|     |                                                                      | REGULATIONS FOR SINGLE-FAMILY HOMES                                                                                                                                               |                                |                                                                                                                                              |
|-----|----------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------|
| 3.3 | Provide appropriate pedestrian facilities along residential streets. | Provide sidewalks on streets near Cedaroak Elementary School for student safety                                                                                                   | City Planners, Property Owners | <ul style="list-style-type: none"> <li>• CDC Amendments</li> </ul>                                                                           |
|     |                                                                      | Provide sidewalks on streets leading to and from the Robinwood Main Street area                                                                                                   | City Planners, City Engineers  | <ul style="list-style-type: none"> <li>• Development Review Process</li> <li>• Cap. Imp. Projects List</li> </ul>                            |
|     |                                                                      | On streets with topographic or environmental constraints, accommodate pedestrians with grade separated asphalt paths in lieu of sidewalks on at least one side of the street.     | City Planners, City Engineers  | <ul style="list-style-type: none"> <li>• Development Review Process</li> <li>• Capital Improvement Projects List</li> </ul>                  |
| 3.4 | Implement “green street” concepts for residential streets.           | Amend the City Community Development Code and Engineering Standards to explicitly allow “green street” designs.                                                                   | City Planners, City Engineers  | <ul style="list-style-type: none"> <li>• Community Development Code Amendments</li> <li>• Engineering Design Standards Amendments</li> </ul> |
|     |                                                                      | Adopt a map of streets within the Robinwood Neighborhood where “green street” designs will be required for all new development and required when streets are improved by the City | City Planners, City Engineers  | <ul style="list-style-type: none"> <li>• Community Development Code Amendments</li> <li>• Capital Improvement Projects List</li> </ul>       |

# Locations of examples A,B, C and D

## Location of CUP-18-01 Marylhurst School



**Example – A**





**Example – B**



**Example – C**



Example – D



**Table 14: Pedestrian and Bicycle Projects**

| Map ID (Fig#)   | Type              | Project Name                                                     | Description                                                                                                                                                                                                                                                                | Estimated Cost           | Project Source                                                              | Comments                                                                            |
|-----------------|-------------------|------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------|-----------------------------------------------------------------------------|-------------------------------------------------------------------------------------|
| 3<br>(7C)       | Sidewalks         | Boca Ratan Sidewalk:<br>Atwater to Bonniebrae                    | 1,600' long, 5.5' wide curb-tight sidewalk along the east side. Existing curbs and storm drains. R/W required, parking will be removed. Completes a connection.                                                                                                            | \$342,000                | CIP, Citizen Request, 2002 TAB "top ten" List                               |                                                                                     |
| 4<br>(7B)       | Pathways          | Canyon Drive<br>Neighborhood Pathway:<br>Dellwood to South Shore | 1,150' long, 5' wide separated asphalt pathway on east side of road. Section between Park Rd and Greentree Rd is in an unconstructed R/W and will need heavy rock section to manage soft soils/natural springs. Adequate R/W. Transit connection on South Shore Blvd.      | \$548,000                | CIP, Palisades Neighborhood Plan, TMP                                       | NA confirmed project in compliance with Palisades Neighborhood Plan.                |
| 5<br>(7A & 7B)  | Bike & Pedestrian | Carman Drive Sidewalks and Bike Lanes:<br>Kruse Way to I-5       | 6,600' long, 6' wide bike lanes, 6' wide separated concrete sidewalks along 70% of length, both sides. Widen roadway from Kruse Way to I-5 (1.2 miles). Subject to road transfer from Clackamas County. Transit connection on Kruse Way and at I-5/Carman Dr. interchange. | \$3,710,000 <sup>1</sup> | CIP, Neighborhood Request, CC TSP                                           | NA concern that sidewalks and bike lanes are too wide.                              |
| 6<br>(7D)       | Pathways          | Cherry Lane Pathway:<br>Chapin Way to Hwy. 43                    | 750' long, 5' wide separated asphalt pathway on south side of road. Include 3'-5' gravel shoulder. Adequate R/W, vegetation removals. Completes connections to Hallinan Elementary School and TriMet bus stop. SRTS route.                                                 | \$89,000                 | CIP, Glenmorrie Neighborhood Plan, TAB Top 10 list                          | NA concern of stormwater impacts from additional impervious surfaces.               |
| 7<br>(7B)       | Pathways          | Childs Rd (west) Pathway:<br>Canal Rd to Sycamore Ave            | 1,550' long, 6' wide separated asphalt pathway on one side of the road. Include storm drain system to accommodate roadside ditch. Larger project listed on Clackamas County TSP. Traverses identified wetlands and tree groves.                                            | \$385,000 <sup>1</sup>   | CIP, Neighborhood Request, CC TSP                                           | NA noted this as high priority. Possible funding from parks.                        |
| 9<br>(7A)       | Sidewalks         | Daniel Way Sidewalk:<br>Kruse Woods Place to Carman Drive        | Replace gravel pathway with 800' long, 6' wide separated sidewalk on west side of road. Remove parking and rebuild landscape islands within West End Building campus. Completes a connection.                                                                              | \$200,000                | CIP, Holly Orchards Neighborhood Request, TMP                               | NA concern that sidewalks are too wide and would like further consultation.         |
| 10<br>(7C)      | Pathways          | E Avenue Pathway:<br>4th to 10th Street                          | 2,000' long, 5' wide separated asphalt pathway that includes storm water drainage improvements. SRTS route. Extends a connection. Part of Metro's Hillsdale to LO Regional Trail.                                                                                          | \$530,000                | CIP, First Addition Neighborhood Plan, Metro Regional, TAB Top 10 list, TMP | Mixed support by NA. Concern for stormwater issues. Not shown on neighborhood plan. |
| 11<br>(7B & 7D) | Pathways          | Fernwood Drive Pathway:<br>South Shore to Marjorie               | 3,300' long, 6' wide asphalt shoulder pathway on west/south side of road. Pathway could impact on-street parking and need extensive storm water system to pipe the ditch lines in the right-of-way. Extends a connection. Transit connection on South Shore.               | \$888,000                | CIP, Palisades Neighborhood Plan, TMP                                       | NA confirmed project in compliance with Palisades Neighborhood Plan.                |



| Map ID (Fig#) | Type              | Project Name                                                              | Description                                                                                                                                                                                                                                                      | Estimated Cost           | Project Source                                   | Comments                                                                                                      |
|---------------|-------------------|---------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------|--------------------------------------------------|---------------------------------------------------------------------------------------------------------------|
| 12 (7A)       | Sidewalks         | Fir Ridge Road Sidewalk: Twin Fir to Wembley Park Road                    | 900' long, 5' wide separated concrete sidewalk, 5' wide landscape strip on the north side of street. Existing curbs. Extends a connection.                                                                                                                       | \$111,000                | CIP, TMP                                         | NA confirmed project consistent with goals of Uplands Neighborhood Association Board.                         |
| 15 (7A & 7C)  | Pathways          | Goodall Road Pathway: Knaus to Country Club                               | 3,000' long, 6' wide asphalt shoulder pathway on the east side of road. R/W needed to accommodate swale. Completes a connection. Subject to road transfer from Clackamas County. Transit connection on Country Club.                                             | \$860,000                | CIP, TMP                                         |                                                                                                               |
| 16 (7B)       | Sidewalks         | Greentree Road Sidewalk: Fernwood to Westview                             | 2,400' long, 5.5' wide curb-tight concrete sidewalk. Use of shoulder areas would eliminate parking. Requires widening, retaining walls and storm drainage modifications.                                                                                         | \$682,000                | CIP, Palisades Neighborhood Plan, TMP            | NA confirmed project in compliance with Palisades Neighborhood Plan.                                          |
| 17 (7B)       | Pathways          | Kelok Pathway: South Shore to Bryant                                      | 1,000' long, 5' wide separated asphalt pathway, 5' landscape strip on one side of the road. Loss of on-street parking.                                                                                                                                           | \$1,586,000              | CIP, TMP                                         | Mixed support. Concern for loss of on-street parking. Option for pedestrian connection at Fir Rd to Cardinal. |
| 18 (7A)       | Pathways          | Kerr Pkwy (south) Pathway: McNary to Boones Ferry Rd                      | 1,400' long, 6' wide separated pathway along the east side. Would occur in cooperation with the Mountain park HOA; extends pathway built along common property. Crash history on Kerr. Transit connection at Kerr and Boones Ferry.                              | \$212,000                | CIP, Mountain Park Homeowner Association Request |                                                                                                               |
| 19 (7A & 7C)  | Bike & Pedestrian | Knaus Rd Pathway: Boones Ferry to Country Club                            | 4,000' long, 6' wide separated asphalt pathway, 5' wide bike lanes. Some sections subject to road transfer with Clackamas County. Closes many gaps created through annexation and redevelopment. Transit connection on Country Club. (See #209)                  | \$4,500,000              | CIP, TMP                                         |                                                                                                               |
| 20 (7A & 7B)  | Pathways          | Lakeview Blvd Pathway: Summit Drive to Iron Mtn. Blvd                     | 1,000' long, 5' wide attached asphalt pathway on east side of roadway. Includes crosswalk at Iron Mtn. roundabout and crossing over RR tracks. R/W required and an extensive (450' long, 20' high) retaining wall necessary; possible road reconstruction.       | \$1,556,000              | CIP, Citizen Request                             |                                                                                                               |
| 21 (7A)       | Pathways          | Lanewood/Douglas Circle Sidewalk & Pathway: Twin Fir to Boones Ferry Road | 600' long, 5' wide curb-tight sidewalk along Lanewood St to Boones Way, both sides.<br>1,600' long, 5' wide attached asphalt pathway from Boones Way to Twin Fir, one side; widening required.<br>Extends a connection. Transit connection on Boones Ferry Road. | \$358,000                | CIP, Lake Grove Neighborhood Association, TMP    |                                                                                                               |
| 22 (7D)       | Sidewalks         | Laurel Street Sidewalk: Cornell to Hallinan                               | 1,600' long, 6' wide curb-tight sidewalk. Closes gaps with existing sidewalks. 450' long retaining wall with guardrail, storm piping on both sides, and 4' roadway (shy) widening required. SRTS route.                                                          | \$1,305,000 <sup>3</sup> | CIP, Hallinan Neighborhood Association Request   | Supported by NA.                                                                                              |

| Map ID (Fig#) | Type      | Project Name                                                              | Description                                                                                                                                                                                                                                                                                                         | Estimated Cost | Project Source                                         | Comments                                                             |
|---------------|-----------|---------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------|--------------------------------------------------------|----------------------------------------------------------------------|
| 23 (7C)       | Pathways  | Library to Adult Community Center Pathway                                 | 530' long, 6' wide attached asphalt pathways on 4th St between E and F Ave (230') and on G Ave between 4th and 5th (200'). Project could be combined with the E Avenue Pathway project to provide connectivity if both facilities remain in the current location. Half-street (10' wide) roadway widening required. | \$312,000      | CIP, First addition Neighborhood Plan, TAB Top 10 list |                                                                      |
| 24 (7B)       | Sidewalks | Meadowlark Ln Sidewalk: Overlook Dr to Ridge Pointe Dr.                   | 700' long, 5.5' wide curb-tight sidewalk on east side of road. Extends a connection. Existing curb and gutter available. Remove some existing landscaping and trees. Rebuild driveway aprons. SRTS route.                                                                                                           | \$240,000      | CIP, Palisades Neighborhood Plan                       | NA confirmed project in compliance with Palisades Neighborhood Plan. |
| 25 (7A)       | Sidewalks | Mercantile Dr. Sidewalk: Hallmark to Boones Ferry Rd                      | 400' long, 5.5' wide curb-tight concrete sidewalks on both sides. Short retaining wall at back of walk with handrail, north side only. Remove all street trees, replace behind walk where space. Existing curbs. Completes a connection. Transit on Boones Ferry.                                                   | \$267,000      | CIP, Waluga Neighborhood Plan                          | NA does not support sidewalks on both sides or removal of any trees. |
| 26 (7D)       | Pathways  | Oak St Pathway: McVey to Palisades Terrace Drive                          | 1,000' long, 5' wide attached asphalt pathway on south side of street; completes connection. Area is fairly flat, R/W ok; Lost Dog Creek culvert may have to be extended and roadway widened. Transit connection on McVey.                                                                                          | \$132,000      | CIP, TMP                                               |                                                                      |
| 27 (7B)       | Sidewalks | Palisades Crest Dr Neighborhood Sidewalk: Hillside Dr to Cooks Butte Park | 1,100' long, 5.5' wide curb-tight concrete sidewalk along the northeast side (where streetlights already exist). Existing curb and gutter. Several utilities would need to be moved and some trees would be removed.                                                                                                | \$146,000      | CIP, Palisades Neighborhood Plan                       | NA confirmed project in compliance with Palisades Neighborhood Plan. |
| 28 (7C & 7D)  | Pathways  | Willamette River Greenway Trail: Roehr Park Pathway Repair/Upgrade        | 1,200' long, 10' wide separated asphalt pathway. Reconstruct and relocate existing 6' path to regional standard of 10' to avoid high water and resolve root heave issues. Part of Metro Regional Trail System (aka William Stafford Pathway). (See #86.)                                                            | \$243,000      | CIP, Parks Department                                  |                                                                      |
| 29 (7A & 7C)  | Sidewalks | Timberline Dr. Sidewalk: Knaus to Bonniebrae Drive                        | 2,700' long, 6.5' wide curb-tight sidewalk on Neighborhood Collector with existing curbs on 2/3 of length, one side only. Multiple driveway apron reconstructs; landscaping removal; mailbox relocations.                                                                                                           | \$454,000      | CIP, TMP                                               |                                                                      |
| 30 (7A & 7B)  | Pathways  | Twin Fir Pathway: Upper to Boones Ferry Road                              | 3,500' long, 5' wide asphalt shoulder pathway; include wide stripe with RPMs. Geotechnical studies, retaining walls, and storm drainage system required. Transit connection on Boones Ferry.                                                                                                                        | \$1,846,000    | CIP, TMP                                               |                                                                      |
| 31 (7B)       | Pathways  | Waluga Dr Pathway: Oakridge to Madrona                                    | 1,200' long, 5' wide separated asphalt pathway on east side of road. Driveway and fence rebuilds, numerous large tree removals. Completes a connection.                                                                                                                                                             | \$280,000      | CIP, Lake Forest Neighborhood Request, TMP             | NA does not support project if tree removal necessary.               |



| Map ID (Fig#)   | Type              | Project Name                                                      | Description                                                                                                                                                                                                                                                                                                                                                                                               | Estimated Cost           | Project Source                                    | Comments                                                                                                                                                 |
|-----------------|-------------------|-------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------|---------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------|
| 32 (7B)         | Pathways          | Washington Court Pathway: Roosevelt Avenue to Boones Ferry Road   | 3,200' long, 5' wide separated asphalt pathway, one side of road. R/W consistent on north side. R/W required, Lake Forest to Bonaire; some areas subject to road transfer from Clackamas County. Encroachment issues and tree removals required. No stormwater piping, structure adjustments only. Transit connection on Boones Ferry.                                                                    | \$488,000                | CIP, TMP                                          |                                                                                                                                                          |
| 33 (7B)         | Sidewalks         | Westview Dr. Sidewalk: Greentree Rd to South Shore                | 350' long, 5.5' wide curb-tight sidewalk along a very narrow section of roadway that would have to be brought up to City standards. Target west side of road. Due to steep banks on both sides, a large retaining wall would be needed. Cantilever driveways will need redesign; several trees need to be removed. Road needs complete geotechnical study and engineering design. Transit on South Shore. | \$881,000                | CIP, Palisades Neighborhood Plan, TMP             | NA confirmed project in compliance with Palisades Neighborhood Plan.                                                                                     |
| 34 (7C)         | Pathways          | Bridgeport to Milwaukie Regional Trail Bridge                     | Feasibility study for new bicycle and pedestrian trail and bridge connecting Lake Oswego via Tryon Cove Park to Milwaukie via Oakgrove. Option to utilize existing railroad bridge across the Willamette River. May study alternative alignments. Listed on Clackamas County TSP. (See #40, 87, 95)                                                                                                       | \$200,000 <sup>1</sup>   | CIP, Metro's Regional Transportation Plan, CC TSP |                                                                                                                                                          |
| 35 (7A, 7B, 7C) | Pathways          | Bridgeport to Milwaukie Regional Trail                            | Feasibility study for bicycle and pedestrian trail that follows the existing Pacific and Western railroad alignment from downtown Lake Oswego to I-5 using northern spur toward Tigard.                                                                                                                                                                                                                   | \$200,000 <sup>1</sup>   | CIP, Metro's Regional Transportation Plan, TMP    |                                                                                                                                                          |
| 36 (7A)         | Bike              | Boones Ferry Road Bike Lanes: Country Club to northern city limit | 3,500' long bike lanes on both sides. Includes extensive widening and retaining walls above and below the roadway grade. NHS/AASHTO standards apply.                                                                                                                                                                                                                                                      | \$5,713,000              | CIP, Metro's Regional Transportation Plan         |                                                                                                                                                          |
| 37 (7B)         | Bike & Pedestrian | Bryant Road Bike Lanes and Pathway: Childs Rd to Boones Ferry Rd  | 7,500' long, combo of bike lanes (widening), pathways, and striping on both sides of roadway. RR crossing reconstruction; retaining wall needed at X-ing. Coordinate with related TSP project: trail connection along canal to Tualatin River.                                                                                                                                                            | \$10,200,000             | CIP, Metro's Regional Transportation Plan         |                                                                                                                                                          |
| 38 (7C & 7D)    | Bike              | Highway 43 Bike Lanes: Terwilliger Blvd to Oak Street             | 5,500' long bike lanes, both sides. NHS/AASHTO/ODOT standards apply.                                                                                                                                                                                                                                                                                                                                      | \$7,587,000 <sup>1</sup> | CIP, Metro's Regional Transportation Plan         |                                                                                                                                                          |
| 39 (7A, 7B, 7C) | Bike & Pedestrian | Iron Mountain/Upper Drive Bike Lanes: 10th Street to Bryant Road  | 14,000' (2.65 mi) long bike lanes, both sides. Widening needed to accommodate bike lanes and pathways. Transit connection at 10th/A Ave. Alternate route for pedestrians along future Iron Mountain Park Trail.                                                                                                                                                                                           | \$6,682,000              | CIP, Metro's Regional Transportation Plan         | NA would like to dovetail this project with Iron Mountain Park improvements, address safety issues with guardrail, and enhance intersection with Summit. |

| Map ID [Fig#]   | Type      | Project Name                                                               | Description                                                                                                                                                                                                                                                                                                        | Estimated Cost         | Project Source                                                       | Comments                                                                                                                                                                                                                 |
|-----------------|-----------|----------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------|----------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 40<br>(7C)      | Pathways  | Willamette River Greenway Trail:<br>Lake Oswego to Portland<br>(via OR 43) | Conduct a refinement study for a 16,600' long (3.15 mi) trail that follows along Highway 43 and connects downtown Lake Oswego to Portland. Alternate route along Willamette Shore Line (Trolley) Trail alignment (See #95). Continuation of Highway 43 Pathway (See #142). (City cost is 10% if grants available). | \$100,000 <sup>1</sup> | CIP, Metro's Regional Transportation Plan, Connecting Clackamas Plan | Project does not yet have consensus with agency partners regarding location/alignment.                                                                                                                                   |
| 41<br>(7A)      | Sidewalks | Douglas Way Sidewalk:<br>Quarry to Hallmark                                | 900' long, 6' wide separated sidewalk with curb, 5' landscape strip on south side of road. Completes a connection. Several driveway rebuilds. SRTS route. (See #157)                                                                                                                                               | \$360,000              | CIP, Waluga Neighborhood Plan, LGVCP                                 | NA no longer supports this project, despite Waluga NA Plan document. A pathway is preferred.                                                                                                                             |
| 42<br>(7C & 7D) | Sidewalks | Durham Street Sidewalk:<br>Alley to Ladd                                   | 100' long, 5' wide separated sidewalk to connect existing sidewalk to George Rogers Park. Suggest pavers to flex and protect adjacent large trees (roots).                                                                                                                                                         | \$25,000               | NP, Old Town Neighborhood Plan                                       | NA agrees with option to utilize pavers to protect trees. Request to utilize trench drains for stormwater that are pedestrian-friendly.                                                                                  |
| 43<br>(7C)      | Pathways  | Lake Forest Dr Pathway:<br>Ellis Ave to Berwick Rd                         | 1,300' long, 5' wide separated asphalt pathway along the south side of the roadway to connect with proposed Berwick Road Pathway. Flat terrain, space already being used as pathway/parking. Removal of parking.                                                                                                   | \$170,000              | NP, Evergreen Neighborhood Plan                                      | Neighborhood notes high priority for neighborhood due to heavy pedestrian use and speeding concerns. Consider realignment of Ellis/Lake Forest, and STOP sign on Ellis, eastbound. Citizen concerns over loss of parking |
| 44<br>(7C)      | Pathways  | Berwick Road Pathway:<br>Berwick Ct. to Ellis Ave                          | 200' long, 5' wide separated asphalt pathway on east side of roadway to connect with proposed Lake Forest Dr Pathway. Flat terrain, space already being used as pathway/parking. Removal of parking.                                                                                                               | \$36,000               | NP, Evergreen Neighborhood Plan, TMP                                 | Neighborhood notes high priority for neighborhood due to heavy pedestrian use.                                                                                                                                           |
| 45<br>(7C)      | Pathways  | Evergreen Road Pathway:<br>4th to 10th                                     | 1,400' long, 5' wide separated asphalt pathway on north side of roadway. Completes a connection. Requires widening; removal of on-street parking.                                                                                                                                                                  | \$173,000              | NP, Evergreen Neighborhood Plan, TMP                                 | Neighborhood notes high priority due to heavy pedestrian use. Connects to completed section along Our Lady of the Lake. Citizen concerns regarding loss of landscaping.                                                  |
| 46<br>(7B)      | Sidewalks | Treetop Ln Sidewalk:<br>Overlook to Fernwood Dr                            | 1,300' long, 5.5' curb-tight concrete sidewalk on east side of roadway. Extends pedestrian route north. Existing curbs on both sides.                                                                                                                                                                              | \$176,000              | NP, Palisades Neighborhood Plan, TMP                                 | NA confirmed project in compliance with Palisades Neighborhood Plan.                                                                                                                                                     |





| Map ID (Fig#) | Type      | Project Name                                                | Description                                                                                                                                                                                                                                                                                      | Estimated Cost           | Project Source                       | Comments                                                                                                                                                                                                                               |
|---------------|-----------|-------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------|--------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 47 (7B)       | Sidewalks | Hillside Dr Sidewalk: Palisades Crest Dr to Fernwood Circle | 1,600' long, 5.5' wide curb-tight concrete sidewalk on one side of roadway. Initiates a route. Existing curbs on both sides.                                                                                                                                                                     | \$180,000                | NP, Palisades Neighborhood Plan      | NA confirmed project in compliance with Palisades Neighborhood Plan.                                                                                                                                                                   |
| 48 (7B)       | Pathways  | Fernwood Circle Pathway: Fernwood Drive to Fernwood Drive   | 1,400' long, 5' wide separated asphalt pathway on one side of street (north and west side) to connect with proposed Hillside Dr Sidewalk and Fernwood Dr Pathway. (no curbs on Fernwood Cir.)                                                                                                    | \$389,000                | NP, Palisades Neighborhood Plan      | NA confirmed project in compliance with Palisades Neighborhood Plan.                                                                                                                                                                   |
| 50 (7B)       | Sidewalks | Roosevelt Ave Sidewalk: Washington Ct to Harrington         | 1,600' long, 6' wide curb-tight concrete sidewalk on one side of roadway, reconstruct portion of roadway. Add sanitary sewer and stormwater infrastructure along entire length. Subject to road transfer from Clackamas County. Initiates a connection, may connect to Washington Court Pathway. | \$1,374,000              | NP, Lake Forest Neighborhood Request |                                                                                                                                                                                                                                        |
| 53 (7B & 7D)  | Sidewalks | Cloverleaf Dr Sidewalk: Banyan to Fernwood Dr               | 1,800' long, 5.5' wide curb-tight concrete sidewalk along school property. Existing curbs on both sides. Minor tree and embankment issues, meander sidewalk around trees. Initiates a connection, may connect to Fernwood Dr. Pathway.                                                           | \$319,000                | NP, Palisades Neighborhood Plan      | NA confirmed project in compliance with Palisades Neighborhood Plan.                                                                                                                                                                   |
| 58 (7C)       | Sidewalks | E Ave Sidewalk: State Street to 1 <sup>st</sup> Street      | 600' long, 5.5' wide curb-tight concrete sidewalk, preferably on north side. Coordinate ex. curb shift to accommodate sidewalk, on-street parking mitigation. Completes a connection. Connects to Metro's Hillsdale to LO Regional Trail.                                                        | \$132,000                | NP, First Addition Neighborhood Plan |                                                                                                                                                                                                                                        |
| 73 (7A)       | Pathways  | Kerr Pkwy (north) Pathway: PCC to Multnomah. Co line        | 2,700' long, 8' wide separated asphalt pathway, 3' landscape strip on west/south side of roadway; create ADA accessible route at Jefferson Pkwy.                                                                                                                                                 | \$1,023,000              | 1997 TSP                             |                                                                                                                                                                                                                                        |
| 74 (7C)       | Pathways  | Hwy 43 Buffered Pathway: Public Storage to Briarwood        | 2,000' striping to create pathway (buffer), east side only. Coordinate with ODOT and Clackamas County. Major widening/retaining wall/guardrail needed between Public Storage and Stampher Road (~300' long); otherwise, plenty of existing shoulder pavement. NHS/AASHTO/ODOT standards apply.   | \$2,225,000 <sup>2</sup> | 1997 TSP                             |                                                                                                                                                                                                                                        |
| 75 (7B)       | Pathways  | Lakeview Blvd Pathway: Jean Rd to 65th Ave                  | 2,300' long and 5' wide separated asphalt pathway or sidewalk, likely on north/west side of roadway. Install storm drain system. May involve loss of parking and minor vegetation removal. Completes a connection.                                                                               | \$563,000                | 1997 TSP                             | Neighborhood notes the undesirable skew, especially for trucks; request realignment of Lakeview at Jean Road. Citizens noted concern of truck traffic diverting off, using Kenny. Citizen desire to retain trees by adjusting designs. |

**Table 4: Pedestrian Plan Improvement Projects**

| Map ID | Location                | Type      | Project Description                                                                                                                           | Priority | Cost (\$1,000) |
|--------|-------------------------|-----------|-----------------------------------------------------------------------------------------------------------------------------------------------|----------|----------------|
| P1     | 4 <sup>th</sup> Avenue  | Sidewalks | Install sidewalks on the south side of the roadway from 14 <sup>th</sup> Street to 12 <sup>th</sup> Street                                    | High     | \$100          |
| P2     | 5 <sup>th</sup> Avenue  | Sidewalks | Install sidewalks on the north side of the roadway from 11 <sup>th</sup> Street to 7 <sup>th</sup> Street                                     | High     | \$250          |
| P3     | 5 <sup>th</sup> Avenue  | Sidewalks | Install sidewalks on the south side of the roadway from 25 feet west of 8th Street to 150 feet east of 8th Street                             | High     | \$25           |
| P4     | 8 <sup>th</sup> Avenue  | Sidewalks | Install sidewalks on the south side of the roadway from 12 <sup>th</sup> Street to 400 feet east of 12 <sup>th</sup> Street                   | High     | \$55           |
| P5     | 13 <sup>th</sup> Street | Sidewalks | Install sidewalks on the east side of the roadway from 100 feet north of Tualatin Avenue to Tualatin Avenue                                   | High     | \$15           |
| P6     | Bittner Street          | Sidewalks | Install sidewalks on the east side of the roadway from Oxford Street to Long Street                                                           | High     | \$180          |
| P7     | Blankenship Road        | Sidewalks | Install sidewalks on the north side of the roadway from 10th Street to approximately 50 feet east of the Willamette Corporate Center driveway | High     | \$65           |
| P8     | Blankenship Road        | Sidewalks | Install sidewalks on the north side of the roadway from approximately 400 feet west of Debok Road to Johnson Road                             | High     | \$90           |
| P9     | Blankenship Road        | Sidewalks | Install sidewalks on the south side of the roadway from 19th Street to approximately 175 feet east of Ostman Road                             | High     | \$110          |
| P10    | Bonnet Drive            | Sidewalks | Install sidewalks on the west side of the roadway from Oregon City Boulevard to Oxford Street                                                 | High     | \$50           |
| P11    | Caufield Street         | Sidewalks | Install sidewalks on both sides of the roadway from Tompkins Street to Randall Street                                                         | High     | \$80           |
| P12    | Cedar Oak Drive         | Sidewalks | Install sidewalks on both sides of the roadway from Old River Drive to 200 feet west of Trillium Drive                                        | High     | \$140          |
| P13    | Cedar Oak Drive         | Sidewalks | Install sidewalks on the north side of the roadway from 200 feet west of Trillium Drive to Trillium Drive                                     | High     | \$25           |
| P14    | Cedar Oak Drive         | Sidewalks | Install sidewalks on the south/east side of the roadway from Trillium Drive to Elmran Drive                                                   | High     | \$200          |
| P15    | Cornwall Street         | Sidewalks | Install sidewalks on both sides of the roadway from Oxford Street to Sunset Avenue                                                            | High     | \$355          |
| P16    | Davenport Street        | Sidewalks | Install sidewalks on both sides of the roadway from Randall Street to Buck Street                                                             | High     | \$65           |
| P17    | Exeter Street           | Sidewalks | Install sidewalks on the both sides of the roadway from Lancaster Street to Sunset Avenue                                                     | High     | \$150          |
| P18    | Exeter Street           | Sidewalks | Install sidewalks on the east side of the roadway from Long Street to Lancaster Avenue                                                        | High     | \$25           |
| P19    | Exeter Street           | Sidewalks | Install sidewalks on the west side of the roadway from Oxford Street to Long Street                                                           | High     | \$90           |
| P20    | Hidden Springs Road     | Sidewalks | Install sidewalks on the south side of the roadway from Carriage Way to Cottonwood Court                                                      | High     | \$145          |
| P21    | Holmes Street           | Sidewalks | Install sidewalks on the west side of the roadway from Buck Street to Perrin Street                                                           | High     | \$60           |
| P22    | Lancaster Street        | Sidewalks | Install sidewalks on the south side of the roadway from Parker Road to Cornwall Street                                                        | High     | \$110          |
| P23    | Lancaster Street        | Sidewalks | Install sidewalks on the north side of the roadway from approximately 175 feet east of Parker Road to Cornwall Street                         | High     | \$90           |
| P24    | Long Street             | Sidewalks | Install sidewalks on both sides of the roadway from Bittner Street to Simpson Street                                                          | High     | \$90           |

| Map ID | Location                   | Type      | Project Description                                                                                                                                  | Priority | Cost (\$1,000) |
|--------|----------------------------|-----------|------------------------------------------------------------------------------------------------------------------------------------------------------|----------|----------------|
| P25    | Long Street                | Sidewalks | Install sidewalks on the north side of the roadway from 125 feet east of Simpson Street to 250 feet east of Simpson Street                           | High     | \$115          |
| P26    | Lowry Drive/Barclay Street | Sidewalks | Install sidewalks on both sides of the roadway from Dillow Drive to Tompkins Street                                                                  | High     | \$305          |
| P27    | Oregon City Boulevard      | Sidewalks | Install sidewalks on the north side of the roadway from Bonnet Drive to 350 feet east of Prospect Street                                             | High     | \$135          |
| P28    | Oxford Street              | Sidewalks | Install sidewalks on the south side of the roadway from Bonnet Drive to Sussex Street                                                                | High     | \$35           |
| P29    | Oxford Street              | Sidewalks | Install sidewalks on the south side of the roadway from Exeter Street to Bittner Street                                                              | High     | \$50           |
| P30    | Parker Road                | Sidewalks | Install sidewalks on both sides of the roadway from approximately 125 feet east of Noble Lane to approximately 100 feet west of Dillon Lane          | High     | \$155          |
| P31    | Parker Road                | Sidewalks | Install sidewalks on the north side of the roadway from approximately 150 feet east of Wild Rose Drive to 475 feet east of Wild Rose Drive           | Medium   | \$75           |
| P32    | Parker Road                | Sidewalks | Install sidewalks on the north side of the roadway from approximately 150 west of Damon Drive to 75 feet west of Chinook Court                       | Medium   | \$70           |
| P33    | Perrin Street              | Sidewalks | Install sidewalks on both sides of the roadway from Holmes Street to Lewis Street                                                                    | High     | \$290          |
| P34    | Prospect Street            | Sidewalks | Install sidewalks on the east side of the roadway from Knox Street to Oregon City Boulevard                                                          | High     | \$135          |
| P35    | Prospect Street            | Sidewalks | Install sidewalks on the west side of the roadway from 125 feet south of Knox Street to Oregon City Boulevard                                        | High     | \$115          |
| P36    | Randall Street             | Sidewalks | Install sidewalks on both sides of the roadway from Caufield Street to Davenport Street                                                              | High     | \$65           |
| P37    | Salamo Road                | Sidewalks | Install sidewalks on the west side of the roadway from approximately 750 feet south of Remington Drive to Barrington Drive                           | High     | \$70           |
| P38    | Salamo Road                | Sidewalks | Install sidewalks on the north side of the roadway from Barrington Drive to 10 <sup>th</sup> Street                                                  | High     | \$380          |
| P39    | Santa Anita Drive          | Sidewalks | Install sidewalks on the east side of the roadway from Hidden Springs Road to Clubhouse Circle                                                       | High     | \$40           |
| P40    | Santa Anita Drive          | Sidewalks | Install sidewalks on the east side of the roadway from approximately 250 feet south of Clubhouse Circle to Pimlico Drive                             | High     | \$50           |
| P41    | Simpson Street             | Sidewalks | Install sidewalks on both sides of the roadway from Long Street to Charman Street                                                                    | High     | \$415          |
| P42    | Skyline Drive              | Sidewalks | Install sidewalks on the north side of the roadway from Summit Street to approximately 150 feet west of Firwood Drive                                | High     | \$55           |
| P43    | Skyline Drive              | Sidewalks | Install sidewalks on the north side of the roadway from approximately 100 feet east of Firwood Drive to approximately 150 feet west of West A Street | High     | \$450          |
| P44    | Summit Street              | Sidewalks | Install sidewalks on the west side of the roadway from approximately 150 feet south of Skyline Drive to Rosemont Road                                | High     | \$40           |
| P45    | Summit Street              | Sidewalks | Install sidewalks on the west side of the roadway from approximately 150 feet south of Rosemont Road to Oxford Street                                | High     | \$90           |
| P46    | Summit Street              | Sidewalks | Install sidewalks on the east side of the roadway from Gloria Drive to Oxford Street                                                                 | High     | \$230          |
| P47    | Summit Street              | Sidewalks | Install sidewalks on both sides of the roadway from Pimlico Drive to 150 feet south of Pimlico Drive                                                 | High     | \$25           |

| Map ID | Location                | Type      | Project Description                                                                                                                                                                                                                         | Priority | Cost (\$1,000) |
|--------|-------------------------|-----------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------|----------------|
| P48    | Summit Street           | Sidewalks | Fill in the 65-foot gap in the sidewalk on the north side of roadway at approximately 350 feet south of Pimlico Drive                                                                                                                       | High     | \$5            |
| P49    | Sunset Avenue           | Sidewalks | Install sidewalks on the north side of the roadway from Cornwall Street to Willamette Falls Drive                                                                                                                                           | High     | \$595          |
| P50    | Sunset Avenue           | Sidewalks | Install sidewalks on the south side of the roadway from Cornwall Street to approximately 150 feet west of Spring Rock Circle                                                                                                                | High     | \$210          |
| P51    | Sussex Street           | Sidewalks | Install sidewalks on both sides of the roadway from Oxford Street to Sunset Avenue                                                                                                                                                          | High     | \$350          |
| P52    | Tompkins Street         | Sidewalks | Install sidewalks on both sides of the roadway from Lowry Drive to Caufield Street                                                                                                                                                          | High     | \$90           |
| P53    | Trillium Drive          | Sidewalks | Install sidewalks on both sides of the roadway from Glen Terrace to 700 feet south of Glen Terrace                                                                                                                                          | High     | \$320          |
| P54    | West A Street           | Sidewalks | Install sidewalks on both sides of the roadway from approximately 250 feet east of Willamette Drive to Terrace Drive                                                                                                                        | High     | \$350          |
| P55    | West A Street           | Sidewalks | Install sidewalks on the north side of the roadway from Terrace Drive to Skyline Drive                                                                                                                                                      | High     | \$35           |
| P56    | Willamette Falls Drive  | Sidewalks | Install sidewalks on the south side of the roadway from West A Street to Sunset Avenue                                                                                                                                                      | High     | \$300          |
| P57    | Willamette Falls Drive  | Sidewalks | Install sidewalks on the south side of the roadway from Sunset Avenue to 10 <sup>th</sup> Street                                                                                                                                            | High     | \$2,565        |
| P58    | Willamette Falls Drive  | Sidewalks | Install sidewalks on the north side of the roadway from Dollar Street (east) to 19 <sup>th</sup> Street                                                                                                                                     | High     | \$195          |
| P59    | Willamette Falls Drive  | Sidewalks | Install sidewalks on the north side of the roadway from Epperly Way to West City Limits                                                                                                                                                     | High     | \$290          |
| P60    | Willamette Falls Drive  | Sidewalks | Install sidewalks on the south side of the roadway from 16th Street to 200 feet west of 16th Street                                                                                                                                         | High     | \$25           |
| P61    | Willamette Falls Drive  | Sidewalks | Install sidewalks on the south side of the roadway from approximately 500-feet east of 19 <sup>th</sup> Street to approximately 150-feet west of 19 <sup>th</sup> Street and from approximately 200-feet east of Ostman Road to Ostman Road | High     | \$185          |
| P62    | Willamette Falls Drive  | Sidewalks | Install sidewalks on the south side of the roadway from Ostman Road to West City Limits                                                                                                                                                     | High     | \$465          |
| P63    | 19 <sup>th</sup> Street | Sidewalks | Install sidewalks on the west side of the roadway from Blankenship Road to Nova Court                                                                                                                                                       | Medium   | \$135          |
| P64    | 19 <sup>th</sup> Street | Sidewalks | Install sidewalks on both sides of the roadway from Nova Court to Dollar Street                                                                                                                                                             | Medium   | \$195          |
| P65    | 19 <sup>th</sup> Street | Sidewalks | Install sidewalks on both sides of the roadway from Dollar Street to High Touch Court                                                                                                                                                       | Medium   | \$140          |
| P66    | 19 <sup>th</sup> Street | Sidewalks | Install sidewalks on the west side of the roadway from High Touch Street to Dollar Street                                                                                                                                                   | Medium   | \$60           |
| P67    | Bland Circle            | Sidewalks | Install sidewalks on the north side of the roadway from Salamo Road to Tannler Drive                                                                                                                                                        | Medium   | \$95           |
| P68    | Bland Circle            | Sidewalks | Install sidewalks on the north side of the roadway from Tannler Drive to approximately 100 feet east of Falcon Drive                                                                                                                        | Medium   | \$55           |
| P69    | Bland Circle            | Sidewalks | Install sidewalks on the north side of the roadway from Falcon Drive to approximately 400 feet north of Fircrest Drive                                                                                                                      | Medium   | \$230          |
| P70    | Carriage Way            | Sidewalks | Install sidewalks on the north-west side of the roadway from approximately 350 feet west of Suncrest Drive to Rosemont Road                                                                                                                 | Medium   | \$265          |

| Map ID | Location            | Type      | Project Description                                                                                                                 | Priority | Cost (\$1,000) |
|--------|---------------------|-----------|-------------------------------------------------------------------------------------------------------------------------------------|----------|----------------|
| P71    | Clark Street        | Sidewalks | Install sidewalks on both sides of the roadway from Skyline Drive to approximately 150 feet north of Windsor Boulevard              | Medium   | \$475          |
| P72    | Failing Street      | Sidewalks | Install sidewalks on the east side of the roadway from approximately 200-feet north of Highway 43 to Buck Street                    | Medium   | \$65           |
| P73    | Fairview Way        | Sidewalks | Install sidewalks on both sides of the roadway from approximately 200-feet east of Highway 43 to approximately 100-west of Rose Way | Medium   | \$135          |
| P74    | Fairview Way        | Sidewalks | Install sidewalks on the south side of the roadway from approximately 100-feet west of Rose Way to Chippewa Court                   | Medium   | \$55           |
| P75    | Fairview Way        | Sidewalks | Install sidewalks on both sides of the roadway from Chippewa Court to the roadway terminus                                          | Medium   | \$175          |
| P76    | Hidden Springs Road | Sidewalks | Install sidewalks on the south side of the roadway from Suncrest Drive to Santa Anita Drive (Maintain existing curb line)           | Medium   | \$80           |
| P77    | Holly Street        | Sidewalks | Install sidewalks on both sides of the roadway from approximately 150-feet east of Highway 43 to River Street                       | Medium   | \$620          |
| P78    | Johnson Road        | Sidewalks | Install sidewalks on west side of the roadway from Blankenship Road to Western City Limits                                          | Medium   | \$390          |
| P79    | Lewis Street        | Sidewalks | Install sidewalks on both sides of the roadway from Highway 43 to Perkins Street                                                    | Medium   | \$305          |
| P80    | Marylhurst Drive    | Sidewalks | Install sidewalks on one side of the roadway from Willamette Drive to Hillcrest Drive (East)                                        | High     | \$340          |
| P81    | Old River Drive     | Sidewalks | Install sidewalks on the east side of the roadway from approximately 100 feet north of Riverside Court to Cedar Oak Drive           | Medium   | \$550          |
| P82    | Old River Drive     | Sidewalks | Install sidewalks on the west side of the roadway from approximately 200 feet north of Riverside Court to Cedar Oak Drive           | Medium   | \$475          |
| P83    | Ostman Road         | Sidewalks | Install sidewalks on the east side of the roadway from Blankenship Road to Michael Drive                                            | Medium   | \$55           |
| P84    | Ostman Road         | Sidewalks | Install sidewalks on both sides of the roadway from Michael Drive to Fields Drive-Short Street                                      | Medium   | \$85           |
| P85    | Ostman Road         | Sidewalks | Install sidewalks on both sides of the roadway from Dollar Street to Willamette Falls Drive                                         | Medium   | \$330          |
| P86    | Pimlico Drive       | Sidewalks | Install sidewalks on the south side of the roadway from Santa Anita Drive to approximately 100 feet west of Palomino Way (west)     | Medium   | \$95           |
| P87    | Pimlico Drive       | Sidewalks | Install sidewalks on the south side of the roadway from Palomino Way (east) to Pimlico Terrace                                      | Medium   | 25             |
| P88    | Pimlico Drive       | Sidewalks | Install sidewalks on both sides of the roadway from Pimlico Terrace to Treetop Lane                                                 | Medium   | \$165          |
| P89    | Pimlico Drive       | Sidewalks | Install sidewalks on the south side of the roadway from Treetop Lane to Willamette Drive                                            | Medium   | \$30           |
| P90    | Rosemont Road       | Sidewalks | Install sidewalks on the south side of the roadway from Santa Anita Drive to Wild Rose Drive                                        | High     | \$250          |
| P91    | Rosemont Road       | Sidewalks | Install sidewalks on both sides of the roadway from Shannon Lane to Summit Street                                                   | Medium   | \$540          |
| P92    | Shady Hollow Way    | Sidewalks | Install sidewalks on the south side of the roadway from approximately 150-feet east of Highway 43 to Arbor Drive                    | Medium   | \$230          |

| Map ID                              | Location               | Type      | Project Description                                                                                                                         | Priority | Cost (\$1,000)  |
|-------------------------------------|------------------------|-----------|---------------------------------------------------------------------------------------------------------------------------------------------|----------|-----------------|
| P93                                 | Suncrest Drive         | Sidewalks | Install sidewalks on the east side of the roadway from approximately 250 feet south of Ridgebrook Drive (north) to Ridgebrook Drive (north) | High     | \$70            |
| P94                                 | Suncrest Drive         | Sidewalks | Install sidewalks on the east side of the roadway from approximately 150 feet north of Ridgebrook Drive (north) to Hillcrest Drive          | High     | \$135           |
| P95                                 | Suncrest Drive         | Sidewalks | Install sidewalks on the west side of the roadway from approximately 250 feet north of Ridgebrook Drive (north) to Hillcrest Drive          | High     | \$135           |
| P96                                 | Tannler Drive          | Sidewalks | Install sidewalks on both sides of the roadway from Blankenship Road to Greene Street                                                       | Medium   | \$235           |
| P97                                 | Clark Street           | Interim   | Install a mixed use shoulder on one side of the roadway from Skyline Drive to approximately 150 feet north of Windsor Boulevard             | Low      | \$185           |
| P98                                 | Johnson Road           | Interim   | Install a mixed use shoulder on one side of the roadway from Blankenship Road to Western City Limits                                        | Low      | \$305           |
| P99                                 |                        |           | Intentionally left blank.                                                                                                                   |          |                 |
| P100                                | Old River Drive        | Interim   | Install a mixed-use shoulder on the east side of the roadway from the northern City limits to Cedar Oak Drive                               | Low      | \$475           |
| P101                                | Hillcrest Drive        | Sidewalks | Install sidewalks on one side of the roadway from Suncrest Drive to Marylhurst Drive                                                        | High     | \$300           |
| P102                                | Willamette Falls Drive | Interim   | Improve pedestrian crossing at Fields Bridge Park                                                                                           | High     | \$20            |
| P103                                | Hidden Springs Road    | Interim   | Improve pedestrian crossing at Suncrest Drive                                                                                               | High     | \$20            |
| P104                                | Citywide               | Study     | Develop citywide policy and methodology to address pedestrian crossing improvements                                                         | High     | \$50            |
| Total High Priority Project Costs   |                        |           |                                                                                                                                             |          | \$13,110        |
| Total Medium Priority Project Costs |                        |           |                                                                                                                                             |          | \$6,435         |
| Total Low Priority Project Costs    |                        |           |                                                                                                                                             |          | \$660           |
| <b>Total Project Costs</b>          |                        |           |                                                                                                                                             |          | <b>\$20,205</b> |

Additional pedestrian improvement projects along the Highway 43 and 10<sup>th</sup> Street corridors are included with the motor vehicle projects.



**EXHIBIT CC-3: AFFIDAVIT AND NOTICE PACKET**



# AFFIDAVIT OF NOTICE

We, the undersigned do hereby certify that, in the interest of the party (parties) initiating a proposed land use, the following took place on the dates indicated below:

## GENERAL

File No. AP-19-02 Applicant's Name Sheila Walker, The Marylhurst School  
Development Name \_\_\_\_\_  
Scheduled Meeting/Decision Date 4-8-19

**NOTICE:** Notices were sent at least 20 days prior to the scheduled hearing, meeting, or decision date per Section 99.080 of the Community Development Code. (check below)

## TYPE A

A. The applicant (date) 3-19-19 (signed) S. Shroyer  
B. Affected property owners (date) 3-19-19 (signed) S. Shroyer  
C. School District/Board (date) \_\_\_\_\_ (signed) \_\_\_\_\_  
D. Other affected gov't. agencies (date) \_\_\_\_\_ (signed) \_\_\_\_\_  
E. Affected neighborhood assns. (date) 3-19-19 (ALL) (signed) S. Shroyer  
F. All parties to an appeal or review (date) 3-19-19 (signed) S. Shroyer

At least 10 days prior to the scheduled hearing or meeting, notice was published/posted:

Tidings (published date) 3-28-19 (signed) S. Shroyer  
City's website (posted date) 3-19-19 (signed) S. Shroyer

## SIGN

At least 10 days prior to the scheduled hearing, meeting or decision date, a sign was posted on the property per Section 99.080 of the Community Development Code.

(date) 3/28/2019 (signed) Dan S Wynn

**NOTICE:** Notices were sent at least 14 days prior to the scheduled hearing, meeting, or decision date per Section 99.080 of the Community Development Code. (check below)

## TYPE B

A. The applicant (date) \_\_\_\_\_ (signed) \_\_\_\_\_  
B. Affected property owners (date) \_\_\_\_\_ (signed) \_\_\_\_\_  
C. School District/Board (date) \_\_\_\_\_ (signed) \_\_\_\_\_  
D. Other affected gov't. agencies (date) \_\_\_\_\_ (signed) \_\_\_\_\_  
E. Affected neighborhood assns. (date) \_\_\_\_\_ (signed) \_\_\_\_\_

Notice was posted on the City's website at least 10 days prior to the scheduled hearing or meeting.  
Date: \_\_\_\_\_ (signed) \_\_\_\_\_

**STAFF REPORT** mailed to applicant, City Council/Planning Commission and any other applicable parties 10 days prior to the scheduled hearing.

(date) 3/28/2019 (signed) Dan S Wynn

**FINAL DECISION** notice mailed to applicant, all other parties with standing, and, if zone change, the County surveyor's office.

(date) \_\_\_\_\_ (signed) \_\_\_\_\_

**CITY OF WEST LINN  
CITY COUNCIL  
PUBLIC HEARING NOTICE  
FILE NO. AP-19-02**

The West Linn City Council will hold a public hearing on **Monday, April 8, 2019**, starting at **6:30 p.m.** in the Council Chambers of City Hall, 22500 Salamo Road, West Linn. The public hearing will be on a proposal approved by the Planning Commission on February 11, 2019 for a Conditional Use Permit, Class II Design Review, and two Class II Variances for the redevelopment of an existing church into a private school serving pre-school through 8<sup>th</sup> Grade at 19915 Old River Drive (File # CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03).

At its meeting on February 19, 2019, the City Council moved to approve a review of the unanimously approved decision. Council requested a review of this decision pursuant to Community Development Code Chapter 99.240 through 99.300.

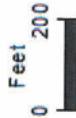
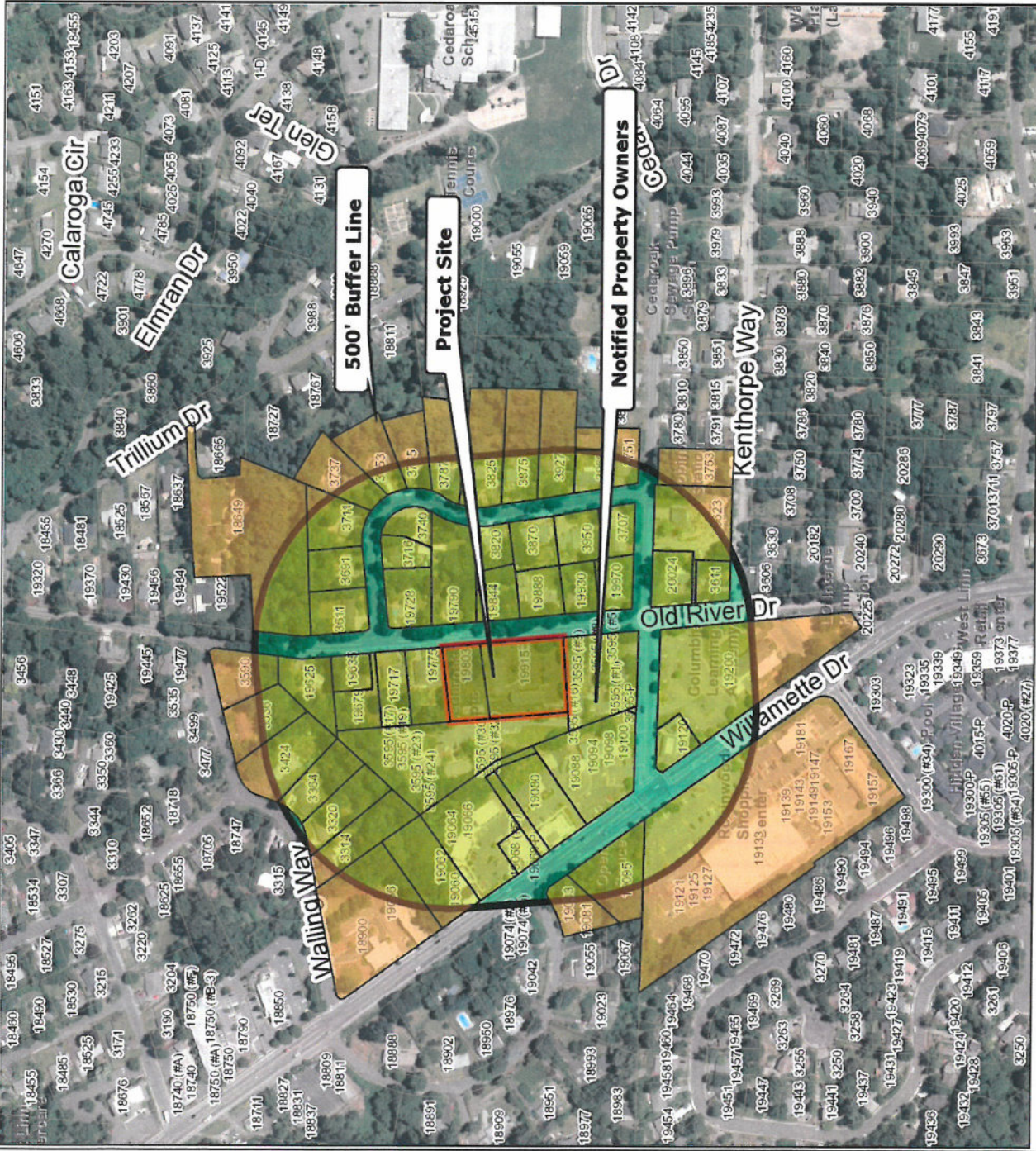
The review of the Planning Commission decision will consider Condition of Approval 2 (Engineering Standards) contained in the Final Decision and Order signed February 11, 2019. An appeal or review is de novo hearing and not limited to the stated grounds for review and all relevant issues may be considered. All evidence presented to the lower approval authority shall be considered and given equal weight as evidence presented on appeal. The Criteria applicable to the Conditional Use Permit, Class II Design Review, and Class II Variances (required parking and required loading area ) approved by the Planning Commission and under review by City Council are in Chapters 11, 41, 42, 44, 46, 48, 54, 55, 60, 75, 92, 96, and 99 of the Community Development Code (CDC). The approval authority may affirm, reverse, or modify the February 11, 2019 decision that is subject of this appeal or review.

You have been notified of this proposal because City records indicate that you had standing on case CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03 or because notice is required by CDC 99.140 and 99.260.

The notice of appeal or review and complete application for AP-19-02 is available for inspection at no cost at City Hall or via the City of West Linn's website <https://westlinnoregon.gov/planning/19915-old-river-drive-marylhurst-school-city-council-call-planning-commission-approval>. Printed copies of these documents may be obtained at City Hall for a minimal charge per page. At least 10 days prior to the hearing, a copy of the staff report will be available for inspection at no cost or copies can be obtained for a minimal charge per page. For further information, please contact Darren Wyss, Associate Planner, at City Hall, 22500 Salamo Road, West Linn, OR 97068, 503-742-6064 or [dwyss@westlinnoregon.gov](mailto:dwyss@westlinnoregon.gov).

Anyone wishing to present written testimony on the issue under review may do so prior to, or at the public hearing. At the public hearing, the City Council will receive a staff presentation and then invite both oral and written testimony from the public. The City Council may continue the public hearing to another meeting to obtain additional information, leave the record open, or close the public hearing and take action on the review as provided by CDC 99.290. Failure to raise an issue in person or in writing at some point prior to the close of the hearing, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes an appeal to the Land Use Board of Appeals (LUBA) based on that issue.

# 1915 Old River Drive Notification Map



Scale 1:4,800 - 1 in = 400 ft  
Scale is based on 8-1/2 x 11 paper size



Map created by: SSSHROYER  
Date Created: 1/Dec-18 11:24 AM

**WEST LINN GIS**

DISCLAIMER: This product is for informational purposes and may not have been prepared for, or be suitable for legal, engineering, or surveying purposes. Users of this information should review or consult the primary data and information sources to ascertain the usability of the information. Map scale is approximate. Source: West Linn GIS (Geographic Information System) Mappoint.



# CITY OF West Linn

## **CITY OF WEST LINN NOTICE OF UPCOMING CITY COUNCIL PUBLIC HEARING**

**PROJECT # AP-19-02  
MAIL: 3/19/19 TIDINGS: 3/28/19**

### **CITIZEN CONTACT INFORMATION**

To lessen the bulk of agenda packets, land use application notice, and to address the worries of some City residents about testimony contact information and online application packets containing their names and addresses as a reflection of the mailing notice area, this sheet substitutes for the photocopy of the testimony forms and/or mailing labels. A copy is available upon request.



**STAFF REPORT  
FOR THE PLANNING COMMISSION**

**FILE NUMBER:** CUP-18-01, DR-18-04, VAR-18-02/03

**HEARING DATE:** February 6, 2019

**REQUEST:** A Conditional Use Permit and Class II Design Review to redevelop an existing church into a private school serving pre-school to 8<sup>th</sup> grade at 19915 Old River Drive, requiring a variance to required parking and loading area.

**APPROVAL CRITERIA:** Community Development Code (CDC) Chapter 11; Chapter 41; Chapter 42; Chapter 44; Chapter 46; Chapter 48; Chapter 54; Chapter 55; Chapter 60; Chapter 75; Chapter 92; Chapter 96; and Chapter 99.

**STAFF REPORT PREPARED BY:** Darren Wyss, Associate Planner

Planning Manager's Initials AB Development Review Engineer's Initials AP

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## GENERAL INFORMATION

**OWNER/APPLICANT:** The Marylhurst School  
1232 Linn Avenue  
Oregon City, OR 97045  
Contact: Sheila Walker

**CONSULTANT:** Cardno  
6720 SW Macadam Avenue, Suite 200  
Portland, OR 97219  
Contact: Kevin Brady

**SITE LOCATION:** 19915 and 19803 Old River Drive

**LEGAL DESCRIPTION:** Clackamas County Assessor's Map 2S-1E-23AA, Taxlots 400 (0.47 acres) and 500 (0.99 acres)

**SITE SIZE:** 1.46 acres

**ZONING:** R-10, Single-Family Residential Detached

**COMP PLAN DESIGNATION:** Low-Density Residential

**120-DAY PERIOD:** This application became complete on December 5, 2018. The 120-day maximum application-processing period ends on April 4, 2019.

**PUBLIC NOTICE:** Notice was mailed to property owners within 500 feet of the subject property and all neighborhood associations January 17, 2019. Notice was published in the West Linn Tidings on January 24, 2019. The property was posted with a notice sign on January 25, 2019. The notice and application were posted on the City's website January 17, 2019. Therefore, public notice requirements of CDC Chapter 99 have been met.

## EXECUTIVE SUMMARY

**Site Conditions:** The proposed development site (Lots 2, 3, and 4 of Cedaroak Park Subdivision) is zoned R-10 and located in the Robinwood Neighborhood. The site contains two existing buildings that were previously used for church purposes. The main building is 4,500 square feet and the annex building is 4,000 square feet. There is an existing parking lot with 47 parking spaces, including 3 handicap spaces. The site also contains a shed and playground equipment. The site is 1.46 acres, rectangular, and bordered by Old River Drive on the east, apartment buildings on the south, parking lot for apartments on the west, and a single-family home on the north. The site is relatively flat and contains four significant trees (3 in the northeast corner of the property). Access to the site is provided by two driveways from Old River Drive.

**Project Description:** The Marylhurst School is requesting approval for a conditional use permit, design review, and two variances for the purpose of redeveloping an existing church property into a private school serving pre-school to 8<sup>th</sup> grade. The variances are for the following:

- Class II Variance to reduce the required off-street parking from 48 spaces to 37 spaces as required in CDC Section 46.090.
- Class II Variance to waive the required off-street loading space as required in CDC Section 46.130.

The proposal is for the project to be conducted in two phases. Phase One is adaptive reuse of the existing buildings on-site with a maximum of 115 primary/middle school students. The main building (4,500 sq. ft.) would be used for the main office and school assemblies, activities, etc., while the annex building (4,000 sq. ft.) would be used for classrooms and administrative offices. A portable building (1,800 sq. ft.) would be located in the parking lot and contain two temporary classrooms. The portable would be removed after the completion of Phase Two.

Phase Two would include the construction of a new, 16,300 square foot, two-story building to house classrooms and a library. Maximum enrollment after completion of Phase Two is 194 pre-school/primary/middle school students. Redesigned parking, open space areas, outdoor recreation areas, pedestrian pathways, and new landscaping will also be included.

**Surrounding Land Use and Zoning:** The site is zoned R-10 and located in the Robinwood Neighborhood. Adjacent land uses and zoning include:

| Direction From Site | Zoning | Land Use                       |
|---------------------|--------|--------------------------------|
| North/East          | R-10   | Single-family residences       |
| South/West          | R-2.1  | Multi-family apartment complex |

### **Applicable Community Development Code Approval Criteria:**

- Chapter 11, Single-Family Residential Detached, R-10;
- Chapter 41, Building Height, Structures on Steep Slopes, Exceptions;
- Chapter 42, Clear Vision Areas;
- Chapter 44, Fences;
- Chapter 46, Off-Street Parking, Loading and Reservoir Areas;
- Chapter 48, Access, Egress and Circulation;
- Chapter 52, Signs;
- Chapter 54, Landscaping;
- Chapter 55, Design Review;
- Chapter 60 Conditional Uses;
- Chapter 75, Variances and Special Waivers;
- Chapter 92, Required Improvements;
- Chapter 96, Street Improvement Construction; and
- Chapter 99, Procedures for Decision Making: Quasi-Judicial.

### **Public comment:**

As of the publication date of this report, staff had received no written public comment.

### **RECOMMENDATION**

Staff recommends approval of application CUP-18-01/DR-18-04/VAR-18-02/03, based on: 1) the findings submitted by the applicant, which are incorporated by this reference, 2) supplementary staff findings included in the Addendum below, and 3) the addition of conditions of approval below. With these findings, the applicable approval criteria are met. The conditions are as follows:

1. **Site Plans.** With the exception of modifications required by these conditions, the project shall substantially conform to all Tentative Plan Sheets.
2. **Engineering Standards.** All public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite stormwater, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. These must be designed, constructed, and completed prior to the issuance of occupancy permits for each phase of the project.



3. **Retaining Wall Fences.** The proposed three-foot fence surrounding the stormwater facility shall maintain a two-foot setback from top of retaining wall for any portion of the wall that exceeds five and one-half feet per Staff Finding 9.
4. **Parking Space Size.** The site plan shall be modified to reconfigure the parking spaces so that 50 percent are nine feet by eighteen feet per Staff Finding 19.
5. **Landscaping Installation.** All required landscaping shall be installed prior to final building certificate of occupancy for each of the two phases per Staff Finding 52
6. **Property Line Adjustment.** The applicant shall consolidate Lots 2, 3, and 4 of the Cedaroak Park Subdivision prior to the issuance of building permits for purposes of meeting building code requirements per Staff Finding 100.
7. **15 Foot Sanitary Sewer Line.** The applicant shall vacate the existing sanitary sewer line easement on the south end of the property and record a new 15 foot easement centered on the existing sanitary sewer line prior to issuance of occupancy permits per Staff Finding 86.
8. **15 Foot Water Line Easement.** The applicant shall dedicate a 15 foot wide easement, centered over the water line on the north side of the property, to the City for on-going operation and maintenance of the existing water line. The easement shall be dedicated to the City, on a form provided by the City, prior to issuance of occupancy permits per Staff Finding 84.
9. **23 Foot Drive Aisle.** Prior to issuance of building permits, the applicant shall modify the site plan to redesign the curb bulb at the end of the student drop-off zone to create a minimum 23 foot drive aisle per Staff Finding 33.
10. **One-Way Drive Aisle.** The site plan shall be modified to show treatments that reduce conflict associated with wrong way movements from motor vehicles in the one-way drive aisles.

**ADDENDUM**  
**PLANNING COMMISSION STAFF REPORT**  
February 6, 2019

**STAFF EVALUATION OF THE PROPOSAL'S COMPLIANCE  
WITH APPLICABLE CODE CRITERIA**

***I. CHAPTER 11, SINGLE-FAMILY RESIDENTIAL DETACHED, R-10***

*11.020 PROCEDURES AND APPROVAL PROCESS*

*(...)*

*C. A conditional use (CDC [11.060](#)) is a use the approval of which is discretionary with the Planning Commission. The approval process and criteria for approval are set forth in Chapter [60](#) CDC, Conditional Uses. If a use is not listed as a conditional use, it may be held to be a similar unlisted use under the provisions of Chapter [80](#) CDC.*

*(...)*

**Staff Finding 1: The application is for a conditional use, design review, and two variances. The Planning Commission will hold a public hearing to make a decision on the joint application. This criterion is met.**

*11.030 PERMITTED USES*

*The following are uses permitted outright in this zoning district:*

*(...)*

*5. Utilities, minor*

*(...)*

**Staff Finding 2: The applicant proposes three stormwater facilities to meet water quality and quantity requirements of the West Linn Public Works Standards. A rain garden planter (1,950 square feet) will accommodate stormwater runoff from the parking area in Phase One of the proposal. A smaller treatment planter (130 square feet) will accommodate stormwater runoff from new public improvements in Phase One. Phase Two will include a 900 square foot rain garden planter to accommodate runoff associated with the new building and new catch basins/stormwater lines along the west boundary of the site to control any potential runoff associated with the existing parking lot on the neighboring property. All facilities will be connected to the existing infrastructure in Old River Drive. The proposed stormwater system has been designed and sized by an Oregon licensed engineer to accommodate the projected peak storm event. The facilities are required by City regulations and will serve the proposed redevelopment of the subject property. The Planning Commission has discussed the issue of major versus minor utility and the applicant was informed of the interpretation at the pre-application conference.**

11.060 CONDITIONAL USES

The following are conditional uses which may be allowed in this zoning district subject to the provisions of Chapter 60 CDC, Conditional Uses.

(...)

7. Schools

(...)

**Staff Finding 3: The proposed Marylhurst School will replace the former use as a church. The subject property is located entirely within the R-10 zoning district, which allows schools as a conditional use. Subject to approval by the Planning Commission, this criterion is met.**

(...)

11.080 DIMENSIONAL REQUIREMENTS, CONDITIONAL USES

Except as may otherwise be established by this code, the appropriate lot or parcel size for a conditional use shall be determined by the approval authority at the time of consideration of the application based upon the criteria set forth in CDC 60.070(A) and (B).

**Staff Finding 4: The applicant has proposed to redevelop the site by using the existing buildings and parking lot in Phase One, while constructing an additional building as part of Phase Two. The design uses the site as efficiently as possible, while also providing the required improvements and accommodating the standard amenities of a primary school. These include parking areas and landscaping, fire access aisles, pedestrian and bicycle facilities, stormwater facilities, play fields and playgrounds, and the retention of significant trees. Subject to approval by the Planning Commission, this criterion is met.**

**II. CHAPTER 41, BUILDING HEIGHT, STRUCTURES ON STEEP SLOPES, EXCEPTIONS**

41.005 DETERMINING HEIGHT OF BUILDING

A. For all zoning districts, building height shall be the vertical distance above a reference datum measured to the highest point of a flat roof or to the deck line of a mansard roof or to the highest gable, ridgeline or peak of a pitched or hipped roof, not including projections above roofs such as cupolas, towers, etc. The reference datum shall be selected by either of the following, whichever yields a greater height of building.

1. For relatively flat sites where there is less than a 10-foot difference in grade between the front and rear of the building, the height of the building shall be measured from grade five feet out from the exterior wall at the front of the building; or

(...)

**Staff Finding 5: The subject property contains two existing buildings. Both the main building and the annex building have a one-foot grade difference between front and back. Both buildings are less than 35 feet and are not proposed to increase in height. The applicant proposes to use a portable classroom building in the parking lot as part of Phase One. The location in the parking lot has zero grade difference between the front and back of the**

portable. The height of the portable is 22 feet as measured from five-feet out from the front exterior wall. Phase two includes the construction of a new two-story building. The maximum grade difference between front and back is four-feet at the north end of the proposed building. The height of the proposed building is 32 feet as measured from five-feet out from the front exterior wall. The maximum height in the R-10 zone is 35 feet. These criteria are met.

### **III. CHAPTER 42, CLEAR VISION AREAS**

#### *42.020 CLEAR VISION AREAS REQUIRED, USES PROHIBITED*

A. A clear vision area shall be maintained on the corners of all property adjacent to an intersection as provided by CDC 42.040 and 42.050.

B. A clear vision area shall contain no planting, fence, wall, structure or temporary or permanent obstruction (except for an occasional utility pole or tree) exceeding three feet in height, measured from the top of the curb, or, where no curb exists, from the street centerline grade, except that trees exceeding this height may be located in this area, provided all branches below eight feet are removed.

**Staff Finding 6: The subject property abuts neither a street intersection nor property corner that requires clear vision areas. However, the proposal has two street and accessway intersections that have only street trees and shrubs proposed to be located within the clear vision areas. The vegetation will be pruned to meet the three and eight-foot requirements. These criteria are met.**

#### *42.050 COMPUTATION; ACCESSWAY LESS THAN 24 FEET IN WIDTH*

*The clear vision area for street and accessway intersections (accessways having less than 24 feet in width) shall be that triangular area whose base extends 30 feet along the street right-of-way line in both directions from the centerline of the accessway at the front setback line of a single-family and two-family residence, and 30 feet back from the property line on all other types of uses.*

**Staff Finding 7: The subject property abuts neither a street intersection nor property corner that requires clear vision areas. However, the proposal has two street and accessway intersections, one 20 feet in width and the other 24 feet in width. The clear vision triangle was calculated for this conditional use permit per CDC 42.050 and has only street trees and shrubs (no walls or fences) proposed to be located within the 30 foot clear vision areas. The applicant shall update the submitted Traffic Impact Study to clarify this method was used. The vegetation will be pruned to maintain the three-foot and eight-foot requirements. These criteria are met.**

## **IV. CHAPTER 44, FENCES**

### *44.020 SIGHT-OBSCURING FENCE; SETBACK AND HEIGHT LIMITATIONS*

*A. A sight- or non-sight-obscuring fence may be located on the property line or in a yard setback area subject to the following:*

*1. The fence is located within:*

*a. A required front yard area, and it does not exceed three feet, except pillars and driveway entry features subject to the requirements of Chapter 42 CDC, Clear Vision Areas, and approval by the Planning Director;*

*b. A required side yard which abuts a street and it is within that portion of the side yard which is also part of the front yard setback area and it does not exceed three feet;*

*c. A required side yard which abuts a street and it is within that portion of the side yard which is not also a portion of the front yard setback area and it does not exceed six feet provided the provisions of Chapter 42 CDC are met;*

*d. A required rear yard which abuts a street and it does not exceed six feet; or*

*e. A required side yard area which does not abut a street or a rear yard and it does not exceed six feet*

**Staff Finding 8: The subject property has an existing six-foot cyclone fence along the side (north and south) property lines and rear (west) property line. The applicant proposes to retain this fencing. The applicant proposes a three-foot fence running south from the northeast corner (front property line) of the property and enclosing the proposed stormwater facility located between the existing main building and Old River Drive. These criteria are met.**

*B. Fence or wall on a retaining wall. When a fence is built on a retaining wall or an artificial berm, the following standards shall apply:*

*1. When the retaining wall or artificial berm is 30 inches or less in height from finished grade, the maximum fence or wall height on top of the retaining wall shall be six feet.*

*2. When the retaining wall or earth berm is greater than 30 inches in height, the combined height of the retaining wall and fence or wall from finished grade shall not exceed eight and one-half feet.*

*3. Fences or walls located on top of retaining walls or earth berms in excess of 30 inches above finished grade may exceed the total allowed combined height of eight and one-half feet; provided, that the fence or wall is located a minimum of two feet from the retaining wall and the fence or wall height shall not exceed six feet.*

**Staff Finding 9: The applicant proposes a retaining wall in Phase One associated with the stormwater facility. The retaining wall will range from one-foot to six-foot high depending on adjacent grade. The applicant proposes a three-foot fence surrounding the stormwater facility. Where the wall is five and one-half feet or lower, the three-foot fence can be located on top of the wall. Where the wall is higher than five and one-half feet, the three-foot fence must be placed two-feet behind top of wall per Condition of Approval 3. Subject to the Conditions of Approval, these criteria are met.**

#### 44.030 SCREENING OF OUTDOOR STORAGE

- A. All service, repair, and storage activities carried on in connection with any commercial, business or industrial activity and not conducted within an enclosed building shall be screened from view of all adjacent properties and adjacent streets by a sight-obscuring fence.
- B. The sight-obscuring fence shall be in accordance with provisions of Chapter 42 CDC, Clear Vision Areas, and shall be subject to the provisions of Chapter 55 CDC, Design Review.

**Staff Finding 10: The applicant proposes no permanent outside storage activities, but an enclosed refuse/recycling area will be provided and screened from view per Chapter 55. Please see Plan Sheet C1.0. These criteria are met.**

#### 44.040 LANDSCAPING

*Landscaping which is located on the fence line and which impairs sight vision shall not be located within the clear vision area as provided in Chapter 42 CDC.*

#### 44.050 STANDARDS FOR CONSTRUCTION

- A. The structural side of the fence shall face the owner's property; and
- B. The sides of the fence abutting adjoining properties and the street shall be maintained.

**Staff Finding 11: No fences or landscaping that restrict sight vision are proposed within required clear vision areas. The fence will be constructed with the structural side facing the subject property and both sides of the fence will be maintained. These criteria are met.**

### **V. CHAPTER 46, OFF-STREET PARKING, LOADING AND RESERVOIR AREAS**

#### 46.060 STORAGE IN PARKING AND LOADING AREAS PROHIBITED

*Required parking spaces shall be available for the parking of passenger automobiles of residents, customers, patrons and employees only, and the required parking spaces shall not be used for storage of vehicles or materials or for the parking of trucks connected with the business or use with the exception of small (under one-ton) delivery trucks or cars.*

**Staff Finding 12: The applicant does not propose the storage of materials or vehicles in the parking lot that would occupy required parking spaces. Phase I includes a portable classroom structure to be located in the parking lot, but still maintains the 21 required parking spaces. This criterion is met.**

#### 46.070 MAXIMUM DISTANCE ALLOWED BETWEEN PARKING AREA AND USE

- A. Off-street parking spaces for single- and two-family dwellings shall be located on the same lot with the dwelling.
- B. Off-street parking spaces for uses not listed in subsection A of this section shall be located not farther than 200 feet from an entryway to the building or use they are required to serve, measured in a straight line from the building, with the following exceptions:  
(...)

3. *Employee parking areas for carpools and vanpools shall be located closer to the entryway to the building than general employee parking.*

*(...)*

5. *All disabled parking shall be placed closest to building entrances than all other parking. Appropriate ADA curb cuts and ramps to go from the parking lot to the ADA-accessible entrance shall be provided unless exempted by ADA code.*

**Staff Finding 13:** The furthest parking space is a maximum 170 feet from the entryway to the existing main and annex buildings and the proposed new building in Phase Two. Disabled parking spaces are proposed to be placed closest to all building entrances in relation to all other parking. The proposal requires no carpool/vanpool spaces (see Staff Finding 16). These criteria are met.

#### 46.080 COMPUTATION OF REQUIRED PARKING SPACES AND LOADING AREA

A. *Where several uses occupy a single structure or unit of land...*

B. *To calculate building square footage as a basis for determining how many parking spaces are needed, the area measured shall be gross floor area under the roof measured from the faces of the structure, including all habitable floors and excluding only space devoted to covered off-street parking or loading.*

C. *Where employees are specified, the employees counted are the persons who work on the premises including proprietors, executives, professional people, production, sales, and distribution employees, during the largest shift.*

D. *Fractional space requirements shall be counted as a whole space.*

E. *On-street parking along the immediate property frontage(s) may be counted toward the minimum parking requirement with approval from the City Engineer.*

*(...)*

**Staff Finding 14:** The proposal is for one use as a private school. The total habitable floor space of the existing main building is 4,500 square feet, the existing annex building is 4,000 square feet, the proposed portable classroom structure is 1,800 square feet, and the proposed new school building is 16,300 square feet. Maximum staff levels after proposed Phase Two is 15 employees. The proposal does not include on-street parking to count towards minimum requirements and all fractional space calculations have been counted as a whole space. Please see Plan Sheets C1.0 and C1.1. These criteria are met.

#### 46.090 MINIMUM PARKING SPACE REQUIREMENTS

*(...)*

B. *Public and semi-public buildings/uses.*

*(...)*

6. *Primary school, middle school, or equivalent private or parochial school.*

*- One space for every employee, plus one space for each 1,000 square feet of floor area.*

*(...)*

8. *Day care, kindergarten, or pre-school facilities.*

*- One space per employee, plus one space for every 300 square feet of floor area.*

**Staff Finding 15:** The proposed use is a private school serving pre-school to 8<sup>th</sup> grade. The proposal is to redevelop the property in two phases. Required parking for Phase One includes:

PHASE 1 PARKING ANALYSIS

| BUILDING                       | PROPOSED USE            | AREA     | PARKING REQ.         | NO. STALLS REQUIRED |
|--------------------------------|-------------------------|----------|----------------------|---------------------|
| EXISTING CHURCH ANNEX BUILDING | MIDDLE SCHOOL CLASSROOM | 4,000 SF | 1 SPACE PER 1,000 SF | 4                   |
| PROPOSED TEMPORARY PORTABLE    | PRIMARY SCHOOL          | 1,800 SF | 1 SPACE PER 1,000 SF | 2                   |
| EXISTING MAIN CHURCH BUILDING  | MULTI-USE COMMONS AREA  | 4,500 SF | 1 SPACE PER 1,000 SF | 5                   |
| STAFF                          | 10<br>(AN ANY ONE TIME) |          |                      | 10                  |
| TOTAL PARKING STALLS REQUIRED  |                         |          |                      | 21                  |
| TOTAL PARKING STALLS PROPOSED  |                         |          |                      | 22                  |

Total number of required parking spaces for Phase One is 21 and the applicant proposes 22 spaces. Please see Plan Sheet C1.0. The criteria are met.

Required parking for Phase Two includes:

PHASE 2 PARKING ANALYSIS

| BUILDING                       | PROPOSED USE            | AREA      | PARKING REQ.         | NO. STALLS REQUIRED |
|--------------------------------|-------------------------|-----------|----------------------|---------------------|
| EXISTING CHURCH ANNEX BUILDING | MIDDLE SCHOOL CLASSROOM | 4,000 SF  | 1 SPACE PER 1,000 SF | 4                   |
| EXISTING MAIN CHURCH BUILDING  | MULTI-USE COMMONS AREA  | 4,500 SF  | 1 SPACE PER 1,000 SF | 5                   |
| 25% PROPOSED NEW ADDITION      | KINDERGARTEN & PRE-K    | 3,300 SF  | 1 SPACE PER 300 SF   | 11                  |
| 75% PROPOSED NEW ADDITION      | PRIMARY SCHOOL          | 13,000 SF | 1 SPACE PER 1,000 SF | 13                  |
| STAFF                          | 15<br>(AN ANY ONE TIME) |           |                      | 15                  |
| TOTAL PARKING STALLS REQUIRED  |                         |           |                      | 48                  |
| TOTAL PARKING STALLS PROPOSED  |                         |           |                      | 37                  |

Total number of required parking spaces for Phase Two is 48. The applicant proposes 37 spaces for Phase Two and has applied for a Class II Variance to reduce the minimum required parking spaces for Phase II. Please refer to Staff Findings 120 to 126 and Plan Sheet C1.1. Subject to approval of the variance, these criteria are met.

(...)

*F. Maximum parking. Parking spaces (except for single-family and two-family residential uses) shall not exceed the minimum required number of spaces by more than 10 percent.*

*G. Parking reductions. An applicant may reduce parking up to 10 percent for development sites within one-quarter mile of a transit corridor or within a mixed-use commercial area, and up to 10 percent for commercial development sites adjacent to multi-family residential sites with the potential to accommodate more than 20 dwelling units.*



H. For office, industrial, and public uses where there are more than 20 parking spaces for employees on the site, at least 10 percent of the required employee parking spaces shall be reserved for carpool use before 9:00 a.m. on weekdays. The spaces will be the closest to the building entrance, except for any disabled parking and those signed for exclusive customer use. The carpool/vanpool spaces shall be clearly marked “Reserved – Carpool/Vanpool Before 9:00 a.m.”

**Staff Finding 16:** The applicant is not requesting a parking reduction based on proximity to transit. The applicant proposes a four percent increase in minimum parking for Phase One and a reduction of minimum parking by variance in Phase Two. Maximum employee parking is 15 after completion of Phase Two, which does not require carpool/vanpool spaces. These criteria are met.

(...)

**46.120 DRIVEWAYS REQUIRED ON SITE**

Any school or other meeting place which is designed to accommodate more than 25 people at one time shall provide a 15-foot-wide driveway designed for continuous forward flow of passenger vehicles for the purpose of loading and unloading passengers. Depending on functional requirements, the width may be increased with Planning Director approval

**Staff Finding 17:** Staff incorporates applicant findings (page 71 of packet). Also, please see Plan Sheet C1.0 and C1.1. The criterion is met.

**46.130 OFF-STREET LOADING SPACES**

Buildings or structures to be built or substantially altered, which receive and distribute material or merchandise by truck, shall provide and maintain off-street loading and maneuvering space. The dimensional standard for loading spaces is a minimum of 14 feet wide by 20 feet long or proportionate to accommodate the size of delivery trucks that typically serve the proposed use as follows:

| Land Use               | Gross Floor Area                 |                                   |
|------------------------|----------------------------------|-----------------------------------|
|                        | At Which First Berth Is Required | At Which Second Berth Is Required |
| (...)                  |                                  |                                   |
| Institutional: Schools | 10,000                           | 100,000                           |
| (...)                  |                                  |                                   |

**Staff Finding 18:** The applicant proposes school buildings with a gross floor area of 24,800 square feet, which requires one loading space. The applicant has applied for a Class II Variance to waive the required loading space. Please refer to Staff Findings 120 to 126. Food delivery trucks are not required as the school does not prepare or provide food service for the student population. Subject to approval of the variance, these criteria are met.

(...)

#### 46.150 DESIGN AND STANDARDS

*The following standards apply to the design and improvement of areas used for vehicle parking, storage, loading, and circulation:*

##### *A. Design Standards.*

- 1. "One standard parking space" means a minimum for a parking stall of eight feet in width and 16 feet in length. These stalls shall be identified as "compact." To accommodate larger cars, 50 percent of the required parking spaces shall have a minimum dimension of nine feet in width and 18 feet in length (nine feet by 18 feet). When multi-family parking stalls back onto a main driveway, the stalls shall be nine feet by 20 feet. Parking for development in water resource areas may have 100 percent compact spaces.*
- 2. Disabled parking and maneuvering spaces shall be consistent with current federal dimensional standards and subsection B of this section and placed nearest to accessible building entryways and ramps.*

(...)

**Staff Finding 19: The applicant proposes 19 parking spaces of eight feet by 16 feet, 16 spaces of nine feet by 18 feet (46%), and two spaces that meet federal ADA standards and are located nearest to accessible building entryways and ramps. The proposal does not meet the 50 percent requirement for 9x18 foot spaces. The applicant shall reconfigure the parking spaces to meet the 50 percent requirement per Condition of Approval 4. Subject to the Conditions of Approval, these criteria are met.**

- 4. Service drives shall be designed and constructed to facilitate the flow of traffic, provide maximum safety of traffic access and egress, and maximum safety of pedestrians and vehicular traffic on the site.*
- 5. Each parking and/or loading space shall have clear access, whereby the relocation of other vehicles to utilize the parking space is not required.*
- 6. Except for single- and two-family residences, any area intended to be used to meet the off-street parking requirements as contained in this chapter shall have all parking spaces clearly marked using a permanent paint. All interior drives and access aisles shall be clearly marked and signed to show direction of flow and maintain vehicular and pedestrian safety. Permeable parking surface spaces may have an alternative delineation for parking spaces.*
- 7. Except for residential parking, and parking for public parks and trailheads, at least 50 percent of all areas used for the parking and/or storage and/or maneuvering of any vehicle, boat and/or trailer shall be improved with asphalt or concrete surfaces according to the same standards required for the construction and acceptance of City streets. The remainder of the areas used for parking may use a permeable paving surface designed to reduce surface runoff. Parking for public parks or trailheads may use a permeable paving surface designed to reduce surface runoff for all parking areas. Where a parking lot contains both paved and unpaved areas, the paved areas shall be located closest to the use which they serve.*

**Staff Finding 20:** The applicant proposal has designed the service drive and loading area to minimize vehicular interaction with pedestrians, bicyclists, and other vehicles, thus maximizing safety. Please see Plan Sheets C1.0 and C1.1. The school provides no bus service. All parking spaces are designed with clear access and no relocation of other vehicles would be required. All parking, loading, and driveway surfaces will be paved and appropriately marked as required. These criteria are met.

(...)

9. *Access drives from the street to off-street parking or loading areas shall be designed and constructed to facilitate the flow of traffic and provide maximum safety for pedestrian and vehicular traffic on the site. The number of access drives shall be limited to the minimum that will allow the property to accommodate and service the anticipated traffic. Access drives shall be clearly and permanently marked and defined through use of rails, fences, walls, or other barriers or markers on frontage not occupied by service drives.*

10. *Access drives shall have a minimum vision clearance as provided in Chapter 42 CDC, Clear Vision Areas.*

11. *Parking spaces along the boundaries of a parking lot or adjacent to interior landscaped areas or sidewalks shall be provided with a wheel stop at least four inches high located two feet back from the front of the parking stall. Such parking spaces may be provided without wheel stops if the sidewalks or landscaped areas adjacent the parking stalls are two feet wider than the minimum width.*

**Staff Finding 21:** The applicant proposes to utilize the two existing access drives that meet City standards for access. The access drives will be clearly defined to provide maximum safety for pedestrians, bicyclists, and vehicular traffic. All access drives meet clear vision area requirements (see Staff Findings 6 and 7). Wheel stops will be provided for all parking spaces. These criteria are met.

12. *Off-street parking and loading areas shall be drained in accordance with plans and specifications approved by the City Engineer. Storm drainage at commercial sites may also have to be collected to treat oils and other residue.*

**Staff Finding 22:** The applicant identifies all stormwater from off-street parking areas to be collected and conveyed to the stormwater facility for treatment (Plan Sheet C3.0). Access roads shall be designed in accordance with the Public Works Standards and shall be provided to within 10 feet of all control structures for maintenance of all stormwater facilities. This criterion is met.

13. *Artificial lighting on all off-street parking facilities shall be designed to deflect all light downward away from surrounding residences and so as not to create a hazard to the public use of any road or street.*

**Staff Finding 23:** The applicant has proposed an illumination plan (pages 154-155 of packet) with on-site lighting that is deflected downward and away from surrounding residences and public rights-of-way. This criterion is met.

14. *Directional arrows and traffic control devices which are placed on parking lots shall be identified.*

(...)

16. *Visitor or guest parking must be identified by painted "GUEST" or "VISITOR."*

17. *The parking area shall have less than a five percent grade. No drainage across adjacent sidewalks or walkways is allowed.*

**Staff Finding 24:** The applicant proposes directional arrows and signage for the access drives. No visitor or guest parking spaces are proposed. To reduce potential conflicts with motor vehicles, the applicant shall install treatments for the one-way drive aisles to reduce wrong way traffic per Condition of Approval 10. The east/west grade of the parking lot is 5.0 percent and the north/south grade of the parking lot is 3.1 percent. The applicant design proposes parking area stormwater collection through four catch basins located appropriately to capture and convey runoff to the stormwater facility for treatment. The design does not propose drainage across adjacent sidewalks or walkways. Please see Plan Sheets C1.0, C1.1, and C3.0. Subject to the Conditions of Approval, these criteria are met.

18. *Commercial, office, industrial, and public parking lots may not occupy more than 50 percent of the main lot frontage of a development site. The remaining frontage shall comprise buildings or landscaping. If over 50 percent of the lineal frontage comprises parking lot, the landscape strip between the right-of-way and parking lot shall be increased to 15 feet wide and shall include terrain variations (e.g., one-foot-high berm) plus landscaping. The defensible space of the parking lot should not be compromised.*

**Staff Finding 25:** The subject property has 325 lineal feet of frontage along Old River Drive. The applicant is proposing 155 lineal feet of parking lot to occupy the lot frontage, which equates to 47.7 percent. Therefore, no increased landscape strips or berms are required. This criterion is met.

19. *Areas of the parking lot improved with asphalt or concrete surfaces shall be designed into areas of 12 or less spaces through the use of defined landscaped area. Groups of 12 or less spaces are defined as:*

a. *Twelve spaces in a row, provided there are no abutting parking spaces, as in the case when the spaces are abutting the perimeter of the lot; or*

b. *Twelve spaces in a group with six spaces abutting together; or*

c. *Two groups of 12 spaces abutting each other, but separated by a 15-foot-wide landscape area including a six-foot-wide walkway.*

(...)

**Staff Finding 26:** The applicant proposal provides three parking areas that meet Criteria a, and two parking areas that meet Criteria b. Please see Plan Sheets C1.0, C1.1, L1.01, and L1.03. These criteria are met.

20. *Pedestrian walkways shall be provided in parking areas having 20 or more spaces. Walkways or sidewalks shall be constructed between major buildings/activity areas...Walkways shall be constructed using a material that visually contrasts with the parking lot and driveway surface. Walkways shall be further identifiable to pedestrians and motorists by grade separation, walls, curbs, surface texture, and/or landscaping. Walkways shall be six feet wide. The arrangement and layout of the paths shall depend on functional requirements.*

**Staff Finding 27:** The applicant proposes new sidewalks along the entire frontage of the subject property (Plan Sheets C1.0 and C1.1). The proposal also includes internal pedestrian walkways to facilitate the movement of people from their vehicles and activity areas to the building entryways. The internal walkways will be six-foot wide, have pavement markings and surface texture, or be accommodated above a curb. This criterion is met.

21. *The parking and circulation patterns are easily comprehended and defined. The patterns shall be clear to minimize traffic hazards and congestion and to facilitate emergency vehicles.*

22. *The parking spaces shall be close to the related use.*

23. *Permeable parking spaces shall be designed and built to City standards.*

**Staff Finding 28:** The proposal has been designed for ease of use and the safety of pedestrians, bicyclists, vehicular traffic, and emergency vehicles. No permeable parking spaces are proposed. The proposed parking spaces have been located to make the most efficient use of the site and all meet the distance standards. These criteria are met.

*B. Accessible parking standards for persons with disabilities. If any parking is provided for the public or visitors, or both, the needs of the people with disabilities shall be based upon the following standards or current applicable federal standards, whichever are more stringent:*

1. *Minimum number of accessible parking space requirements (see following table):*

| <i>MINIMUM REQUIRED NUMBER OF TOTAL PARKING SPACES</i> | <i>TOTAL NUMBER OF ACCESSIBLE SPACES</i> | <i>NUMBER OF VAN-ACCESSIBLE SPACES REQUIRED, OF TOTAL</i> | <i>SPACES SIGNED "WHEELCHAIR USE ONLY"</i> |
|--------------------------------------------------------|------------------------------------------|-----------------------------------------------------------|--------------------------------------------|
| <i>(...)</i>                                           |                                          |                                                           |                                            |
| 26 – 50                                                | 2                                        | 1                                                         | –                                          |
| <i>(...)</i>                                           |                                          |                                                           |                                            |

**Staff Finding 29:** The proposal is required to provide a minimum of 48 parking spaces, which then requires two accessible spaces. The two spaces shall include one van-accessible space. The applicant has proposed two accessible spaces, including one van-accessible space (Plan Sheets C1.0 and C1.1). These criteria are met.

2. *Location of parking spaces. Parking spaces for the individual with a disability that serve a particular building shall be located on the shortest possible accessible circulation route to an accessible entrance to a building. In separate parking structures or lots that do not serve a particular building, parking spaces for the persons with disabilities shall be located on the shortest possible circulation route to an accessible pedestrian entrance of the parking facility.*
3. *Accessible parking space and aisle shall meet ADA vertical and horizontal slope standards.*
4. *Where any differences exist between this section and current federal standards, those standards shall prevail over this code section.*
5. *One in every eight accessible spaces, but not less than one, shall be served by an access aisle 96 inches wide.*
6. *Van-accessible parking spaces shall have an additional sign marked "Van Accessible" mounted below the accessible parking sign. A van-accessible parking space reserved for wheelchair users shall have a sign that includes the words "Wheelchair Use Only." Van-accessible parking shall have an adjacent eight-foot-wide aisle. All other accessible stalls shall have a six-foot-wide aisle. Two vehicles may share the same aisle if it is between them. The vertical clearance of the van space shall be 96 inches*

**Staff Finding 30:** The applicant proposal has located the accessible parking spots nearest the building entryway. All accessible spaces meet ADA standards (Plan Sheets C1.0 and C1.1). One accessible space has a 108-inch access aisle that will be signed "Van Accessible". The remaining accessible space has nine foot wide access aisles. These criteria are met.

(...)

*D. Bicycle facilities and parking.*

1. *Provisions shall be made for pedestrian and bicycle ways if such facilities are shown on an adopted plan.*
2. *Bicycle parking facilities shall either be lockable enclosures in which the bicycle is stored, or secure stationary racks which accommodate bicyclist's locks securing the frame and both wheels. The bicycle parking shall be no more than 50 feet from the entrance to the building, well-lit, observable, and properly signed.*

**Staff Finding 31:** The subject property does not include any planned bicycle pathways. The West Linn Transportation System Plan includes a bike lane on Old River Drive with a medium priority (Project B21). Due to the location of the existing travel lanes in the Old River Drive right-of-way, the applicant proposes sidewalk, planter strip, curb/gutter, and 14 feet of asphalt from the current street centerline. A shared use pavement marking will be required as part of public improvements. The applicant proposes to provide 34 secure stationary racks that are well-lit, observable, and properly signed. The applicant proposes all of the spaces to be within 30 feet of an entryway. These criteria are met.

3. Bicycle parking must be provided in the following amounts:

**Staff Finding 32: Staff incorporates applicant findings. These criteria are met.**

(...)

F. (See Figures 1 and 2 below.) Minimum Standards for Parking Lot Layout

| ANGLE OF PARKING | DIRECTION OF PARKING | AISLE WIDTH |       | DIMENSION 'A' |       | DIMENSION 'B' |      |
|------------------|----------------------|-------------|-------|---------------|-------|---------------|------|
|                  |                      | STALL WIDTH |       | STALL WIDTH   |       | STALL WIDTH   |      |
|                  |                      | 9.0'        | 8.0'  | 9.0'          | 8.0'  | 9.0'          | 8.0' |
| (...)            |                      |             |       |               |       |               |      |
| 90°              | DRIVE-IN             | 23.0'       | 23.0' | 18.0'         | 16.0' | 9.0'          | 8.0' |
| (...)            |                      |             |       |               |       |               |      |

**Staff Finding 33: The proposal is for all parking spaces to be drive-in at a 90 degree angle, which requires a drive aisle width of 23 feet regardless of whether the space is standard or compact. The applicant proposes a minimum 23 foot drive aisle for all parking spaces except the section of drive aisle at the end of the student drop-off zone that is only 20 feet wide. The applicant shall redesign the curb bulb to create a 23 foot drive aisle per Condition of Approval 9. Subject to the Conditions of Approval, this criterion is met.**

**VI. CHAPTER 48, ACCESS, EGRESS AND CIRCULATION**

48.020 APPLICABILITY AND GENERAL PROVISIONS

(...)

**Staff Finding 34: Staff incorporates applicant findings (page 78 of packet). These criteria are met.**

48.025 ACCESS CONTROL

B. Access Control Standards

1. Traffic impact analysis requirements. The City or other agency with access jurisdiction may require a traffic study prepared by a qualified professional to determine access, circulation and other transportation requirements. (See also CDC [55.125](#), Traffic Impact Analysis.)

**Staff Finding 35: The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the**

analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.

*2. The City or other agency with access permit jurisdiction may require the closing or consolidation of existing curb cuts or other vehicle access points, recording of reciprocal access easements (i.e., for shared driveways), development of a frontage street, installation of traffic control devices, and/or other mitigation as a condition of granting an access permit, to ensure the safe and efficient operation of the street and highway system. Access to and from off-street parking areas shall not permit backing onto a public street.*

**Staff Finding 36: The applicant proposes to utilize the existing two access drives from Old River Road. The design has a flow through parking lot and no parking areas that back onto a public street. This criterion is met.**

*3. Access options. When vehicle access is required for development (i.e., for off-street parking, delivery, service, drive-through facilities, etc.), access shall be provided by one of the following methods (planned access shall be consistent with adopted public works standards and TSP). These methods are "options" to the developer/subdivider.*

*a) Option 1. Access is from an existing or proposed alley or mid-block lane. If a property has access to an alley or lane, direct access to a public street is not permitted.*

*b) Option 2. Access is from a private street or driveway connected to an adjoining property that has direct access to a public street (i.e., "shared driveway"). A public access easement covering the driveway shall be recorded in this case to assure access to the closest public street for all users of the private street/drive.*

*c) Option 3. Access is from a public street adjacent to the development lot or parcel. If practicable, the owner/developer may be required to close or consolidate an existing access point as a condition of approving a new access. Street accesses shall comply with the access spacing standards in subsection (B) (6) of this section.*

**Staff Finding 37: The applicant proposes two access points to subject property via Option 3. The applicant has designed the access to meet all City standards or regulations that enhance safety and convenience for all travel modes. These criteria are met.**

*4. Subdivisions fronting onto an arterial street.*

*(...)*

*5. Double-frontage lots.*

*(...)*

**Staff Finding 38: The subject property has access to only one public street and therefore is not a double-frontage lot. These criteria do not apply.**



6. *Access spacing.*

a. *The access spacing standards found in Chapter 8 of the adopted Transportation System Plan (TSP) shall be applicable to all newly established public street intersections and non-traversable medians.*

b. *Private drives and other access ways are subject to the requirements of CDC 48.060.*

**Staff Finding 39: The proposal does not create any new intersections or non-traversable medians. CDC 48.060 is addressed in Staff Findings 46 to 49. These criteria are met.**

(...)

C. *Street connectivity and formation of blocks required.*

*In order to promote efficient vehicular and pedestrian circulation throughout the City, land divisions and large site developments shall produce complete blocks bounded by a connecting network of public and/or private streets, in accordance with the following standards:*

(...)

**Staff Finding 40: The applicant proposal is bounded by existing development and does not create any new blocks. This criterion is not applicable.**

2. *Street standards. Public and private streets shall also conform to Chapter 92 CDC, Required Improvements, and to any other applicable sections of the West Linn Community Development Code and approved TSP.*

**Staff Finding 41: Proposed street designs and improvements are consistent with the provisions of the West Linn Community Development Code and the West Linn Transportation System Plan. The applicant reviewed City regulations in the design of accessways and street cross-sections to enhance safety and convenience for all travel modes. This criterion is met.**

(...)

**48.030 MINIMUM VEHICULAR REQUIREMENTS FOR RESIDENTIAL USES**

(...)

E. *Access and/or service drives for multi-family dwellings shall be fully improved with hard surface pavement:*

(...)

3. *Minimum vertical clearance of 13 feet, six inches.*

4. *Appropriate turnaround facilities per Fire Chief's standards for emergency vehicles when the drive is over 150 feet long. Fire Department turnaround areas shall not exceed seven percent grade unless waived by the Fire Chief.*

5. *The grade shall not exceed 10 percent on average, with a maximum of 15 percent.*

6. *A minimum centerline turning radius of 45 feet for the curve.*

**Staff Finding 42: The applicant proposal has no vertical clearance obstructions. Neither of the two access drives exceed 150 feet. Both have turnaround facilities that meet Fire Department**

recommendations. The maximum grade of the parking areas/turnarounds is 5.0 percent. These criteria are met.

#### *48.040 MINIMUM VEHICLE REQUIREMENTS FOR NON-RESIDENTIAL USES*

*Access, egress, and circulation system for all non-residential uses shall not be less than the following:*

- A. Service drives for non-residential uses shall be fully improved with hard surface pavement:*
  - 1. With a minimum of 24-foot width when accommodating two-way traffic; or*
  - 2. With a minimum of 15-foot width when accommodating one-way traffic. Horizontal clearance shall be two and one-half feet wide on either side of the driveway.*
  - 3. Meet the requirements of CDC 48.030(E)(3) through (6).*
  - 4. Pickup window driveways may be 12 feet wide unless the Fire Chief determines additional width is required.*

**Staff Finding 43: The applicant proposes the northern access drive to be asphalt, 20 feet wide, and one-way. The southern access drive will be asphalt, 24 feet wide, and two-way. No pickup windows are proposed. Please also refer to Staff Finding 42. These criteria are met.**

- B. All non-residential uses shall be served by one or more service drives as determined necessary to provide convenient and safe access to the property and designed according to CDC 48.030(A). In no case shall the design of the service drive or drives require or facilitate the backward movement or other maneuvering of a vehicle within a street, other than an alley.*
- C. All on-site maneuvering and/or access drives shall be maintained pursuant to CDC 46.130.*
- D. Gated accessways to non-residential uses are prohibited unless required for public safety or security.*

**Staff Finding 44: The subject property does not take access from an arterial street, thus 48.030(A) does not apply. The proposed design does not require or facilitate any backward movement or maneuvering within the street. All access drives and parking lot maneuvering aisles will be maintained pursuant to 46.130 as shown in Staff Findings 18. The applicant proposes no vehicle gate. These criteria are met.**

#### *48.050 ONE-WAY VEHICULAR ACCESS POINTS*

*Where a proposed parking facility plan indicates only one-way traffic flow on the site, it shall be accommodated by a specific driveway serving the facility, and the entrance drive shall be situated closest to oncoming traffic, and the exit drive shall be situated farthest from oncoming traffic.*

**Staff Finding 45: The applicant proposes the northern access drive as one-way ingress and the southern access drive as two-way ingress/egress. This configuration eliminates the need for an arriving vehicle to make a left turn across exiting traffic. This criterion is met.**

*48.060 WIDTH AND LOCATION OF CURB CUTS AND ACCESS SEPARATION REQUIREMENTS*

*A. Minimum curb cut width shall be 16 feet.*

*B. Maximum curb cut width shall be 36 feet, except along Highway 43 in which case the maximum curb cut shall be 40 feet. For emergency service providers, including fire stations, the maximum shall be 50 feet.*

**Staff Finding 46: The applicant proposes two accessways requiring a curb cut. The northern access curb cut is proposed at 26 feet and the southern access at 30 feet. These criteria are met.**

*C. No curb cuts shall be allowed any closer to an intersecting street right-of-way line than the following:*

*1. On an arterial when intersected by another arterial, 150 feet.*

*(...)*

*6. On a local street when intersecting any other street, 35 feet.*

**Staff Finding 47: The applicant proposes two curb cuts on Old River Drive, a neighborhood route, to provide access to the site. The southern curb cut is a distance of 190 feet from the closest intersection with Cedar Oak Drive. These criteria are met.**

*D. There shall be a minimum distance between any two adjacent curb cuts on the same side of a public street, except for one-way entrances and exits, as follows:*

*1. On an arterial street, 150 feet.*

*2. On a collector street, 75 feet.*

*3. Between any two curb cuts on the same lot or parcel on a local street, 30 feet.*

**Staff Finding 48: The subject property is adjacent to a neighborhood route and contains two curb cuts that are a distance of 87 feet from one another. These criteria are met.**

*E. A rolled curb may be installed in lieu of curb cuts and access separation requirements.*

*F. Curb cuts shall be kept to the minimum, particularly on Highway 43. Consolidation of driveways is preferred. The standard on Highway 43 is one curb cut per business if consolidation of driveways is not possible.*

*G. Adequate line of sight pursuant to engineering standards should be afforded at each driveway or accessway.*

**Staff Finding 49: The applicant has designed the placement of curb cuts to meet City regulations. All accessways meet clear vision area requirements (refer to Staff Findings 6 and 7) and will adhere to engineering standards per Condition of Approval 2. These criteria are met.**

**VII. CHAPTER 54, LANDSCAPING**

54.020 APPROVAL CRITERIA

A. Every development proposal requires inventorying existing site conditions which include trees and landscaping. In designing the new project, every reasonable attempt should be made to preserve and protect existing trees and to incorporate them into the new landscape plan. Similarly, significant landscaping (e.g., bushes, shrubs) should be integrated. The rationale is that saving a 30-foot-tall mature tree helps maintain the continuity of the site, they are qualitatively superior to two or three two-inch caliper street trees, they provide immediate micro-climate benefits (e.g., shade), they soften views of the street, and they can increase the attractiveness, marketability, and value of the development.

B. To encourage tree preservation, the parking requirement may be reduced by one space for every significant tree that is preserved in the parking lot area for a maximum reduction of 10 percent of the required parking. The City Parks Supervisor or Arborist shall determine the significance of the tree and/or landscaping to determine eligibility for these reductions.

**Staff Finding 50: The applicant proposes the retention of 75 percent of significant tree canopy coverage on the subject property and is not seeking a parking reduction based on significant trees preserved in the parking lot area. Please see Plan Sheets L1.01 and L1.03. These criteria are met.**

C. Developers must also comply with the municipal code chapter on tree protection.

D. Heritage trees. Heritage trees are trees which, because of their age, type, notability, or historical association, are of special importance. Heritage trees are trees designated by the City Council following review of a nomination. A heritage tree may not be removed without a public hearing at least 30 days prior to the proposed date of removal. Development proposals involving land with heritage tree(s) shall be required to protect and save the tree(s). Further discussion of heritage trees is found in the municipal code.

**Staff Finding 51: The subject property contains no heritage trees. The applicant will comply with municipal code tree protection requirements during and after site development. These criteria are met.**

E. Landscaping – By type, location and amount.

(...)

2. Non-residential uses. A minimum of 20 percent of the gross site area shall be landscaped. Parking lot landscaping may be counted in the percentage.

**Staff Finding 52: Staff incorporates applicant findings (page 83 of packet). Landscaping shall be installed prior to final building certificate of occupancy per Condition of Approval 5. Please see Plan Sheets L1.01 and L1.03. These criteria are met.**

3. All uses (residential uses (non-single-family) and non-residential uses):

a. The landscaping shall be located in defined landscaped areas which are uniformly distributed throughout the parking or loading area. There shall be one shade tree planted for every eight parking spaces. These trees shall be evenly distributed throughout the parking lot to

*provide shade. Parking lots with over 20 spaces shall have a minimum 10 percent of the interior of the parking lot devoted to landscaping. Pedestrian walkways in the landscaped areas are not to be counted in the percentage. The perimeter landscaping, explained in subsection (E)(3)(d) of this section, shall not be included in the 10 percent figure. Parking lots with 10 to 20 spaces shall have a minimum five percent of the interior of the parking lot devoted to landscaping. The perimeter landscaping, as explained above, shall not be included in the five percent. Parking lots with fewer than 10 spaces shall have the standard perimeter landscaping and at least two shade trees. Non-residential parking areas paved with a permeable parking surface may reduce the required minimum interior landscaping by one-third for the area with the permeable parking surface only.*

**Staff Finding 53: Staff incorporates applicant findings (page 83 of packet). Please see Plan Sheets L1.01 and L1.03. These criteria are met.**

- b. The landscaped areas shall not have a width of less than five feet.*
- c. The soils, site, proposed soil amendments, and proposed irrigation system shall be appropriate for the healthy and long-term maintenance of the proposed plant species.*

**Staff Finding 54: Staff incorporates applicant findings (page 83 of packet). Please see Plan Sheets L1.01 and L1.03. These criteria are met.**

- d. A parking, loading, or service area which abuts a street shall be set back from the right-of-way line by perimeter landscaping in the form of a landscaped strip at least 10 feet in width. When a parking, loading, or service area or driveway is contiguous to an adjoining lot or parcel, there shall be an intervening five-foot-wide landscape strip. The landscaped area shall contain:
  - 1) Street trees spaced as appropriate to the species, not to exceed 50 feet apart on the average;*
  - 2) Shrubs, not to reach a height greater than three feet, six inches, spaced no more than five feet apart on the average; or*
  - 3) Vegetative ground cover such as grass, wildflowers, or other landscape material to cover 100 percent of the exposed ground within two growing seasons. No bark mulch shall be allowed except under the canopy of low level shrubs.**

**Staff Finding 55: The proposal provides a nine-foot landscape strip between the parking area and the Old River Drive right-of-way. The applicant seeks a Director's Exemption of 10% reduction in landscaping requirements per CDC 55.170(D). Please see Staff Finding 99. The landscape strip will contain shrubs not to reach a height greater than three-feet. The south end of the parking area abuts an adjoining parcel and provides a five-foot landscape strip with appropriate shrubs. Please see Plan Sheets L1.01 and L1.03. These criteria are met.**

- e. If over 50 percent of the lineal frontage of the main street or arterial adjacent to the development site comprises parking lot, the landscape strip between the right-of-way and*

*parking lot shall be increased to 15 feet in width and shall include terrain variations (e.g., one-foot-high berm) plus landscaping. This extra requirement only applies to one street frontage.*

**Staff Finding 56:** The subject property has 325 lineal feet of frontage along Old River Drive. The applicant is proposing 155 lineal feet of parking lot to occupy the lot frontage, which equates to 47.7 percent. Therefore, no increased landscape strips or berms are required. Please see Plan Sheets L1.01 and L1.03. This criterion is met.

*f. A parking, loading, or service area which abuts a property line shall be separated from the property line by a landscaped area at least five feet in width and which shall act as a screen and noise buffer, and the adequacy of the screen and buffer shall be determined by the criteria set forth in CDC 55.100(C) and (D), except where shared parking is approved under CDC 46.050.*

**Staff Finding 57:** The south end of the parking area abuts an adjoining parcel and provides a five-foot landscape strip with shrubs to act as a screen and noise buffer. Please refer to Staff Findings 78 to 79. Please see Plan Sheets L1.01 and L1.03. This criterion is met.

*g. All areas in a parking lot not used for parking, maneuvering, or circulation shall be landscaped.*

*h. The landscaping in parking areas shall not obstruct lines of sight for safe traffic operation.*

**Staff Finding 58:** Staff incorporates applicant findings (pages 84-85 of packet). Please see Plan Sheets L1.01 and L1.03. These criteria are met.

*i. Outdoor storage areas, service areas (loading docks, refuse deposits, and delivery areas), and above-ground utility facilities shall be buffered and screened to obscure their view from adjoining properties and to reduce noise levels to acceptable levels at the property line. The adequacy of the buffer and screening shall be determined by the criteria set forth in CDC 55.100(C)(1).*

**Staff Finding 59:** The applicant proposal includes a recycling/refuse enclosure in the southwest corner of the parking lot. The enclosure will be surrounded by a six-foot wooden framed opaque enclosure. The enclosure will have a curbed, three-foot landscape area with a 36 inch continuous hedge on both the rear and sides. These criteria are met.

*j. Crime prevention shall be considered and plant materials shall not be located in a manner which prohibits surveillance of public and semi-public areas (shared or common areas).*

*k. Irrigation facilities shall be located so that landscaped areas can be properly maintained and so that the facilities do not interfere with vehicular or pedestrian circulation.*

**Staff Finding 60:** Staff incorporates applicant findings (page 85 of packet). These criteria are met.

*l. For commercial, office, multi-family, and other sites, the developer shall select trees that possess the following characteristics:*

- 1) Provide generous "spreading" canopy for shade.*
- 2) Roots do not break up adjacent paving.*
- 3) Tree canopy spread starts at least six feet up from grade in, or adjacent to, parking lots, roads, or sidewalks unless the tree is columnar in nature.*
- 4) No sticky leaves or sap-dripping trees (no honey-dew excretion).*
- 5) No seed pods or fruit-bearing trees (flowering trees are acceptable).*
- 6) Disease-resistant.*
- 7) Compatible with planter size.*
- 8) Drought-tolerant unless irrigation is provided.*
- 9) Attractive foliage or form all seasons.*

*m. Plant materials (shrubs, ground cover, etc.) shall be selected for their appropriateness to the site, drought tolerance, year-round greenery and coverage, staggered flowering periods, and avoidance of nuisance plants (Scotch broom, etc.).*

**Staff Finding 61: Staff incorporates applicant findings (page 86 of packet). These criteria are met.**

(...)

#### **54.030 PLANTING STRIPS FOR MODIFIED AND NEW STREETS**

*All proposed changes in width in a public street right-of-way or any proposed street improvement shall, where feasible, include allowances for planting strips. Plans and specifications for planting such areas shall be integrated into the general plan of street improvements. This chapter requires any multi-family, commercial, or public facility which causes change in public right-of-way or street improvement to comply with the street tree planting plan and standards.*

**Staff Finding 62: The applicant proposal will meet street standards per Condition of Approval 2. These criteria are met.**

#### **54.040 INSTALLATION**

- A. All landscaping shall be installed according to accepted planting procedures.*
- B. The soil and plant materials shall be of good quality.*
- C. Landscaping shall be installed in accordance with the provisions of this code.*
- D. Certificates of occupancy shall not be issued unless the landscaping requirements have been met or other arrangements have been made and approved by the City such as the posting of a bond.*

#### **54.050 PROTECTION OF STREET TREES**

*Street trees may not be topped or trimmed unless approval is granted by the Parks Supervisor or, in emergency cases, when a tree imminently threatens power lines.*

54.060 MAINTENANCE

A. The owner, tenant and their agent, if any, shall be jointly and severally responsible for the maintenance of all landscaping which shall be maintained in good condition so as to present a healthy, neat, and orderly appearance and shall be kept free from refuse and debris.

B. All plant growth in interior landscaped areas shall be controlled by pruning, trimming, or otherwise so that:

1. It will not interfere with the maintenance or repair of any public utility;
2. It will not restrict pedestrian or vehicular access; and
3. It will not constitute a traffic hazard because of reduced visibility.

**Staff Finding 63: The applicant proposal will comply. These criteria are met.**

54.070 SPECIFICATION SUMMARY

| <b>Area/Location</b>                                                                                   | <b>Landscaping Req'd.</b> |
|--------------------------------------------------------------------------------------------------------|---------------------------|
| 1. Between parking lot and R-O-W.                                                                      | 10 ft.                    |
| 2. Between parking lot and other lot.                                                                  | 5 ft.                     |
| 3. Between parking lot and R-O-W if parking lot comprises more than 50 percent of main R-O-W frontage. | 15 ft.                    |
| 4. Percentage of residential/multi-family site to be landscaped.                                       | 25%                       |
| 5. Percentage of non-residential (commercial/industrial/office) site to be landscaped.                 | 20%                       |
| 6. Percentage of 10 – 25 car parking lot to be landscaped (excluding perimeter).                       | 5%                        |
| 7. Percentage of 1 – 9 car parking lot to be landscaped (excluding perimeter).                         | 0%                        |
| 8. Percentage of 26+ car parking lot to be landscaped (excluding perimeter).                           | 10%                       |

**Staff Finding 64: Please see Staff Findings 52 to 58. These criteria are met.**



### **VIII. CHAPTER 55, DESIGN REVIEW**

#### **55.100 APPROVAL STANDARDS – CLASS II DESIGN REVIEW**

##### **B. Relationship to the natural and physical environment.**

1. *The buildings and other site elements shall be designed and located so that all heritage trees, as defined in the municipal code, shall be saved. Diseased heritage trees, as determined by the City Arborist, may be removed at his/her direction.*

**Staff Finding 65: The subject site contains no heritage trees. This criteria does not apply.**

2. *All heritage trees...all trees and clusters of trees (“cluster” is defined as three or more trees with overlapping driplines; however, native oaks need not have an overlapping dripline) that are considered significant by the City Arborist...shall be protected pursuant to the criteria of subsections (B)(2)(a) through (f) of this section..*

a. *Non-residential and residential projects on Type I and II lands shall protect all heritage trees and all significant trees and tree clusters by either the dedication of these areas or establishing tree conservation easements...*

**Staff Finding 66: The subject property is primarily Type III or IV lands (95%). No significant trees proposed to be removed from Type I or II lands. These criteria are met.**

b. *Non-residential and residential projects on non-Type I and II lands shall set aside up to 20 percent of the area to protect trees and tree clusters that are determined to be significant, plus any heritage trees...*

**Staff Finding 67: Staff incorporates applicant findings (see page xx of packet). These criteria are met.**

c. *Where stubouts of streets occur on abutting properties, and the extension of those streets will mean the loss of significant trees, tree clusters, or heritage trees, it is understood that tree loss may be inevitable. In these cases, the objective shall be to minimize tree loss. These provisions shall also apply in those cases where access, per construction code standards, to a lot or parcel is blocked by a row or screen of significant trees or tree clusters.*

**Staff Finding 68: No street stubouts occur on abutting properties, nor do significant trees block access to the subject property. This criterion is met.**

d. *For both non-residential and residential development, the layout shall achieve at least 70 percent of maximum density for the developable net area. The developable net area excludes all Type I and II lands and up to 20 percent of the remainder of the site for the purpose of protection of stands or clusters of trees as defined in subsection (B)(2) of this section.*

e. *For arterial and collector street projects...*

f. *If the protection of significant tree(s) or tree clusters is to occur in an area of grading that is necessary for the development of street grades...compensate for the removal of the tree(s) on*

*an “inch by inch” basis (e.g., a 48-inch Douglas fir could be replaced by 12 trees, each four-inch). The mix of tree sizes and types shall be approved by the City Arborist.*

**Staff Finding 69: The applicant proposal utilizes over 70 percent of the site for building footprint, parking areas, play fields and playgrounds, required landscape areas, access aisles, stormwater facilities, pedestrian and bicycle facilities, and the retention of significant trees. The subject property is not adjacent to arterial or collector streets. No significant trees will be removed for right-of-way improvements. These criteria are met.**

- 3. The topography and natural drainage shall be preserved to the greatest degree possible.*
- 4. The structures shall not be located in areas subject to slumping and sliding. The Comprehensive Plan Background Report’s Hazard Map, or updated material as available and as deemed acceptable by the Planning Director, shall be the basis for preliminary determination.*
- 5. There shall be adequate distance between on-site buildings and on-site and off-site buildings on adjoining properties to provide for adequate light and air circulation and for fire protection.*

**Staff Finding 70: Staff incorporates applicant findings (pages 92-93 of packet). These criteria are met.**

*6. Architecture.*

*a. The proposed structure(s) scale shall be compatible with the existing structure(s) on site and on adjoining sites. Contextual design is required. Contextual design means respecting and incorporating prominent architectural styles, building lines, roof forms, rhythm of windows, building scale and massing of surrounding buildings in the proposed structure. The materials and colors shall be complementary to the surrounding buildings.*

**Staff Finding 71: Staff incorporates applicant findings (pages 95-96 of packet). This criterion is met.**

- b. While there has been discussion in Chapter 24 CDC about transition, it is appropriate that new buildings should architecturally transition in terms of bulk and mass to work with, or fit, adjacent existing buildings. This transition can be accomplished by selecting designs that “step down” or “step up” from small to big structures and vice versa (see figure below). Transitions may also take the form of carrying building patterns and lines (e.g., parapets, windows, etc.) from the existing building to the new one.*
- c. Contrasting architecture shall only be permitted when the design is manifestly superior to adjacent architecture in terms of creativity, design, and workmanship, and/or it is adequately separated from other buildings by distance, screening, grade variations, or is part of a development site that is large enough to set its own style of architecture.*
- d. Human scale is a term that seeks to accommodate the users of the building and the notion that buildings should be designed around the human scale (i.e., their size and the average range of their perception). Human scale shall be accommodated in all designs by, for example, multi-light windows that are broken up into numerous panes, intimately scaled entryways, and visual*

*breaks (exaggerated eaves, indentations, ledges, parapets, awnings, engaged columns, etc.) in the facades of buildings, both vertically and horizontally.*

**Staff Finding 72: Staff incorporates applicant findings (pages 95-96 of packet). These criteria are met.**

*e. The main front elevation of commercial and office buildings shall provide at least 60 percent windows or transparency at the pedestrian level to create more interesting streetscape and window shopping opportunities. One side elevation shall provide at least 30 percent transparency. Any additional side or rear elevation, which is visible from a collector road or greater classification, shall also have at least 30 percent transparency...*

**Staff Finding 73: Staff incorporates applicant findings (pages 95-96 of packet). This criterion is met.**

*f. Variations in depth and roof line are encouraged for all elevations.*

*To vary the otherwise blank wall of most rear elevations, continuous flat elevations of over 100 feet in length should be avoided by indents or variations in the wall. The use of decorative brick, masonry, or stone insets and/or designs is encouraged. Another way to vary or soften this elevation is through terrain variations such as an undulating grass area with trees to provide vertical relief.*

*g. Consideration of the micro-climate (e.g., sensitivity to wind, sun angles, shade, etc.) shall be made for building users, pedestrians, and transit users, including features like awnings.*

*h. The vision statement identified a strong commitment to developing safe and attractive pedestrian environments with broad sidewalks, canopied with trees and awnings*

*i. Sidewalk cafes, kiosks, vendors, and street furniture are encouraged. However, at least a four-foot-wide pedestrian accessway must be maintained per Chapter 53 CDC, Sidewalk Use.*

**Staff Finding 74: Staff incorporates applicant findings (pages 95-96 of packet). These criteria are met.**

*7. Transportation Planning Rule (TPR) compliance. The automobile shall be shifted from a dominant role, relative to other modes of transportation, by the following means:*

*a. Commercial and office development shall be oriented to the street. At least one public entrance shall be located facing an arterial street; or....facing the local street with highest traffic levels...*

*(...)*

*c. Commercial, office, and multi-family projects shall be built as close to the adjacent main right-of-way as practical to facilitate safe pedestrian and transit access...*

**Staff Finding 75: Staff incorporates applicant findings (page 99 of packet). These criteria are met.**

d. *Accessways, parking lots, and internal driveways shall accommodate pedestrian circulation and access by specially textured, colored, or clearly defined footpaths at least six feet wide. Paths shall be eight feet wide when abutting parking areas or travel lanes. Paths shall be separated from parking or travel lanes by either landscaping, planters, curbs, bollards, or raised surfaces...*

**Staff Finding 76: The applicant design proposes clearly defined, six foot pedestrian facilities throughout both parking areas to facilitate safe circulation. The abutting sidewalks are eight-foot wide and separated by a curb. This criterion is met.**

e. *Paths shall provide direct routes that pedestrians will use between buildings, adjacent rights-of-way, and adjacent commercial developments. They shall be clearly identified. They shall be laid out to attract use and to discourage people from cutting through parking lots and impacting environmentally sensitive areas.*

f. *At least one entrance to the building shall be on the main street, or as close as possible to the main street. The entrance shall be designed to identify itself as a main point of ingress/egress.*

g. *Where transit service exists, or is expected to exist, there shall be a main entrance within a safe and reasonable distance of the transit stop. A pathway shall be provided to facilitate a direct connection.*

h. *Projects shall bring at least part of the project adjacent to or near the main street right-of-way in order to enhance the height-to-width ratio along that particular street. (The "height-to-width ratio" is an architectural term that emphasizes height or vertical dimension of buildings adjacent to streets. The higher and closer the building is, and the narrower the width of the street, the more attractive and intimate the streetscape becomes.) For every one foot in street width, the adjacent building ideally should be one to two feet higher. This ratio is considered ideal in framing and defining the streetscape.*

i. *These architectural standards shall apply to public facilities such as reservoirs, water towers, treatment plants, fire stations, pump stations, power transmission facilities, etc. It is recognized that many of these facilities, due to their functional requirements, cannot readily be configured to meet these architectural standards. However, attempts shall be made to make the design sympathetic to surrounding properties by landscaping, setbacks, buffers, and all reasonable architectural means.*

j. *Parking spaces at trailheads shall be located so as to preserve the view of, and access to, the trailhead entrance from the roadway. The entrance apron to the trailhead shall be marked: "No Parking," and include design features to foster trail recognition.*

**Staff Finding 77: Staff incorporates applicant findings (page 99 of packet). This criterion is met.**

C. *Compatibility between adjoining uses, buffering, and screening.*

1. *In addition to the compatibility requirements contained in Chapter 24 CDC, buffering shall be provided between different types of land uses; for example, buffering between single-family homes and apartment blocks. However, no buffering is required between single-family homes*

*and duplexes or single-family attached units. The following factors shall be considered in determining the adequacy of the type and extent of the buffer:*

- a. The purpose of the buffer, for example to decrease noise levels, absorb air pollution, filter dust, or to provide a visual barrier.*
  - b. The size of the buffer required to achieve the purpose in terms of width and height.*
  - c. The direction(s) from which buffering is needed.*
  - d. The required density of the buffering.*
  - e. Whether the viewer is stationary or mobile.*
- 2. On-site screening from view from adjoining properties of such things as service areas, storage areas, and parking lots shall be provided and the following factors will be considered in determining the adequacy of the type and extent of the screening:*
- a. What needs to be screened?*
  - b. The direction from which it is needed.*
  - c. How dense the screen needs to be.*
  - d. Whether the viewer is stationary or mobile.*
  - e. Whether the screening needs to be year-round.*
- 3. Rooftop air cooling and heating systems and other mechanical equipment shall be screened from view from adjoining properties.*

**Staff Finding 78: Staff incorporates applicant findings. In addition, the applicant proposal includes a recycling/refuse enclosure in the southwest corner of the parking lot. The enclosure will be surrounded by a six-foot wooden framed opaque enclosure. The enclosure will have a curbed, three-foot landscape area with a 36 inch continuous hedge on both the rear and sides. These criteria are met.**

*D. Privacy and noise.*

- 1. Structures which include residential dwelling units shall provide private outdoor areas for each ground floor unit which is screened from view from adjoining units.*
- 2. Residential dwelling units shall be placed on the site in areas having minimal noise exposure to the extent possible. Natural-appearing sound barriers shall be used to lessen noise impacts where noise levels exceed the noise standards contained in West Linn Municipal Code Section 5.487.*
- 3. Structures or on-site activity areas which generate noise, lights, or glare shall be buffered from adjoining residential uses in accordance with the standards in subsection C of this section where applicable.*
- 4. Businesses or activities that can reasonably be expected to generate noise in excess of the noise standards contained in West Linn Municipal Code Section 5.487 shall undertake and submit appropriate noise studies and mitigate as necessary to comply with the code. (See CDC [55.110\(B\)\(11\)](#) and [55.120\(M\)](#).)*

*If the decision-making authority reasonably believes a proposed use may generate noise exceeding the standards specified in the municipal code, then the authority may require the applicant to supply professional noise studies from time to time during the user's first year of operation to monitor compliance with City standards and permit requirements.*

**Staff Finding 79: Staff incorporates applicant findings (page 100 of packet). In addition, the proposal does not include residential dwelling units and complies with subsection C (see Staff Finding 78). These criteria are met.**

(...)

*G. Demarcation of public, semi-public, and private spaces. The structures and site improvements shall be designed so that public areas such as streets or public gathering places, semi-public areas, and private outdoor areas are clearly defined in order to establish persons having a right to be in the space, to provide for crime prevention, and to establish maintenance responsibility. These areas may be defined by:*

- 1. A deck, patio, fence, low wall, hedge, or draping vine;*
- 2. A trellis or arbor;*
- 3. A change in level;*
- 4. A change in the texture of the path material;*
- 5. Sign; or*
- 6. Landscaping.*

**Staff Finding 80: Staff incorporates applicant findings (page 102 of packet). These criteria are met.**

*H. Public transit.*

*1. Provisions for public transit may be required where the site abuts an existing or planned public transit route. The required facilities shall be based on the following:*

- a. The location of other transit facilities in the area.*
- b. The size and type of the proposed development.*
- c. The rough proportionality between the impacts from the development and the required facility.*

*2. The required facilities shall be limited to such facilities as the following:*

- a. A waiting shelter with a bench surrounded by a three-sided covered structure, with transparency to allow easy surveillance of approaching buses.*
- b. A turnout area for loading and unloading designed per regional transit agency standards.*
- c. Hard-surface paths connecting the development to the waiting and boarding areas.*
- d. Regional transit agency standards shall, however, prevail if they supersede these standards.*

*3. The transit stop shall be located as close as possible to the main entrance to the shopping center, public or office building, or multi-family project. The entrance shall not be more than 200 feet from the transit stop with a clearly identified pedestrian link.*

*4. All commercial business centers (over three acres) and multi-family projects (over 40 units) may be required to provide for the relocation of transit stops to the front of the site if the existing stop is within 200 to 400 yards of the site and the exaction is roughly proportional to the impact of the development. The commercial or multi-family project may be required to provide new facilities in those cases where the nearest stop is over 400 yards away. The transit stop shall be built per subsection (H)(2) of this section.*

**Staff Finding 81: The subject property does not abut an existing or planned public transit route and no provisions are required. These criteria are met.**

*1. Public facilities. An application may only be approved if adequate public facilities will be available to provide service to the property prior to occupancy.*

*1. Streets. Sufficient right-of-way and slope easement shall be dedicated to accommodate all abutting streets to be improved to the City's Improvement Standards and Specifications. The City Engineer shall determine the appropriate level of street and traffic control improvements to be required, including any off-site street and traffic control improvements, based upon the transportation analysis submitted. The City Engineer's determination of developer obligation, the extent of road improvement and City's share, if any, of improvements and the timing of improvements shall be made based upon the City's systems development charge ordinance and capital improvement program, and the rough proportionality between the impact of the development and the street improvements...*

**Staff Finding 82: Old River Drive is classified as a Neighborhood Route in the West Linn Transportation System Plan and has an existing right-of-way (ROW) width of 60 feet. There are no existing pedestrian/bicycle facilities in the ROW. The ROW is sufficient to accommodate proposed street improvements for a Neighborhood Route without Parking. The applicant has submitted a Traffic Impact Study and designed the infrastructure to accommodate anticipated traffic load and pedestrian facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.**

*2. Storm detention and treatment and geologic hazards. Per the submittals required by CDC 55.130 and 92.010(E), all proposed storm detention and treatment facilities must comply with the standards for the improvement of public and private drainage systems located in the West Linn Public Works Design Standards, there will be no adverse off-site impacts caused by the development (including impacts from increased intensity of runoff downstream or constrictions causing ponding upstream), and the applicant must provide sufficient factual data to support the conclusions of the submitted plan.*

*Per the submittals required by CDC 55.130(E), the applicant must demonstrate that the proposed methods of rendering known or potential hazard sites safe for development, including proposed geotechnical remediation, are feasible and adequate to prevent landslides or other damage to property and safety. The review authority may impose conditions, including limits on type or intensity of land use, which it determines are necessary to mitigate known risks of landslides or property damage.*

**Staff Finding 83: The applicant has submitted a Stormwater Management Report, prepared by a licensed engineer, which complies with the West Linn Public Works Design Standards, shows no adverse off-site impacts, and provides sufficient factual data to support the conclusions of the plan (pages 162 to 171 of packet). The subject property does not contain**

any known landslide hazards. Any geotechnical hazards associated with on-site soil structure can be remediated per the Carlson Geotechnical report dated July 19, 2018 (pages 172 to 238 of packet). The applicant shall comply with the requirements and install improvements to meet the West Linn Public Works Design Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.

3. *Municipal water. A registered civil engineer shall prepare a plan for the provision of water which demonstrates to the City Engineer's satisfaction the availability of sufficient volume, capacity, and pressure to serve the proposed development's domestic, commercial, and industrial fire flows. All plans will then be reviewed by the City Engineer.*

**Staff Finding 84: Water is available in Old River Drive to serve the proposed development. A registered civil engineer prepared the water provision plan, including a new six-inch fire water service to provide complete sprinkler systems for both existing and proposed buildings. The City's public water system has sufficient capacity and pressure in this area. Private water system sizing will be analyzed with the building permit. There is an existing six-inch water line on the north side of the property. The applicant shall dedicate a 15 foot wide easement, centered over the water line, to the City for on-going operation and maintenance of the existing water line. The easement shall be dedicated to the City, on a form provided by the City, prior to issuance of any occupancy permits per Condition of Approval 8. Subject to the Conditions of Approval, these criteria are met.**

4. *Sanitary sewers. A registered civil engineer shall prepare a sewerage collection system plan which demonstrates sufficient on-site capacity to serve the proposed development. The City Engineer shall determine whether the existing City system has sufficient capacity to serve the development.*

**Staff Finding 85: The applicant has submitted a plan prepared by a registered civil engineer that will gravity flow to the existing sanitary sewer line in Old River Drive. The system will be built to appropriate standards. The City public sanitary sewer system has sufficient capacity to service the proposed use. These criteria are met.**

5. *Solid waste and recycling storage areas. Appropriately sized and located solid waste and recycling storage areas shall be provided. Metro standards shall be used.*

**Staff Finding 86: The applicant proposal provides a screened solid waste and recycling area that meets Metro standards. An existing sanitary sewer line and easement exists near the location of the refuse/recycling area. The applicant shall vacate the existing easement and dedicate a new 15 foot wide easement centered over the sanitary sewer line before construction of the refuse/recycling area per Condition of Approval 7. Subject to the Conditions of Approval, these criteria are met.**



*J. Crime prevention and safety/defensible space.*

- 1. Windows shall be located so that areas vulnerable to crime can be surveyed by the occupants.*
- 2. Interior laundry and service areas shall be located in a way that they can be observed by others.*
- 3. Mailboxes, recycling, and solid waste facilities shall be located in lighted areas having vehicular or pedestrian traffic.*

**Staff Finding 87: Staff incorporates applicant findings (page 106 of packet). In addition, the applicant has provided a proposed lighting plan that illuminates all areas vulnerable to crime (pages 154-155 of packet). These criteria are met.**

- 4. The exterior lighting levels shall be selected and the angles shall be oriented towards areas vulnerable to crime.*
- 5. Light fixtures shall be provided in areas having heavy pedestrian or vehicular traffic and in potentially dangerous areas such as parking lots, stairs, ramps, and abrupt grade changes.*
- 6. Fixtures shall be placed at a height so that light patterns overlap at a height of seven feet which is sufficient to illuminate a person. All commercial, industrial, residential, and public facility projects undergoing design review shall use low or high pressure sodium bulbs and be able to demonstrate effective shielding so that the light is directed downwards rather than omni-directional. Omni-directional lights of an ornamental nature may be used in general commercial districts only.*

**Staff Finding 88: The applicant has provided a proposed lighting plan that illuminates all areas vulnerable to crime. The applicant has proposed limited lighting in parking areas to reduce impact to surrounding properties. The light fixtures will comply with bulb standards and be directed downward. These criteria are met.**

- 7. Lines of sight shall be reasonably established so that the development site is visible to police and residents.*
- 8. Security fences for utilities (e.g., power transformers, pump stations, pipeline control equipment, etc.) or wireless communication facilities may be up to eight feet tall in order to protect public safety. No variances are required regardless of location.*

**Staff Finding 89: Staff incorporates applicant findings (page 106 of packet). These criteria are met.**

*K. Provisions for persons with disabilities.*

- 1. The needs of a person with a disability shall be provided for. Accessible routes shall be provided between all buildings and accessible site facilities. The accessible route shall be the most practical direct route between accessible building entries, accessible site facilities, and the accessible entry to the site. An accessible route shall connect to the public right-of-way and to at least one on-site or adjacent transit stop (if the area is served by transit). All facilities shall*

*conform to, or exceed, the Americans with Disabilities Act (ADA) standards, including those included in the Uniform Building Code.*

**Staff Finding 90: Staff incorporates applicant findings (page 106 of packet). These criteria are met.**

*L. Signs.*

*(...)*

- 2. The signs, graphics, and letter styles shall be designed to be compatible with surrounding development, to contribute to a sense of project identity, or, when appropriate, to reflect a sense of the history of the area and the architectural style.*
- 3. The sign graphics and letter styles shall announce, inform, and designate particular areas or uses as simply and clearly as possible.*
- 4. The signs shall not obscure vehicle driver's sight distance.*
- 5. Signs indicating future use shall be installed on land dedicated for public facilities (e.g., parks, water reservoir, fire halls, etc.).*
- 6. Signs and appropriate traffic control devices and markings shall be installed or painted in the driveway and parking lot areas to identify bicycle and pedestrian routes.*

**Staff Finding 91: The applicant does not propose any signage with this application. These criteria are met.**

*M. Utilities. The developer shall make necessary arrangements with utility companies or other persons or corporations affected for the installation of underground lines and facilities. Electrical lines and other wires, including but not limited to communication, street lighting, and cable television, shall be placed underground, as practical. The design standards of Tables 1 and 2 above, and of subsection 5.487 of the West Linn Municipal Code relative to existing high ambient noise levels shall apply to this section.*

**Staff Finding 92: The City finds it practical for the applicant to underground all existing overhead utility lines as part of the public improvements and all new services will also be installed underground per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.**

*(...)*

*O. Refuse and recycling standards.*

- 1. All commercial, industrial and multi-family developments over five units requiring Class II design review shall comply with the standards set forth in these provisions. Modifications to these provisions may be permitted if the Planning Commission determines that the changes are consistent with the purpose of these provisions and the City receives written evidence from the local franchised solid waste and recycling firm that they are in agreement with the proposed modifications.*
- 2. Compactors, containers, and drop boxes shall be located on a level Portland cement concrete pad, a minimum of four inches thick, at ground elevation or other location compatible*

*with the local franchise collection firm's equipment at the time of construction. The pad shall be designed to discharge surface water runoff to avoid ponding.*

**Staff Finding 93: Staff incorporates applicant findings (page 108 of packet). These criteria are met.**

3. *Recycling and solid waste service areas.*

a. *Recycling receptacles shall be designed and located to serve the collection requirements for the specific type of material.*

b. *The recycling area shall be located in close proximity to the garbage container areas and be accessible to the local franchised collection firm's equipment.*

c. *Recycling receptacles or shelters located outside a structure shall have lids and be covered by a roof constructed of water and insect-resistive material. The maintenance of enclosures, receptacles and shelters is the responsibility of the property owner.*

d. *The location of the recycling area and method of storage shall be approved by the local fire marshal.*

e. *Recycling and solid waste service areas shall be at ground level and/or otherwise accessible to the franchised solid waste and recycling collection firm.*

f. *Recycling and solid waste service areas shall be used only for purposes of storing solid waste and recyclable materials and shall not be a general storage area to store personal belongings of tenants, lessees, property management or owners of the development or premises.*

g. *Recyclable material service areas shall be maintained in a clean and safe condition.*

**Staff Finding 94: The applicant will provide a recycling/refuse area in the southwest corner of the parking lot. The area will be maintained in a clean and safe condition. An existing sanitary sewer line and easement exists near the location of the refuse/recycling area. The applicant shall vacate the existing easement and dedicate a new 15 foot wide easement centered over the sanitary sewer line before construction of the refuse/recycling area per Condition of Approval 7. Subject to the Conditions of Approval, these criteria are met.**

4. *Special wastes or recyclable materials.*

a. *Environmentally hazardous wastes defined in ORS 466.005 shall be located, prepared, stored, maintained, collected, transported, and disposed in a manner acceptable to the Oregon Department of Environmental Quality.*

b. *Containers used to store cooking oils, grease or animal renderings for recycling or disposal shall not be located in the principal recyclable materials or solid waste storage areas. These materials shall be stored in a separate storage area designed for such purpose.*

**Staff Finding 95: The proposal does not include the need for any special waste or recyclable material storage. The private school does not prepare or provide food services for the students. These criteria are met.**

5. *Screening and buffering.*

a. *Enclosures shall include a curbed landscape area at least three feet in width on the sides and rear. Landscaping shall include, at a minimum, a continuous hedge maintained at a height of 36 inches.*

b. *Placement of enclosures adjacent to residentially zoned property and along street frontages is strongly discouraged. They shall be located so as to conceal them from public view to the maximum extent possible.*

c. *All dumpsters and other trash containers shall be completely screened on all four sides with an enclosure that is comprised of a durable material such as masonry with a finish that is architecturally compatible with the project. Chain link fencing, with or without slats, will not be allowed.*

**Staff Finding 96: The applicant proposal includes a recycling/refuse enclosure in the southwest corner of the parking lot. The enclosure will be surrounded by a six-foot wooden framed opaque enclosure. The enclosure will have a curbed, three-foot landscape area with a 36 inch continuous hedge on both the rear and sides. These criteria are met.**

6. *Litter receptacles.*

a. *Location. Litter receptacles may not encroach upon the minimum required walkway widths.*

b. *Litter receptacles may not be located within public rights-of-way except as permitted through an agreement with the City in a manner acceptable to the City Attorney or his/her designee.*

**Staff Finding 97: The applicant does not propose any litter receptacles in the public right-of-way. Any receptacles on the subject property will be located to not encroach upon required walkway widths. These criteria are met.**

55.125 TRANSPORTATION ANALYSIS

*Certain development proposals required that a Traffic Impact Analysis (TIA) be provided which may result in modifications to the site plan or conditions of approval to address or minimize any adverse impacts created by the proposal. The purpose, applicability and standards of this analysis are found in CDC 85.170(B)(2).*

**Staff Finding 98: The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.**

55.170 EXCEPTIONS TO UNDERLYING ZONE, YARD, PARKING, SIGN PROVISIONS, AND LANDSCAPING PROVISIONS

D. *The Planning Director may grant an exception to the landscaping requirements in the applicable zone based on findings that the following criteria will be met:*

1. *A minor exception that is not greater than 10 percent of the required landscaped area.*
2. *A more efficient use of the site.*
3. *The preservation of natural features that have been incorporated into the overall design of the project.*
4. *No adverse effect to adjoining property.*

**Staff Finding 99:** The applicant is requesting an exception to the 10 foot required landscape strip that separates a parking area abutting a street (CDC 54.E.3(d)). The applicant proposes a reduced setback of nine-feet, which is a 10 percent reduction. The reduction allows for a more efficient use of the site because the proposal is to utilize the existing parking area layout and with the installation of a sidewalk and planter strip, the landscape strip will be reduced to nine-feet. The properties on the opposite side of the street will not experience any visual encroachment from the minor reduction in the landscape strip. Staff supports this exception request. Subject to approval of the exception, these criteria are met.

## **IX. CHAPTER 60, CONDITIONAL USES**

### *60.070 APPROVAL STANDARDS AND CONDITIONS*

*A. The Planning Commission shall approve, approve with conditions, or deny an application for a conditional use, except for a manufactured home subdivision in which case the approval standards and conditions shall be those specified in CDC 36.030, or to enlarge or alter a conditional use based on findings of fact with respect to each of the following criteria:*

1. *The site size and dimensions provide:*
  - a. *Adequate area for the needs of the proposed use; and*
  - b. *Adequate area for aesthetic design treatment to mitigate any possible adverse effect from the use on surrounding properties and uses.*

**Staff Finding 100:** Staff incorporates applicant findings (page 115 of packet). In addition, the subject property is comprised of three lots of record (Lots 2, 3, and 4 of Cedaroak Park Subdivision). City Building Codes do not allow the construction of buildings across property lines. The applicant must apply for and get approval for a Property Line Adjustment to consolidate the three lots into one. This must be completed before the issuance of building permits per Condition of Approval 6. Subject to the Conditions of Approval, these criteria are met.

2. *The characteristics of the site are suitable for the proposed use considering size, shape, location, topography, and natural features.*

**Staff Finding 101:** Staff incorporates applicant findings (page 115 of packet). These criteria are met.

3. *The granting of the proposal will provide for a facility that provides an overall benefit to the City.*

**Staff Finding 102: Staff incorporates applicant findings (page 115 of packet). These criteria are met.**

4. *Adequate public facilities will be available to provide service to the property at the time of occupancy.*
5. *The applicable requirements of the zone are met, except as modified by this chapter.*
6. *The supplementary requirements set forth in Chapters 52 to 55 CDC, if applicable, are met.*
7. *The use will comply with the applicable policies of the Comprehensive Plan.*

**Staff Finding 103: Staff incorporates applicant findings (pages 115 to 119 of packet). In addition, the applicant will install street improvements per Condition of Approval 2 to ensure adequate transportation facilities are available. The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.**

*B. An approved conditional use or enlargement or alteration of an existing conditional use shall be subject to the development review provisions set forth in Chapter 55 CDC.*

**Staff Finding 104: Please see Staff Findings 68 to 108. These criteria are met.**

*C. The Planning Commission may impose conditions on its approval of a conditional use which it finds are necessary to assure the use is compatible with other uses in the vicinity. These conditions may include, but are not limited to, the following:*

1. *Limiting the hours, days, place, and manner of operation.*

**Staff Finding 105: Staff incorporates applicant findings (page 120 of packet). These criteria are met.**

2. *Requiring design features which minimize environmental impacts such as noise, vibration, air pollution, glare, odor, and dust.*

**Staff Finding 106: Staff incorporates applicant findings (page 120 of packet). These criteria are met.**

3. *Requiring additional setback areas, lot area, or lot depth, or width.*

**Staff Finding 107: The subject property is located in the R-10 zone and proposes the following:**

|                    | R-10 Standards | School Proposal |
|--------------------|----------------|-----------------|
| Front Yard Setback | 20 ft.         | 45 ft.          |
| Rear Yard Setback  | 20 ft.         | 20 ft.          |
| Side Yard Setback  | 7.5 ft.        | 23 ft. & 67 ft. |
| Lot Area           | 10,000 sq. ft. | 64,669 sq. ft.  |
| Lot Depth          | n/a            | 200 ft.         |
| Lot Width          | 50 ft.         | 325 ft.         |

These criteria are met.

4. *Limiting the building height, size or lot coverage, or location on the site.*

**Staff Finding 108:** The subject property is located in the R-10 zone and proposes the following:

|                     | R-10 Standards | School Proposal |
|---------------------|----------------|-----------------|
| Building Height     | 35 ft.         | 32 ft.          |
| Lot Coverage        | 35%            | 23%             |
| Floor-to-Area Ratio | 0.45           | 0.39            |

These criteria are met.

5. *Designating the size, number, location and design of vehicle access points.*

**Staff Finding 109:** Staff incorporates applicant findings (page 120 of packet). These criteria are met.

6. *Requiring street right-of-way to be dedicated and the street to be improved including all steps necessary to address future street improvements identified in the adopted Transportation System Plan.*

**Staff Finding 110:** Staff incorporates applicant findings (page 120 of packet). In addition, the applicant will install street improvements per Condition of Approval 2 to address street improvements identified in the Transportation System Plan. These criteria are met.

7. *Requiring participation in making the intersection improvement or improvements identified in the Transportation System Plan when a traffic analysis (compiled as an element of a conditional use application for the property) indicates the application should contribute toward.*

**Staff Finding 111:** The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant

**the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.**

*8. Requiring landscaping, screening, drainage, and surfacing of parking and loading areas.*

**Staff Finding 112: Staff incorporates applicant findings (page 120 of packet). These criteria are met.**

*9. Limiting the number, size, location, height, and lighting of signs.*

**Staff Finding 113: Staff incorporates applicant findings (page 120 of packet). These criteria are met.**

*10. Limiting or setting standards for the location and intensity of outdoor lighting.*

**Staff Finding 114: Staff incorporates applicant findings (page 120 of packet). These criteria are met.**

*11. Requiring berming, screening, or landscaping and the establishment of standards for their installation and maintenance.*

**Staff Finding 115: Staff incorporates applicant findings (page 120 of packet). These criteria are met.**

*12. Requiring and designating the size, height, location, and materials for fences.*

**Staff Finding 116: Staff incorporates applicant findings (page 120 of packet). These criteria are met.**

*13. Requiring the protection and preservation of existing trees, soils, vegetation, watercourses, habitat areas, and drainage areas.*

**Staff Finding 117: Staff incorporates applicant findings (page 120 of packet). These criteria are met.**

*D. Aggregate extraction uses shall also be subject to the provisions of ORS [541.605](#).*

*E. The Historic Review Board shall review an application for a conditional use, or to enlarge a conditional use on a property designated as a historic resource, based on findings of fact that the use will:*

- 1. Preserve or improve a historic resource which would probably not be preserved or improved otherwise; and*
- 2. Utilize existing structures rather than new structures.*



**Staff Finding 118: Staff incorporates applicant findings (page 120 of packet). These criteria are met.**

## **X. CHAPTER 75, VARIANCES AND SPECIAL WAIVERS**

### *75.020 CLASSIFICATION OF VARIANCES*

*A. Class I Variance. Class I variances provide minor relief from certain code provisions where it can be demonstrated that the modification will not harm adjacent properties, and it conforms with any other code requirements. Class I variances are allowed for the following code provisions:*

- 1. Required Yard and Minimum Lot Dimensional Requirements. (...)*
- 2. Off-street parking dimensional and minimum number of space requirements may be modified up to 10 percent (...)*
- 3. Dimensional sign requirements may be modified up to 10 percent (...)*
- 4. Landscaping requirements in the applicable zone may be modified up to 10 percent (...)*

**Staff Finding 119: The applicant did not request any Class I Variances. These criteria are not applicable.**

*B. Class II Variance. Class II variances may be utilized when strict application of code requirements would be inconsistent with the general purpose of the CDC and would create a burden upon a property owner with no corresponding public benefit. A Class II variance will involve a significant change from the code requirements and may create adverse impacts on adjacent property or occupants. It includes any variance that is not classified as a Class I variance or special waiver.*

**Staff Finding 120: The applicant is requesting two Class II Variances to allow a reduction of off-street parking spaces from 48 to 37 as required in CDC Section 46.090 and to waive the required loading space as required by CDC Section 46.130. These criteria are met.**

*1. Class II Variance Approval Criteria. The approval authority may impose appropriate conditions to ensure compliance with the criteria. The appropriate approval authority shall approve a variance request if all the following criteria are met and corresponding findings of fact prepared.*

*a. The variance is the minimum variance necessary to make reasonable use of the property. To make this determination, the following factors may be considered, together with any other relevant facts or circumstances:*

- 1) Whether the development is similar in size, intensity and type to developments on other properties in the City that have the same zoning designation.*

**Staff Finding 121: Staff incorporates applicant findings (pages 123-125 of packet). This criterion is met.**

2) *Physical characteristics of the property such as lot size or shape, topography, or the existence of natural resources.*

**Staff Finding 122: Staff incorporates applicant findings (pages 123-125 of packet). This criterion is met.**

3) *The potential for economic development of the subject property.*

**Staff Finding 123: Staff incorporates applicant findings (pages 123-125 of packet). This criterion is met.**

*b. The variance will not result in violation(s) of any other code standard, and the variance will meet the purposes of the regulation being modified.*

**Staff Finding 124: Staff incorporates applicant findings (pages 123-125 of packet). This criterion is met.**

*c. The need for the variance was not created by the applicant and/or owner requesting the variance.*

**Staff Finding 125: Staff incorporates applicant findings (pages 123-125 of packet). This criterion is met.**

*d. If more than one variance is requested, the cumulative effect of the variances results in a project that is consistent with the overall purpose of the zone.*

**Staff Finding 126: Staff incorporates applicant findings (pages 123-125 of packet). This criterion is met.**

## ***XI. CHAPTER 92, REQUIRED IMPROVEMENTS***

### ***92.010 PUBLIC IMPROVEMENTS FOR ALL DEVELOPMENT***

*The following improvements shall be installed at the expense of the developer and meet all City codes and standards:*

- A. Streets within subdivisions.*
- B. Extension of streets to subdivisions*
- C. Local and minor collector streets*
- D. Monuments*

**Staff Finding 127: The applicant shall install improvements to meet the West Linn Public Works Design Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.**

*E. Storm detention and treatment. For Type I, II and III lands (refer to definitions in Chapter 02 CDC), a registered civil engineer must prepare a storm detention and treatment plan, at a scale sufficient to evaluate all aspects of the proposal, and a statement that demonstrates:*

- 1. The location and extent to which grading will take place indicating general contour lines, slope ratios, slope stabilization proposals, and location and height of retaining walls, if proposed.*
- 2. All proposed storm detention and treatment facilities comply with the standards for the improvement of public and private drainage systems located in the West Linn Public Works Design Standards.*
- 3. There will be no adverse off-site impacts, including impacts from increased intensity of runoff downstream or constrictions causing ponding upstream.*
- 4. There is sufficient factual data to support the conclusions of the plan.*
- 5. Per CDC 99.035, the Planning Director may require the information in subsections (E)(1), (2), (3) and (4) of this section for Type IV lands if the information is needed to properly evaluate the proposed site plan.*

**Staff Finding 128: The applicant has submitted a Stormwater Management Report, prepared by a licensed engineer, which complies with the West Linn Public Works Design Standards, shows no adverse off-site impacts, and provides sufficient factual data to support the conclusions of the plan. Included is a grading plan with general contour lines and location/height of retaining walls. The applicant shall comply with the requirements and install improvements to meet the West Linn Public Works Design Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.**

*F. Sanitary sewers*

*(...)*

*G. Water system*

*(...)*

*H. Sidewalks.*

*(...)*

**Staff Finding 129: The applicant has designed the sanitary sewer, water system, and sidewalks to comply with City of West Linn Public Works Design Standards. The applicant shall install all improvements to meet the Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.**

*I. Bicycle routes.*

*J. Street name signs.*

*K. Dead-end street signs.*

*L. Signs indicating future use.*

*M. Street lights.*

**Staff Finding 130: The applicant shall comply with the requirements and install improvements to meet the West Linn Public Works Design Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.**

*N. Utilities.*

*O. Curb cuts and driveways.*

*P. Street trees.*

*Q. Joint mailbox facilities*

**Staff Finding 131: The applicant shall comply with the requirements and install improvements to meet the West Linn Public Works Design Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.**

*92.030 IMPROVEMENT PROCEDURES*

*(...)*

**Staff Finding 132: The applicant shall comply with the requirements and install improvements to meet the West Linn Public Works Design Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.**

***XII. CHAPTER 96, STREET IMPROVEMENT CONSTRUCTION***

*96.010 CONSTRUCTION REQUIRED*

*A. New construction.*

*(...)*

*B. Remodeling of an existing building.*

*(...)*

**Staff Finding 133: The applicant proposal includes construction of half-street improvements along the portion of Old River Drive that is adjacent to the subject property. The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2. Installation of all street improvements will meet the West Linn Public Works Design Standards per Condition of Approval 2.**

*96.020 STANDARDS*

*Street improvements shall be installed according to the City standards and shall be completed prior to the issuance of any occupancy permit for the new or remodeled structure or building. In unimproved areas of the City, the City Engineer may grant a time extension of the provisions of*

*this section; provided, that the applicant provides sufficient security in amount and quantity satisfactory to the City Attorney to assure payment of such improvement costs.*

**Staff Finding 134:** The applicant proposal includes construction of half-street improvements along the portion of Old River Drive that is adjacent to the subject property. The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2. Installation of street improvements will be complete before occupancy and meet the West Linn Public Works Design Standards per Condition of Approval 2.

### ***XIII. CHAPTER 99, PROCEDURES FOR DECISION MAKING: QUASI-JUDICIAL***

#### *99.030 APPLICATION PROCESS: WHO MAY APPLY, PRE-APPLICATION CONFERENCE, REQUIREMENTS, REFUSAL OF APPLICATION, FEES*

*(...)*

*B. Pre-application conferences.*

*1. Subject to subsection (B)(4) of this section, a pre-application conference is required for, but not limited to, each of the following applications:*

*(...)*

*d. Conditional uses;*

*e. Design review (Class I and Class II);*

*(...)*

*o. Variances;*

*(...)*

**Staff Finding 135:** The applicant attended a pre-application conference with City staff on January 18, 2018. These criteria are met.

#### *99.038 NEIGHBORHOOD CONTACT REQUIRED FOR CERTAIN APPLICATIONS*

*Prior to submittal of an application for any subdivision, conditional use permit, (...)*

**Staff Finding 136:** The applicant had neighborhood contact by attending the Robinwood Neighborhood Meeting on July 10, 2018. Required documentation pertaining to this meeting are included in the applicant submittal. These criteria are met.

#### *99.060 APPROVAL AUTHORITY*

*B. Planning Commission authority. The Planning Commission shall have the authority to:*

*(...)*

*2. Approve, deny, or approve with conditions the following applications:*

*(...)*

*b. A conditional use (Chapter 60 CDC).*

(...)

e. *Class II variance or special waiver (Chapter 75 CDC).*

(...)

h. *Design review, Class II (Chapter 55 CDC).*

(...)

**Staff Finding 137: The applicant proposal will be heard by the Planning Commission at a public hearing scheduled for February 6, 2019.**

*99.080 NOTICE*

*Notice shall be given in the following ways:*

A. *Class A Notice. (...)*

**Staff Finding 138: The applicant proposal has been properly noticed by the City. Please see Staff Report for the Planning Commission Exhibit PC-3. These criteria are met.**

**EXHIBIT PC-1: APPLICANT SUBMITTAL**

## **EXHIBIT PC-2: STREET IMPROVEMENT MEMOS**



**DEVELOPMENT REVIEW APPLICATION**

|                                                 |                                                                     |                        |
|-------------------------------------------------|---------------------------------------------------------------------|------------------------|
| <b>For Office Use Only</b>                      |                                                                     |                        |
| STAFF CONTACT<br><i>Darren Wyss</i>             | PROJECT NO(S)<br><i>WP-18-01 / DR-18-04 / VAR-18-02 / VAR-18-03</i> |                        |
| NON-REFUNDABLE FEE(S)<br><i>2900, 1450, 500</i> | REFUNDABLE DEPOSIT(S)<br><i>16,500</i>                              | TOTAL<br><i>21,350</i> |

**Type of Review (Please check all that apply):**

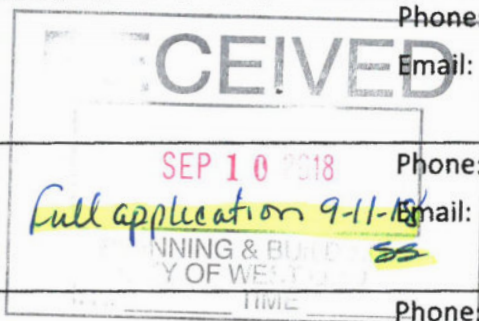
- |                                                                             |                                                                           |                                                                          |
|-----------------------------------------------------------------------------|---------------------------------------------------------------------------|--------------------------------------------------------------------------|
| <input type="checkbox"/> Annexation (ANX)                                   | <input type="checkbox"/> Historic Review                                  | <input type="checkbox"/> Subdivision (SUB)                               |
| <input type="checkbox"/> Appeal and Review (AP) *                           | <input type="checkbox"/> Legislative Plan or Change                       | <input type="checkbox"/> Temporary Uses *                                |
| <input checked="" type="checkbox"/> Conditional Use (CUP) <i>4500 + 200</i> | <input type="checkbox"/> Lot Line Adjustment (LLA) */**                   | <input type="checkbox"/> Time Extension *                                |
| <input checked="" type="checkbox"/> Design Review (DR) <i>12000 + 300</i>   | <input type="checkbox"/> Minor Partition (MIP) (Preliminary Plat or Plan) | <input checked="" type="checkbox"/> Variance (VAR) <i>2900 + 1450</i>    |
| <input type="checkbox"/> Easement Vacation                                  | <input type="checkbox"/> Non-Conforming Lots, Uses & Structures           | <input type="checkbox"/> Water Resource Area Protection/Single Lot (WAP) |
| <input type="checkbox"/> Extraterritorial Ext. of Utilities                 | <input type="checkbox"/> Planned Unit Development (PUD)                   | <input type="checkbox"/> Water Resource Area Protection/Wetland (WAP)    |
| <input type="checkbox"/> Final Plat or Plan (FP)                            | <input type="checkbox"/> Pre-Application Conference (PA) */**             | <input type="checkbox"/> Willamette & Tualatin River Greenway (WRG)      |
| <input type="checkbox"/> Flood Management Area                              | <input type="checkbox"/> Street Vacation                                  | <input type="checkbox"/> Zone Change                                     |
| <input type="checkbox"/> Hillside Protection & Erosion Control              |                                                                           |                                                                          |

Home Occupation, Pre-Application, Sidewalk Use, Sign Review Permit, and Temporary Sign Permit applications require different or additional application forms, available on the City website or at City Hall.

|                                                                               |                             |
|-------------------------------------------------------------------------------|-----------------------------|
| <b>Site Location/Address:</b><br>19915 Old River Drive<br>West Linn, OR 97068 | Assessor's Map No.: 21E23AA |
|                                                                               | Tax Lot(s): 400, 500        |
|                                                                               | Total Land Area: 0.99 acres |

**Brief Description of Proposal:** The owner/applicant is proposing to redevelop the site and change the use from a church to a school as part of the proposed phased development plan. The proposed school will ultimately have a two-story building built on the vacant northern portion of the site, a new/re-designed parking lot including vehicle and school bus access ways, outdoor recreation facilities, open space areas, pedestrian pathways, and new landscaped areas.

|                                                                                      |                                            |                                       |                     |                                     |
|--------------------------------------------------------------------------------------|--------------------------------------------|---------------------------------------|---------------------|-------------------------------------|
| <b>Applicant Name:</b> Sheila Walker<br><small>(please print)</small>                | Address: 1232 Linn Avenue                  | City State Zip: Oregon City, OR 97045 | Phone: 503-650-0978 | Email: info@themarylhurstschool.org |
| <b>Owner Name</b> (required): The Marylhurst School<br><small>(please print)</small> | Address: 1232 Linn Avenue                  | City State Zip: Oregon City, OR 97045 | Phone: 503-650-0978 | Email: info@themarylhurstschool.org |
| <b>Consultant Name:</b> Cardno (Attn: Kevin Brady)<br><small>(please print)</small>  | Address: 6720 SW Macadam Avenue, Suite 200 | City State Zip: Portland, OR 97219    | Phone: 503-419-2500 | Email: kevin.brady@cardno.com       |



- All application fees are non-refundable (excluding deposit). Any overruns to deposit will result in additional billing.
  - The owner/applicant or their representative should be present at all public hearings.
  - A denial or approval may be reversed on appeal. No permit will be in effect until the appeal period has expired.
  - Three (3) complete hard-copy sets (single sided) of application materials must be submitted with this application. One (1) complete set of digital application materials must also be submitted on CD in PDF format. If large sets of plans are required in application please submit only two sets.
- \* No CD required / \*\* Only one hard-copy set needed

The undersigned property owner(s) hereby authorizes the filing of this application, and authorizes on site review by authorized staff. I hereby agree to comply with all code requirements applicable to my application. Acceptance of this application does not infer a complete submittal. All amendments to the Community Development Code and to other regulations adopted after the application is approved shall be enforced where applicable. Approved applications and subsequent development is not vested under the provisions in place at the time of the initial application.

*Sheila Walker*      8/14/18      *Patricia E. Lewis*      8-14-18  
Applicant's signature      Date      Owner's signature (required)      Date



**REQUEST FOR WAIVER OF STREET IMPROVEMENTS  
PAYMENT OF FEE-IN-LIEU**

22500 Salamo Rd. Box 800; West Linn, OR 97068  
Phone: (503)722-5500 Fax: (503)656-4106  
Email: [road\\_permit@westlinnoregon.gov](mailto:road_permit@westlinnoregon.gov)



Complete and sign all fields and the statement below indicating your application for a waiver of street improvements and the option to make a payment in lieu of construction of street improvements as allowed by West Linn Community Development Code section 96.010.

| APPLICANT INFORMATION |                                 |                                                                    |    |      |       | PROJECT INFORMATION |  |                      |
|-----------------------|---------------------------------|--------------------------------------------------------------------|----|------|-------|---------------------|--|----------------------|
| Applicant Name:       |                                 | Owner: New Life Church Robinwood, Applicant: the Marylhurst School |    |      |       | Project Address     |  | 1995 Old River Drive |
| Address:              |                                 | (applicant address) 1232 Linn Ave                                  |    |      |       | Permit #            |  |                      |
| City:                 | Oregon City                     | State:                                                             | OR | Zip: | 97045 | Project description |  |                      |
| Phone:                | 503.804.6405                    | Fax:                                                               |    |      |       |                     |  |                      |
| Email:                | Sheilaw@themarylhurstschool.org |                                                                    |    |      |       |                     |  |                      |

NEW LIFE CHURCH ROBINWOOD  
I, [redacted], the legal owner(s) of property at 1995 Old River Drive hereby apply for a waiver of street improvements in accordance with section 92.020 of the West Linn Community Development Code and agree to make a payment in-lieu of constructing said street improvements.

Applicant may provide three cost estimates to the City for approval or provide quantities to be assessed by City staff at recent construction values. A final payment calculation will be provided by the City.

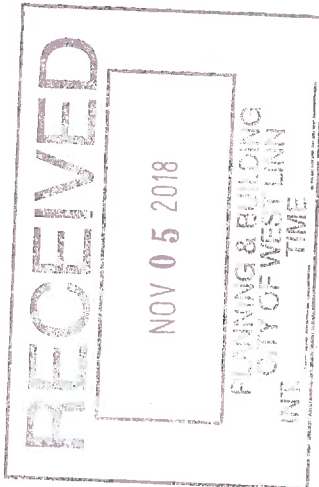
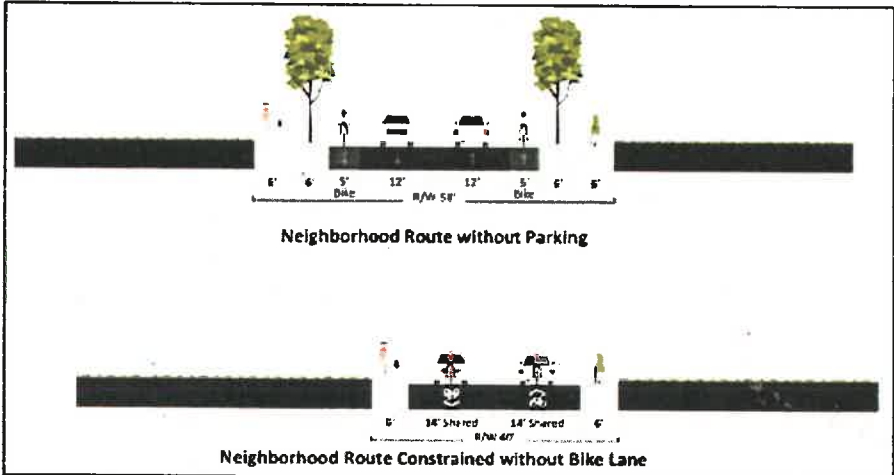
Owner(s) Signature:

Dale Austen for New Life Church Robinwood DocuSigned by: oct-17-2018 | 16:32 PD

|                                     |           |            |
|-------------------------------------|-----------|------------|
| Print                               | Signature | Date       |
| Sheila Walker The Marylhurst School |           | 10/17/2018 |
| Print                               | Signature | Date       |

Note: Engineering estimate developed below for future improvements on Old River Road frontage is based on a hybrid of the two sections below:

1. Add additional 2' lane width (base rock & paving) for total 14' wide shared lane.
2. Remove existing 12' wide half-street to centerline and replace with new 4" AC & 12" base rock.
3. Provide standard concrete curb & gutter. Existing storm drain to remain in place.
4. Provide 5' wide landscape strip including street trees and storm treatment planters.
5. Provide 6' wide concrete sidewalk.



| PROJECT QUANTITIES               |          |      |           |                    |
|----------------------------------|----------|------|-----------|--------------------|
|                                  | Quantity | Unit | Cost/Unit | Total Cost         |
| Mobilization                     | 1        | LS   | 10,000.00 | 10,000.00          |
| Sawcut AC                        | 335      | LF   | 2.50      | 837.50             |
| Remove Existing AC               | 433      | SY   | 7.50      | 3,247.50           |
| 10-inches of 1-1/2" Crushed Rock | 505      | SY   | 17.50     | 8,837.50           |
| 2-inches of 3/4"-0 Crushed Rock  | 505      | SY   | 5.00      | 2,525.00           |
| 4" Level 3 1/2" Dense HMAc       | 505      | SY   | 25.00     | 12,625.00          |
| Curb and Gutter                  | 325      | LF   | 25.00     | 8,125.00           |
| Concrete Sidewalk                | 1,950    | SF   | 6.00      | 11,700.00          |
| Concrete Inlet                   | 2        | EACH | 1,500.00  | 3,000.00           |
| Storm Manhole (Junction Boxes)   | 2        | EACH | 1,000.00  | 2,000.00           |
| Storm Pipe (Beehive inlets)      | 2        | LF   | 1,000.00  | 2,000.00           |
| Planter/Swale Soil/Landscape     | 325      | LF   | 20.00     | 6,500.00           |
| Street Tree (Estimated)          | 10       | EACH | 250.00    | 2,500.00           |
| Traffic Control                  | 1        | LS   | 5,000.00  | 5,000.00           |
| Erosion Control                  | 1        | LS   | 2,500.00  | 2,500.00           |
| <b>TOTAL COST</b>                |          |      |           | <b>\$81,397.50</b> |

| PUBLIC WORKS – ENGINEERING DIVISION – STAFF ONLY                    |                                                                                                                                           |
|---------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------|
| <input type="checkbox"/>                                            | The West Linn Transportation System Plan does not include street improvements in this area and a fee-in-lieu payment is <b>approved</b> . |
| <input checked="" type="checkbox"/>                                 | The West Linn Transportation System Plan includes street improvements in this area and a waiver of construction is <b>not approved</b> .  |
| <b>Total final street/sidewalk fee-in-lieu for this project is:</b> |                                                                                                                                           |
|                                                                     | \$                                                                                                                                        |
| CITY APPROVAL - DENIED                                              |                                                                                                                                           |
| PI Program Manager Approval:                                        | Date: 11/16/2018                                                                                                                          |

# The Marylhurst School

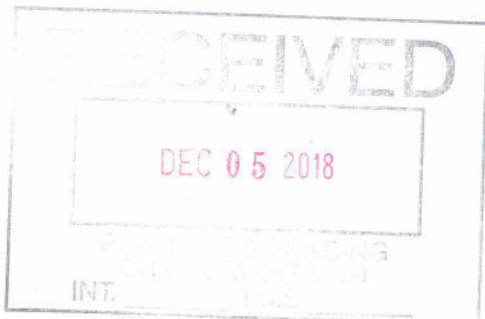
West Linn, Oregon

2<sup>nd</sup> Re-submittal: December 5, 2018

**An Application for:**  
Class 3 Conditional Use  
Class 2 Design Review  
Class 2 Variances

**Applicant:**  
**The Marylhurst School**  
1232 Linn Avenue  
Oregon City, OR 97045  
Phone: 503-650-0978  
Contact: Sheila Walker  
info@themarylhurstchurch.org

**Prepared by:**  
**Cardno**  
6720 SW Macadam Avenue, Suite 200  
Portland, Oregon 97219  
Phone: 503-419-2500  
Contact: Kevin Brady  
kevin.brady@cardno.com



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**EXHIBITS**

- Exhibit A Land Use Application Form**
- Exhibit B Title Documents**
- Exhibit C City Pre-Application Conference Notes**
- Exhibit D Preliminary Development Plans (Revised)**
- Exhibit E Preliminary Drainage Report**
- Exhibit F Traffic Impact Analysis**
- Exhibit G Geotechnical Report**
- Exhibit H Neighborhood Meeting Materials**
- Exhibit I Narrative (Revised)**
- Exhibit J Arborist Report and Tree Protection Plan**
- Exhibit K Parking Variance Supporting Materials**



# I. INTRODUCTION

## GENERAL INFORMATION

**Applicant:** **Marylhurst School**  
1232 Linn Avenue  
Oregon City, OR 97045  
Phone: 503-650-0978  
Contact: Sheila Walker  
Email: info@themarylhurstschool.org

**Applicant's Representative:** **Cardno**  
6720 SW Macadam Avenue, Suite 200  
Portland, Oregon 97219  
Phone: 503-419-2500  
Contact: Kevin Brady  
Email: kevin.brady@cardno.com

**Tax Lot Information:** Tax Map 21E23AA, Tax Lot 400, 500

**Site Address:** 19915 Old River Drive, West Linn, Oregon 97068

**Lot Area:** 0.99 Acres

**Current Zoning District:** R-10 Single Family Residential

## **SUMMARY OF PROPOSAL**

The applicant, The Marylhurst School, is requesting approval for a redevelopment of an existing church to a school in the R-10 zone. The Marylhurst School property is at 19915 Old River Road and is legally identified as 21E23AA, tax lots 500 and 400. The site has previously operated as a church, including a preschool program for 195 children. The applicant is proposing to change the use to a school as part of the proposed development plan.

The existing northernmost building will be used for school assemblies, activities, etc., and the church use will be discontinued. A new two-story school will be built on the vacant north and west portions of the site and will accommodate 12 classrooms serving pre-school through grade eight. Overall the proposal includes: a new/redeveloped school building; new and/or re-designed parking; outdoor recreation facilities; open space areas; pedestrian pathways; and new landscaped areas. The applicant will also apply for a minor partition that will serve to consolidate the lots of the subject site.

Phase One will consist of the adaptive reuse of the existing structures on site. The existing northernmost building will be used for school assemblies, activities, etc., and the church use will be discontinued. The existing southernmost building will be used for classrooms, administrative offices and small teaching break out spaces. A portable classroom will be located in the proposed parking lot area on a relatively temporary basis and will house two (2) additional classrooms.

Phase Two will consist of a new two-story school to be built on the vacant north portion of the site, oriented parallel to the west property line. The new facility will accommodate 12 classrooms serving pre-school through grade eight. Overall the proposal includes: a new/redeveloped school building; new and/or re-designed parking; outdoor recreation facilities; open space areas; pedestrian pathways; and new landscaped areas. The portable classroom will be removed as part of the Phase Two development.

**II. CITY OF WEST LINN CODE**

**CHAPTER 5: GENERAL**

**05.020 CLASSIFICATION OF ZONES**

All areas within the corporate limits of the City of West Linn are hereby divided into zone districts, and the use of each tract and ownership of land within the corporate limits shall be limited to those uses permitted by the zoning classification applicable to each such tract as hereinafter designated. The zoning districts within the City of West Linn are hereby classified and designated as follows:

| ZONING DISTRICT                    | ZONE DESIGNATION | DWELLING UNITS PER NET ACRE | LOT SIZE PER UNIT IN SQUARE FEET |
|------------------------------------|------------------|-----------------------------|----------------------------------|
| Low Density                        |                  |                             |                                  |
| Single-Family Residential detached | R-10             | 4.35                        | 10,000                           |

**Response:** The subject site is zoned R-10 on the West Linn Official Zoning Map.

**CHAPTER 11: SINGLE-FAMILY RESIDENTIAL DETACHED, R-10**

**11.020 PROCEDURES AND APPROVAL PROCESS**

C. A conditional use (CDC 11.060) is a use the approval of which is discretionary with the Planning Commission. The approval process and criteria for approval are set forth in Chapter 60 CDC, Conditional Uses. If a use is not listed as a conditional use, it may be held to be a similar unlisted use under the provisions of Chapter 80 CDC..070

**Response:** The proposed use is considered School, therefore, the use is considered a Conditional Use. As a Conditional Use, the proposal is addressed below under Section 11.060, Conditional Uses.

D. The following code provisions may be applicable in certain situations:

5. Chapter 75 CDC, Variance.

**Response:** The proposed use and development include components that cannot meet two of the relevant standard(s) in the Code. These include Section 46.130 (loading bay standards) and Section 46.090 (minimum parking spaces). Therefore, this application includes requests for Variances for Section 46.130 (loading bay standards) and Section 46.090 (minimum parking spaces). The narrative herein addresses the relevant criteria and standards for these Variances below.

**11.060 CONDITIONAL USES**

The following are conditional uses which may be allowed in this zoning district subject to the provisions of Chapter 60 CDC, Conditional Uses.

7. Schools.

**Response:** The proposed use is considered School, therefore, the use is considered a Conditional Use. As a Conditional Use, the proposal is addressed below under Section 11.060, Conditional Uses. Therefore, this application includes a request for a Conditional Use, and the narrative herein addresses the relevant criteria and standards for the Conditional Use below.

## 11.080 DIMENSIONAL REQUIREMENTS, CONDITIONAL USES

Except as may otherwise be established by this code, the appropriate lot or parcel size for a conditional use shall be determined by the approval authority at the time of consideration of the application based upon the criteria set forth in CDC 60.070(A) and (B).

**Response:** The existing lot is currently configured and no changes to the current configuration are proposed. However, the applicant is proposing lot consolidation for the subject site and will submit a separate minor partition application. The current overall size and shape of the subject site adequately accommodates the proposed use, as indicated on the Site Plans for Phase I and II in Exhibit D, Sheets C1.0 and C1.1.

## Chapter 34: ACCESSORY STRUCTURES, ACCESSORY DWELLING UNITS, AND ACCESSORY USES

### 34.020 ACCESSORY USES

Accessory uses are permitted uses which are customary and incidental to principal uses permitted in the zone and shall be permitted outright, or by prescribed conditions as identified below, and may be either attached or separated from the principal dwelling. Accessory uses on designated historic resources are subject to additional regulations in CDC 25.060(B).

**Response:** No accessory uses are proposed in conjunction with the School as a primary use. Therefore, this Section does not apply.

## CHAPTER 41: BUILDING HEIGHT, STRUCTURES ON STEEP LOTS, EXCEPTIONS

### 41.005 DETERMINING HEIGHT OF BUILDING

- A. For all zoning districts, building height shall be the vertical distance above a reference datum measured to the highest point of a flat roof or to the deck line of a mansard roof or to the highest gable, ridgeline or peak of a pitched or hipped roof, not including projections above roofs such as cupolas, towers, etc. The reference datum shall be selected by either of the following, whichever yields a greater height of building.
1. For relatively flat sites where there is less than a 10-foot difference in grade between the front and rear of the building, the height of the building shall be measured from grade five feet out from the exterior wall at the front of the building; or
  2. For steeper lots where there is more than a 10-foot difference in grade between the front and rear of the building, the height of the building is measured from grade at a point five feet out from the exterior wall on the lowest side (front or rear) of the building. One then measures vertically to the peak or ridgeline of the roof to determine the height.

**Response:** The site is considered a relatively flat site, with less than a 10-foot grade differential between the front and rear of the buildings. The maximum height for the proposed Conditional Use in the R-10 zone is 35 feet, per Section 11.070.6. The proposed height for the new modular building structure in Phase I is approximately 22 feet. The proposed height for the new addition in Phase II is approximately 33 feet. Therefore, the maximum building height requirement is met.

# CHAPTER 42: CLEAR VISION AREAS

## 42.020 CLEAR VISION AREAS REQUIRED, USES PROHIBITED

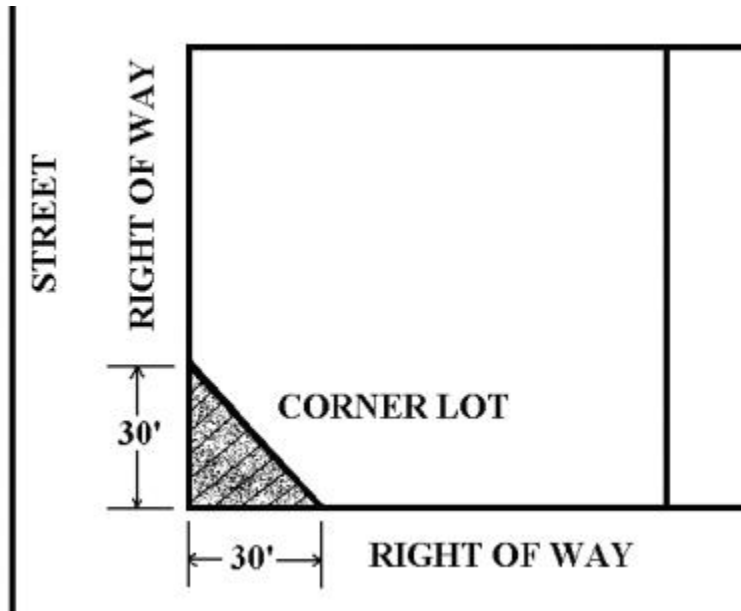
- A. A clear vision area shall be maintained on the corners of all property adjacent to an intersection as provided by CDC 42.040 and 42.050.
- B. A clear vision area shall contain no planting, fence, wall, structure or temporary or permanent obstruction (except for an occasional utility pole or tree) exceeding three feet in height, measured from the top of the curb, or, where no curb exists, from the street centerline grade, except that trees exceeding this height may be located in this area, provided all branches below eight feet are removed.

**Response:** The subject site is not considered a corner lot, therefore, this Section does not apply. However, clear vision areas are proposed to be maintained at both of the driveway curb cuts at the intersections with Old River Road. Clear vision areas are visually provided on the plan set.

## 42.040 COMPUTATION; STREET AND ACCESSWAY 24 FEET OR MORE IN WIDTH

The clear vision area for all street intersections and street and accessway intersections (accessways having 24 feet or more in width) shall be that triangular area formed by the right-of-way or property lines along such lots and a straight line joining the right-of-way or property line at points which are 30 feet distant from the intersection of the right-of-way line and measured along such lines.

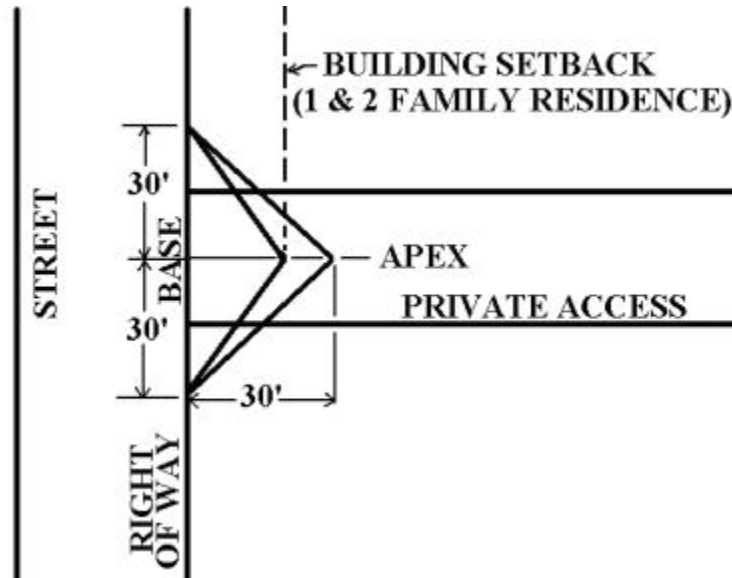
Clear vision area for corner lots and driveways 24 feet or more in width:



## 42.050 COMPUTATION; ACCESSWAY LESS THAN 24 FEET IN WIDTH

The clear vision area for street and accessway intersections (accessways having less than 24 feet in width) shall be that triangular area whose base extends 30 feet along the street right-of-way line in both directions from the centerline of the accessway at the front setback line of a single-family and two-family residence, and 30 feet back from the property line on all other types of uses.

Clear vision area for corner lots and driveways less than 24 feet in width:



**Response:** The applicant proposes two driveways to the site and the associated parking area. The first driveway is a 24-foot wide two-way access from Old River Road at the southeast corner of the site. The second driveway is a 20-foot wide one-way ingress that will serve as the main circulation access point for those using the designated drop-off zone at the front of the school. Both driveways meet the standards indicated in this Section and the associated figures, as depicted in the Site Plan on Sheet C1.0 of the Preliminary Development Plans in Exhibit D.

## CHAPTER 44: FENCES

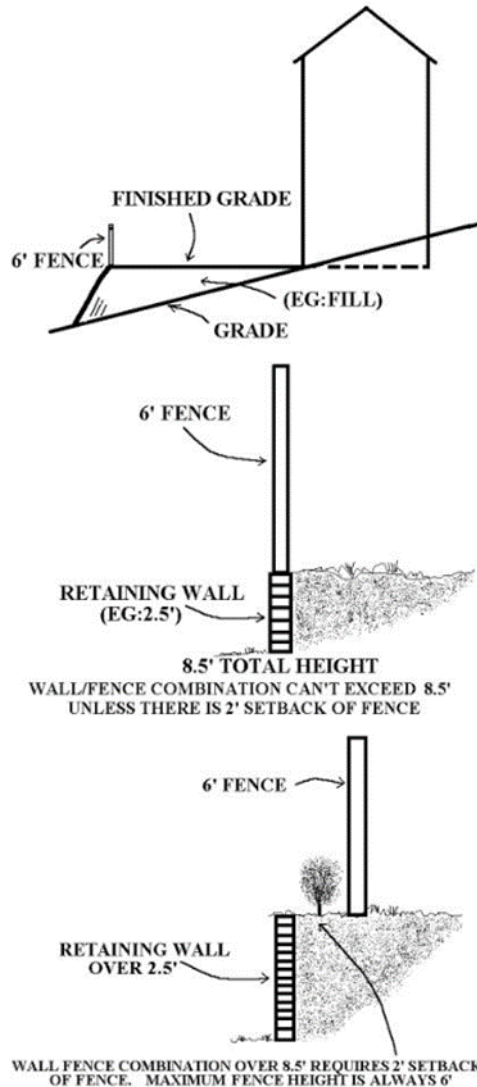
### 44.020 SIGHT-OBSCURING FENCE; SETBACK AND HEIGHT LIMITATIONS

- A. A sight- or non-sight-obscuring fence may be located on the property line or in a yard setback area subject to the following:
  1. The fence is located within:
    - a. A required front yard area, and it does not exceed three feet, except pillars and driveway entry features subject to the requirements of Chapter 42 CDC, Clear Vision Areas, and approval by the Planning Director;
    - b. A required side yard which abuts a street and it is within that portion of the side yard which is also part of the front yard setback area and it does not exceed three feet;
    - c. A required side yard which abuts a street and it is within that portion of the side yard which is not also a portion of the front yard setback area and it does not exceed six feet provided the provisions of Chapter 42 CDC are met;
    - d. A required rear yard which abuts a street and it does not exceed six feet; or
    - e. A required side yard area which does not abut a street or a rear yard and it does not exceed six feet.

**Response:** Some new fencing is proposed for Phase II, including retention of existing 6-foot high cyclone fencing along most of the perimeter of the subject site. All proposed and existing 6-foot high fencing is proposed along rear or side yards. A short section of 3-foot high fencing is proposed along the front of the site with portions within the front yard. Therefore, all the existing and proposed fencing proposed for the project meets the standards in this Section.

**B. Fence or wall on a retaining wall. When a fence is built on a retaining wall or an artificial berm, the following standards shall apply:**

1. When the retaining wall or artificial berm is 30 inches or less in height from finished grade, the maximum fence or wall height on top of the retaining wall shall be six feet.
2. When the retaining wall or earth berm is greater than 30 inches in height, the combined height of the retaining wall and fence or wall from finished grade shall not exceed eight and one-half feet.
3. Fences or walls located on top of retaining walls or earth berms in excess of 30 inches above finished grade may exceed the total allowed combined height of eight and one-half feet; provided, that the fence or wall is located a minimum of two feet from the retaining wall and the fence or wall height shall not exceed 6 ft.



**Response:** There are two retaining wall locations on the site; (1) partially around the front storm planter and (2) partially around the back storm planter. Maximum exposed height of the walls is approximately 5'. All fencing proposed on top of retaining walls will be provided for fall protection and will be limited to 4' tall.

#### **44.030 SCREENING OF OUTDOOR STORAGE**

- A. All service, repair, and storage activities carried on in connection with any commercial, business or industrial activity and not conducted within an enclosed building shall be screened from view of all adjacent properties and adjacent streets by a sight-obscuring fence.**

**Response:** All proposed service, repair or storage activities will be accomplished off-site or conducted within enclosed areas of the site. A storage shed at the rear of the site will be retained for enclosed storage purposes.

#### **44.040 LANDSCAPING**

**Landscaping which is located on the fence line and which impairs sight vision shall not be located within the clear vision area as provided in Chapter 42 CDC.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L1.01, L1.02 and L1.03 in Exhibit D, Preliminary Development Plans. No landscaping is located within clear vision areas of driveways.

## **CHAPTER 46: OFF-STREET PARKING, LOADING, AND RESERVOIR AREAS**

### **46.020 APPLICABILITY AND GENERAL PROVISIONS**

- A. At the time a structure is erected or enlarged, or the use of a structure or unit of land is changed within any zone, parking spaces, loading areas and reservoir areas shall be provided in accordance with the requirements of this chapter unless other requirements are otherwise established as a part of the development approval process.**
- B. The provision and maintenance of off-street parking and loading spaces are the continuing obligation of the property owner.**
- C. No building or other permit shall be issued until plans are approved that show the property that is and will remain available for exclusive use as off-street parking and loading space as required by this chapter.**
- D. Required parking spaces and loading areas shall be improved to the standards contained in this chapter and shall be available for use at the time of the final building inspection except as provided in CDC 46.150.**

**Response:** The applicant is proposing to develop the site in two phases. The first phase (Phase I) will include the use of existing buildings and parking on the site, with the addition of a 28 feet x 64 feet modular classroom and associated ADA-compliant access facilities. Both the Phase I and Phase II parking shall be upgraded to meet current width, depth and paving requirements, as well as complete compliance with required landscaping. The applicant proposes parking through both phases of parking, with maintenance provided by the owner. This proposed parking shall be used exclusively for parking for the proposed School use. Therefore, the applicant is proposing to provide parking for the site in accordance with Chapter 46, with further specific compliance indicated below. See Site Plans on Sheets C1.0 and C1.1 of the Preliminary Development Plans in Exhibit D.



#### 46.030 SUBMITTAL REQUIREMENTS

For any application requiring design review approval, which includes parking areas, the applicant shall submit, within the design review package, a plan drawn to scale showing all the elements necessary to indicate that the requirements of Chapter 55 CDC are met and it shall include but not be limited to:

- A. The delineation of individual parking and loading spaces and their dimensions;
- B. The identification of compact parking spaces;
- C. The location of the circulation area necessary to serve spaces;
- D. The access point(s) to streets, alleys, and properties to be served;
- E. The location of curb cuts;
- F. The location and dimensions of all landscaping, including the type and size of plant material to be used, as well as any other landscape material incorporated into the overall plan;
- G. The proposed grading and drainage plans and the slope (percentage) of parking lot;
- H. Specifications as to signs and bumper guards;
- I. Identification of disabled parking spaces;
- J. Location of pedestrian walkways and crossings; and
- K. Location of bicycle racks.

**Response:** All proposed parking is indicated on the Phase I and Phase II Site Plans on Sheets C1.0 and C1.1 in Exhibit D, Preliminary Development Plans. These plans include all of the required information identified in this Section, including depiction of spaces and dimensions, disabled stalls, access points, landscaping, vehicle circulation, pedestrian walkways and location of bicycle racks.

#### 46.040 APPROVAL STANDARDS

Approval shall be based on the standards set forth in this chapter and Chapter 48 CDC, Access, Egress and Circulation; Chapter 52 CDC, Signs; and Chapter 54 CDC, Landscaping.

**Response:** All of the Sections in Chapter 46 are addressed herein, while the Chapters identified in Section 46.040 are addressed below.

#### 46.070 MAXIMUM DISTANCE ALLOWED BETWEEN PARKING AREA AND USE

- A. Off-street parking spaces for single- and two-family dwellings shall be located on the same lot with the dwelling.

**Response:** No single- and two family dwellings are proposed as part of this project, therefore, this Section does not apply.

- B. Off-street parking spaces for uses not listed in subsection A of this section shall be located not farther than 200 feet from an entryway to the building or use they are required to serve, measured in a straight line from the building, with the following exceptions:

- 1. Shared parking areas for commercial uses which require more than 40 parking spaces may provide for the spaces in excess of the required 40 spaces up to a distance of 300 feet from the entryway to the commercial building or use.

2. Industrial and manufacturing uses which require in excess of 40 spaces may locate the required spaces in excess of the 40 spaces up to a distance of 300 feet from the entryway to the building.
3. Employee parking areas for carpools and vanpools shall be located closer to the entryway to the building than general employee parking.
4. Stacked or valet parking is allowed if an attendant is present to move vehicles. If stacked parking is used for required parking spaces, the applicant shall ensure that an attendant will always be present when the lot is in operation. The requirements for minimum or maximum spaces and all parking area development standards continue to apply for stacked parking.
5. All disabled parking shall be placed closest to building entrances than all other parking. Appropriate ADA curb cuts and ramps to go from the parking lot to the ADA-accessible entrance shall be provided unless exempted by ADA code.

**Response:** All proposed parking is within 200 feet of entryways of the school building, therefore, this standard is met.

#### **46.080 COMPUTATION OF REQUIRED PARKING SPACES AND LOADING AREA**

- A. Where several uses occupy a single structure or unit of land, a combination of uses is included in one business, or a combination of uses in the same or separate buildings share a common parking area as in the case of a shopping center, the total off-street parking spaces and loading area shall be the sum of the requirements of the several uses, computed separately. For example, parking for an auto sales and repair business would be calculated using the “retail-bulky” calculation for the sales area and the “service and repair” calculation for the repair area. In another example, parking for a shopping center with a grocery store, a restaurant, and a medical office would be calculated using the “general retail store” calculation for the grocery store, the “restaurant” calculation for the restaurant, and the “medical/dental clinics” calculation for the medical office. The total number of required parking spaces may be reduced by up to 10 percent to account for cross-patronage (when a customer visits several commercial establishments during one visit to the commercial center) of adjacent businesses or services in a commercial center with five or more separate commercial establishments.
- B. To calculate building square footage as a basis for determining how many parking spaces are needed, the area measured shall be gross floor area under the roof measured from the faces of the structure, including all habitable floors and excluding only space devoted to covered off-street parking or loading.
- C. Where employees are specified, the employees counted are the persons who work on the premises including proprietors, executives, professional people, production, sales, and distribution employees, during the largest shift.
- D. Fractional space requirements shall be counted as a whole space.
- E. On-street parking along the immediate property frontage(s) may be counted toward the minimum parking requirement with approval from the City Engineer.

**Response:** There is only one use proposed for the subject site, which is considered School under the description of Uses in Chapter 12 (R-7 Zone). The use is further described in Section 46.090 as a ‘Public and semi-public building/use’. This proposed use, as well as the total gross floor area of the proposed development was used in determining required parking for the site.

**46.090 MINIMUM PARKING SPACE REQUIREMENTS**

**B. Public and semi-public buildings/ uses.**

|                                                                                                 |                                                                                      |
|-------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------|
| 6. Primary school, middle school, or equivalent private or parochial school.                    | One space for every employee, plus 1 space for each 1,000 square feet of floor area. |
| 7. Senior high, college, or commercial trade school, or equivalent private or parochial school. | 0.2 spaces per staff and student.                                                    |
| 8. Day care, kindergarten, or pre-school facilities.                                            | One space per employee, plus one space for every 300 square feet of floor area.      |

**Response:** There is only one use proposed for the subject site, which is considered School under the description of Uses in Chapter 12 (R-7 Zone). The use is further described in Section 46.090 as a 'Public and semi-public building/use'. This proposed use, as well as the total gross floor area of the proposed development was used in determining required parking for the site.

All proposed parking is indicated on the Site Plan on Sheets C1.0 and C1.1 in Exhibit D, Preliminary Development Plans. The Site Plan includes a Parking Count Table that identifies the required and proposed amounts of parking for the project. For the eventual development based on Phase II, 48 parking spaces are required and 37 are proposed. Due to various site constraints and overall requirements for parking for the proposed use and associated gross floor area, it is not possible to provide the required amount of parking for the proposal. The applicant is requesting a Variance for this Section, which is addressed below.

**F. Maximum parking. Parking spaces (except for single-family and two-family residential uses) shall not exceed the minimum required number of spaces by more than 10 percent.**

**Response:** The applicant is proposing less than the minimum number of parking spaces required, therefore, maximum parking requirements are met.

**G. Parking reductions. An applicant may reduce parking up to 10 percent for development sites within one-quarter mile of a transit corridor or within a mixed-use commercial area, and up to 10 percent for commercial development sites adjacent to multi-family residential sites with the potential to accommodate more than 20 dwelling units.**

**Response:** There is a Tri-Met Bus Stop for route #35 located .2 miles from the site Tri-Met Bus Route #35 on Willamette Drive that connects to the Oregon City Transit Center, the Lake Oswego Transit Center, and the Rose Quarter Transit Center. Buses run approximately every half hour on weekdays when school is in session. In addition, there is a park and ride located at the intersection of Highway 43 and Cedar Oak Road that provides additional transit options. Therefore, the applicant is allowed to reduce parking up to 10%. However, the 10% reduction does not allow for the minimum number of spaces the applicant is proposing. Therefore, the applicant is requesting a Variance for the minimum parking, which is addressed below.

**H. For office, industrial, and public uses where there are more than 20 parking spaces for employees on the site, at least 10 percent of the required employee parking spaces shall be reserved for carpool use before 9:00 a.m. on weekdays. The spaces will be the closest to the building entrance, except for any disabled parking and those signed for exclusive customer use. The carpool/vanpool spaces shall be clearly marked "Reserved – Carpool/Vanpool Before 9:00 a.m."**

**Response:** The applicant does not propose more than 20 spaces for employees on the site, therefore, this standard does not apply.

- I. Existing developments along transit streets or near transit stops may redevelop up to 10 percent of the existing parking spaces to provide transit-oriented facilities, including bus pullouts, bus stops and shelters, park and ride stations, and other similar facilities.

**Response:** Transit Orientated facilities are not provided because there currently is no transit on Old River Road.

**46.120 DRIVEWAYS REQUIRED ON SITE**

Any school or other meeting place which is designed to accommodate more than 25 people at one time shall provide a 15-foot-wide driveway designed for continuous forward flow of passenger vehicles for the purpose of loading and unloading passengers. Depending on functional requirements, the width may be increased with Planning Director approval.

**Response:** The applicant proposes two driveways to the site and the associated parking area. The first driveway is a 24-foot wide two-way access from Old River Road at the southeast corner of the site. The second driveway is a 20-foot wide one-way ingress that will serve as the main circulation access point for those using the designated drop-off zone at the front of the school. Therefore, this requirement is met.

**46.130 OFF-STREET LOADING SPACES**

Buildings or structures to be built or substantially altered, which receive and distribute material or merchandise by truck, shall provide and maintain off-street loading and maneuvering space. The dimensional standard for loading spaces is a minimum of 14 feet wide by 20 feet long or proportionate to accommodate the size of delivery trucks that typically serve the proposed use as follows:

| GROSS FLOOR AREA      |                                  |                                   |
|-----------------------|----------------------------------|-----------------------------------|
| Land Use              | At Which First Berth is Required | At Which Second Berth is Required |
| <b>Institutional:</b> |                                  |                                   |
| <b>Schools</b>        | <b>10,000</b>                    | <b>100,000</b>                    |

**Response:** Due to the nature of the operations and scale of the school use, site constraints and overall requirements for parking, circulation and landscaping for the proposed use, it is not desirable nor possible to include a dedicated loading space that would only be used occasionally. The applicant is requesting a Variance for this Section, which is addressed below.

**46.150 DESIGN AND STANDARDS**

The following standards apply to the design and improvement of areas used for vehicle parking, storage, loading, and circulation:

**A. Design standards.**

1. "One standard parking space" means a minimum for a parking stall of eight feet in width and 16 feet in length. These stalls shall be identified as "compact." To accommodate larger cars, 50 percent of the required parking spaces shall have a minimum dimension of nine feet in width and 18 feet in length (nine feet by 18 feet). When multi-family parking stalls back onto a main driveway, the stalls shall be nine feet by 20 feet. Parking for development in water resource areas may have 100 percent compact spaces.
2. Disabled parking and maneuvering spaces shall be consistent with current federal dimensional standards and subsection B of this section and placed nearest to accessible building entryways and ramps.

4. Service drives shall be designed and constructed to facilitate the flow of traffic, provide maximum safety of traffic access and egress, and maximum safety of pedestrians and vehicular traffic on the site.
5. Each parking and/or loading space shall have clear access, whereby the relocation of other vehicles to utilize the parking space is not required.
6. Except for single- and two-family residences, any area intended to be used to meet the off-street parking requirements as contained in this chapter shall have all parking spaces clearly marked using a permanent paint. All interior drives and access aisles shall be clearly marked and signed to show direction of flow and maintain vehicular and pedestrian safety. Permeable parking surface spaces may have an alternative delineation for parking spaces.
7. Except for residential parking, and parking for public parks and trailheads, at least 50 percent of all areas used for the parking and/or storage and/or maneuvering of any vehicle, boat and/or trailer shall be improved with asphalt or concrete surfaces according to the same standards required for the construction and acceptance of City streets. The remainder of the areas used for parking may use a permeable paving surface designed to reduce surface runoff. Parking for public parks or trailheads may use a permeable paving surface designed to reduce surface runoff for all parking areas. Where a parking lot contains both paved and unpaved areas, the paved areas shall be located closest to the use which they serve.
8. Off-street parking spaces for single- and two-family residences shall be improved with an asphalt or concrete surface, or a permeable parking surface designed to reduce surface runoff, to specifications as approved by the Building Official. Other parking facilities for two- and single-family homes that are to accommodate additional vehicles, boats, recreational vehicles, and trailers, etc., need not be paved. All parking for multi-family residential development shall be paved with concrete or asphalt. Driveways shall measure at least 20 feet from the back of sidewalk to garage or the end of the parking pad to accommodate cars and sport utility vehicles without the vehicles blocking the public sidewalk.
9. Access drives from the street to off-street parking or loading areas shall be designed and constructed to facilitate the flow of traffic and provide maximum safety for pedestrian and vehicular traffic on the site. The number of access drives shall be limited to the minimum that will allow the property to accommodate and service the anticipated traffic. Access drives shall be clearly and permanently marked and defined through use of rails, fences, walls, or other barriers or markers on frontage not occupied by service drives.
10. Access drives shall have a minimum vision clearance as provided in Chapter 42 CDC, Clear Vision Areas.
11. Parking spaces along the boundaries of a parking lot or adjacent to interior landscaped areas or sidewalks shall be provided with a wheel stop at least four inches high located two feet back from the front of the parking stall. Such parking spaces may be provided without wheel stops if the sidewalks or landscaped areas adjacent the parking stalls are two feet wider than the minimum width.
12. Off-street parking and loading areas shall be drained in accordance with plans and specifications approved by the City Engineer. Storm drainage at commercial sites may also have to be collected to treat oils and other residue.
13. Artificial lighting on all off-street parking facilities shall be designed to deflect all light downward away from surrounding residences and so as not to create a hazard to the public use of any road or street.
14. Directional arrows and traffic control devices which are placed on parking lots

- 15. The maximum driveway grade for single-family housing shall be 15 percent. The 15 percent shall be measured along the centerline of the driveway only. Grades elsewhere along the driveway shall not apply. Variations require approval of a Class II variance by the Planning Commission pursuant to Chapter 75 CDC. Regardless, the last 18 feet in front of the garage must maintain a maximum grade of 12 percent as measured along the centerline of the driveway only. Grades elsewhere along the driveway shall not apply.
- 16. Visitor or guest parking must be identified by painted "GUEST" or "VISITOR."
- 17. The parking area shall have less than a five percent grade. No drainage across adjacent sidewalks or walkways is allowed.

**Response:**

The proposed parking area meets all of the design standards indicated in Standards 1 through 17. All standard spaces are a minimum of 9-feet wide by 18-feet long, with the remaining 18 compact spaces proposed to be a minimum of 8-feet wide by 16-feet long. All proposed ADA parking is designed to meet all federal dimensional standards. All dimensional standards are met for drive aisles, including the one-way driveway intended for users of the drop-off zone at the front of the building. All parking spaces are clearly identified and the entire parking area intended for vehicle use will be paved with asphalt. Access drives are also marked with directional arrows and signage. Wheel stops are provided for all parking spaces, and the parking lot will be lit with artificial lighting to provide for greater safety and security during evening hours. The overall grade for the parking area is generally 5%.

All proposed parking is indicated on the Site Plan on Sheets C1.0 and C1.1 in Exhibit D, Preliminary Development Plans.

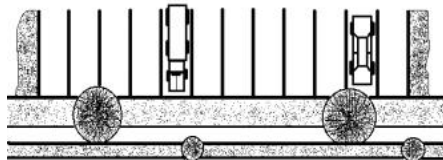
- 18. Commercial, office, industrial, and public parking lots may not occupy more than 50 percent of the main lot frontage of a development site. The remaining frontage shall comprise buildings or landscaping. If over 50 percent of the lineal frontage comprises parking lot, the landscape strip between the right-of-way and parking lot shall be increased to 15 feet wide and shall include terrain variations (e.g., one-foot-high berm) plus landscaping. The defensible space of the parking lot should not be compromised.

**Response:**

The proposed use is considered 'semi-public', as the school is a private entity serving a limited public. Since the use is not considered commercial, office, industrial this standard would not apply to this proposal

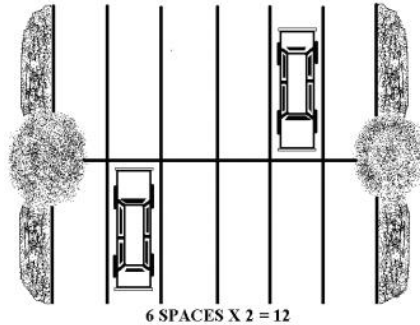
- 19. Areas of the parking lot improved with asphalt or concrete surfaces shall be designed into areas of 12 or less spaces through the use of defined landscaped area. Groups of 12 or less spaces are defined as:

- a. Twelve spaces in a row, provided there are no abutting parking spaces, as in the case when the spaces are abutting the perimeter of the lot; or

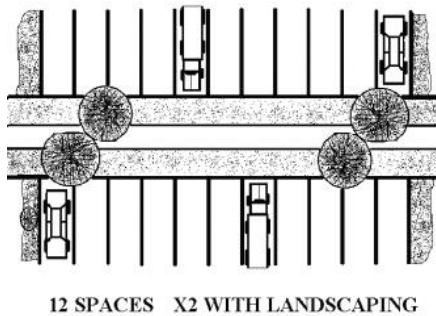


12 SPACES IN A ROW

- b. Twelve spaces in a group with six spaces abutting together; or



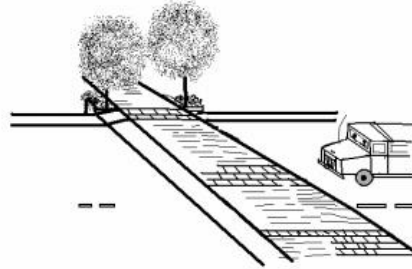
- c. Two groups of 12 spaces abutting each other, but separated by a 15-foot-wide landscape area including a six-foot-wide walkway.



- d. Parking areas improved with a permeable parking surface may be designed using the configurations shown in subsections (A)(19)(a), (b) and (c) of this section except that groups of up to 18 spaces are allowed.
- e. The requirements of this chapter relating to total parking lot landscaping, landscaping buffers, perimeter landscaping, and landscaping the parking lot islands and interior may be waived or reduced pursuant to CDC 32.110(F) in a WRA application without a variance being required.

**Response:** The proposed parking area is designed to insure that no more than 12 spaces are clustered together. In addition, the parking area is designed with perimeter and island landscaping to insure that the parking areas are softened by vegetation. All proposed parking is indicated on the Site Plan on Sheets C1.0 and C1.1 in Exhibit D, Preliminary Development Plans.

20. Pedestrian walkways shall be provided in parking areas having 20 or more spaces. Walkways or sidewalks shall be constructed between major buildings/activity areas (an example in multi-family housing: between recreation center, swimming pool, manager's office, park or open space areas, parking lots, etc.) within a development, between adjacent developments and the new development, as feasible, and between major buildings/activity areas within the development and adjacent streets and all adjacent transit stops. Internal parking lot circulation and design should maintain ease of access for pedestrians from streets and transit stops. Walkways shall be constructed using a material that visually contrasts with the parking lot and driveway surface. Walkways shall be further identifiable to pedestrians and motorists by grade separation, walls, curbs, surface texture (surface texture shall not interfere with safe use of wheelchairs, baby carriages, shopping carts, etc.), and/or landscaping. Walkways shall be six feet wide. The arrangement and layout of the paths shall depend on functional requirements.



RAISED SIDEWALK/TEXTURED SURFACE  
 AUTOMOBILE BECOMES SUBSERVIENT  
 TO THE PEDESTRIAN

- 21. The parking and circulation patterns are easily comprehended and defined. The patterns shall be clear to minimize traffic hazards and congestion and to facilitate
- 22. The parking spaces shall be close to the related use.
- 23. Permeable parking spaces shall be designed and built to City standards.

**Response:**

A pedestrian circulations system has been provided as part of the overall site design. This pedestrian walkway system includes both circulation within the site and connection to the adjacent right-of-way and the proposed asphalt pathway along Old River Road. The pedestrian walkway system also includes a walkway at the center of the parking area connecting the majority of the parking spaces with the rest of the pedestrian walkway system. All proposed pedestrian walkways and parking is indicated on the Site Plan on Sheets C1.0 and C1.1 in Exhibit D, Preliminary Development Plans.

**B. Accessible parking standards for persons with disabilities. If any parking is provided for the public or visitors, or both, the needs of the people with disabilities shall be based upon the following standards or current applicable federal standards, whichever are more stringent:**

**1. Minimum number of accessible parking space requirements (see following table):**

| MINIMUM REQUIRED NUMBER OF TOTAL PARKING SPACES | TOTAL NUMBER OF ACCESSIBLE SPACES | NUMBER OF VAN-ACCESSIBLE SPACES REQUIRED, OF TOTAL | SPACES SIGNED "WHEELCHAIR USE ONLY" |
|-------------------------------------------------|-----------------------------------|----------------------------------------------------|-------------------------------------|
| 1 – 25                                          | 1                                 | 1                                                  | –                                   |
| 26 – 50                                         | 2                                 | 1                                                  | –                                   |

- 2. Location of parking spaces. Parking spaces for the individual with a disability that serve a particular building shall be located on the shortest possible accessible circulation route to an accessible entrance to a building. In separate parking structures or lots that do not serve a particular building, parking spaces for the persons with disabilities shall be located on the shortest possible circulation route to an accessible pedestrian entrance of the parking facility.
- 3. Accessible parking space and aisle shall meet ADA vertical and horizontal slope standards.
- 4. Where any differences exist between this section and current federal standards, those standards shall prevail over this code section.
- 5. One in every eight accessible spaces, but not less than one, shall be served by an access aisle 96 inches wide.



**Response:** The total number of parking spaces proposed is 37, therefore, 2 accessible spaces are required. The applicant proposes 2 ADA accessible parking spaces near the front of the building, therefore, this requirement is met.

**C. Landscaping in parking areas. Reference Chapter 54 CDC, Landscaping.**

**Response:** Requirements and proposals for landscaping for the project are addressed in Chapter 54, below.

**D. Bicycle facilities and parking.**

1. Provisions shall be made for pedestrian and bicycle ways if such facilities are shown on an adopted plan.
2. Bicycle parking facilities shall either be lockable enclosures in which the bicycle is stored, or secure stationary racks which accommodate bicyclist’s locks securing the frame and both wheels. The bicycle parking shall be no more than 50 feet from the entrance to the building, well-lit, observable, and properly signed.
3. Bicycle parking must be provided in the following amounts:

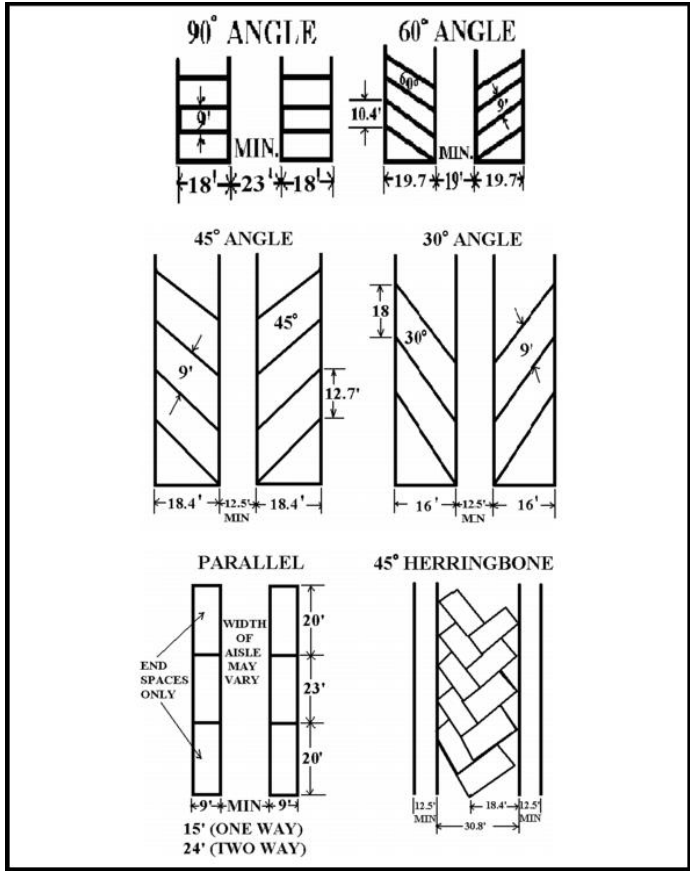
| LAND USE CATEGORY                    | MINIMUM REQUIRED BICYCLE PARKING SPACES | MINIMUM COVERED AMOUNT |
|--------------------------------------|-----------------------------------------|------------------------|
| Institutional                        |                                         |                        |
| Schools – Elementary                 | 2 spaces per classroom                  | 50%                    |
| Schools – Jr. High or Middle Schools | 4 spaces per classroom                  | 50%                    |

**Response:** Phase 1 has (5) Elementary and (2) Middle School classrooms. This requires a total of (18) bike parking spaces, of which (9) are required to be covered. During this phase (18) spaces are provided and all are covered. This more than meets the requirement for covered spaces and meets the requirement for the total number of spaces.

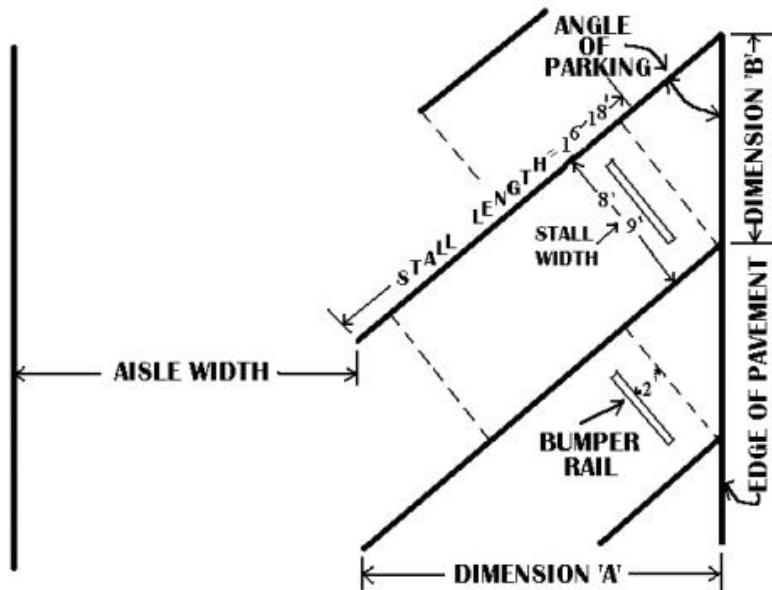
Phase 2 has (11) Elementary and (3) Middle School classroom. This requires a total of (34) bike parking spaces, of which (17) are required to be covered. As noted Phase 1 provides (18) spaces. During Phase 2 (9) more racks are provided. There are (7) racks provided for (2) bikes each or (14) spaces. In addition, (2) of the racks allow for parking on just one side for (2) more spaces. The combination of eighteen (18) Phase 1 spaces and sixteen (16) Phase 2 spaces provides for a total of thirty-four (34) spaces, of which twenty-four (24) of the spaces are covered. This more than meets the requirement for covered spaces and meets the requirement for total number of spaces.

**F. (See Figures 1 and 2 below.)**

**Figure 1. MINIMUM STANDARDS FOR PARKING LOT LAYOUT**



Minimum distance for parking stalls



| ANGLE OF PARKING | DIRECTION OF PARKING | AISLE WIDTH |       | DIMENSION 'A' |       | DIMENSION 'B' |       |
|------------------|----------------------|-------------|-------|---------------|-------|---------------|-------|
|                  |                      | STALL WIDTH |       | STALL WIDTH   |       | STALL WIDTH   |       |
|                  |                      | 9.0'        | 8.0'  | 9.0'          | 8.0'  | 9.0'          | 8.0'  |
| 30°              | DRIVE-IN             | 12.5'       | 12.5' | 16.8'         | 13.8' | 18.0'         | 16.0' |
| 45°              | DRIVE-IN             | 12.5'       | 12.5' | 19.1'         | 17.0' | 12.7'         | 11.3' |
| 60°              | DRIVE-IN             | 19.0'       | 18.0' | 20.1'         | 17.8' | 10.4'         | 9.2'  |
| 60°              | BACK-IN              | 17.0'       | 17.0' | 20.1'         | 17.8' | 10.4'         | 9.2'  |
| 90°              | DRIVE-IN             | 23.0'       | 23.0' | 18.0'         | 16.0' | 9.0'          | 8.0'  |
| 90°              | BACK-IN              | 22.0'       | 22.0' | 18.0'         | 16.0' | 9.0'          | 8.0'  |

**Response:** All proposed parking is 90 degree drive-in. All proposed 2-way aisle widths are 24 feet. Based on the angle and direction of proposed parking, the required and proposed standard spaces are a minimum of 9-feet wide by 18-feet long, with the remaining 18 compact spaces proposed to be a minimum of 8-feet wide by 16-feet long. Therefore, the standards for this Section are met for all proposed parking.

## CHAPTER 48: ACCESS, EGRESS AND CIRCULATION

### 48.020 APPLICABILITY AND GENERAL PROVISIONS

- A. The provisions of this chapter do not apply where the provisions of the Transportation System Plan or land division chapter are applicable and set forth differing standards.
- B. All lots shall have access from a public street or from a platted private street approved under the land division chapter.
- C. No building or other permit shall be issued until scaled plans are presented to the City and approved by the City as provided by this chapter, and show how the access, egress, and circulation requirements are to be fulfilled. Access to State or County roads may require review, approval, and permits from the appropriate authority.
- D. Should the owner or occupant of a lot, parcel or building enlarge or change the use to which the lot, parcel or building is put, resulting in increasing any of the requirements of this chapter, it shall be unlawful and a violation of this code to begin or maintain such altered use until the provisions of this chapter have been met, and, if required, until the appropriate approval authority under Chapter 99 CDC has approved the change.
- E. Owners of two or more uses, structures, lots, parcels, or units of land may agree to utilize jointly the same access and egress when the combined access and egress of both uses, structures, or parcels of land satisfies the requirements as designated in this code; provided, that satisfactory legal evidence is presented to the City Attorney in the form of deeds, easements, leases, or contracts to establish joint use. Copies of said instrument shall be placed on permanent file with the City Recorder.
- F. Property owners shall not be compelled to access their homes via platted stems of flag lots if other driveways and easements are available and approved by the City Engineer.

**Response:** The proposal indicates that access to the site will include access from Old River Road, and the applicant proposes two driveways to the site and the associated parking area. The first driveway is a 24-foot wide two-way access from Old River Road at the southeast corner of the site. The second driveway is a 20-foot wide one-way ingress that will serve as the main circulation access point for those using the designated drop-off zone at the front of the school.

## 48.025 ACCESS CONTROL

### B. Access control standards.

1. **Traffic impact analysis requirements.** The City or other agency with access jurisdiction may require a traffic study prepared by a qualified professional to determine access, circulation and other transportation requirements. (See also CDC 55.125, Traffic Impact Analysis.)
2. **The City or other agency with access permit jurisdiction may require the closing or consolidation of existing curb cuts or other vehicle access points, recording of reciprocal access easements (i.e., for shared driveways), development of a frontage street, installation of traffic control devices, and/or other mitigation as a condition of granting an access permit, to ensure the safe and efficient operation of the street and highway system. Access to and from off-street parking areas shall not permit backing onto a public street.**
3. **Access options.** When vehicle access is required for development (i.e., for off-street parking, delivery, service, drive-through facilities, etc.), access shall be provided by one of the following methods (planned access shall be consistent with adopted public works standards and TSP). These methods are “options” to the developer/subdivider.
  - a) **Option 1.** Access is from an existing or proposed alley or mid-block lane. If a property has access to an alley or lane, direct access to a public street is not permitted.
  - b) **Option 2.** Access is from a private street or driveway connected to an adjoining property that has direct access to a public street (i.e., “shared driveway”). A public access easement covering the driveway shall be recorded in this case to assure access to the closest public street for all users of the private street/drive.
  - c) **Option 3.** Access is from a public street adjacent to the development lot or parcel. If practicable, the owner/developer may be required to close or consolidate an existing access point as a condition of approving a new access. Street accesses shall comply with the access spacing standards in subsection (B)(6) of this section.
4. **Subdivisions fronting onto an arterial street.** New residential land divisions fronting onto an arterial street shall be required to provide alleys or secondary (local or collector) streets for access to individual lots. When alleys or secondary streets cannot be constructed due to topographic or other physical constraints, access may be provided by consolidating driveways for clusters of two or more lots (e.g., includes flag lots and mid-block lanes).
5. **Double-frontage lots.** When a lot or parcel has frontage onto two or more streets, access shall be provided first from the street with the lowest classification. For example, access shall be provided from a local street before a collector or arterial street. When a lot or parcel has frontage opposite that of the adjacent lots or parcels, access shall be provided from the street with the lowest classification.
6. **Access spacing.**
  - a. **The access spacing standards found in the adopted Transportation System Plan (TSP) shall be applicable to all newly established public street intersections and non-traversable medians. Deviation from the access spacing standards may be granted by the City Engineer if conditions are met as described in the access spacing variances section in the adopted TSP.**

b. Private drives and other access ways are subject to the requirements of CDC 48.060.

7. Number of access points. For single-family (detached and attached), two-family, and duplex housing types, one street access point is permitted per lot or parcel, when alley access cannot otherwise be provided; except that two access points may be permitted corner lots (i.e., no more than one access per street), subject to the access spacing standards in subsection (B)(6) of this section. The number of street access points for multiple family, commercial, industrial, and public/institutional developments shall be minimized to protect the function, safety and operation of the street(s) and sidewalk(s) for all users. Shared access may be required, in conformance with subsection (B)(8) of this section, in order to maintain the required access spacing, and minimize the number of access points.
8. Shared driveways. The number of driveway and private street intersections with public streets shall be minimized by the use of shared driveways with adjoining lots where feasible. The City shall require shared driveways as a condition of land division or site design review, as applicable, for traffic safety and access management purposes in accordance with the following standards:

**Response:** The applicant has provided a Traffic Impact Analysis as part of this application. See Traffic Impact Analysis, Exhibit F. The applicant proposes two driveways to the site and the associated parking area. The first driveway is a 24-foot wide two-way access from Old River Road at the southeast corner of the site. The second driveway is a 20-foot wide one-way ingress that will serve as the main circulation access point for those using the designated drop-off zone at the front of the school. This proposed design provides the greatest efficiency and safety for movement of vehicles, bicycles and pedestrians. No shared driveways are proposed as part of this application.

#### 48.040 MINIMUM VEHICULAR REQUIREMENTS FOR NON-RESIDENTIAL USES

Access, egress, and circulation system for all non-residential uses shall not be less than the following

- A. Service drives for non-residential uses shall be fully improved with hard surface pavement:
  1. With a minimum of 24-foot width when accommodating two-way traffic; or
  2. With a minimum of 15-foot width when accommodating one-way traffic. Horizontal clearance shall be two and one-half feet wide on either side of the driveway.
  3. Meet the requirements of CDC 48.030(E)(3) through (6).

**Response:** The applicant proposes two driveways to the site and the associated parking area. All parking areas and vehicle circulation areas will be hard-surfaced with asphalt. The first driveway is a 24-foot wide two-way access from Old River Road at the southeast corner of the site. The second driveway is a 20-foot wide one-way ingress that will serve as the main circulation access point for those using the designated drop-off zone at the front of the school. This proposed design provides the greatest efficiency and safety for movement of vehicles, bicycles and pedestrians.

The requirements of CDC 48.030(E)(3) through (6) are applicable to multi-family developments only, therefore, those standards do not apply to this project.

- B. All non-residential uses shall be served by one or more service drives as determined necessary to provide convenient and safe access to the property and designed according to CDC 48.030(A). In no case shall the design of the service drive or drives require or facilitate the backward movement or other maneuvering of a vehicle within a street, other than an alley.

- C. All on-site maneuvering and/or access drives shall be maintained pursuant to CDC 46.130.

**D. Gated accessways to non-residential uses are prohibited unless required for public safety or security.**

**Response:** The applicant proposes two driveways to the site and the associated parking area. All parking areas and vehicle circulation areas will be hard-surfaced with asphalt. The first driveway is a 24-foot wide two-way access from Old River Road at the southeast corner of the site. The second driveway is a 20-foot wide one-way ingress that will serve as the main circulation access point for those using the designated drop-off zone at the front of the school. This proposed design provides the greatest efficiency and safety for movement of vehicles, bicycles and pedestrians. No gates are proposed as part of this project.

**48.050 ONE-WAY VEHICULAR ACCESS POINTS**

**Where a proposed parking facility plan indicates only one-way traffic flow on the site, it shall be accommodated by a specific driveway serving the facility, and the entrance drive shall be situated closest to oncoming traffic, and the exit drive shall be situated farthest from oncoming traffic.**

**Response:** The applicant proposes two (2) driveways to the site and the associated parking area. Therefore, the standards for one-way vehicular access points does not apply. All parking areas and vehicle circulation areas will be hard-surfaced with asphalt. The first driveway is a 24-foot wide two-way access from Old River Road at the southeast corner of the site. The second driveway is a 20-foot wide one-way ingress that will serve as the main circulation access point for those using the designated drop-off zone at the front of the school. This proposed design provides the greatest efficiency and safety for movement of vehicles, bicycles and pedestrians.

**48.060 WIDTH AND LOCATION OF CURB CUTS AND ACCESS SEPARATION REQUIREMENTS**

- A. Minimum curb cut width shall be 16 feet.**
- B. Maximum curb cut width shall be 36 feet, except along Highway 43 in which case the maximum curb cut shall be 40 feet. For emergency service providers, including fire stations, the maximum shall be 50 feet.**
- C. No curb cuts shall be allowed any closer to an intersecting street right-of-way line than the following:**
  - 1. On an arterial when intersected by another arterial, 150 feet.**
  - 2. On an arterial when intersected by a collector, 100 feet.**
  - 3. On an arterial when intersected by a local street, 100 feet.**
  - 4. On a collector when intersecting an arterial street, 100 feet.**
  - 5. On a collector when intersected by another collector or local street, 35 feet.**
  - 6. On a local street when intersecting any other street, 35 feet.**
- D. There shall be a minimum distance between any two adjacent curb cuts on the same side of a public street, except for one-way entrances and exits, as follows:**
  - 1. On an arterial street, 150 feet.**
  - 2. On a collector street, 75 feet.**
  - 3. Between any two curb cuts on the same lot or parcel on a local street, 30 feet.**
- E. A rolled curb may be installed in lieu of curb cuts and access separation requirements.**
- F. Curb cuts shall be kept to the minimum, particularly on Highway 43. Consolidation of driveways is preferred. The standard on Highway 43 is one curb cut per business if consolidation of driveways is not possible.**

**Response:** The distance from the two-way driveway curb cut and the closest intersection is 220 feet. The distance from the one-way driveway curb cut and the closest intersection is 314 feet. The distance between these two proposed curb cuts is 94 feet.

**G. Adequate line of sight pursuant to engineering standards should be afforded at each driveway or accessway.**

**Response:** Line of sight has been analyzed and included in the Traffic Impact Analysis in Exhibit F, with an adequate line of sight identified in the findings.

## **CHAPTER 54: LANDSCAPING**

### **54.020 APPROVAL CRITERIA**

**A. Every development proposal requires inventorying existing site conditions which include trees and landscaping. In designing the new project, every reasonable attempt should be made to preserve and protect existing trees and to incorporate them into the new landscape plan. Similarly, significant landscaping (e.g., bushes, shrubs) should be integrated. The rationale is that saving a 30-foot-tall mature tree helps maintain the continuity of the site, they are qualitatively superior to two or three two-inch caliper street trees, they provide immediate micro-climate benefits (e.g., shade), they soften views of the street, and they can increase the attractiveness, marketability, and value of the development.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans.

**B. To encourage tree preservation, the parking requirement may be reduced by one space for every significant tree that is preserved in the parking lot area for a maximum reduction of 10 percent of the required parking. The City Parks Supervisor or Arborist shall determine the significance of the tree and/or landscaping to determine eligibility for these reductions.**

**Response:** The City Arborist has identified 4 significant trees on the site, and one of these trees is proposed to be removed. No significant trees are located in the parking area as part of the overall proposed development.

**C. Developers must also comply with the municipal code chapter on tree protection.**

**Response:** All aspects of the chapter on tree protection has been considered during site design. Tree preservation is indicated in the written "Arborist Report and Tree Protection Plan" and on Sheets L0.01 and L0.02 of the Preliminary Development Plans, Exhibit D.

**D. Heritage trees. Heritage trees are trees which, because of their age, type, notability, or historical association, are of special importance. Heritage trees are trees designated by the City Council following review of a nomination. A heritage tree may not be removed without a public hearing at least 30 days prior to the proposed date of removal. Development proposals involving land with heritage tree(s) shall be required to protect and save the tree(s). Further discussion of heritage trees is found in the municipal code.**

**Response:** The City Arborist has indicated that there are no heritage trees on the site, therefore, this standard does not apply.

**E. Landscaping – By type, location and amount.**

**1. Residential uses (non-single-family). A minimum of 25 percent of the gross area including parking, loading and service areas shall be landscaped, and may include the open space and recreation area requirements under CDC 55.100. Parking lot landscaping may be counted in the percentage.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans.

**2. Non-residential uses. A minimum of 20 percent of the gross site area shall be landscaped. Parking lot landscaping may be counted in the percentage.**

**Response:**

All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. Overall landscaping for the site has been calculated at 40%. This calculation is based on 64,429 of gross site area and 26,350 of total landscape areas, including landscaped areas provided and required as part of parking. Therefore, the site is landscaped per the 20% minimum landscaped area requirement.

**3. All uses (residential uses (non-single-family) and non-residential uses):**

- a. The landscaping shall be located in defined landscaped areas which are uniformly distributed throughout the parking or loading area. There shall be one shade tree planted for every eight parking spaces. These trees shall be evenly distributed throughout the parking lot to provide shade. Parking lots with over 20 spaces shall have a minimum 10 percent of the interior of the parking lot devoted to landscaping. Pedestrian walkways in the landscaped areas are not to be counted in the percentage. The perimeter landscaping, explained in subsection (E)(3)(d) of this section, shall not be included in the 10 percent figure. Parking lots with 10 to 20 spaces shall have a minimum five percent of the interior of the parking lot devoted to landscaping. The perimeter landscaping, as explained above, shall not be included in the five percent. Parking lots with fewer than 10 spaces shall have the standard perimeter landscaping and at least two shade trees. Non-residential parking areas paved with a permeable parking surface may reduce the required minimum interior landscaping by one-third for the area with the permeable parking surface only.**

**Response:**

All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. The site landscaping is evenly distributed throughout the site and parking areas, with no less than 5 feet in dimension for landscaped areas. Trees are proposed at a minimum ratio of one tree per eight parking spaces, with 37 parking spaces proposed and 11 proposed shade trees. 12% of the parking area interior is landscaped, thereby meeting the 10 percent requirement.

- b. The landscaped areas shall not have a width of less than five feet.**

**Response:**

All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. The site landscaping is evenly distributed throughout the site, with all landscape areas no less than 5 feet in dimension.

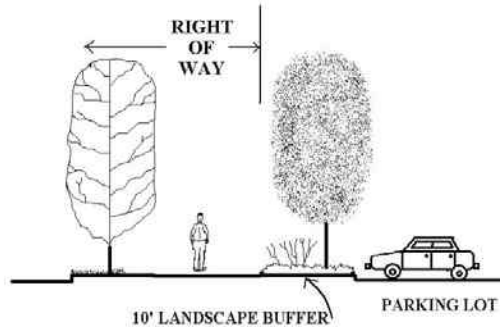
- c. The soils, site, proposed soil amendments, and proposed irrigation system shall be appropriate for the healthy and long-term maintenance of the proposed plant species.**

**Response:**

All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. The Landscape Plan includes details on soils, soil amendments and irrigation system. All of these elements insure healthy and long-term maintenance of the proposed plant species.

- d. A parking, loading, or service area which abuts a street shall be set back from the right-of-way line by perimeter landscaping in the form of a landscaped strip at least 10 feet in width. When a parking, loading, or service area or driveway is contiguous to an adjoining lot or parcel, there shall be an intervening five-foot-wide landscape strip. The landscaped area shall contain:**





- 1) **Street trees spaced as appropriate to the species, not to exceed 50 feet apart on the average;**
- 2) **Shrubs, not to reach a height greater than three feet, six inches, spaced no more than five feet apart on the average; or**
- 3) **Vegetative ground cover such as grass, wildflowers, or other landscape material to cover 100 percent of the exposed ground within two growing seasons. No bark mulch shall be allowed except under the canopy of low level shrubs.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. The proposed parking area comprises over 50 percent of the frontage, therefore, there is a proposed setback between the parking area and the right-of-way. However, this area is 9 feet (one foot less than the 10 foot requirement). The proposed landscape strip in this setback area along the right-of-way is planted with trees, shrubs and ground cover. A Variance is being requested for this standard (see below in Variance section).

- e. **If over 50 percent of the lineal frontage of the main street or arterial adjacent to the development site comprises parking lot, the landscape strip between the right-of-way and parking lot shall be increased to 15 feet in width and shall include terrain variations (e.g., one-foot-high berm) plus landscaping. This extra requirement only applies to one street frontage.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. The proposed parking area comprises less than 50 percent of the frontage, therefore, the increase in landscape width is not required.

- f. **A parking, loading, or service area which abuts a property line shall be separated from the property line by a landscaped area at least five feet in width and which shall act as a screen and noise buffer, and the adequacy of the screen and buffer shall be determined by the criteria set forth in CDC 55.100(C) and (D), except where shared parking is approved under CDC 46.050.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. The proposed landscaped areas along the perimeter of the site abutting residential uses are intended to provide adequate screening and buffering. Under the criteria set forth in CDC 55.100(C) and (D), compatibility between adjoining uses, privacy and noise are all considered in terms of the level of screening and buffering that is considered adequate.

- g. **All areas in a parking lot not used for parking, maneuvering, or circulation shall be landscaped.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. Any areas not proposed for parking,

maneuvering, or circulation will be landscaped, except those areas required for public easement.

- h. The landscaping in parking areas shall not obstruct lines of sight for safe traffic operation.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. A clear vision triangle has been provided as part of these plans. The triangles indicate safe clear vision with the proposed landscaping adjacent to driveway access areas.

- i. Outdoor storage areas, service areas (loading docks, refuse deposits, and delivery areas), and above-ground utility facilities shall be buffered and screened to obscure their view from adjoining properties and to reduce noise levels to acceptable levels at the property line. The adequacy of the buffer and screening shall be determined by the criteria set forth in CDC 55.100(C)(1).**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. No loading docks, outdoor storage areas or above-ground utilities are proposed, therefore, screening or buffering requirements are not applicable under this standard.

- j. Crime prevention shall be considered and plant materials shall not be located in a manner which prohibits surveillance of public and semi-public areas (shared or common areas).**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. Crime prevention and surveillance were considered by the landscape architect during design development of the landscape plan. No portions of the landscaped areas provide any substantial refuge for potential criminals, and most of the site will be visible from the public street.

- k. Irrigation facilities shall be located so that landscaped areas can be properly maintained and so that the facilities do not interfere with vehicular or pedestrian circulation.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. All landscaped areas are proposed to be served by an automatically controlled irrigation system that is accessible without any interference with vehicular or pedestrian circulation.

- i. For commercial, office, multi-family, and other sites, the developer shall select trees that possess the following characteristics:**
  - 1) Provide generous “spreading” canopy for shade.**
  - 2) Roots do not break up adjacent paving.**
  - 3) Tree canopy spread starts at least six feet up from grade in, or adjacent to, parking lots, roads, or sidewalks unless the tree is columnar in nature.**
  - 4) No sticky leaves or sap-dripping trees (no honey-dew excretion).**
  - 5) No seed pods or fruit-bearing trees (flowering trees are acceptable).**
  - 6) Disease-resistant.**
  - 7) Compatible with planter size.**
  - 8) Drought-tolerant unless irrigation is provided.**

**9) Attractive foliage or form all seasons.**

Plant materials (shrubs, ground cover, etc.) shall be selected for their appropriateness to the site, drought tolerance, year-round greenery and coverage, staggered flowering periods, and avoidance of nuisance plants (Scotch broom, etc.).

**Response:** Plants will be properly planted according to the Planting Plan that is part of the overall Landscape Plan in Exhibit D. Plant types have been carefully selected to insure that the standards and requirements in this Section are met.

**54.030 PLANTING STRIPS FOR MODIFIED AND NEW STREETS**

All proposed changes in width in a public street right-of-way or any proposed street improvement shall, where feasible, include allowances for planting strips. Plans and specifications for planting such areas shall be integrated into the general plan of street improvements. This chapter requires any multi-family, commercial, or public facility which causes change in public right-of-way or street improvement to comply with the street tree planting plan and standards.

**Response:** See Sheets L1.01-L1.03 for compliance with requirements. Street trees are provided on the property side of the site along the back of the sidewalk.

**54.040 INSTALLATION**

- A. All landscaping shall be installed according to accepted planting procedures.
- B. The soil and plant materials shall be of good quality.
- C. Landscaping shall be installed in accordance with the provisions of this code.

**Response:** Installation requirements are noted in the plan set. See plan sheets L1.01-L1.03 for details on planting.

**54.050 PROTECTION OF STREET TREES**

Street trees may not be topped or trimmed unless approval is granted by the Parks Supervisor or, in emergency cases, when a tree imminently threatens power lines.

**Response:** No existing street trees are along this site.

**54.060 MAINTENANCE**

- A. The owner, tenant and their agent, if any, shall be jointly and severally responsible for the maintenance of all landscaping which shall be maintained in good condition so as to present a healthy, neat, and orderly appearance and shall be kept free from refuse and debris.
- B. All plant growth in interior landscaped areas shall be controlled by pruning, trimming, or otherwise so that:
  - 1. It will not interfere with the maintenance or repair of any public utility;
  - 2. It will not restrict pedestrian or vehicular access; and
  - 3. It will not constitute a traffic hazard because of reduced visibility.

**Response:** See plans L1.01 and L1.03. Landscape maintenance requirements are understood by the owner.

**54.070 SPECIFICATION SUMMARY**

| AREA/ LOCATION                        | LANSCAPING REQUIRED |
|---------------------------------------|---------------------|
| 1. Between parking lot and R-O-W.     | 10 ft.              |
| 2. Between parking lot and other lot. | 5 ft.               |

|                                                                                                        |        |
|--------------------------------------------------------------------------------------------------------|--------|
| 3. Between parking lot and R-O-W if parking lot comprises more than 50 percent of main R-O-W frontage. | 15 ft. |
| 4. Percentage of residential/ multi-family site to be landscaped.                                      | 25%    |
| 5. Percentage of non-residential (commercial/ industrial/ office) site to be landscaped.               | 20%    |
| 6. Percentage of 10-25 car parking lot to be landscaped (excluding perimeter).                         | 5%     |
| 7. Percentage of 1-9 car parking lot to be landscaped (excluding perimeter).                           | 0%     |
| 8. Percentage of 26+ car parking lot to be landscaped (excluding perimeter).                           | 10%    |

**Response:** All proposed landscaping meets the standards identified in Chapter 54. See Landscape Plans, Sheets L1.01-L1.03

## CHAPTER 55: DESIGN REVIEW

### 55.020 CLASSES OF DESIGN REVIEW

- B. Class II Design Review.** Class II design review applies to all uses/activities except those uses/activities listed under Class I design review, and the exemptions of CDC 55.025. Class II design review applies to the proposed improvements listed in this section when the proposed improvement (e.g., new sidewalk) is part of a major commercial, office, industrial, public, or multi-family construction project (e.g., a new shopping center).

**Response:** This project qualifies as a Class II Design Review, therefore, the applicant is also addressing Chapter 55, Design Review, as part of this narrative and application package. This Type II Design Review will be reviewed concurrently with the Type III Conditional Use application.

### 55.025 EXEMPTIONS

The following activities are exempt from the provisions of this chapter:

- A. Detached single-family residential construction;
- B. Accessory structures;
- C. One to two duplexes or single-family attached structures except as indicated otherwise in this chapter;
- D. Architectural replacements in kind, or replacement of building materials that are equal or superior to existing materials (in terms of performance or quality) but that do not alter the architectural style of the structure. Retrofitted awnings, changes in color schemes, wall art, and freestanding statuary or art under five feet tall are exempt from design review, but shall be subject to Planning Director review under the provisions of CDC 99.060(A)(2), prescribed conditions, and the approval criteria of CDC 55.100(B)(6)(a) and (b).

**Response:** This project does not qualify under any of the exemptions listed above, therefore, is subject to Class II Design Review.

### 55.030 ADMINISTRATION AND APPROVAL PROCESS

- A. A pre-application conference is required before submitting a development plan application for design review as provided by CDC 99.030(B).
- B. The application shall be submitted by the record owner(s) of the property, authorized agent, or condemner.

- C. Action on the development plan application shall be as provided by Chapter 99 CDC, Procedures for Decision-Making: Quasi-Judicial, and the following:
  - 1. The Planning Director for Class I design review applications, or Planning Commission for Class II design review applications, shall approve, approve with conditions, or deny the application based on findings related to the applicable criteria set forth in CDC 99.110 and this chapter.
  - 2. A decision by the Planning Director may be reviewed by the City Council.
- D. Substantial modifications made to the approved development plan will require reapplication (e.g., more or fewer lots, different architectural design, etc.).

**Response:** A pre-application conference was held on January 18, 2018, per this requirement, and is referred to as PA-18-04. Notes were provided by the City and are included in this application in Exhibit C, Pre-Application Conference Notes. The application form for this application has been signed by the current owner (owner of record) and shall be reviewed as a Class II Quasi-Judicial Procedure.

#### 55.040 EXPIRATION OR EXTENSION OF APPROVAL

If substantial construction has not occurred within three years from the date of approval of the development plan, the approved proposal will be void, unless an extension is granted under CDC 99.325.

**Response:** The applicant intends to complete all proposed improvements within 3 years of the date of the approval. Otherwise, the applicant will apply for a two-year extension, if necessary.

#### 55.050 DESIGN REVIEW AMENDMENT TRIGGER

Amendments to design review shall be required when 10 percent or more of the housing type changes (e.g., from single-family units to multi-family units) from the tentatively approved design review plan, or when there is more than a 10 percent change in the number of units, or when the layout of streets and lots significantly changes, or adjusting more than 20 percent of the building footprint or site plan, or significant changes to the architecture that modify the style, mass, or result in elimination of significant design features. Changes in color or materials would not require an amendment unless the colors were non-earth tones and the materials were of poorer quality (for example, going from tile roof to composition roofing) than originally approved. Changes to the project/site plan to meet conditions of approval or legislative changes shall not trigger an amendment.

**Response:** No housing is proposed as part of this proposal. The applicant understands that a Design Review Amendment would be triggered if changes were proposed after approval of 20% or more, as indicated in this Section.

#### 55.060 STAGED OR PHASED DEVELOPMENT

The applicant may elect to develop the site in stages. Staged development shall be subject to the provisions of CDC 99.125.

**Response:** The applicant is not proposing a staged or phased development.

#### 55.070 SUBMITTAL REQUIREMENTS

- A. The design review application shall be initiated by the property owner or the owner's agent, or condemnor.
- B. A pre-application conference, per CDC 99.030(B), shall be a prerequisite to the filing of an
- C. Documentation of any required meeting with the respective City-recognized neighborhood association per CDC 99.038.
- D. The applicant shall submit a completed application form and:

1. The development plan for a Class I design review shall contain the following elements:
  - a. A site analysis (CDC 55.110) only if the site is undeveloped;
  - b. A site plan (CDC 55.120);
  - c. Architectural drawings, including building envelopes and all elevations (CDC 55.140) only if architectural work is proposed; and
  - d. Pursuant to CDC 55.085, additional submittal material may be required.

One original application form must be submitted. One copy at the original scale and one copy reduced to 11 inches by 17 inches or smaller of all drawings and plans must be submitted. One copy of all other items must be submitted. The applicant shall also submit one copy of the complete application in a digital format acceptable to the City. When the application submittal is determined to be complete, additional copies may be required as determined by the Community Development Department.

2. The development plan for a Class II design review shall contain the following elements:
  - a. A site analysis (CDC 55.110);
  - b. A site plan (CDC 55.120);
  - c. A grading plan (CDC 55.130);
  - d. Architectural drawings, indicating floor plan and elevation (CDC 55.140);
  - e. A landscape plan (CDC 55.150);
  - f. A utility plan appropriate to respond to the approval criteria of CDC 55.100(I)(1) through (5) relating to streets, drainage, municipal water, sanitary sewers, solid waste, and recycling storage;
  - g. A light coverage plan with photometric data, including the location and type of outdoor lighting, with specific consideration given to compliance with CDC 55.100(J) pertaining to crime prevention and, if applicable, CDC 46.150(A)(13) pertaining to parking lot lighting;
  - h. If staff determines before or during the pre-application conference that the land use is expected to generate noise that may exceed DEQ standards, the application shall include a noise study conducted by a licensed acoustical engineer that demonstrates that the application and associated noise sources will meet DEQ standards. Typical noise sources of concern include, but are not limited to, vehicle drive-throughs, parking lots, HVAC units, and public address systems; and
  - i. Documents as required per the Tree Technical Manual.

**Response:**

A pre-application conference was held on January 18, 2018, per this requirement, and is referred to as PA-18-04. Notes were provided by the City and are included in this application in Exhibit C, Pre-Application Conference Notes. The application form for this application has been signed by the current owner (owner of record) and shall be reviewed as a Class II Quasi-Judicial Procedure.

A Site Analysis has been included in this narrative and is addressed below under 55.110, Site Analysis. As part of the Preliminary Development Plans in Exhibit D, the applicant has included a site plan, grading plan, utility plan, and landscape plan. The applicant has also included elevations depicting the floor plans and architectural exterior of proposed new buildings. The applicant is not aware of any specific elements of the proposed use that would generate excessive noise, and none were indicated by staff at the Pre-Application Conference.

3. A narrative, based on the standards contained in this code, which supports any requested exceptions as provided under CDC 55.170.
4. Submit full written responses to approval criteria of CDC 55.100 for Class II design review, or CDC 55.090 for Class I design review, plus all applicable referenced approval criteria.

**Response:** A narrative with full written responses to all applicable criteria, standards and requirements of the CDC are included in this narrative, herein, including those indicated in Sections 55.170 and 55.100, below.

- E. The applicant shall submit samples of all exterior building materials and colors in the case of new buildings or building remodeling.

**Response:** A materials board showing exterior building materials and colors is provided.

- F. The applicant shall pay the required deposit and fee.

**Response:** As part of this application submittal, the applicant is submitting the appropriate fee to the City of West Linn for the applications requested.

### 55.100 APPROVAL STANDARDS

The approval authority shall make findings with respect to the following criteria when approving, approving with conditions, or denying a Class II design review application:

- A. The provisions of the following chapters shall be met:

1. Chapter 34 CDC, Accessory Structures, Accessory Dwelling Units, and Accessory Uses.
2. Chapter 38 CDC, Additional Yard Area Required; Exceptions to Yard Requirements; Storage in Yards; Projections into Yards.
3. Chapter 40 CDC, Building Height Limitations, Exceptions.
4. Chapter 42 CDC, Clear Vision Areas.
5. Chapter 44 CDC, Fences.
6. Chapter 46 CDC, Off-Street Parking, Loading and Reservoir Areas.
7. Chapter 48 CDC, Access, Egress and Circulation.
8. Chapter 54 CDC, Landscaping.

**Response:** All of the applicable provisions of the Chapters indicated in Section 55.100 are addressed in various sections of this narrative herein.

- B. Relationship to the natural and physical environment.

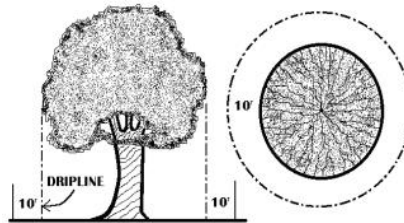
1. The buildings and other site elements shall be designed and located so that all heritage trees, as defined in the municipal code, shall be saved. Diseased heritage trees, as determined by the City Arborist, may be removed at his/her direction.

**Response:** There are no designated heritage trees on this site, per City records and the City Arborist.

2. All heritage trees, as defined in the municipal code, all trees and clusters of trees (“cluster” is defined as three or more trees with overlapping driplines; however, native oaks need not have an overlapping dripline) that are considered significant by the City Arborist, either individually or in consultation with certified arborists or similarly qualified professionals, based on accepted arboricultural standards including consideration of their size, type, location, health, long term survivability, and/or numbers, shall be protected pursuant to the criteria of subsections (B)(2)(a) through (f) of this section. In cases where there is a difference of opinion on the significance of a tree or tree cluster, the City Arborist’s findings shall prevail. It is

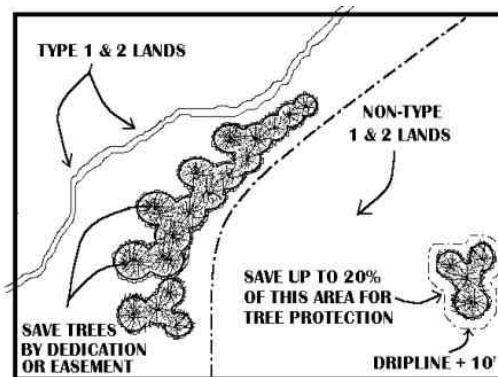
important to acknowledge that all trees are not significant and, further, that this code section will not necessarily protect all trees deemed significant.

- a. Non-residential and residential projects on Type I and II lands shall protect all heritage trees and all significant trees and tree clusters by limiting development in the protected area. The protected area includes the protected tree, its dripline, and an additional 10 feet beyond the dripline, as depicted in the figure below. Development of Type I and II lands shall require the careful layout of streets, driveways, building pads, lots, and utilities to avoid heritage trees and significant trees and tree clusters, and other natural resources pursuant to this code. The method for delineating the protected trees or tree clusters (“dripline plus 10 feet”) is explained in subsection (B)(2)(b) of this section. Exemptions of subsections (B)(2)(c), (e), and (f) of this section shall apply.



**PROTECTED AREA = DRIPLINE + 10 FEET**

- b. Non-residential and residential projects on non-Type I and II lands shall set aside up to 20 percent of the protected areas for significant trees and tree clusters, plus any heritage trees. Therefore, in the event that the City Arborist determines that a significant tree cluster exists at a development site, then up to 20 percent of the non-Type I and II lands shall be devoted to the protection of those trees by limiting development in the protected areas. The exact percentage is determined by establishing the driplines of the trees or tree clusters that are to be protected. In order to protect the roots which typically extend further, an additional 10-foot measurement beyond the dripline shall be added. The square footage of the area inside this “dripline plus 10 feet” measurement shall be the basis for calculating the percentage (see figure below). The City Arborist will identify which tree(s) are to be protected. Development of non-Type I and II lands shall also require the careful layout of streets, driveways, building pads, lots, and utilities to avoid significant trees, tree clusters, heritage trees, and other natural resources pursuant to this code. Exemptions of subsections (B)(2)(c), (e), and (f) of this section shall apply. Please note that in the event that more than 20 percent of the non-Type I and II lands comprise significant trees or tree clusters, the developer shall not be required to save the excess trees, but is encouraged to do so.



**METHOD OF PERCENTAGE CALCULATION**

**E.G., DRIPLINE + 10 FT. AREA = 2,500 SQ. FT. OR 18% OF TOTAL NON-TYPE I AND II LAND DENSITY CALCULATIONS FOR THIS PARCEL WILL BE BASED ON REMAINING NET SQ. FOOTAGE OF SITE (EXCLUDING THE 2,500 SQ. FT.)**



- c. Where stub outs of streets occur on abutting properties, and the extension of those streets will mean the loss of significant trees, tree clusters, or heritage trees, it is understood that tree loss may be inevitable. In these cases, the objective shall be to minimize tree loss. These provisions shall also apply in those cases where access, per construction code standards, to a lot or parcel is blocked by a row or screen of significant trees or tree clusters.
- d. For both non-residential and residential development, the layout shall achieve at least 70 percent of maximum density for the developable net area. The developable net area excludes all Type I and II lands and up to 20 percent of the remainder of the site for the purpose of protection of stands or clusters of trees as defined in subsection (B)(2) of this section.
- e. For arterial and collector street projects, including Oregon Department of Transportation street improvements, the roads and graded areas shall avoid tree clusters where possible. Significant trees, tree clusters, and heritage tree loss may occur, however, but shall be minimized.
- f. If the protection of significant tree(s) or tree clusters is to occur in an area of grading that is necessary for the development of street grades, per City construction codes, which will result in an adjustment in the grade of over or under two feet, which will then threaten the health of the tree(s), the applicant will submit evidence to the Planning Director that all reasonable alternative grading plans have been considered and cannot work. The applicant will then submit a mitigation plan to the City Arborist to compensate for the removal of the tree(s) on an "inch by inch" basis (e.g., a 48-inch Douglas fir could be replaced by 12 trees, each four-inch). The mix of tree sizes and types shall be approved by the City Arborist.

**Response:**

The City Arborist has identified 4 significant trees on the site, and one of these trees is proposed to be removed. Therefore, 75% of significant trees are proposed for protection. Tree protection zones are designated around all trees being preserved. The only trees proposed for removal are diseased, in poor form, or are being displaced by new improvements. The significant tree proposed for removal will be mitigated per the standards herein. See Arborist Report for details.

**3. The topography and natural drainage shall be preserved to the greatest degree possible.**

**Response:**

The natural topography and associated drainage of the site will be significantly preserved. Buildings, parking areas and other development will be located and designed so that natural grades will be substantially maintained. However, in order to insure a more safe and usable playground area, some grading is proposed. In addition, grading will be necessary for building foundations and the storm water facility. See Site Plan, Grading Plan and Utility Plan as part of the Preliminary Development Plans in Exhibit D.

**4. The structures shall not be located in areas subject to slumping and sliding. The Comprehensive Plan Background Report's Hazard Map, or updated material as available and as deemed acceptable by the Planning Director, shall be the basis for preliminary determination.**

**Response:**

No slumping or sliding has been identified on the site, nor indicated in the Comprehensive Plan Background Report's Hazard Map. See Geotechnical Report, Exhibit G.

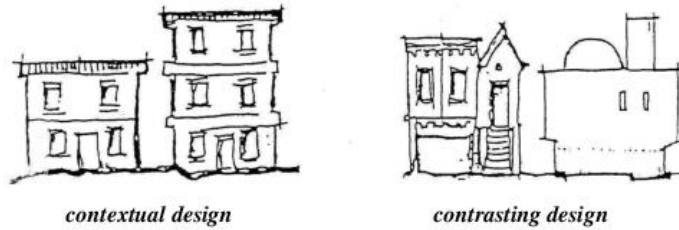
**5. There shall be adequate distance between on-site buildings and on-site and off-site buildings on adjoining properties to provide for adequate light and air circulation and for fire protection.**

**Response:**

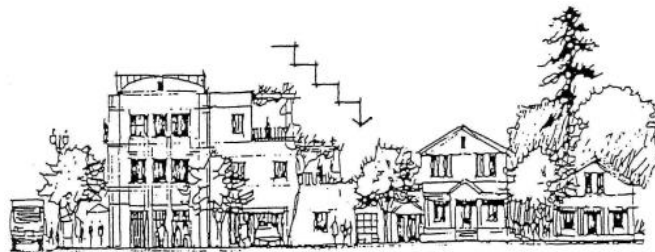
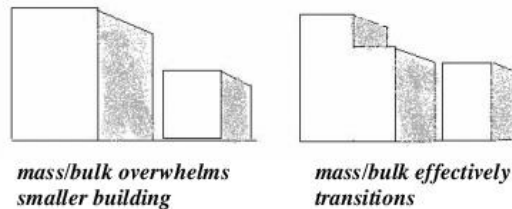
The applicant has designed the mass and height of the building that are part of this redevelopment project, as well as the location of the buildings, to balance the requirements of the anticipated school programs with the desire to minimize impacts associated with noise and adequacy of light and air. The abutting properties are those properties to the south, west and north of the subject site; properties to the east are separated from the site by right-of-way. All proposed buildings and development meet the required setbacks which are intended to insure that adequate light and air, as well as fire suppression access, are all maintained.

**6. Architecture.**

- a. **The proposed structure(s) scale shall be compatible with the existing structure(s) on site and on adjoining sites. Contextual design is required. Contextual design means respecting and incorporating prominent architectural styles, building lines, roof forms, rhythm of windows, building scale and massing of surrounding buildings in the proposed structure. The materials and colors shall be complementary to the surrounding buildings.**



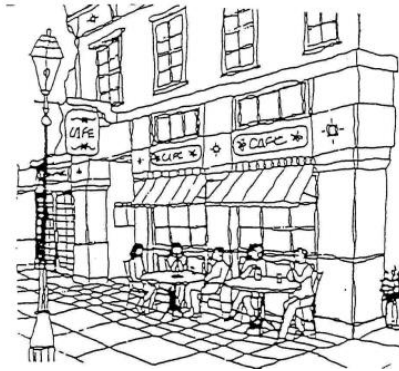
- b. **While there has been discussion in Chapter 24 CDC about transition, it is appropriate that new buildings should architecturally transition in terms of bulk and mass to work with, or fit, adjacent existing buildings. This transition can be accomplished by selecting designs that “step down” or “step up” from small to big structures and vice versa (see figure below). Transitions may also take the form of carrying building patterns and lines (e.g., parapets, windows, etc.) from the existing building to the new one.**



- c. **Contrasting architecture shall only be permitted when the design is manifestly superior to adjacent architecture in terms of creativity, design, and workmanship, and/or it is adequately separated from other buildings by distance, screening, grade variations, or is part of a development site that is large enough to set its own style of architecture.**

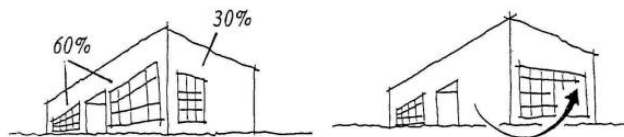
- d. Human scale is a term that seeks to accommodate the users of the building and the notion that buildings should be designed around the human scale (i.e., their size and the average range of their perception). Human scale shall be accommodated in all designs by, for example, multi-light windows that are broken up into numerous panes, intimately scaled entryways, and visual breaks (exaggerated eaves, indentations, ledges, parapets, awnings, engaged columns, etc.) in the facades of buildings, both vertically and horizontally.

The human scale is enhanced by bringing the building and its main entrance up to the edge of the sidewalk. It creates a more dramatic and interesting streetscape and improves the “height and width” ratio referenced in this section.

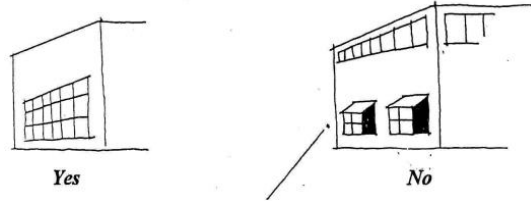


*Human scale is captured in this example*

- e. The main front elevation of commercial and office buildings shall provide at least 60 percent windows or transparency at the pedestrian level to create more interesting streetscape and window shopping opportunities. One side elevation shall provide at least 30 percent transparency. Any additional side or rear elevation, which is visible from a collector road or greater classification, shall also have at least 30 percent transparency. Transparency on other elevations is optional. The transparency is measured in lineal fashion. For example, a 100-foot-long building elevation shall have at least 60 feet (60 percent of 100 feet) in length of windows. The window height shall be, at minimum, three feet tall. The exception to transparency would be cases where demonstrated functional constraints or topography restrict that elevation from being used. When this exemption is applied to the main front elevation, the square footage of transparency that would ordinarily be required by the above formula shall be installed on the remaining elevations at pedestrian level in addition to any transparency required by a side elevation, and vice versa. The rear of the building is not required to include transparency. The transparency must be flush with the building elevation.



***60 percent of lineal street facing or main elevation is windows. 30 percent of one side elevation is windows. You may transfer windows from the side to front, or vice versa.***



*(Windows not at eye level and/or not flush with building.)*

- f. Variations in depth and roof line are encouraged for all elevations.
- f. To vary the otherwise blank wall of most rear elevations, continuous flat elevations of over 100 feet in length should be avoided by indents or variations in the wall. The use of decorative brick, masonry, or stone insets and/or designs is encouraged. Another way to vary or soften this elevation is through terrain variations such as an undulating grass area with trees to provide vertical relief.
- g. Consideration of the micro-climate (e.g., sensitivity to wind, sun angles, shade, etc.) shall be made for building users, pedestrians, and transit users, including features like awnings.
- h. The vision statement identified a strong commitment to developing safe and attractive pedestrian environments with broad sidewalks, canopied with trees and awnings.



*Trees, awnings, and building orientation enhance the micro-climate*

- i. Sidewalk cafes, kiosks, vendors, and street furniture are encouraged. However, at least a four-foot-wide pedestrian accessway must be maintained per Chapter 53 CDC, Sidewalk Use.

**Response:**

The form of the new building compliments the existing structures as viewed from Old River Road, and the simplicity of the shape of the new structure does not compete with the shape of the other structures. The taller portions of the new structure have been set back from Old River Road and from the front façade of the existing buildings, resulting in the new structure having the scale and appearance of a one story building. Both the existing building and the new building will be similar in height at the front eave line facing Old River Road. The buildings will also share a similar width and scale dimension facing Old River Road. In addition, the new parking area will be landscaped along the perimeter to provide additional visual aesthetics to the overall site and character of the district, which is primarily school activities associated with school buildings and outdoor areas.

Exterior Design: The form of the new building compliments the existing structure as viewed from Old River Road, and the simplicity of the shape of the new structure does not compete with the shape of the existing structure. The utilization of significant window glazing elements are used to designate the connection of the interior with the exterior campus.

Massing: The taller portions of the new structure have been set back from Old River Road and from the front façade of the existing structure, resulting in the new structure having the scale and appearance of a one story building

Arrangement: For the new structures, the front yard setback from Old River Road exceeds the setback requirement. Both the Phase I and Phase II new structures are set back 20 feet or greater from the rear property line, 10 feet or greater from the side property lines, and 95 feet or greater from the front property line, allowing for adequate levels of light and air. The façade and building line of the new structure is aligned with the façade and building line of the other existing structures. This allows for all of the structure to retain their prominence as elements of a small scale campus visible to the street.

Proportion: The height and width of the front facade of the new building in Phase II that fronts on Old River Road is similar to, and compatible with, the front facade of the existing structures. The overall building program includes a two story 12 classroom building and a library. The two story portion of the new building is placed to the rear of the site to minimize it's scale, with the smaller scaled library placed in front behind the existing trees.

Detail: The existing buildings are detailed with simplicity, and there are very simple and humble materials included in the design. There is little use of ornament, simple and minimal use of trim, and a fairly direct expression of structure, particularly at the porches and eaves. The new building will also be detailed in a very similar manner, with a simple and minimal material pallet, restrained use of ornament and trim, and minimal expression of structure at the porches and eaves. The intention of restrained expression of these elements will allow the landscape and outdoor learning and outdoor play areas to stand out on the site and as viewed from the public right-of-way.

Scale: The new Phase II building will be only approximately 6 feet taller in height than the existing buildings facing Old River Road. The new building has a linear organization with hallways used as extended learning spaces and the smaller scaled library facing the outdoor learning and play areas and Old River Road. Right angle and linear placement of the structure relative to existing structures provides adequate space for connectivity, while also providing a comfortable scale. This allows for all of the structures to appear to be elements of a small scale campus visible to the street.

Color: The existing structures are painted a light tan color, while both the Phase I and Phase II new structures will be earth toned in color. The Phase II earth toned color may be achieved through natural cedar wood siding and/or painted earth tone siding. The result will be a campus of buildings where the new building and the existing buildings will form a neutral earth toned background behind the landscape, outdoor learning and play areas, and the pedestrian network.

Texture: The primary consideration of texture is the exterior siding and some compatibility with the existing structure. The existing structures have a combination of smooth wood lap siding, smooth wood trim, and smooth concrete block. The exterior of the Phase I modular classroom building will be smooth lap siding to relate to the existing structures and adjacent residential structures. The exterior of the Phase II structure will also have natural cedar wood siding with trim and/or painted siding with trim, which may vary in orientation and width to break up the scale of the larger building facades.

Materials: Exterior materials are chosen to relate to the existing neighborhood residential earth toned structures and to align with the school's mission of sustainability and outdoor learning. Exterior siding may be natural cedar wood siding or painted siding. Larger exterior windows connect students to the outside learning and play areas.

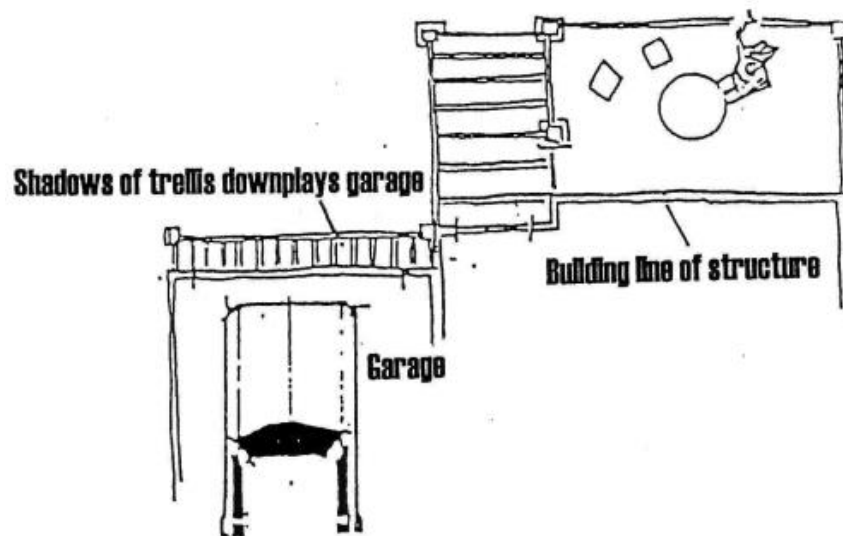
**7. Transportation. The automobile shall be shifted from a dominant role, relative to other modes of transportation, by the following means:**

- a. Commercial and office development shall be oriented to the street. At least one public entrance shall be located facing an arterial street; or, if the project does not front on an arterial, facing a collector street; or, if the**

project does not front on a collector, facing the local street with highest traffic levels. Parking lots shall be placed behind or to the side of commercial and office development. When a large and/or multi-building development is occurring on a large undeveloped tract (three plus acres), it is acceptable to focus internally; however, at least 20 percent of the main adjacent right-of-way shall have buildings contiguous to it unless waived per subsection (B)(7)(c) of this section. These buildings shall be oriented to the adjacent street and include pedestrian-oriented transparencies on those elevations.

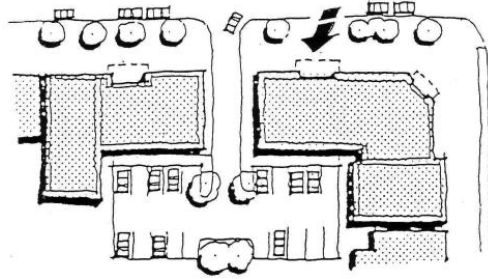
For individual buildings on smaller individual lots, at least 30 lineal feet or 50 percent of the building must be adjacent to the right-of-way unless waived per subsection (B)(7)(c) of this section. The elevations oriented to the right-of-way must incorporate pedestrian-oriented transparency.

- b. Multi-family projects shall be required to keep the parking at the side or rear of the buildings or behind the building line of the structure as it would appear from the right-of-way inside the multi-family project. For any garage which is located behind the building line of the structure, but still facing the front of the structure, architectural features such as patios, patio walls, trellis, porch roofs, overhangs, pergolas, etc., shall be used to downplay the visual impact of the garage, and to emphasize the rest of the house and front entry.



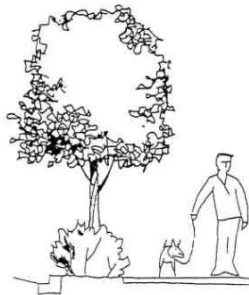
The parking may be positioned inside small courtyard areas around which the units are built. These courtyard spaces encourage socialization, defensible space, and can provide a central location for landscaping, particularly trees, which can provide an effective canopy and softening effect on the courtyard in only a few years. Vehicular access and driveways through these courtyard areas is permitted.

- c. Commercial, office, and multi-family projects shall be built as close to the adjacent main right-of-way as practical to facilitate safe pedestrian and transit access. Reduced frontages by buildings on public rights-of-way may be allowed due to extreme topographic (e.g., slope, creek, wetlands, etc.) conditions or compelling functional limitations, not just inconveniences or design challenges.



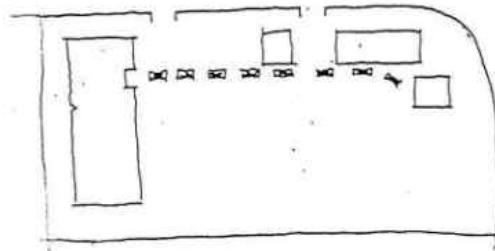
*Entrance from right-of-way*

- d. Accessways, parking lots, and internal driveways shall accommodate pedestrian circulation and access by specially textured, colored, or clearly defined footpaths at least six feet wide. Paths shall be eight feet wide when abutting parking areas or travel lanes. Paths shall be separated from parking or travel lanes by either landscaping, planters, curbs, bollards, or raised surfaces. Sidewalks in front of storefronts on the arterials and main store entrances on the arterials identified in CDC 85.200(A)(3) shall be 12 feet wide to accommodate pedestrians, sidewalk sales, sidewalk cafes, etc. Sidewalks in front of storefronts and main store entrances in commercial/OBC zone development on local streets and collectors shall be eight feet wide.



*Landscaping*

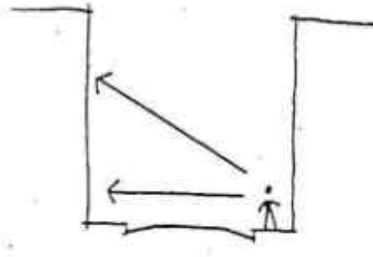
- e. Paths shall provide direct routes that pedestrians will use between buildings, adjacent rights-of-way, and adjacent commercial developments. They shall be clearly identified. They shall be laid out to attract use and to discourage people from cutting through parking lots and impacting environmentally sensitive areas.



*Direct pedestrian route required (--)*

- f. At least one entrance to the building shall be on the main street, or as close as possible to the main street. The entrance shall be designed to identify itself as a main point of ingress/egress.
- g. Where transit service exists, or is expected to exist, there shall be a main entrance within a safe and reasonable distance of the transit stop. A pathway shall be provided to facilitate a direct connection.

- h. Projects shall bring at least part of the project adjacent to or near the main street right-of-way in order to enhance the height-to-width ratio along that particular street. (The “height-to-width ratio” is an architectural term that emphasizes height or vertical dimension of buildings adjacent to streets. The higher and closer the building is, and the narrower the width of the street, the more attractive and intimate the streetscape becomes.) For every one foot in street width, the adjacent building ideally should be one to two feet higher. This ratio is considered ideal in framing and defining the streetscape.



*1:1 height to width ratio is ideal (example only)*

- i. These architectural standards shall apply to public facilities such as reservoirs, water towers, treatment plants, fire stations, pump stations, power transmission facilities, etc. It is recognized that many of these facilities, due to their functional requirements, cannot readily be configured to meet these architectural standards. However, attempts shall be made to make the design sympathetic to surrounding properties by landscaping, setbacks, buffers, and all reasonable architectural means.
- j. Parking spaces at trailheads shall be located so as to preserve the view of, and access to, the trailhead entrance from the roadway. The entrance apron to the trailhead shall be marked: “No Parking,” and include design features to foster trail recognition.

**Response:**

Though not technically a commercial or office development, the new campus redevelopment will focus on a connection between the front of the new building and Old River Road. The new front door of the school faces out towards Old River Road, with various connections to the street and to other portions of the site, including the parking area. The existing buildings on the site will be retained and the new building is located with the intention and purpose of maintaining and enhancing a campus character of the site. Due to retention of the existing buildings, it was also necessary to locate the parking area in the existing location, which is essentially at the side of the overall campus. Overall, the site design includes a pedestrian network providing connectivity among existing buildings, parking areas, the front of the new building and the adjacent street.

**C. Compatibility between adjoining uses, buffering, and screening.**

1. In addition to the compatibility requirements contained in Chapter 24 CDC, buffering shall be provided between different types of land uses; for example, buffering between single-family homes and apartment blocks. However, no buffering is required between single-family homes and duplexes or single-family attached units. The following factors shall be considered in determining the adequacy of the type and extent of the buffer:
- The purpose of the buffer, for example to decrease noise levels, absorb air pollution, filter dust, or to provide a visual barrier.
  - The size of the buffer required to achieve the purpose in terms of width and height.
  - The direction(s) from which buffering is needed.



- d. **The required density of the buffering.**
- e. **Whether the viewer is stationary or mobile.**

**Response:**

Abutting properties are not significantly impacted by the redevelopment project beyond the existing scale of the church campus, as there will be only a slight increase in height from the existing height of some portions of the existing church campus buildings, as well as minor changes in overall building coverage of the site. Most of those abutting properties will see no impact related to the project, as most of the renovation and additions proposed are essentially within the existing developed area. Besides temporary construction noise impacts, the future enrollment for the campus is expected to stay relatively moderate and similar to peak use periods at the existing church, so noise impacts will not increase as part of this final redevelopment of the campus. However, the applicant is proposing buffering and effective screening along the perimeter of the parking areas to further protect private areas of adjoining properties from noise impacts.

Overall, the proposal includes the maintenance of existing landscaping and new landscaping in those areas that provide more robust buffering and screening to meet Code requirements. See Landscape Plan on Sheets L0.01-L1.03 of the Preliminary Development Plans in Exhibit D.

- 2. **On-site screening from view from adjoining properties of such things as service areas, storage areas, and parking lots shall be provided and the following factors will be considered in determining the adequacy of the type and extent of the screening:**
  - a. **What needs to be screened?**
  - b. **The direction from which it is needed.**
  - c. **How dense the screen needs to be.**
  - d. **Whether the viewer is stationary or mobile.**
  - e. **Whether the screening needs to be year-round.**

**Response:**

6 foot tall chain link fencing and gates surround the trash area. The south and west sides, where the area is adjacent to landscape areas, are screened with closely planted hedging (Arborvitae).

- 3. **Rooftop air cooling and heating systems and other mechanical equipment shall be screened from view from adjoining properties.**

**Response:**

Interior mechanical units will be utilized for the majority of the Phase II new building. Any outdoor mechanical units will be screened from view from adjoining properties.

**D. Privacy and noise.**

- 1. **Structures which include residential dwelling units shall provide private outdoor areas for each ground floor unit which is screened from view from adjoining units.**
- 2. **Residential dwelling units shall be placed on the site in areas having minimal noise exposure to the extent possible. Natural-appearing sound barriers shall be used to lessen noise impacts where noise levels exceed the noise standards contained in West Linn Municipal Code Section 5.487.**
- 3. **Structures or on-site activity areas which generate noise, lights, or glare shall be buffered from adjoining residential uses in accordance with the standards in subsection C of this section where applicable.**
- 4. **Businesses or activities that can reasonably be expected to generate noise in excess of the noise standards contained in West Linn Municipal Code Section 5.487 shall undertake and submit appropriate noise studies and mitigate as necessary to comply with the code. (See CDC 55.110(B)(11) and 55.120(M).)**

**If the decision-making authority reasonably believes a proposed use may generate noise exceeding the standards specified in the municipal code, then the authority may require the applicant to supply professional noise studies from time to time during the user's first year of operation to monitor compliance with City standards and permit requirements.**

**Response:** Abutting properties are not significantly impacted by the redevelopment project beyond the existing church campus, as there will be only a slight increase in height from the existing height of some portions of the existing church campus buildings. Most of those abutting properties will see no impact related to the project, as most of the renovation and additions proposed are essentially within the existing building envelopes, and additions are all to the northwest of the existing building area. Besides temporary construction noise impacts, the future enrollment for the campus is expected to stay relatively the same, so noise impacts will not increase as part of this final redevelopment of the campus. However, the applicant is proposing buffering and effective screening along the perimeter of the parking areas, as well as other portions of the property boundary, to further protect private areas of adjoining properties from visual or noise impacts.

**E. Private outdoor area. This section only applies to multi-family projects.**

- 1. In addition to the requirements of residential living, unit shall have an outdoor private area (patio, terrace, porch) of not less than 48 square feet in area;**
- 2. The outdoor space shall be oriented towards the sun where possible; and**
- 3. The area shall be screened or designed to provide privacy for the users of the space.**
- 4. Where balconies are added to units, the balconies shall not be less than 48 square feet, if they are intended to be counted as private outdoor areas.**

**Response:** The project does not include any multi-family elements, therefore, this Section does not apply.

**F. Shared outdoor recreation areas. This section only applies to multi-family projects and projects with 10 or more duplexes or single-family attached dwellings on lots under 4,000 square feet. In those cases, shared outdoor recreation areas are calculated on the duplexes or single-family attached dwellings only. It also applies to qualifying PUDs under the provisions of CDC 24.170.**

- 1. In addition to the requirements of subsection E of this section, usable outdoor recreation space shall be provided in residential developments for the shared or common use of all the residents in the following amounts:**
  - a. Studio up to and including two-bedroom units: 200 square feet per unit.**
  - b. Three or more bedroom units: 300 square feet per unit.**
- 2. The required recreation space may be provided as follows:**
  - a. It may be all outdoor space; or**
  - b. It may be part outdoor space and part indoor space; for example, an outdoor tennis court and indoor recreation room; and**
  - c. Where some or all of the required recreation area is indoor, such as an indoor recreation room, then these indoor areas must be readily accessible to all residents of the development subject to clearly posted restrictions as to hours of operation and such regulations necessary for the safety of minors.**
  - d. In considering the requirements of this subsection F, the emphasis shall be on usable recreation space. No single area of outdoor recreational space**

shall encompass an area of less than 250 square feet. All common outdoor recreational space shall be clearly delineated and readily identifiable as such. Small, marginal, and incidental lots or parcels of land are not usable recreation spaces. The location of outdoor recreation space should be integral to the overall design concept of the site and be free of hazards or constraints that would interfere with active recreation.

3. The shared space shall be readily observable to facilitate crime prevention and safety.

**Response:** The project does not include any multi-family elements, therefore, this Section does not apply.

**G. Demarcation of public, semi-public, and private spaces. The structures and site improvements shall be designed so that public areas such as streets or public gathering places, semi-public areas, and private outdoor areas are clearly defined in order to establish persons having a right to be in the space, to provide for crime prevention, and to establish maintenance responsibility. These areas may be defined by:**

1. A deck, patio, fence, low wall, hedge, or draping vine;
2. A trellis or arbor;
3. A change in level;
4. A change in the texture of the path material;
5. Sign; or
6. Landscaping.

**Use of gates to demarcate the boundary between a public street and a private access driveway is prohibited.**

**Response:** The new design includes an open play area oriented toward Old River Road and the neighborhood. This area is intended for the sole use of the school patrons during operating hours, due to safety and security issues, however, this open area will be open to the public after school hours.

**H. Public transit.**

1. Provisions for public transit may be required where the site abuts an existing or planned public transit route. The required facilities shall be based on the following:
  - a. The location of other transit facilities in the area.
  - b. The size and type of the proposed development.
  - c. The rough proportionality between the impacts from the development and the required facility.
2. The required facilities shall be limited to such facilities as the following:
  - a. A waiting shelter with a bench surrounded by a three-sided covered structure, with transparency to allow easy surveillance of approaching buses.
  - b. A turnout area for loading and unloading designed per regional transit agency standards.
  - c. Hard-surface paths connecting the development to the waiting and boarding areas.
  - d. Regional transit agency standards shall, however, prevail if they supersede these standards.

3. The transit stop shall be located as close as possible to the main entrance to the shopping center, public or office building, or multi-family project. The entrance shall not be more than 200 feet from the transit stop with a clearly identified pedestrian link.
4. All commercial business centers (over three acres) and multi-family projects (over 40 units) may be required to provide for the relocation of transit stops to the front of the site if the existing stop is within 200 to 400 yards of the site and the exaction is roughly proportional to the impact of the development. The commercial or multi-family project may be required to provide new facilities in those cases where the nearest stop is over 400 yards away. The transit stop shall be built per subsection (H)(2) of this section.

**Response:**

It is not likely that the students and staff use will use public transit to a significant level, as most members of the school come from locations around the region and not necessarily West Linn. It is estimated that approximately 5% of all staff and students will use public transit in the form of Tri-Met bus service. There is an existing transit facilities (bus stop) near the intersection of Willamette /Drive and Cedar Oak Drive, approximately .2 miles from the subject property. In addition, there is a park and ride located at the intersection of Highway 43 and Cedar Oak Road that provides additional transit options.

- I. **Public facilities. An application may only be approved if adequate public facilities will be available to provide service to the property prior to occupancy.**
  1. **Streets. Sufficient right-of-way and slope easement shall be dedicated to accommodate all abutting streets to be improved to the City's Improvement Standards and Specifications. The City Engineer shall determine the appropriate level of street and traffic control improvements to be required, including any off-site street and traffic control improvements, based upon the transportation analysis submitted. The City Engineer's determination of developer obligation, the extent of road improvement and City's share, if any, of improvements and the timing of improvements shall be made based upon the City's systems development charge ordinance and capital improvement program, and the rough proportionality between the impact of the development and the street improvements.**

In determining the appropriate sizing of the street in commercial, office, multi-family, and public settings, the street should be the minimum necessary to accommodate anticipated traffic load and needs and should provide substantial accommodations for pedestrians and bicyclists. Road and driveway alignment should consider and mitigate impacts on adjacent properties and in neighborhoods in terms of increased traffic loads, noise, vibrations, and glare.

The realignment or redesign of roads shall consider how the proposal meets accepted engineering standards, enhances public safety, and favorably relates to adjacent lands and land uses. Consideration should also be given to selecting an alignment or design that minimizes or avoids hazard areas and loss of significant natural features (drainage ways, wetlands, heavily forested areas, etc.) unless site mitigation can clearly produce a superior landscape in terms of shape, grades, and reforestation, and is fully consistent with applicable code restrictions regarding resource areas.

Streets shall be installed per Chapter 85 CDC standards. The City Engineer has the authority to require that street widths match adjacent street widths. Sidewalks shall be installed per CDC 85.200(A)(3) for commercial and office projects, and CDC 85.200(A)(16) and 92.010(H) for residential projects, and applicable provisions of this chapter. Where streets bisect or traverse water resource areas (WRAs) the street width shall be reduced to the appropriate "constrained" cross-section width indicated in the TSP or alternate configurations which are appropriate to site

**conditions, minimize WRA disturbance or are consistent with an adopted transportation system plan. The street design shall also be consistent with habitat friendly provisions of CDC 32.060(I).**

**Based upon the City Manager's or Manager's designee's determination, the applicant shall construct or cause to be constructed, or contribute a proportionate share of the costs, for all necessary off-site improvements identified by the transportation analysis commissioned to address CDC 55.125 that are required to mitigate impacts from the proposed development. Proportionate share of the costs shall be determined by the City Manager or Manager's designee, who shall assume that the proposed development provides improvements in rough proportion to identified impacts of the development.**

**Response:**

The subject site fronts along the Old River Road right-of-way, which is classified as a Neighborhood Route that can adequately serve this development and the associated neighborhood. The current width of this right-of-way is 60 feet and the proposed width is 60 feet, therefore, there is no right-of-way dedication required and none proposed. The applicant is proposing an alternative design for sidewalk based on commentary from the neighbors at the neighborhood meeting. The proposed design is a 6-foot wide asphalt pedestrian (multi-modal) path along the entire frontage of the site. This alternative design allows for a softer aesthetic along the frontage of the site, while still providing a pathway for multiple modes of alternative transportation. The alternative design considers and mitigates for impacts associated with the standard design on adjacent properties and in neighborhoods in terms of aesthetic, safety, traffic, noise, vibrations, and glare. The alternative to required street standards is further addressed in Chapter 85, below.

Phase I of the proposed project includes only the modular classroom, while Phase II includes the full build-out of the project. As part of Phase II, the applicant is requesting that the City Engineer either allow the proposed alternative right-of-way improvements, or approve the waiver request and allow the applicant to pay a fee-in-lieu of developing the right-of-way, per current Public Works standards. See attached Waiver Request form.

- 2. Storm detention and treatment and geologic hazards. Per the submittals required by CDC 55.130 and 92.010(E), all proposed storm detention and treatment facilities must comply with the standards for the improvement of public and private drainage systems located in the West Linn Public Works Design Standards, there will be no adverse off-site impacts caused by the development (including impacts from increased intensity of runoff downstream or constrictions causing ponding upstream), and the applicant must provide sufficient factual data to support the conclusions of the submitted plan.**

**Per the submittals required by CDC 55.130(E), the applicant must demonstrate that the proposed methods of rendering known or potential hazard sites safe for development, including proposed geotechnical remediation, are feasible and adequate to prevent landslides or other damage to property and safety. The review authority may impose conditions, including limits on type or intensity of land use, which it determines are necessary to mitigate known risks of landslides or property damage.**

**Response:**

All required public facilities currently serve the site, including storm water, water, sanitary sewer and waste/recycling. A new storm water management system is also proposed to serve on-site flow and collection through a surface rain garden and onsite detention system for the new development areas. This new system will discharge onsite drainage to the existing public storm drainage system in Old River Road.

In addition, the City Engineer has indicated that at this location it may be possible to eliminate detention entirely. The applicant is submitting a Stormwater Management Report to the City Engineer to determine the level of downstream capacity and the need for on-site detention. Preliminary analysis on the downstream public system indicates that there is adequate

capacity. The applicant requests review of the report by the City Engineer so that the detailed analysis within the revised storm water report will provide the analysis needed to waive detention requirements by the City Engineer.

3. **Municipal water. A registered civil engineer shall prepare a plan for the provision of water which demonstrates to the City Engineer's satisfaction the availability of sufficient volume, capacity, and pressure to serve the proposed development's domestic, commercial, and industrial fire flows. All plans will then be reviewed by the City Engineer.**
4. **Sanitary sewers. A registered civil engineer shall prepare a sewerage collection system plan which demonstrates sufficient on-site capacity to serve the proposed development. The City Engineer shall determine whether the existing City system has sufficient capacity to serve the development.**
5. **Solid waste and recycling storage areas. Appropriately sized and located solid waste and recycling storage areas shall be provided. Metro standards shall be used.**

**Response:**

All required public facilities currently serve the site, including storm water, water, sanitary sewer and waste/recycling. A new 6" fire water service is proposed to provide a complete sprinkler system for both the existing and proposed new buildings. This new service will connect to the existing 8" public water line in Old River Road. There is an 8-inch sanitary sewer line in Old River Road that will continue to serve the site. There is an existing solid waste and recycling area on-site. This area will temporarily be relocated in Phase I to make room for the proposed temporary portable classroom building. The area is shown on the site drawings and will be enclosed within 6' high chain link fencing with privacy slats. The area will be relocated in Phase II as shown on the site drawings within a similar fencing system with privacy slats.

**J. Crime prevention and safety/defensible space.**

1. **Windows shall be located so that areas vulnerable to crime can be surveyed by the occupants.**
2. **Interior laundry and service areas shall be located in a way that they can be observed by others.**
3. **Mailboxes, recycling, and solid waste facilities shall be located in lighted areas having vehicular or pedestrian traffic.**
4. **The exterior lighting levels shall be selected and the angles shall be oriented towards areas vulnerable to crime.**
5. **Light fixtures shall be provided in areas having heavy pedestrian or vehicular traffic and in potentially dangerous areas such as parking lots, stairs, ramps, and abrupt grade changes.**
6. **Fixtures shall be placed at a height so that light patterns overlap at a height of seven feet which is sufficient to illuminate a person. All commercial, industrial, residential, and public facility projects undergoing design review shall use low or high pressure sodium bulbs and be able to demonstrate effective shielding so that the light is directed downwards rather than omni-directional. Omni-directional lights of an ornamental nature may be used in general commercial districts only.**
7. **Lines of sight shall be reasonably established so that the development site is visible to police and residents.**
8. **Security fences for utilities (e.g., power transformers, pump stations, pipeline control equipment, etc.) or wireless communication facilities may be up to eight feet tall in order to protect public safety. No variances are required regardless of location.**

**Response:** The new building is designed with a substantial amount of glazing, both on the first and second floors of the building, which allows for surveillance from most portions of the new building. The proximity of all buildings and areas of pedestrian activity to the street enables substantial visibility (lines of sight) between the site and Old River Road. On-site lighting is proposed as light fixtures attached to buildings. These fixtures will be placed in areas that may require extra illumination, such as stairs or entryways. This site lighting will also partially illuminate the proposed parking area. However, light fixtures specific to parking areas are not proposed, as this portion of the site is visible from the street, partially illuminated and relatively safe. In addition, some concern has been expressed from neighbors about parking lot illumination impact on adjacent properties.

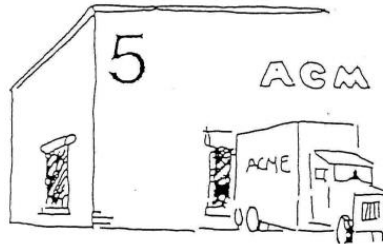
**K. Provisions for persons with disabilities.**

1. **The needs of a person with a disability shall be provided for. Accessible routes shall be provided between all buildings and accessible site facilities. The accessible route shall be the most practical direct route between accessible building entries, accessible site facilities, and the accessible entry to the site. An accessible route shall connect to the public right-of-way and to at least one on-site or adjacent transit stop (if the area is served by transit). All facilities shall conform to, or exceed, the Americans with Disabilities Act (ADA) standards, including those included in the Uniform Building Code.**

**Response:** The pedestrian network on-site includes accessible routes among the buildings and with the proposed parking area. This network also provides a direct connection between the main building and the public right-of-way.

**L. Signs.**

1. **Based on considerations of crime prevention and the needs of emergency vehicles, a system of signs for identifying the location of each residential unit, store, or industry shall be established.**



2. **The signs, graphics, and letter styles shall be designed to be compatible with surrounding development, to contribute to a sense of project identity, or, when appropriate, to reflect a sense of the history of the area and the architectural style.**
3. **The sign graphics and letter styles shall announce, inform, and designate particular areas or uses as simply and clearly as possible.**
4. **The signs shall not obscure vehicle driver's sight distance.**
5. **Signs indicating future use shall be installed on land dedicated for public facilities (e.g., parks, water reservoir, fire halls, etc.).**
6. **Signs and appropriate traffic control devices and markings shall be installed or painted in the driveway and parking lot areas to identify bicycle and pedestrian routes.**

**Response:** Signage will be submitted as a separate application.

**M. Utilities. The developer shall make necessary arrangements with utility companies or other persons or corporations affected for the installation of underground lines and facilities.**

**Electrical lines and other wires, including but not limited to communication, street lighting, and cable television, shall be placed underground, as practical. The design standards of Tables 1 and 2 above, and of subsection 5.487 of the West Linn Municipal Code relative to existing high ambient noise levels shall apply to this section.**

**Response:** The proposed site is within a developed neighborhood and franchise utilities such as power, telephone and communication/telecom are already installed along the street and within the site. Any new service lateral upgrades into the site will be coordinated with the appropriate service providers.)

**N. Wireless communication facilities (WCFs). (This section only applicable to WCFs.) WCFs as defined in Chapter 57 CDC may be required to go through Class I or Class II design review. The approval criteria for Class I design review is that the visual impact of the WCF shall be minimal to the extent allowed by Chapter 57 CDC. Stealth designs shall be sufficiently camouflaged so that they are not easily seen by passersby in the public right-of-way or from any adjoining residential unit. WCFs that are classified as Class II design review must respond to all of the approval criteria of this chapter.**

**Response:** No Wireless Communication Facilities are part of this proposal, therefore, this Section does not apply.

**O. Refuse and recycling standards.**

- 1. All commercial, industrial and multi-family developments over five units requiring Class II design review shall comply with the standards set forth in these provisions. Modifications to these provisions may be permitted if the Planning Commission determines that the changes are consistent with the purpose of these provisions and the City receives written evidence from the local franchised solid waste and recycling firm that they are in agreement with the proposed modifications.**
- 2. Compactors, containers, and drop boxes shall be located on a level Portland cement concrete pad, a minimum of four inches thick, at ground elevation or other location compatible with the local franchise collection firm's equipment at the time of construction. The pad shall be designed to discharge surface water runoff to avoid ponding.**
- 3. Recycling and solid waste service areas.**
  - a. Recycling receptacles shall be designed and located to serve the collection requirements for the specific type of material.**
  - b. The recycling area shall be located in close proximity to the garbage container areas and be accessible to the local franchised collection firm's equipment.**
  - c. Recycling receptacles or shelters located outside a structure shall have lids and be covered by a roof constructed of water and insect-resistive material. The maintenance of enclosures, receptacles and shelters is the responsibility of the property owner.**
  - d. The location of the recycling area and method of storage shall be approved by the local fire marshal.**
  - e. Recycling and solid waste service areas shall be at ground level and/or otherwise accessible to the franchised solid waste and recycling collection firm.**
  - f. Recycling and solid waste service areas shall be used only for purposes of storing solid waste and recyclable materials and shall not be a general storage area to store personal belongings of tenants, lessees, property management or owners of the development or premises.**



- g. Recyclable material service areas shall be maintained in a clean and safe condition.
4. Special wastes or recyclable materials.
    - a. Environmentally hazardous wastes defined in ORS 466.005 shall be located, prepared, stored, maintained, collected, transported, and disposed in a manner acceptable to the Oregon Department of Environmental Quality.
    - b. Containers used to store cooking oils, grease or animal renderings for recycling or disposal shall not be located in the principal recyclable materials or solid waste storage areas. These materials shall be stored in a separate storage area designed for such purpose.
  5. Screening and buffering.
    - a. Enclosures shall include a curbed landscape area at least three feet in width on the sides and rear. Landscaping shall include, at a minimum, a continuous hedge maintained at a height of 36 inches.
    - b. Placement of enclosures adjacent to residentially zoned property and along street frontages is strongly discouraged. They shall be located so as to conceal them from public view to the maximum extent possible.
    - c. All dumpsters and other trash containers shall be completely screened on all four sides with an enclosure that is comprised of a durable material such as masonry with a finish that is architecturally compatible with the project. Chain link fencing, with or without slats, will not be allowed.
  6. Litter receptacles.
    - a. Location. Litter receptacles may not encroach upon the minimum required walkway widths.
    - b. Litter receptacles may not be located within public rights-of-way except as permitted through an agreement with the City in a manner acceptable to the City Attorney or his/her designee.
    - c. Number. The number and location of proposed litter receptacles shall be based on the type and size of the proposed uses. However, at a minimum, for non-residential uses, at least one external litter receptacle shall be provided for every 25 parking spaces for first 100 spaces, plus one receptacle for every additional 100 spaces.

**Response:** There is an existing solid waste and recycling area on-site. The area is shown on the site drawings and will be enclosed within 6' high chain link fencing with privacy slats.

## 55.110 SITE ANALYSIS

The site analysis shall include:

- A. A vicinity map showing the location of the property in relation to adjacent properties, roads, pedestrian and bike ways, transit stops and utility access.
- B. A site analysis on a drawing at a suitable scale (in order of preference, one inch equals 10 feet to one inch equals 30 feet) which shows:
  1. The property boundaries, dimensions, and gross area.
  2. Contour lines at the following minimum intervals:
    - a. Two-foot intervals for slopes from zero to 25 percent; and
    - b. Five- or 10-foot intervals for slopes in excess of 25 percent.

3. Tables and maps identifying acreage, location and type of development constraints due to site characteristics such as slope, drainage and geologic hazards, including a slope analysis which identifies portions of the site according to the land types (I, II, III and IV) defined in Chapter 02 CDC.
4. The location and width of adjoining streets.
5. The drainage patterns and drainage courses on the site and on adjacent lands.
6. Potential natural hazard areas including:
  - a. Floodplain areas pursuant to the site's applicable FEMA Flood Map panel;
  - b. Water resource areas as defined by Chapter 32 CDC;
  - c. Landslide areas designated by the Natural Hazard Mitigation Plan, Map 16; and
  - d. Landslide vulnerable analysis areas, designated by the Natural Hazard Mitigation Plan, Map 17.
7. Resource areas including:
  - a. Wetlands;
  - b. Riparian corridors;
  - c. Streams, including intermittent and ephemeral streams;
  - d. Habitat conservation areas; and
  - e. Large rock outcroppings.
8. Potential historic landmarks and registered archaeological sites. The existence of such sites on the property shall be verified from records maintained by the Community Development Department and other recognized sources.
9. Identification information including the name and address of the owner, developer, project designer, lineal scale and north arrow.
10. Identify Type I and II lands in map form. Provide a table which identifies square footage of Type I and II lands also as percentage of total site square footage.

Certain development proposals required that a Traffic Impact Analysis (TIA) be provided which may result in modifications to the site plan or conditions of approval to address or minimize any adverse impacts created by the proposal. The purpose, applicability and standards of this analysis are found in CDC 85.170(B)(2).

**Response:** The site analysis for this project is embedded in this narrative under various the Sections addressing compliance with CDC standards, requirements and criteria. In addition, the Site Analysis can also be found represented in the various Sheets of the Preliminary Development Plans in Exhibit D

## 55.120 SITE PLAN

The site plan shall be at the same scale as the site analysis (CDC 55.110) and shall show:

- A. The applicant's entire property and the surrounding property to a distance sufficient to determine the relationship between the applicant's property and proposed development and adjacent property and development.
- B. Boundary lines and dimensions for the perimeter of the property and the dimensions for all proposed lot or parcel lines.
- C. Streams and stream corridors.

- D. Identification information, including the name and address of the owner, developer, project designer, lineal scale and north arrow.
- E. The location, dimensions, and names of all existing and proposed streets, public pathways, easements on adjacent properties and on the site, and all associated rights-of-way.
- F. The location, dimensions and setback distances of all:
  - 1. Existing and proposed structures, improvements, and utility facilities on site; and
  - 2. Existing structures and driveways on adjoining properties.
- G. The location and dimensions of:
  - 1. The entrances and exits to the site;
  - 2. The parking and circulation areas;
  - 3. Areas for waste disposal, recycling, loading, and delivery;
  - 4. Pedestrian and bicycle routes, including designated routes, through parking lots and to adjacent rights-of-way;
  - 5. On-site outdoor recreation spaces and common areas;
  - 6. All utilities, including storm water detention and treatment; and
  - 7. Sign locations.
- H. The location of areas to be landscaped. Certain development proposals required that a Traffic Impact Analysis (TIA) be provided which may result in modifications to the site plan or conditions of approval to address or minimize any adverse impacts created by the proposal. The purpose, applicability and standards of this analysis are found in CDC 85.170(B)(2).

**Response:** All of the Site Plan requirements indicated in this Section can be found on the Site Plan on Sheet C1.0 and C1.1 of the Preliminary Development Plans in Exhibit D.

### 55.125 TRANSPORTATION ANALYSIS

Certain development proposals required that a Traffic Impact Analysis (TIA) be provided which may result in modifications to the site plan or conditions of approval to address or minimize any adverse impacts created by the proposal. The purpose, applicability and standards of this analysis are found in CDC 85.170(B)(2). (Ord. 1584, 2008)

**Response:** A transportation analysis in the form of a Traffic Impact Study has been provided in this application package as Exhibit F.

### 55.130 GRADING AND DRAINAGE PLANS

For Type I, II and III lands (refer to definitions in Chapter 02 CDC), a registered civil engineer must prepare a grading plan and a storm detention and treatment plan pursuant to CDC 92.010(E), at a scale sufficient to evaluate all aspects of the proposal, and a statement that demonstrates:

- A. The location and extent to which grading will take place indicating general contour lines, slope ratios, slope stabilization proposals, and location and height of retaining walls, if proposed.
- B. All proposed storm detention and treatment facilities comply with the standards for the improvement of public and private drainage systems located in the West Linn Public Works Design Standards.
- C. There is sufficient factual data to support the conclusions of the plan.

- D. Per CDC 99.035, the Planning Director may require the information in subsections A, B and C of this section for Type IV lands if the information is needed to properly evaluate the proposed site plan.
- E. For Type I, II and III lands (refer to definitions in Chapter 02 CDC), the applicant must provide a geologic report, with text, figures and attachments as needed to meet the industry standard of practice, prepared by a certified engineering geologist and/or a geotechnical professional engineer, that includes:
  - 1. Site characteristics, geologic descriptions and a summary of the site investigation conducted;
  - 2. Assessment of engineering geological conditions and factors;
  - 3. Review of the City of West Linn's Natural Hazard Mitigation Plan and applicability to the site; and
  - 4. Conclusions and recommendations focused on geologic constraints for the proposed land use or development activity, limitations and potential risks of development, recommendations for mitigation approaches and additional work needed at future development stages including further testing and monitoring.
- F. Identification information, including the name and address of the owner, developer, project designer, and the project engineer.

**Response:**

A Preliminary Drainage Report (Exhibit E), Grading Plan (Sheet C2.0 in Exhibit D) and Utility Plan (Sheet C3.0 in Exhibit D) are all included in this application package. The Preliminary Drainage Report (and associated calculations) both convey and outline compliance with the standards, definitions and requirements of CDC Chapter 2 and Section 92.010(E). A conveyance network of underground piping will both treat and convey runoff to public storm mains located at the perimeter of the site and in public-right-of-way. The storm drainage system is completely separate from the sanitary sewer system. Filter strips are proposed to treat new impervious sidewalk along Old River Road frontage and/or a storm water planter treats the runoff from public right-of-way on Old River Road.

All required public facilities currently serve the site, including storm water, water, sanitary sewer and waste/recycling. A new storm water management system is also proposed to serve on-site flow and collection through a surface rain garden and onsite detention system for the new development areas. This new system will discharge onsite drainage to the existing public storm drainage system in Old River Road.

In addition, the City Engineer has indicated that at this location it may be possible to eliminate detention entirely. The applicant is submitting a Stormwater Management Report to the City Engineer to determine the level of downstream capacity and the need for on-site detention. Preliminary analysis on the downstream public system indicates that there is adequate capacity. The applicant requests review of the report by the City Engineer so that the detailed analysis within the revised storm water report will provide the analysis needed to waive detention requirements by the City Engineer.

**55.140 ARCHITECTURAL DRAWINGS**

**This section does not apply to single-family residential subdivisions or partitions, or up to two duplexes or single-family attached dwellings.**

**Architectural drawings shall be submitted showing:**

- A. Building elevations and sections tied to curb elevation;
- B. Building materials: color and type; and
- C. The name of the architect or designer.

**Response:** Architectural drawings are included on Sheets A301 and A302 in the Preliminary Development Plans in Exhibit D. These drawings include elevations, building materials and the name of the architect.

### **55.150 LANDSCAPE PLAN**

This section does not apply to detached single-family residential subdivisions or partitions, or up to two duplexes or single-family attached dwellings.

- A. The landscape plan shall be prepared and shall show the following:**
1. Preliminary underground irrigation system, if proposed;
  2. The location and height of fences and other buffering of screening materials, if proposed;
  3. The location of terraces, decks, patios, shelters, and play areas, if proposed;
  4. The location, size, and species of the existing and proposed plant materials, if proposed; and
  5. Building and pavement outlines.
- B. The landscape plan shall be accompanied by:**
1. The erosion controls that will be used, if necessary;
  2. Planting list; and
  3. Supplemental information as required by the Planning Director or City Arborist.

**Response:** A Landscape Plan is included on Sheets L0.01-L1.03 of the Preliminary Development Plans in Exhibit D. These plans include landscaped areas, outdoor areas, irrigation system, screening information, building outlines and a planting list.

### **55.170 EXCEPTIONS TO UNDERLYING ZONE, YARD, PARKING, SIGN PROVISIONS, AND LANDSCAPING PROVISIONS**

- A. The Planning Director may grant an exception to the dimensional building setback or yard requirements in the applicable zone based on findings that the approval will satisfy the following criteria:**
1. A minor exception that is not greater than 20 percent of the required setback.
  2. A more efficient use of the site.
  3. The preservation of natural features that have been incorporated into the overall design of the project.
  4. No adverse affect to adjoining properties in terms of light, air circulation, noise levels, privacy, and fire hazard.
  5. Safe vehicular and pedestrian access to the site and safe on-site vehicular and pedestrian circulation.

**Response:** The applicant is not requesting adjustments to the underlying zone, yard, parking, sign provisions, nor landscaping provisions.

- B. The Planning Director may grant an exception to the off-street parking dimensional and minimum number of space requirements in the applicable zone so long as the following criteria are met:**
1. The minor exception is not greater than 10 percent of the required parking;
  2. The application is for a use designed for a specific purpose which is intended to be permanent in nature (for example, a nursing home) and which has a low demand for off-street parking; or

3. There is an opportunity for sharing parking and there is written evidence that the property owners are willing to enter into a legal agreement; or
4. Public transportation is available to the site reducing the standards and will not adversely affect adjoining uses, and there is a community interest in the preservation of particular natural feature(s) of the site which make it in the public interest to grant an exception to parking standards.

**Response:** A variance is requested for reduced required minimum number of parking spaces.

### 55.180 MAINTENANCE

**All on-site improvements shall be the ongoing responsibility of the property owner or occupant.**

**Response:** The applicant understands the ongoing responsibility of the property owner or occupant for all on-site improvements.

## CHAPTER 60: CONDITIONAL USES

### 60.030 ADMINISTRATION AND APPROVAL PROCESS

- A. Conditional use applications shall be decided by the Planning Commission in the manner set forth in CDC 99.060(B). A petition for review by the Council may be filed as provided by CDC 99.240(B).

**Response:** The applicant understands that the application shall be decided by the Planning Commission, as set forth in CDC 99.060(B).

- B. All approved conditional use applications in new buildings, or buildings with a major modification, shall be subject to design review under the provisions of Chapter 55 CDC, and in the manner set forth in CDC 99.060(B).

**Response:** This application includes new and existing buildings. The applicant understands that the application for Conditional Use requires and additional application for Class II Design Review. This application package includes both Conditional Use and Design Review applications and associated required material.

- C. All approved conditional use applications within existing buildings shall not be subject to design review.

**Response:** This application includes new and existing buildings. The applicant understands that the application for Conditional Use requires and additional application for Class II Design Review. This application package includes both Conditional Use and Design Review applications and associated required material.

### 60.040 TIME LIMIT ON A CONDITIONAL USE APPROVAL

**Approval of a conditional use that required a design review shall be subject to the time limitations set forth in CDC 55.040. Approval of a conditional use that did not require design review shall be void unless either the use is commenced or an extension is granted per CDC 99.325 within three years of the approval.**

**Response:** The applicant understands that approval of a conditional use that required a design review shall be subject to the time limitations set forth in CDC 55.040., which is 3 years. Further, the applicant understands that an approval of a conditional use and associated design review shall be void unless either the use is commenced or a 2-year extension is granted, per CDC 99.325, within three years of the approval.

## 60.050 BUILDING PERMITS FOR AN APPROVED CONDITIONAL USE

Building permits for all or any portion of a conditional use shall be issued only on the basis of the conditional use plan and conditions as approved by the Planning Commission.

**Response:** The applicant understands that approval of a conditional use by the Planning Commission will include specific approvals and conditions, and that subsequent building permits will be based on those approvals and conditions.

## 60.060 APPLICATION

- A. A conditional use application shall be initiated by the property owner or the owner's authorized agent.
- B. A prerequisite to the filing of an application is a pre-application conference at which time the Director shall explain the requirements and provide the appropriate forms as specified in CDC 99.030(B) and (C).
- C. A prerequisite to the filing of an application is a meeting with the respective City-recognized neighborhood association, per CDC 99.038, at which time the applicant will present his/her proposal and receive comments.
- D. An application for a conditional use shall include the completed application form and:
  - 1. A narrative which addresses the approval criteria set forth in CDC 60.070 and which sustains the applicant's burden of proof;
  - 2. A site plan as provided by CDC 60.080; and
  - 3. If site modification or construction is proposed, a storm detention and treatment plan and narrative pursuant to CDC 92.010(E).

One original application form must be submitted. One copy at the original scale and one copy reduced to 11 inches by 17 inches or smaller of all drawings and plans must be submitted. One copy of all other items must be submitted. The applicant shall also submit one copy of the complete application in a digital format acceptable to the City. When the application submittal is determined to be complete, additional copies may be required as determined by the Community Development Department.

- E. The applicant shall pay the requisite fee.

**Response:** The application form for this application has been signed by the current owner (owner of record) and the application shall be reviewed as a Class II Quasi-Judicial Procedure. A pre-application conference was held on January 18, 2018, per this requirement, and is referred to as PA-18-04. Notes were provided by the City and are included in this application in Exhibit C, Pre-Application Conference Notes. Neighborhood Meeting Materials are also included with this application package in Exhibit H. In addition, a narrative herein is also provided as Exhibit I. The Preliminary Drainage Report has been provided in Exhibit E, with design of storm water facilities depicted in the Utility Plan on Sheet C3.0 in Exhibit D, Preliminary Development Plans. The required sizes and number of copies are all included as part of the application package, including the correct fee amount of \$20,130 made out to the City of West Linn.

## 60.070 APPROVAL STANDARDS AND CONDITIONS

- A. The Planning Commission shall approve, approve with conditions, or deny an application for a conditional use, except for a manufactured home subdivision in which case the approval standards and conditions shall be those specified in CDC 36.030, or to enlarge or alter a conditional use based on findings of fact with respect to each of the following criteria:

1. **The site size and dimensions provide:**
  - a. **Adequate area for the needs of the proposed use; and**
  - b. **Adequate area for aesthetic design treatment to mitigate any possible adverse effect from the use on surrounding properties and uses.**

**Response:** The overall proposed project development area is approximately 38,500 square feet (paving and roof areas) and the overall site size is 64,430 square feet (approximately 1.48 acres). This equates to approximately 59.8% of the overall site needed for the proposed development area and associated school use. The proposed use requires 48 parking spaces and 37 spaces are provided. The parking includes all required landscaping and screening, as well as pedestrian connections and vehicle accessways. Therefore, the size of the site is more than adequate to accommodate the needs of the proposed use.

2. **The characteristics of the site are suitable for the proposed use considering size, shape, location, topography, and natural features.**

**Response:** The overall proposed project development area is approximately 38,500 square feet (paving and roof areas) and the overall site size is 64,430 square feet (approximately 1.48 acres). This equates to approximately 59.8% of the overall site needed for the proposed development area and associated school use. The proposed increase in floor area for Phase I is approximately 24% beyond the existing floor area. The proposed development is located on an existing developed site that is relatively uniform in shape as a square or rectangle and is served by adjacent streets and utilities. The site has been used as an institutional use (church and/or school) in a residential neighborhood since the 1960's. Therefore, the size, shape and location of the site is more than adequate to accommodate the relatively minor impacts of the proposed use. The site slopes from the northwest to the southeast, with some grading or cut/fill required. There are no significant natural features on, or adjacent to, the overall site. No significant amount of additional traffic will be generated from the campus redevelopment, with a limited number of additional trips anticipated. All required parking for the school use is provided on the existing site through the proposed parking lot areas.

3. **The granting of the proposal will produce a facility that provides an overall benefit to the City.**

**Response:** This small neighborhood school will provide many overall benefits to the City. The school will provide a local opportunity for education and community connection, including the use of the facility as a community center for a variety of local events. Short-term economic benefits include salaries for instructors, with long-term benefits including the advantages of education and training of future residents of the City.

4. **Adequate public facilities will be available to provide service to the property at the time of occupancy.**

**Response:** All required public facilities currently serve the site, including storm water, water and sanitary sewer (West Linn Public Works), waste/recycling (West Linn Refuse), electrical (PGE), and Gas (NW Natural). A new storm water management system is also proposed to serve on-site flow and collection through a surface rain garden and onsite underground detention system for the new development areas. This new system will discharge onsite drainage to the existing public storm drainage system in Old River Road. A new 6" fire water service is proposed to provide a complete sprinkler system for both the existing and proposed new buildings. This new service will connect to the existing 8" public water line in Old River Road.. There is an 8-inch sanitary sewer line in Old River Road that will continue to serve the site. There is an existing solid waste and recycling area on-site. This area will temporarily be relocated in Phase I to make room for the proposed temporary portable classroom building. The area is shown on the site drawings and will be enclosed within 6' high chain link fencing with privacy slats. The area will be relocated in Phase II as shown on the site drawings within a similar fencing system with privacy slats.



**5. The applicable requirements of the zone are met, except as modified by this chapter.**

**Response:** All applicable requirements of the underlying R-10 zoning district are met, through both the Conditional Use approval criteria and the associated R-10 standards, which are addressed in this narrative. However, two Variances are requested for loading space requirements and minimum distance between parking and building front entrance. In addition, an Adjustment for minimum parking is also requested in this application. All of these requests are addressed in this narrative.

**6. The supplementary requirements set forth in Chapters 52 to 55 CDC and CDC 92.010(E) are met, if applicable.**

**Response:** The supplementary requirements set forth in the other chapters of this code, including, but not limited to, Chapters 52 to 55 and CDC 92.010(E), are addressed in this narrative.

**7. The use will comply with the applicable policies of the Comprehensive Plan.**

**Policy 4 (Section 1: Air Quality – GOAL 6: Air, Water, and Land Resources Quality**

**Encourage the use of alternative modes of transportation, including mass transit, walking, and bicycling.**

**In the design of the school, the supporting transportation infrastructure, and pathway improvements will facilitate safe and convenient multi-modal access.**

**Response:** The proposal includes an on-site pedestrian network connecting vehicle areas with entryways, bike storage, classrooms, and active open space (playground). A 6-foot asphalt pathway along the front of the school is also proposed, with provision of right-of-way for a future bike lane. There is an existing transit facilities (bus stop) near the intersection of Willamette /Drive and Cedar Oak Drive, approximately .2 miles from the subject property. The proposal also includes a pick-up drop-off area that provides safe and convenient multi-modal access to the entryway.

**Policy 1 (Section 2: Water Quality – GOAL 6: Air, Water, and Land Resources Quality)**

**Require that new development be designed and constructed to prevent degradation of surface and ground water quality by runoff.**

**Appropriate erosion control and water quality measures will be taken to comply with this policy and related regulations. These measures will be reviewed by the city as part of the building permit process.**

**Response:** Appropriate erosion control and water quality measures will be taken to comply with this policy and related regulations. These measures will be reviewed by the city as part of the building permit process. A new 6" fire water service is proposed to provide a complete sprinkler system for both the existing and proposed new buildings. This new service will connect to the existing 8" public water line in Old River Road.

**Policy 4 (Water Quality)**

**Require that new development be connected to the City's sanitary sewer system.**

**Response:** There is an 8-inch sanitary sewer line in Old River Road that will continue to serve the site.

**Policy 2 (Section 4: Noise Control)**

**Require development proposals that are expected to generate noise to incorporate landscaping and other techniques to reduce noise impacts to levels compatible with surrounding land uses.**

**Response:** All proposed landscaping is indicated on the Landscape Plan on Sheets L0.01-L1.03 in Exhibit D, Preliminary Development Plans. This proposed landscaping is partially designed to buffer and screen abutting properties for aesthetic and noise control purposes. In addition, buildings are located to provide a courtyard effect that will also buffer noise from abutting properties.

**Policy 3 (Section 4: Noise Control)**

**Require new commercial, industrial, and public facilities to be designed and landscaped to meet Department of Environmental Quality (DEQ) and City noise standards.**

**Response:** The noise policies will be satisfied because the proposed improvements will not appreciably change use patterns on the site or increase associated noise. The building function, orientation, and capacity will remain essentially as it is today.

**Policy 4 (Section 4: Noise Control)**

**As part of the land use application submittal for a noise-generating use, require the applicant to include a statement from a licensed acoustical engineer, and, if necessary, from DEQ, declaring that all applicable standards can be met.**

**Response:** Noise policies 2, 3, and 4 above will be satisfied because the proposed improvements will not appreciably change use patterns on the site or increase associated noise. Most important, the building function, orientation, and capacity will remain essentially as it is today. The noise policies will be satisfied because the proposed improvements will not appreciably change use patterns on the site or increase associated noise.

**Policy 3 (Section 3: Storm Drainage - GOAL 11: Public Facilities and Services)**

**Protect downstream areas from increased storm water runoff by managing runoff from upstream development and impacts on adjacent natural drainageways and their associated vegetation.**

**Response:** The proposed site work has been designed to meet this policy. The proposed site work will not have any appreciable impact on storm water runoff because the amount of impervious surface will remain virtually the same as it is today. A new storm water management system is proposed to serve on-site flow and collection through a surface rain garden and onsite underground detention system for the new development areas. This new system will discharge onsite drainage to the existing public storm drainage system in Old River Road. The subject site is highly constrained, and although there may technically be room for a detention pond, the size and location (due to easements and offsets) would preclude any functional or significant play area for the students and neighbors. The City's Public Works Code allows for the possibility of underground detention if surface facilities are not practicable.

All required public facilities currently serve the site, including storm water, water, sanitary sewer and waste/recycling. A new storm water management system is also proposed to serve on-site flow and collection through a surface rain garden and onsite detention system for the new development areas. This new system will discharge onsite drainage to the existing public storm drainage system in Old River Road.

In addition, the City Engineer has indicated that at this location it may be possible to eliminate detention entirely. The applicant is submitting a Stormwater Management Report to the City Engineer to determine the level of downstream capacity and the need for on-site detention. Preliminary analysis on the downstream public system indicates that there is adequate capacity. The applicant requests review of the report by the City Engineer so that the detailed analysis within the revised storm water report will provide the analysis needed to waive detention requirements by the City Engineer.

**Policy 1: (Section 7: Schools - GOAL 11: Public Facilities and Services)**

**Encourage the School District to build schools on collectors or arterial streets and, where possible, along transit lines.**

**Response:** As noted in this application, a neighborhood institutional use (public facility) has been in this location for a long time, and it is well-integrated with the neighborhood. Continued access to the school has been provided without undue impacts on the neighborhood. The multi-modal access improvements coupled with building orientation and new landscaping and buffering will enhance both accessibility and compatibility with the surrounding neighborhood.

**Policy 2: (Section 7: Schools - GOAL 11: Public Facilities and Services)**

**Encourage the use of energy-responsive materials and processes in the design of schools where economically feasible.**

**Response:** As noted in the project description, the school will employ energy-saving design features. In addition, the school will be required to meet current building and energy codes, which will result in vastly superior energy and resource conservation compared to the existing building.

**Policy 4: (Section 7: Schools - GOAL 11: Public Facilities and Services)**

**School design, use, and parking will be responsive to and compatible with surrounding neighborhoods and existing land uses.**

**Response:** As noted in this application, an institutional use has been in this location for a long time, and it is well-integrated with the neighborhood. The proposed school will further enhance its relationship with the neighborhood by having a similar intensity of use, public street improvements and multi-modal accessibility, improved building design and orientation and additional landscaping and buffering.

**Policy 4: Bicycles (GOAL 12: Transportation)**

**Require new commercial, industrial, and institutional development to provide on-site facilities for bicycle parking and storage.**

**Response:** The proposed bicycle parking spaces will continue to provide improved parking convenience for cyclists, including a combination of covered and uncovered spaces near the front entrance.

**Policy 1b: Pedestrians (GOAL 12: Transportation)**

**Provide connections to schools, recreation facilities, community centers, and transit facilities.**

**Response:** The public street and on-site walkway system will be enhanced significantly, including new pathway improvements along the frontage and on-site pedestrian network.

**Policy 1c: Pedestrians (GOAL 12: Transportation)**

**Use off-street pedestrian "short-cut" pathways to provide routes where physical constraints or existing development preclude the construction of streets with sidewalks.**

**Response:** An on-site pedestrian network is included as part of site design. In addition, a 6-foot wide asphalt multipurpose path is also proposed along the frontage of the site.

**Policy 1e: Pedestrians (GOAL 12: Transportation)**

**Eliminate gaps in the existing walkway network and provide pedestrian linkages between neighborhoods.**

**Response:** The existing church facility does not have full half-street improvements including sidewalk. In partnership with the City, the Applicant will provide a 6-wide asphalt pedestrian pathway for

the frontage along Old River Road that abuts the school property. These improvements, along with the pathways noted above, will greatly improve the safety and convenience of walking or bicycling to school.

**Policy 2: Pedestrians (GOAL 12: Transportation)**

**Employ a variety of methods to promote safe and convenient pedestrian access in addition to, or instead of, sidewalks in older developed areas of West Linn without sidewalks.**

**Response:** The existing church facility does not have full half-street improvements, including sidewalk. In partnership with the City and as an exception to the cross-section required under the Public Works standards, the Applicant will provide a 6-wide asphalt pedestrian pathway for the frontage along Old River Road that abuts the school property. These improvements, along with the on-site pathways noted above, will greatly improve the safety and convenience of walking or bicycling to school.

**Policy 6: (GOAL 13: Energy Conservation)**

**Encourage the use of energy-conscious design and materials in all public facilities.**

**Response:** As noted in the project description, the building design incorporates methods to reduce energy demand for lighting, heating, and cooling. It also features a roof design that can accommodate future solar energy equipment.

**Policy 7: (GOAL 13: Energy Conservation)**

**Encourage the construction and maintenance of sidewalks and bike paths/ways to promote alternative modes of transportation.**

**Response:** The existing church facility does not have full half-street improvements, including sidewalk. In partnership with the City and as an exception to the cross-section required under the Public Works standards, the Applicant will provide a 6-wide asphalt pedestrian pathway for the frontage along Old River Road that abuts the school property. These improvements, along with the on-site pathways noted above, will greatly improve the safety and convenience of walking or bicycling to school.

Overall, the proposed development of the site complies with the applicable policies of the Comprehensive Plan.

**B. An approved conditional use or enlargement or alteration of an existing conditional use shall be subject to the development review provisions set forth in Chapter 55 CDC.**

**Response:** Chapter 55 is addressed in a separate section of this narrative.

**C. The Planning Commission may impose conditions on its approval of a conditional use which it finds are necessary to assure the use is compatible with other uses in the vicinity. These conditions may include, but are not limited to, the following:**

1. Limiting the hours, days, place, and manner of operation.
2. Requiring design features which minimize environmental impacts such as noise, vibration, air pollution, glare, odor, and dust.
3. Requiring additional setback areas, lot area, or lot depth, or width.
4. Limiting the building height, size or lot coverage, or location on the site.
5. Designating the size, number, location and design of vehicle access points.
6. Requiring street right-of-way to be dedicated and the street to be improved including all steps necessary to address future street improvements identified in the adopted Transportation System Plan.

7. Requiring participation in making the intersection improvement or improvements identified in the Transportation System Plan when a traffic analysis (compiled as an element of a conditional use application for the property) indicates the application should contribute toward.
8. Requiring landscaping, screening, drainage, and surfacing of parking and loading areas.
9. Limiting the number, size, location, height, and lighting of signs.
10. Limiting or setting standards for the location and intensity of outdoor lighting.
11. Requiring berming, screening, or landscaping and the establishment of standards for their installation and maintenance.
12. Requiring and designating the size, height, location, and materials for fences.
13. Requiring the protection and preservation of existing trees, soils, vegetation, watercourses, habitat areas, and drainage areas.

**Response:** The applicant understands that approval of a conditional use by the Planning Commission will include specific approvals and conditions, and that subsequent building permits will be based on those approvals and conditions, including those identified in this Section.

**D. Aggregate extraction uses shall also be subject to the provisions of ORS 541.605.**

**Response:** No aggregate extraction is proposed, therefore, this Section does not apply.

**E. The Historic Review Board shall review an application for a conditional use, or to enlarge a conditional use on a property designated as a historic resource, based on findings of fact that the use will:**

1. Preserve or improve a historic resource which would probably not be preserved or improved otherwise; and
2. Utilize existing structures rather than new structures.

**Response:** The subject property does not include any historic resources, therefore, this Section does not apply.

**60.080 SITE PLAN AND MAP**

**A. All site plans and maps shall include the name, address, and telephone number of the applicant, the scale of the site plan, north arrow, and a vicinity map.**

**B. The applicant shall submit a site plan drawn to an appropriate scale (in order of preference, one inch equals 10 feet to one inch equals 30 feet) which contains the following information:**

1. The subdivision name, block, and lot number or the section, township, range, and tax lot number.
2. The lot or parcel boundaries, dimensions, and gross area.
3. The applicant's property and the surrounding property to a distance sufficient to determine the relationship between the applicant's property and proposed development to the adjacent property and development.
4. The location, dimensions, and names of all existing and platted streets and other public ways and easements on adjacent property and on the site.
5. The location, dimensions, and setback distances of all:
  - a. Existing structures, improvements, utilities, and drainage facilities on adjoining properties;

- b. Existing structures, improvements, utilities, and drainage facilities to remain on the site; and
  - c. Proposed structures or changes to existing structures, improvements, utilities, and drainage facilities.
6. The existing and proposed dimensions of:
- a. The entrances and exits to the site;
  - b. The parking and circulation areas;
  - c. Loading and service areas for waste disposal, loading and delivery;
  - d. Pedestrian and bicycle circulation area;
  - e. On-site outdoor recreation spaces and common areas; and
  - f. Above-ground utilities.
7. The location of areas to be landscaped and the proposed landscape plan.
8. The location of all trees having a six-inch caliper at a height of five feet.
- C. The applicant shall submit the site plan on a map showing two-foot contours up to 20 percent grade and 10-foot contours on grades above 20 percent.

**Response:** All of the Site Plan requirements indicated in this Section can be found on the Site Plans on Sheets C1.0 and C1.1 of the Preliminary Development Plans in Exhibit D.

#### **60.100 ADDITIONAL CRITERIA FOR SCHOOLS AND OTHER GOVERNMENT FACILITIES**

Schools and other government facilities that attract a regular and significant volume of users shall, to the greatest extent possible, be centrally located relative to the majority of the population that they will serve and be serviceable by sidewalks and bike routes/lanes. Police and fire stations shall meet these standards to the greatest extent possible but it is acknowledged that access to arterials remains a key locational determinant for those uses.

**Response:** The proposed school is centrally located based on the clientele and enrollment demographics. Most attendees of the school will come from within 10 miles of the facility. Enrollment is constituted as follows: approximately 1/3 of the families come from West Linn; 1/3 of the families come from Sellwood (Portland), and 1/3 of the families come from Oregon City.

## **CHAPTER 75: VARIANCES AND SPECIAL WAIVERS**

### **75.020 CLASSIFICATION OF VARIANCES**

A. **Class I Variance.** Class I variances provide minor relief from certain code provisions where it can be demonstrated that the modification will not harm adjacent properties, and it conforms with any other code requirements. Class I variances are allowed for the following code provisions:

1. **Required Yard and Minimum Lot Dimensional Requirements.** Required yards may be modified up to 20 percent, lot dimensions by up to 10 percent and lot area by up to five percent if the decision-making authority finds that the resulting approval:
  - a. Provides for a more efficient use of the site;
  - b. Preserves and incorporates natural features into the overall design of the project;
  - c. Does not adversely affect adjoining properties in terms of light, air circulation, noise levels, privacy, and fire hazards; and

- d. Provides for safe vehicular and pedestrian access to the site and safe on-site vehicular and pedestrian circulation.
- 2. Off-street parking dimensional and minimum number of space requirements may be modified up to 10 percent if the decision-making authority finds that the use is designed for a specific purpose, which is intended to be permanent in nature.
- 3. Dimensional sign requirements may be modified up to 10 percent if the decision-making authority finds that the proposed larger sign is:
  - a. Necessary for adequate identification of the use on the property; and
  - b. Compatible with the overall site plan, the structural improvements, and with the structures and uses on adjoining properties.
- 4. Landscaping requirements in the applicable zone may be modified up to 10 percent if the decision-making authority finds that the resulting approval:
  - a. Provides for a more efficient use of the site;
  - b. Preserves and incorporates natural features into the overall design of the project; and
  - c. Will have no adverse effect on adjoining property.

**Response:** A Class I Variance is being requested for landscaping requirements by 10%. The applicable criteria is addressed below.

- 4. Landscaping requirements in the applicable zone may be modified up to 10 percent if the decision-making authority finds that the resulting approval:
  - c. Provides for a more efficient use of the site;
  - d. Preserves and incorporates natural features into the overall design of the project; and
  - c. Will have no adverse effect on adjoining property.

**Response:** The minimum required landscaping between the parking area and the street is 10 feet. The proposed landscape area is 9 feet. The proposed difference is 10%. This allowance of an extra foot allows additional room for site development, including driveway aisles and parking spaces. The overall proposal preserves and incorporates natural features on the site, including trees. The reduction of one foot of landscape area will be imperceptible, therefore, there will no adverse effect on adjoining properties.

**B. Class II Variance. Class II variances may be utilized when strict application of code requirements would be inconsistent with the general purpose of the CDC and would create a burden upon a property owner with no corresponding public benefit. A Class II variance will involve a significant change from the code requirements and may create adverse impacts on adjacent property or occupants. It includes any variance that is not classified as a Class I variance or special waiver.**

**Response:** Two Variances are being requested and they do not qualify under the Class I Variance criteria listed in 75.020.A.1-4, therefore, this Section does apply. The two requested Variances will be considered Class II, and are for the following standards:

- 1) CDC 46.090, Minimum Off-Street Parking Space Requirements; and
- 2) CDC 46.130, Loading Bay Requirements

Each of these Variances is addressed separately, below.

**CDC 46.090, Minimum Off-Street Parking Space Requirements**

**1. Class II Variance Approval Criteria. The approval authority may impose appropriate conditions to ensure compliance with the criteria. The appropriate approval**

**authority shall approve a variance request if all the following criteria are met and corresponding findings of fact prepared.**

- a. The variance is the minimum variance necessary to make reasonable use of the property. To make this determination, the following factors may be considered, together with any other relevant facts or circumstances:**
  - 1) Whether the development is similar in size, intensity and type to developments on other properties in the City that have the same zoning designation.**
  - 2) Physical characteristics of the property such as lot size or shape, topography, or the existence of natural resources.**
  - 3) The potential for economic development of the subject property.**

**Response:**

The minimum required parking spaces is 48, and the proposed number of parking spaces is 37. This is indicated in detail in the parking analysis on Sheets C1.0 and C1.1. The 37 spaces (versus 48) is the minimum variance necessary to make reasonable use of the property.

The lot size is just large enough to accommodate the school program, vehicular and pedestrian areas, storm water treatment and detention, tree preservation and landscape requirements, pedestrian network and outdoor learning and play areas. The steep topography of the site also significantly restricts the addition of additional parking. The proposed development is similar in size, intensity and type to developments on other properties in the City with the same zoning designation. The application is for redevelopment of an existing public facility use. Existing parking at the site currently meets CDC requirements for Phase II. However, the City of West Linn requirement for a pick-up drop-off area of students reduces the number of parking spaces. The attached memo from Lancaster Engineering (Exhibit K) addresses city required vehicular circulation improvements that reduce available parking but will increase site safety, queuing, and both vehicular and pedestrian circulation.

- b. The variance will not result in violation(s) of any other code standard, and the variance will meet the purposes of the regulation being modified.**

**Response:**

The proposed reduction in minimum parking does not result in violation of other code standards. Applicable Code standards, including specific standards in Chapter 46, are being met. Compliance with these standards are addressed in this narrative and are depicted visually in the plan set. The application eliminates some existing stalls in the interest of providing greater pick-up and drop-off area for parents. The extended pick-up/drop-off area reduces the need for greater parking and provides a safer circulation for pedestrians and vehicles.

- c. The need for the variance was not created by the applicant and/or owner requesting the variance.**

**Response:**

The need for the variance was generated by the City as a response to requirements for student drop off and pick up circulation. The vehicular circulation required by the City eliminates 12 parking stalls that previously counted towards addressing the minimum number of spaces required. In addition, the lot size of the subject site is relatively constrained when considering building size needs, preservation of trees, a necessary pedestrian network and required parking areas. Finally, a requirement for minimum parking space distances would diminish the ability to provide all other elements required of the development, thereby jeopardizing the project feasibility.

- d. If more than one variance is requested, the cumulative effect of the variances results in a project that is consistent with the overall purpose of the zone.**



**Response:** Two variances are being requested, however, the cumulative effect of the variances is still consistent with the Conditional Use in an R-10 zone. The purpose of the zone is to allow primarily residential use and development, with other uses allowed through a Conditional Use. If the Conditional Use is approved and associated standards are met, then it can be deduced that the effect of the variances is consistent with the overall purpose of the R-10 zone, which allows for schools as a Conditional Use.

**CDC 46.130. Loading Bay Requirements**

**1. Class II Variance Approval Criteria. The approval authority may impose appropriate conditions to ensure compliance with the criteria. The appropriate approval authority shall approve a variance request if all the following criteria are met and corresponding findings of fact prepared.**

**a. The variance is the minimum variance necessary to make reasonable use of the property. To make this determination, the following factors may be considered, together with any other relevant facts or circumstances:**

- 1) Whether the development is similar in size, intensity and type to developments on other properties in the City that have the same zoning designation.**
- 2) Physical characteristics of the property such as lot size or shape, topography, or the existence of natural resources.**
- 3) The potential for economic development of the subject property.**

**Response:** The variance being requested is the minimum needed to negate the requirement for a loading bay that is not functionally necessary for the proposed use. The existing church does not have a loading bay and was never necessary. This is also true for the proposed use. The standard in CDC under 46.130 states that for a building over 10,000 square feet, a loading space with minimum dimensions of 14 feet wide and 20 feet long is required. Nearly all of the deliveries required for the operation of the school can be accomplished by a large van that could fit in one of the standard parking lot spaces on a temporary basis. Due to site size and requirements for certain building square footage to accommodate minimal enrollment feasibility, it is not practicable to include a truck loading space as part of the development.

The site size is similar to other commercial properties along Willamette Drive that do not have designated loading spaces. Again, the lot size of the subject site is relatively constrained when considering building size needs, preservation of trees, a necessary pedestrian network and required parking areas. Finally, a requirement for truck loading would diminish the ability to provide all other elements required of the development, thereby jeopardizing the project feasibility.

**b. The variance will not result in violation(s) of any other code standard, and the variance will meet the purposes of the regulation being modified.**

**Response:** The variance being requested does not result in violation of any other applicable Code standard, as all other applicable Code standards are addressed in this narrative indicating compliance. The purpose of this regulation is to ensure that a designated space is provided for normal on-site loading. The proposed parking spaces can provide the adequate space needed for temporary on-site loading.

**c. The need for the variance was not created by the applicant and/or owner requesting the variance.**

**Response:** The variance being requested is not necessarily resultant of any action by the owner. The normal functioning of the school simply does not require a relatively large loading space for normal operation.

- d. **If more than one variance is requested, the cumulative effect of the variances results in a project that is consistent with the overall purpose of the zone.**

**Response:** Two variances are being requested, however, the cumulative effect of the variances is still consistent with the Conditional Use in an R-10 zone. The purpose of the zone is to allow primarily residential use and development, with other uses allowed through a Conditional Use. If the Conditional Use is approved and associated standards are met, then it can be deduced that the effect of the variances is consistent with the overall purpose of the R-10 zone, which allows for schools as a Conditional Use.

- C. **Special Waivers. Special waivers are only applicable in mixed use and non-residential zoning districts. Special waivers may be granted by the approval authority when it can be shown that the proposed site design provides a superior means of furthering the intent and purpose of the regulation to be waived. A special waiver involves a waiver of a standard to permit a specific proposed development. It does not require demonstration of a hardship. It is a request to modify specific requirements in order to provide a superior site design that would not otherwise be possible under the standard requirements of the code.**

**Response:** No Special Waiver is being requested, therefore, this Section does not apply.

#### **75.030 ADMINISTRATION AND APPROVAL PROCESS**

- A. **Class I variances shall be decided by the Planning Director in the manner set forth in CDC 99.060(A). An appeal may be taken as provided by CDC 99.240(A).**
- B. **Class II variances and special waivers shall be decided by the Planning Commission in the manner set forth in CDC 99.060(B). A petition for review by the Council may be filed as provided by CDC 99.240(B).**

**Response:** Both requested Class II Variances are included with this application package and will be reviewed by the Planning Commission as part of this Quasi-Judicial Procedure, per CDC 99.060(A).

#### **75.040 TIME LIMIT ON A VARIANCE AND SPECIAL WAIVERS**

**Approval of a variance or special waiver shall be void after three years unless substantial construction has taken place or an extension is granted per Chapter 99 CDC.**

**Response:** The applicant understands that approval of the variances shall be subject to the time limitations set forth in CDC 55.040., which is 3 years. Further, the applicant understands that an approval of the variances shall be void unless either the use is commenced or a 2-year extension is granted, per CDC 99.325, within three years of the approval.

#### **75.050 APPLICATION**

- A. **A variance request shall be initiated by the property owner or the owner's authorized agent.**
- B. **A prerequisite to the filing of an application for a Class II variance or special waiver is a pre-application conference at which time the Planning Director shall explain the requirements and provide the appropriate form(s).**
- C. **An application for a variance shall include the completed application form and:**
  - 1. **A narrative which addresses the approval criteria set forth in CDC 75.020, and which sustains the applicant's burden of proof.**
  - 2. **A site plan as provided by CDC 75.060.**

**One original application form must be submitted. One copy at the original scale and one copy reduced to 11 inches by 17 inches or smaller of all drawings and plans must**

be submitted. One copy of all other items must be submitted. The applicant shall also submit one copy of the complete application in a digital format acceptable to the City. When the application submittal is determined to be complete, additional copies may be required as determined by the Community Development Department.

- D. Requests for more than one Class II variance for the same lot or parcel shall be consolidated in one application and reviewed concurrently by the City.
- E. Not more than two Class II variances may be approved for any one lot or parcel in a continuous 12-month period.
- F. The applicant shall pay the requisite fee.

**Response:** The application form for this application has been signed by the current owner (owner of record) and the application shall be reviewed as a Class II Quasi-Judicial Procedure. A pre-application conference was held on January 18, 2018, per this requirement, and is referred to as PA-18-04. Notes were provided by the City and are included in this application in Exhibit C, Pre-Application Conference Notes. In addition, a narrative herein is also provided as Exhibit I. The Site Plan on Sheets C1.0 and C1.1 in Exhibit D, Preliminary Development Plans, is also included. The required sizes and number of copies are all included as part of the application package, including the correct fee amount of \$20,130 made out to the City of West Linn.

## 75.060 SITE PLANS AND MAP

- A. All plot plans and maps shall include the name, address, and telephone number of the applicant; the scale; north arrow; and a vicinity map.
- B. The applicant shall submit a plot plan drawn to an appropriate scale (in order of preference: one inch equals 10 feet to one inch equals 30 feet) which shows the following:
  - 1. The subdivision name, block, and lot number or the section, township, range, and tax lot number.
  - 2. In the case of a request for a variance to a lot or parcel dimensional or building setback requirement:
    - a. The lot or parcel configuration and dimensions, and the location of all existing structures; the setback distances and the location of all structures on abutting units of land, and the setback distances; and
    - b. The proposed variances.
  - 3. In the case of a request for a variance to the building height provisions:
    - a. An elevation drawing of the structure and the proposed variances; and
    - b. A drawing(s) to scale showing the impact on adjoining properties; for example, will the height variance, if granted, block a viewpoint from an adjoining property of a significant land feature.

**Response:** All of the Site Plan requirements indicated in this Section can be found on the Site Plans on Sheet C1.0 and C1.1 of the Preliminary Development Plans in Exhibit D.

## CHAPTER 91 IMPROVEMENT GUARANTEE

### 91.010 IMPROVEMENTS

- A. Before approval by the Planning Director and the City Engineer of a final subdivision, partition plat, building permit, or construction plans (other than plans for required improvements), the developer shall:

1. Install required improvements and repair existing streets and other public facilities damaged in the development of the property; or
2. The developer shall also provide reimbursement to the City for costs of processing inspection, professional services, etc., of said required improvements by the City. Monthly costs of the City shall be billed against the six percent of construction cost deposit made by the developer to the City prior to construction of required improvements. The developer shall ensure that the deposit balance remains positive. If the developer is notified that the balance is negative, the developer has seven calendar days to correct the overage and provide additional deposit as specified by the City Engineer. Failure of the developer to correct the situation by that date will result in the issuance of a stop work order by the City which shall remain in force until said fees are paid in full and additional deposit provided.

**Response:** The applicant (developer) will install all required improvements, per the decision and associated conditions of approval.

- B. The City shall install all street name signs and traffic control devices for the initial signing of a new development, with said costs to be reimbursed by the developer.**

**Response:** It is not anticipated that either street signs or traffic control devices will be required as part of this proposal and application.

- C. Upon written acceptance by the City of required improvements, the developer shall execute a maintenance bond with a surety company authorized to transact business in the State; such bond to be in a form approved by the City Attorney. The maintenance bond shall guarantee satisfactory performance required and installed improvements included in the subdivision or partition for a maximum period of 18 months from the date of written approval/acceptance by the City of said improvements. The amount of said maintenance bond shall be in an amount equivalent to 20 percent of the total installation cost of required improvements. The maintenance bond shall also provide financial guarantee for any damage caused to said improvement during the period of the maintenance bond.**

**Response:** This application does not include a partition or subdivision, therefore, this Section is not applicable.

- D. Until such time as all required improvements within the subdivision or partition have been accepted by the City, the developer shall be solely responsible for the cleanup of debris, dirt, and foreign materials derived from this development or project upon sidewalks and roadways. To guarantee performance of this responsibility, the developer shall provide a cash deposit in the amount of five percent of the total installation of the improvements. The developer shall be responsible for all safety and cleaning all debris, dirt, and foreign material derived from his or her development or project by 5:00 p.m. of each workday; except that if said debris, dirt, or foreign material is found by the City Engineer to constitute an immediate traffic or safety hazard, it shall be immediately removed by the developer. The developer shall furnish the City with information as to where the developer or a designated subordinate may be reached at all times by the City regarding the performance of such cleanup work. Failure of the developer to clean up debris, dirt, or foreign material as hereinabove stated shall give the City the right to clean up said debris, dirt, or foreign material utilizing City crews, or to hire an independent contractor to do the same, and deduct same costs from the five percent cash deposit. The City shall bill the developer for all such cleanup services at the rate of twice the actual City labor costs incurred plus 35 percent of such actual labor costs reflecting utilization of City equipment. In the event that the City hires a private contractor to perform these services, the City shall bill the developer the actual cost incurred by the private contractor plus 50 percent of said actual costs reflecting the administrative costs incurred. The deposit shall be kept in a positive balance within the same criteria as the deposit noted in subsection (A)(2) of this section with the same ramifications for failure.**

**Response:** This application does not include a partition or subdivision, therefore, this Section is not applicable.

- E. Before the City accepts any required improvements within a subdivision or major partition and releases the performance bond, the developer shall furnish to the City certification of a registered civil engineer that said improvements have been installed and meet all applicable City, State, and federal requirements.**

**Response:** This application does not include a partition or subdivision, therefore, this Section is not applicable.

## **CHAPTER 96: STREET IMPROVEMENT CONSTRUCTION**

### **96.010 CONSTRUCTION REQUIRED**

#### **A. New construction.**

- 1. Building permits shall not be issued for the construction of any new building or structure, or for the remodeling of any existing building or structure, which results in an increase in size or includes a change in use, including building permits for single-family dwellings but excepting building permits for alteration or addition to an existing single-family dwelling, unless the applicant for said building permit agrees to construct street improvements as required by the land use decision authorizing the construction activity. The placement of new curbs and the drainage facilities required shall be determined by the City Manager or the Manager's designee.**

**Response:** The applicant (developer) will install all required improvements, per the decision and associated conditions of approval, as part of Phase I of the development.

- 3. An applicant for a building permit may apply for a waiver of street improvements and the option to make a payment in lieu of construction. The option is available if the City Manager or the Manager's designee determines the transportation system plan does not include the street improvement for which the waiver is requested.**

**Response:** The applicant (developer) will install all required improvements, per the decision and associated conditions of approval, as part of Phase I of the development.

- 4. When an applicant applies for and is granted a waiver of street improvements under subsection (A)(3) of this section, the applicant shall pay an in-lieu fee equal to the estimated cost, accepted by the City Engineer, of the otherwise required street improvements. As a basis for this determination, the City Engineer shall consider the cost of similar improvements in recent development projects and may require up to three estimates from the applicant. The in-lieu fee shall be used for in kind or related improvements.**

**Response:** The applicant (developer) is not requesting a waiver of street improvements, nor exercising the option to make a payment in lieu of construction. The applicant (developer) will install all required improvements, per the decision and associated conditions of approval, as part of Phase I of the development.

### **96.020 STANDARDS**

**Street improvements shall be installed according to the City standards and shall be completed prior to the issuance of any occupancy permit for the new or remodeled structure or building. In unimproved areas of the City, the City Engineer may grant a time extension of the provisions of this section; provided, that the applicant provides sufficient security in amount and quantity satisfactory to the City Attorney to assure payment of such improvement costs.**

**Response:** The applicant (developer) will install all required improvements, per the decision and associated conditions of approval, as part of Phase I of the development.

### **III. CONCLUSION**

Based upon the findings of this narrative and the submitted exhibits, the applicant has demonstrated compliance with relevant sections of the West Linn Code. Therefore, the applicant requests that this submitted application be approved.



# Technical Memorandum



**LANCASTER  
ENGINEERING**

**To:** City of West Linn  
**From:** Jessica Hajar  
**Date:** September 7, 2018  
**Subject:** Marylhurst School Parking Variance Memorandum

321 SW 4th Ave., Suite 400  
Portland, OR 97204  
phone: 503.248.0313  
fax: 503.248.9251  
lancasterengineering.com

This document is written to supplement the Marylhurst School's Variance for a Reduction in Required Parking. The Marylhurst School is a private school (K-8) that is proposed at 19915 Old River Drive in West Linn, Oregon. The City of West Linn has required a new designated drop-off and pick-up area within the project site that will result in the elimination of 12 parking stalls. Based on the revised site plan shown in Figure 1, the site will provide a total of 37 parking spaces. A summary of the required parking stalls for the site is shown in Figure 2 on page 2.

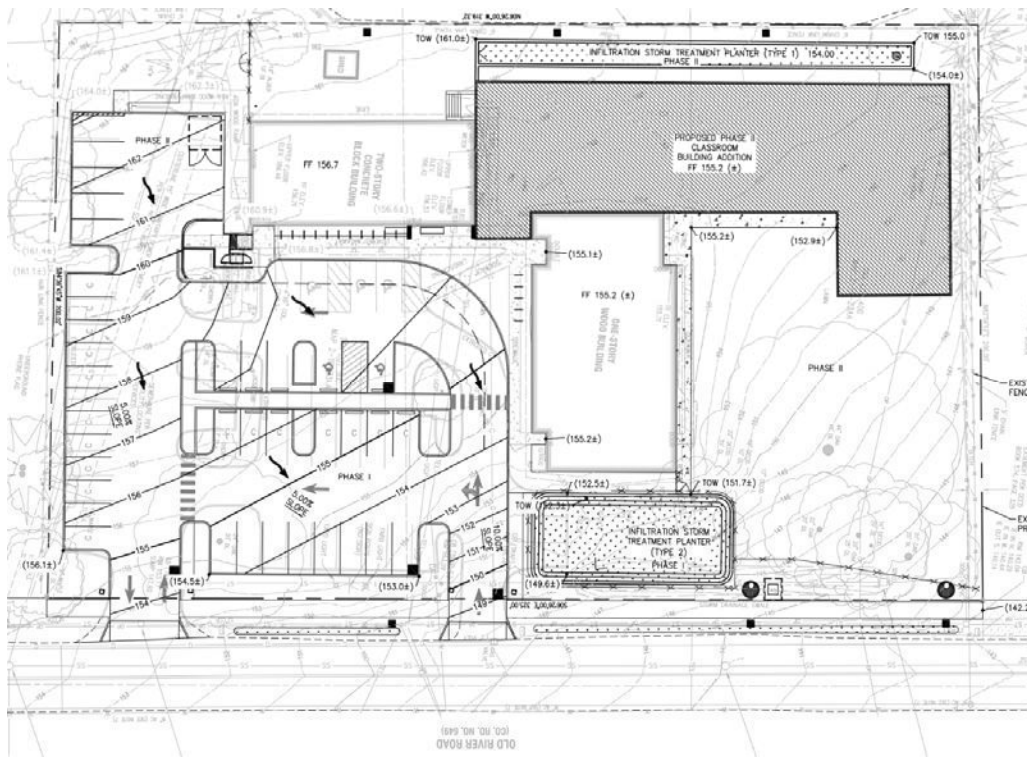


Figure 1 - Site Plan





| BUILDING                             | PROPOSED USE            | AREA      | PARKING REQ.         | NO. STALLS REQUIRED |
|--------------------------------------|-------------------------|-----------|----------------------|---------------------|
| EXISTING CHURCH ANNEX BUILDING       | MIDDLE SCHOOL CLASSROOM | 4,000 SF  | 1 SPACE PER 1,000 SF | 4                   |
| EXISTING MAIN CHURCH BUILDING        | MULTI-USE COMMONS AREA  | 4,500 SF  | 1 SPACE PER 1,000 SF | 5                   |
| 25% PROPOSED NEW ADDITION            | KINDERGARTEN & PRE-K    | 3,300 SF  | 1 SPACE PER 300 SF   | 11                  |
| 75% PROPOSED NEW ADDITION            | PRIMARY SCHOOL          | 13,000 SF | 1 SPACE PER 1,000 SF | 13                  |
| STAFF                                | 15<br>(AN ANY ONE TIME) |           |                      | 15                  |
| <b>TOTAL PARKING STALLS REQUIRED</b> |                         |           |                      | <b>48</b>           |
| <b>TOTAL PARKING STALLS PROPOSED</b> |                         |           |                      | <b>37</b>           |

Figure 2 - Required Parking Stalls

### Queuing Analysis

Queues were examined for the site access under Year 2021 Background plus Phase Two scenarios. The queue lengths were calculated using a Synchro/SimTraffic simulation, with the reported values based on the 95<sup>th</sup> percentile queue lengths. This means that during the peak hour, 95 percent of the time the queue lengths will be less than or equal to the reported values.

Table 1 - Queuing Analysis Summary

| Intersection                           | Movement | AM Queue Length | AFTN Queue Length | PM Queue Length |
|----------------------------------------|----------|-----------------|-------------------|-----------------|
| Old Rive Road at Southern Site Access  | EBLR     | 53 feet         | 45 feet           | 43 feet         |
| Old River Road at Northern Site Access | NBLT     | 15 feet         | 14 feet           | 14 feet         |

The longest queue length occurs for the eastbound approach, and therefore the queuing will occur within the site, and not interfere with traffic circulation on Old River Road. The northern site access has a northbound 95<sup>th</sup> percentile queue length of approximately one vehicle during all analysis scenarios.



It is typical for school facilities to experience the afternoon peak period (pick-up) during a peak of about 15 minutes. However, the Marylhurst School has several different academic programs which begin and end at varying times throughout the day. Due to the unique schedule at Marylhurst School, it is expected that these peaks will occur at staggered times throughout the day. Therefore, it is not expected that queue lengths will exceed the area provided on-site or degrade the traffic flow on Old River Drive. Detailed queuing analysis worksheets are provided in the appendix.

### *Safety*

The reduction in parking stalls will increase the curb area of the drop-off and pick-up area. This will increase the efficiency of the drop-off/pick-up process, allowing more students to enter/exit the vehicles at one time, while also reducing the amount of on-foot circulation that will occur within the parking lot and possible conflicts between vehicles and crossing students.

### *Conclusions*

The proposed revision to the site plan is expected to enhance the functionality and safety of the on-site circulation of the school. Due to the physical characteristics of the site, there is not available area to relocate the stalls within the site. Additionally, the variance is required as a result of a city requirement, and therefore falls under Approval Criteria 75.020.B.1.c for Class II Variances.



September 7, 2018  
Page 4 of 4

***Appendix***

Queuing and Blocking Report  
 Background 2021 + Phase 2 AM Peak Hour

08/13/2018

Intersection: 1: Willamette Drive & Cedar Oak Drive

| Movement              | WB  | WB  | SE  | SE  | NW  |
|-----------------------|-----|-----|-----|-----|-----|
| Directions Served     | L   | R   | L   | T   | TR  |
| Maximum Queue (ft)    | 153 | 74  | 92  | 118 | 720 |
| Average Queue (ft)    | 70  | 17  | 32  | 44  | 612 |
| 95th Queue (ft)       | 131 | 50  | 73  | 95  | 870 |
| Link Distance (ft)    |     | 390 |     | 541 | 666 |
| Upstream Blk Time (%) |     |     |     |     | 32  |
| Queuing Penalty (veh) |     |     |     |     | 0   |
| Storage Bay Dist (ft) | 170 |     | 110 |     |     |
| Storage Blk Time (%)  | 0   |     | 0   | 0   |     |
| Queuing Penalty (veh) | 0   |     | 1   | 0   |     |

Intersection: 2: Old River Road & Cedar Oak Drive

| Movement              | EB  | WB  | NB  | SB  |
|-----------------------|-----|-----|-----|-----|
| Directions Served     | LTR | LTR | LTR | LTR |
| Maximum Queue (ft)    | 37  | 4   | 60  | 70  |
| Average Queue (ft)    | 3   | 0   | 21  | 34  |
| 95th Queue (ft)       | 19  | 3   | 50  | 55  |
| Link Distance (ft)    | 390 | 573 | 293 | 213 |
| Upstream Blk Time (%) |     |     |     |     |
| Queuing Penalty (veh) |     |     |     |     |
| Storage Bay Dist (ft) |     |     |     |     |
| Storage Blk Time (%)  |     |     |     |     |
| Queuing Penalty (veh) |     |     |     |     |

Intersection: 3: Old River Road & Southern Site Access

| Movement              | EB  | NB  |
|-----------------------|-----|-----|
| Directions Served     | LR  | LT  |
| Maximum Queue (ft)    | 61  | 5   |
| Average Queue (ft)    | 33  | 0   |
| 95th Queue (ft)       | 53  | 4   |
| Link Distance (ft)    | 161 | 213 |
| Upstream Blk Time (%) |     |     |
| Queuing Penalty (veh) |     |     |
| Storage Bay Dist (ft) |     |     |
| Storage Blk Time (%)  |     |     |
| Queuing Penalty (veh) |     |     |

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Intersection: 4: Old River Road & Northern Site Access

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|                       |     |
|-----------------------|-----|
| Movement              | NB  |
| Directions Served     | LT  |
| Maximum Queue (ft)    | 24  |
| Average Queue (ft)    | 2   |
| 95th Queue (ft)       | 15  |
| Link Distance (ft)    | 185 |
| Upstream Blk Time (%) |     |
| Queuing Penalty (veh) |     |
| Storage Bay Dist (ft) |     |
| Storage Blk Time (%)  |     |
| Queuing Penalty (veh) |     |

---

Network Summary

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Network wide Queuing Penalty: 1

Queuing and Blocking Report  
 Background 2021 + Phase 2 AFTN Peak Hour

09/07/2018

Intersection: 1: Willamette Drive & Cedar Oak Drive

| Movement              | WB  | WB  | SE  | SE  | NW  |
|-----------------------|-----|-----|-----|-----|-----|
| Directions Served     | L   | R   | L   | T   | TR  |
| Maximum Queue (ft)    | 123 | 160 | 154 | 541 | 453 |
| Average Queue (ft)    | 78  | 29  | 30  | 227 | 191 |
| 95th Queue (ft)       | 131 | 106 | 94  | 480 | 367 |
| Link Distance (ft)    |     | 390 |     | 541 | 666 |
| Upstream Blk Time (%) |     |     |     | 3   | 0   |
| Queuing Penalty (veh) |     |     |     | 0   | 0   |
| Storage Bay Dist (ft) | 100 |     | 150 |     |     |
| Storage Blk Time (%)  | 9   | 0   |     | 9   |     |
| Queuing Penalty (veh) | 5   | 0   |     | 3   |     |

Intersection: 2: Old River Road & Cedar Oak Drive

| Movement              | EB  | WB  | NB  | SB  |
|-----------------------|-----|-----|-----|-----|
| Directions Served     | LTR | LTR | LTR | LTR |
| Maximum Queue (ft)    | 37  | 4   | 40  | 46  |
| Average Queue (ft)    | 4   | 0   | 15  | 30  |
| 95th Queue (ft)       | 21  | 3   | 40  | 44  |
| Link Distance (ft)    | 390 | 573 | 293 | 213 |
| Upstream Blk Time (%) |     |     |     |     |
| Queuing Penalty (veh) |     |     |     |     |
| Storage Bay Dist (ft) |     |     |     |     |
| Storage Blk Time (%)  |     |     |     |     |
| Queuing Penalty (veh) |     |     |     |     |

Intersection: 3: Old River Road & Southern Site Access

| Movement              | EB  |
|-----------------------|-----|
| Directions Served     | LR  |
| Maximum Queue (ft)    | 52  |
| Average Queue (ft)    | 28  |
| 95th Queue (ft)       | 45  |
| Link Distance (ft)    | 189 |
| Upstream Blk Time (%) |     |
| Queuing Penalty (veh) |     |
| Storage Bay Dist (ft) |     |
| Storage Blk Time (%)  |     |
| Queuing Penalty (veh) |     |

---

Intersection: 4: Old River Road & Northern Site Access

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|                       |     |
|-----------------------|-----|
| Movement              | NB  |
| Directions Served     | LT  |
| Maximum Queue (ft)    | 16  |
| Average Queue (ft)    | 2   |
| 95th Queue (ft)       | 14  |
| Link Distance (ft)    | 156 |
| Upstream Blk Time (%) |     |
| Queuing Penalty (veh) |     |
| Storage Bay Dist (ft) |     |
| Storage Blk Time (%)  |     |
| Queuing Penalty (veh) |     |

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Network Summary

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Network wide Queuing Penalty: 8

Queuing and Blocking Report  
Background 2021 + Phase 2 PM Peak Hour

09/07/2018

Intersection: 1: Willamette Drive & Cedar Oak Drive

| Movement              | WB  | WB  | SE  | SE  | NW  |
|-----------------------|-----|-----|-----|-----|-----|
| Directions Served     | L   | R   | L   | T   | TR  |
| Maximum Queue (ft)    | 118 | 17  | 113 | 480 | 314 |
| Average Queue (ft)    | 63  | 2   | 29  | 203 | 133 |
| 95th Queue (ft)       | 110 | 10  | 85  | 399 | 249 |
| Link Distance (ft)    |     | 390 |     | 541 | 666 |
| Upstream Blk Time (%) |     |     |     | 1   |     |
| Queuing Penalty (veh) |     |     |     | 0   |     |
| Storage Bay Dist (ft) | 100 |     | 150 |     |     |
| Storage Blk Time (%)  | 3   |     |     | 8   |     |
| Queuing Penalty (veh) | 1   |     |     | 3   |     |

Intersection: 2: Old River Road & Cedar Oak Drive

| Movement              | EB  | NB  | SB  |
|-----------------------|-----|-----|-----|
| Directions Served     | LTR | LTR | LTR |
| Maximum Queue (ft)    | 22  | 42  | 50  |
| Average Queue (ft)    | 1   | 15  | 27  |
| 95th Queue (ft)       | 10  | 41  | 46  |
| Link Distance (ft)    | 390 | 293 | 203 |
| Upstream Blk Time (%) |     |     |     |
| Queuing Penalty (veh) |     |     |     |
| Storage Bay Dist (ft) |     |     |     |
| Storage Blk Time (%)  |     |     |     |
| Queuing Penalty (veh) |     |     |     |

Intersection: 3: Old River Road & Southern Site Access

| Movement              | EB  |
|-----------------------|-----|
| Directions Served     | LR  |
| Maximum Queue (ft)    | 55  |
| Average Queue (ft)    | 16  |
| 95th Queue (ft)       | 43  |
| Link Distance (ft)    | 244 |
| Upstream Blk Time (%) |     |
| Queuing Penalty (veh) |     |
| Storage Bay Dist (ft) |     |
| Storage Blk Time (%)  |     |
| Queuing Penalty (veh) |     |



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Intersection: 4: Old River Road & Nothern Site Access

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|                       |     |
|-----------------------|-----|
| Movement              | NB  |
| Directions Served     | LT  |
| Maximum Queue (ft)    | 31  |
| Average Queue (ft)    | 2   |
| 95th Queue (ft)       | 14  |
| Link Distance (ft)    | 103 |
| Upstream Blk Time (%) |     |
| Queuing Penalty (veh) |     |
| Storage Bay Dist (ft) |     |
| Storage Blk Time (%)  |     |
| Queuing Penalty (veh) |     |

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Network Summary

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Network wide Queuing Penalty: 4

# The Marylhurst School

|               |               |               | Monday   |        | Tuesday  |        | Wednesday |        | Thursday |        | Friday   |        |
|---------------|---------------|---------------|----------|--------|----------|--------|-----------|--------|----------|--------|----------|--------|
| # of Students | Grades        |               | Drop Off | Pickup | Drop Off | Pickup | Drop Off  | Pickup | Drop Off | Pickup | Drop Off | Pickup |
| 15            | Middle School | Drive Through | 8:30     | 3:15   | 8:30     | 3:15   | 8:30      | 3:15   | 8:30     | 3:15   | 8:30     | 3:15   |
| 72            | Primary       | Drive Through | 8:30     | 2:30   | 8:30     | 2:30   | 8:30      | 2:30   | 8:30     | 2:30   | 8:30     | 2:30   |
| 18            | Threes/Fours  | Park & Drop   | 9:00     | 1:00   | 9:00     | 1:00   | 9:00      | 1:00   | 9:00     | 1:00   | No Class |        |
| Full-Time     | Faculty       | 10            |          |        |          |        |           |        |          |        |          |        |

\*Incentives for teachers who take public transportation

\*\*Some families carpool

\*\*\*On average 20 Extended care children stay later; get picked up between 2:30 - 5:00

\*\*\*On average 10 Extended care children arrive early; get dropped off between 7:30 - 8:00

Variables:

- Our teachers currently park in a park and ride across the street from our current location. There is a park and ride on Cedar Oak where teachers could potentially park.
- 36 of our families have multiple children that go to this school so they would arrive in the same car.
- There are 11 students who have parents who teach at our school. They would be included in the faculty parking rather than parent drop off.

|           | Morning:  | 8:00-8:30 | 8:30-9:00 | 9:00-9:30 | 9:30-10:00 | 10:00-10:30 | 10:30-11:00 | 11:00-11:30 | 11:30-12:00 |
|-----------|-----------|-----------|-----------|-----------|------------|-------------|-------------|-------------|-------------|
| Staff: 14 | Monday    |           |           |           |            |             |             |             |             |
| Staff: 14 | Tuesday   |           |           |           |            |             |             |             |             |
| Staff: 14 | Wednesday |           |           |           |            |             |             |             |             |
| Staff: 14 | Thursday  |           |           |           |            |             |             |             |             |
| Staff: 14 | Friday    |           |           |           |            |             |             |             |             |

|           | Afternoon: | 12:00-12:30 | 12:30-1:00 | 1:00-1:30 | 1:30-2:00 | 2:00-2:30 | 2:30-3:00 | 3:00-3:30 | 3:30-4:00 |
|-----------|------------|-------------|------------|-----------|-----------|-----------|-----------|-----------|-----------|
| Staff: 14 | Monday     |             |            |           |           |           |           |           |           |
| Staff: 14 | Tuesday    |             |            |           |           |           |           |           |           |
| Staff: 14 | Wednesday  |             |            |           |           |           |           |           |           |
| Staff: 14 | Thursday   |             |            |           |           |           |           |           |           |
| Staff: 14 | Friday     |             |            |           |           |           |           |           |           |

|                     |
|---------------------|
| Staff Only          |
| 10 - 15 cars parked |
| 15 - 20 Cars parked |
|                     |
|                     |

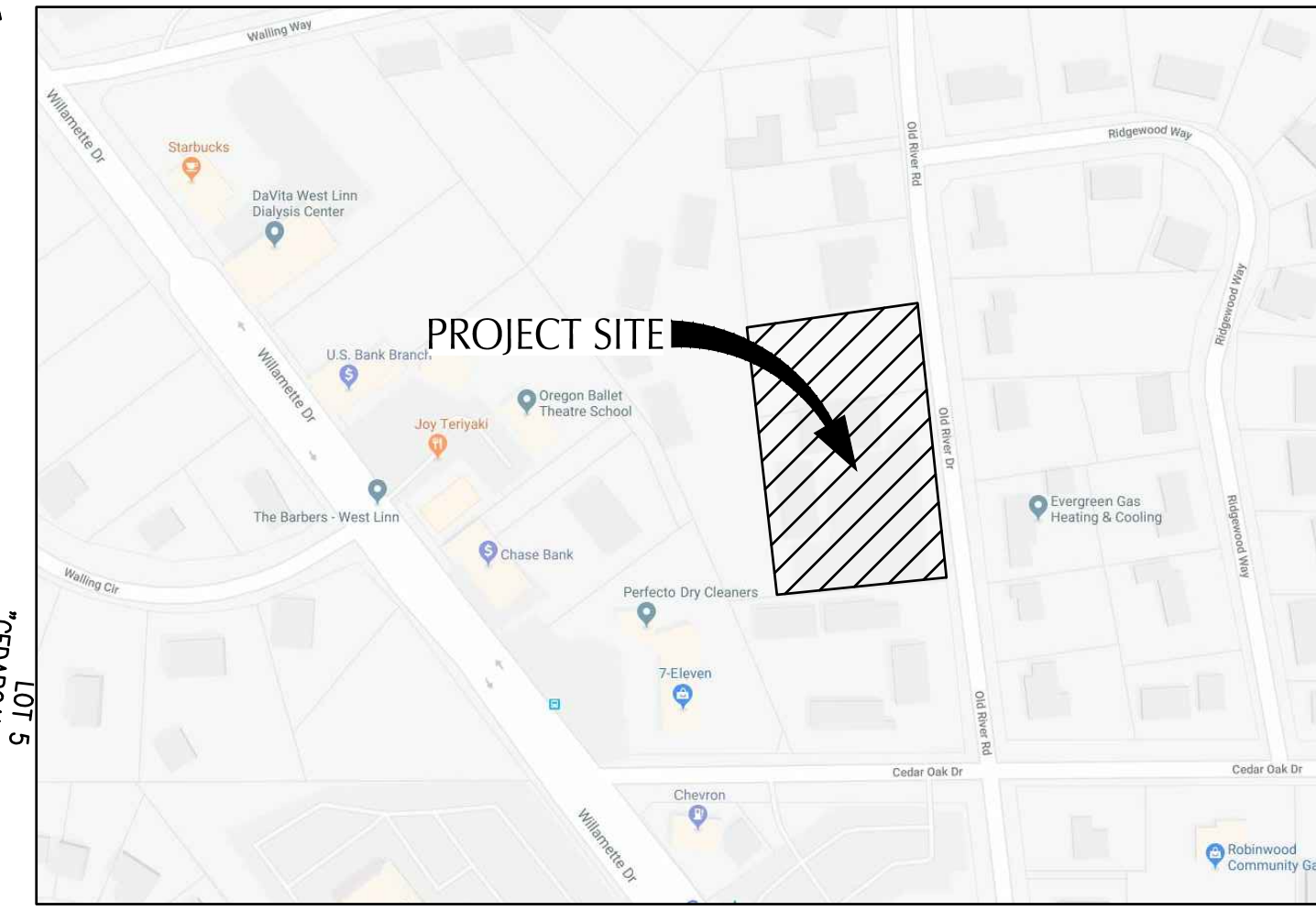
**Special School Events:**

- Parent Association meetings; once a month alternating am meeting and pm meeting
- Back to School Night; one for Primary & one for Preschool both in September
- Work Party; twice a year on a Saturday one October & one April
- Info night; once a year November
- Winter Performance; once a year December
- Preschool Celebration; once a year December
- Open House; once a year January
- Preschool Picnic; once a year June
- Parent Education Night; possibly twice a year month varies

**THE MARYLHURST SCHOOL**

NEW LIFE CHURCH SITE  
 1915 Old Lower River Road  
 West Linn, Oregon, 97068

**LAND USE APPLICATION**



VICINITY MAP

SCALE: NTS

GROSS AREA = 64,429 SF = 1.48 AC



**LEGEND**

- |        |                        |     |                            |
|--------|------------------------|-----|----------------------------|
| AB     | AREA DRAIN             | --- | BUILDING EAVE              |
| □      | BOLLARD                | --- | CURB                       |
| DECOD. | DECIDUOUS              | --- | DITCH                      |
| DL     | DRIP LINE RADIUS       | --- | EDGE OF PAVEMENT           |
| ●      | FOUND MONUMENT         | G   | GAS                        |
| ⊙      | MAILBOX                | OP  | OVERHEAD POWER             |
| ⊙      | ROOF DRAIN             | SS  | SANITARY SEWER             |
| ⊙      | SIGN, DISABLED PARKING | ST  | STORM SEWER                |
| ⊙      | SPRINKLER VALVE        | T   | UNDERGROUND COMMUNICATIONS |
| ⊙      | TREE STUMP             | UGP | UNDERGROUND POWER          |
| ⊙      | UTILITY POLE           | TEL | OVERHEAD COMMUNICATIONS    |
| ⊙      | UTILITY POLE ANCHOR    | W   | WATER                      |
| ⊙      | WATER METER            |     |                            |
| ⊙      | WATER VALVE            |     |                            |
| COL    | WOOD COLUMN            |     |                            |
- 
- |         |          |        |
|---------|----------|--------|
| ASPHALT | CONCRETE | GRAVEL |
|---------|----------|--------|

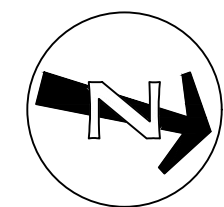
| Issue                | Revision | Date     |
|----------------------|----------|----------|
| LAND USE APPLICATION |          | 10.30.18 |

**EXISTING CONDITIONS**

Scale AS INDICATED  
 Date 08.10.18

Sheet No.

**C0.1**



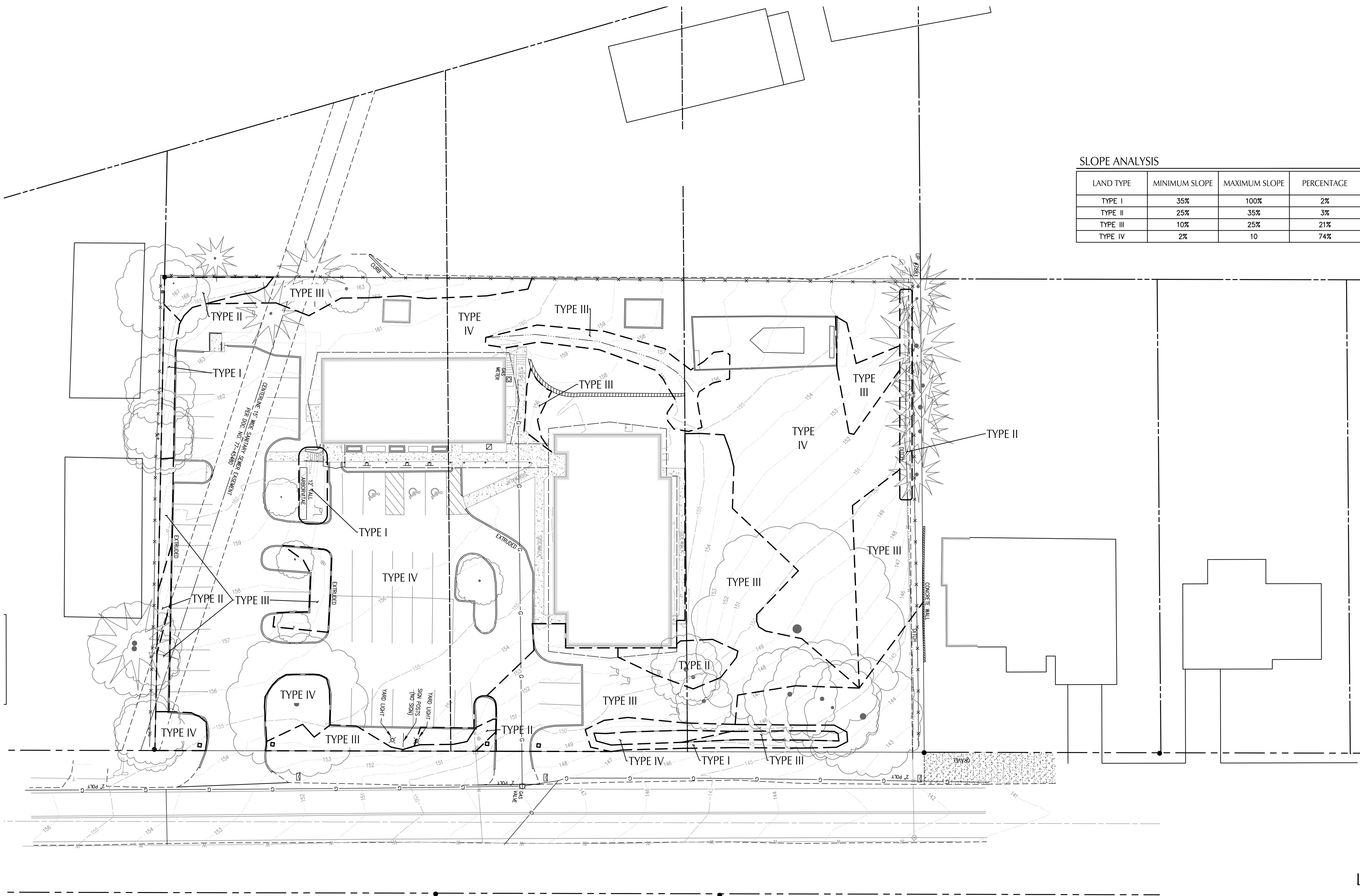
**SLOPE ANALYSIS**

| LAND TYPE | MINIMUM SLOPE | MAXIMUM SLOPE | PERCENTAGE |
|-----------|---------------|---------------|------------|
| TYPE I    | 35%           | 100%          | 2%         |
| TYPE II   | 25%           | 35%           | 3%         |
| TYPE III  | 10%           | 25%           | 21%        |
| TYPE IV   | 2%            | 10            | 74%        |

**THE MARYLHURST SCHOOL**

NEW LIFE CHURCH SITE  
 19915 Old Lower River Road  
 West Linn, Oregon, 97068

**LAND USE APPLICATION**



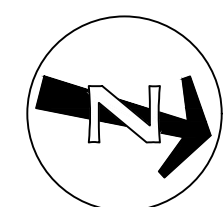
| Issue                | Revision | Date     |
|----------------------|----------|----------|
| LAND USE APPLICATION |          | 10.30.18 |

**LAND TYPE SLOPE ANALYSIS**

Scale AS INDICATED  
 Date 08.10.18

Sheet No.

**C0.2**



PHASE 1 PARKING ANALYSIS

| BUILDING                             | PROPOSED USE            | AREA     | PARKING REQ.         | NO. STALLS REQUIRED |
|--------------------------------------|-------------------------|----------|----------------------|---------------------|
| EXISTING CHURCH ANNEX BUILDING       | MIDDLE SCHOOL CLASSROOM | 4,000 SF | 1 SPACE PER 1,000 SF | 4                   |
| PROPOSED TEMPORARY PORTABLE          | PRIMARY SCHOOL          | 1,800 SF | 1 SPACE PER 1,000 SF | 2                   |
| EXISTING MAIN CHURCH BUILDING        | MULTI-USE COMMONS AREA  | 4,500 SF | 1 SPACE PER 1,000 SF | 5                   |
| STAFF                                | 10 (AN ANY ONE TIME)    |          |                      | 10                  |
| <b>TOTAL PARKING STALLS REQUIRED</b> |                         |          |                      | <b>21</b>           |
| <b>TOTAL PARKING STALLS PROPOSED</b> |                         |          |                      | <b>22</b>           |

NOTES

- GROSS SITE AREA = 1.48 AC
- SEE LANDSCAPE PLANS FOR FENCING LAYOUT

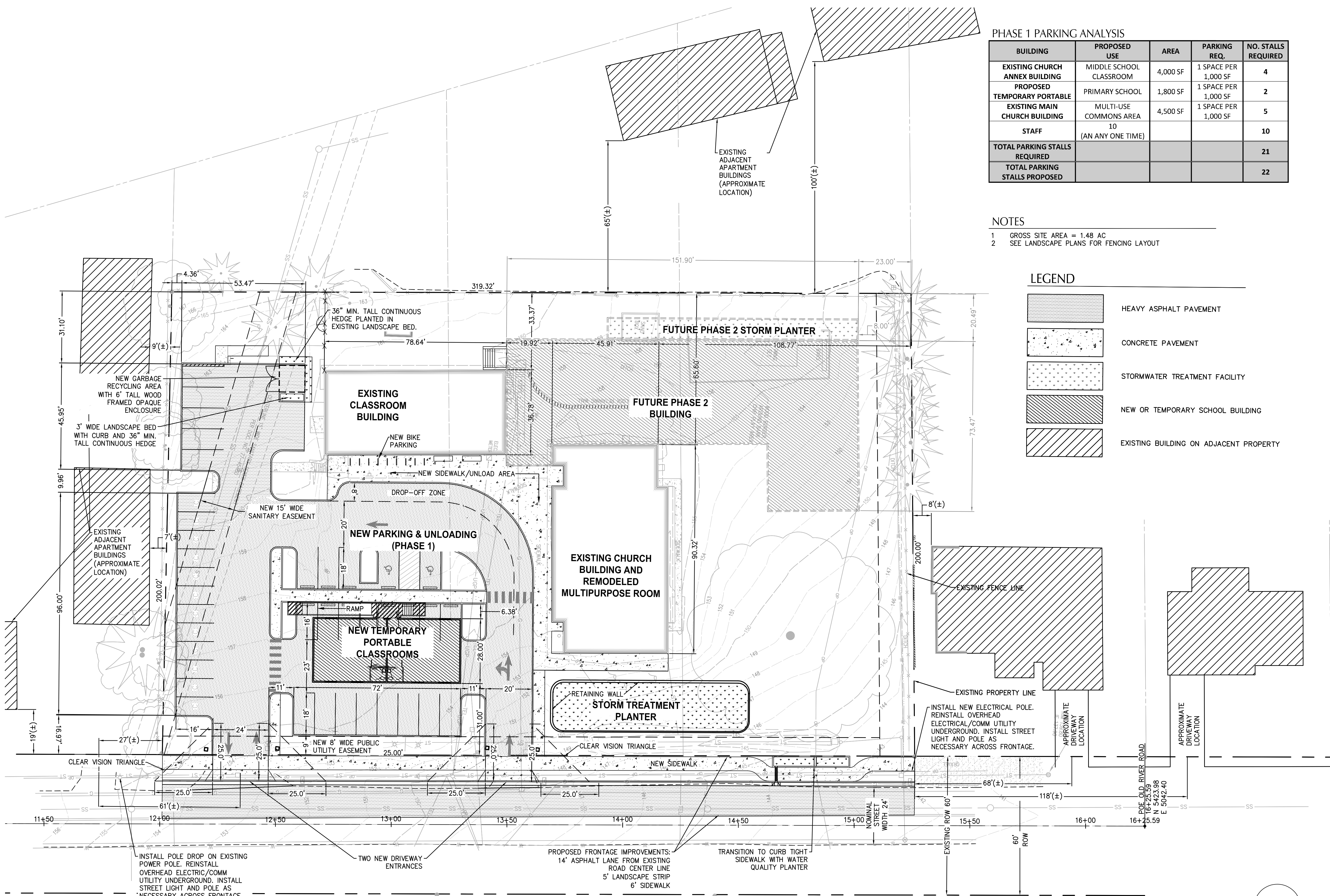
LEGEND

|  |                                        |
|--|----------------------------------------|
|  | HEAVY ASPHALT PAVEMENT                 |
|  | CONCRETE PAVEMENT                      |
|  | STORMWATER TREATMENT FACILITY          |
|  | NEW OR TEMPORARY SCHOOL BUILDING       |
|  | EXISTING BUILDING ON ADJACENT PROPERTY |

THE MARYLHURST SCHOOL

NEW LIFE CHURCH SITE  
19915 Old Lower River Road  
West Linn, Oregon, 97068

LAND USE APPLICATION



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|----------------------|----------|----------|
| LAND USE APPLICATION |          | 10.30.18 |

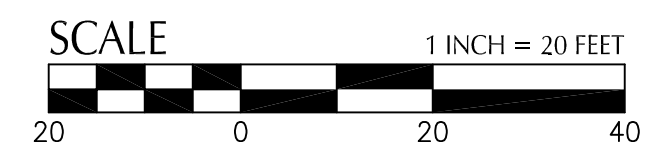
SITE PLAN - PHASE I

Scale AS INDICATED

Date 08.10.18

Sheet No.

C1.0



PHASE 2 PARKING ANALYSIS

| BUILDING                             | PROPOSED USE            | AREA      | PARKING REQ.         | NO. STALLS REQUIRED |
|--------------------------------------|-------------------------|-----------|----------------------|---------------------|
| EXISTING CHURCH ANNEX BUILDING       | MIDDLE SCHOOL CLASSROOM | 4,000 SF  | 1 SPACE PER 1,000 SF | 4                   |
| EXISTING MAIN CHURCH BUILDING        | MULTI-USE COMMONS AREA  | 4,500 SF  | 1 SPACE PER 1,000 SF | 5                   |
| 25% PROPOSED NEW ADDITION            | KINDERGARTEN & PRE-K    | 3,300 SF  | 1 SPACE PER 300 SF   | 11                  |
| 75% PROPOSED NEW ADDITION            | PRIMARY SCHOOL          | 13,000 SF | 1 SPACE PER 1,000 SF | 13                  |
| STAFF                                | 15 (AN ANY ONE TIME)    |           |                      | 15                  |
| <b>TOTAL PARKING STALLS REQUIRED</b> |                         |           |                      | <b>48</b>           |
| <b>TOTAL PARKING STALLS PROPOSED</b> |                         |           |                      | <b>37</b>           |

NOTES

1 GROSS SITE AREA = 1.48 AC

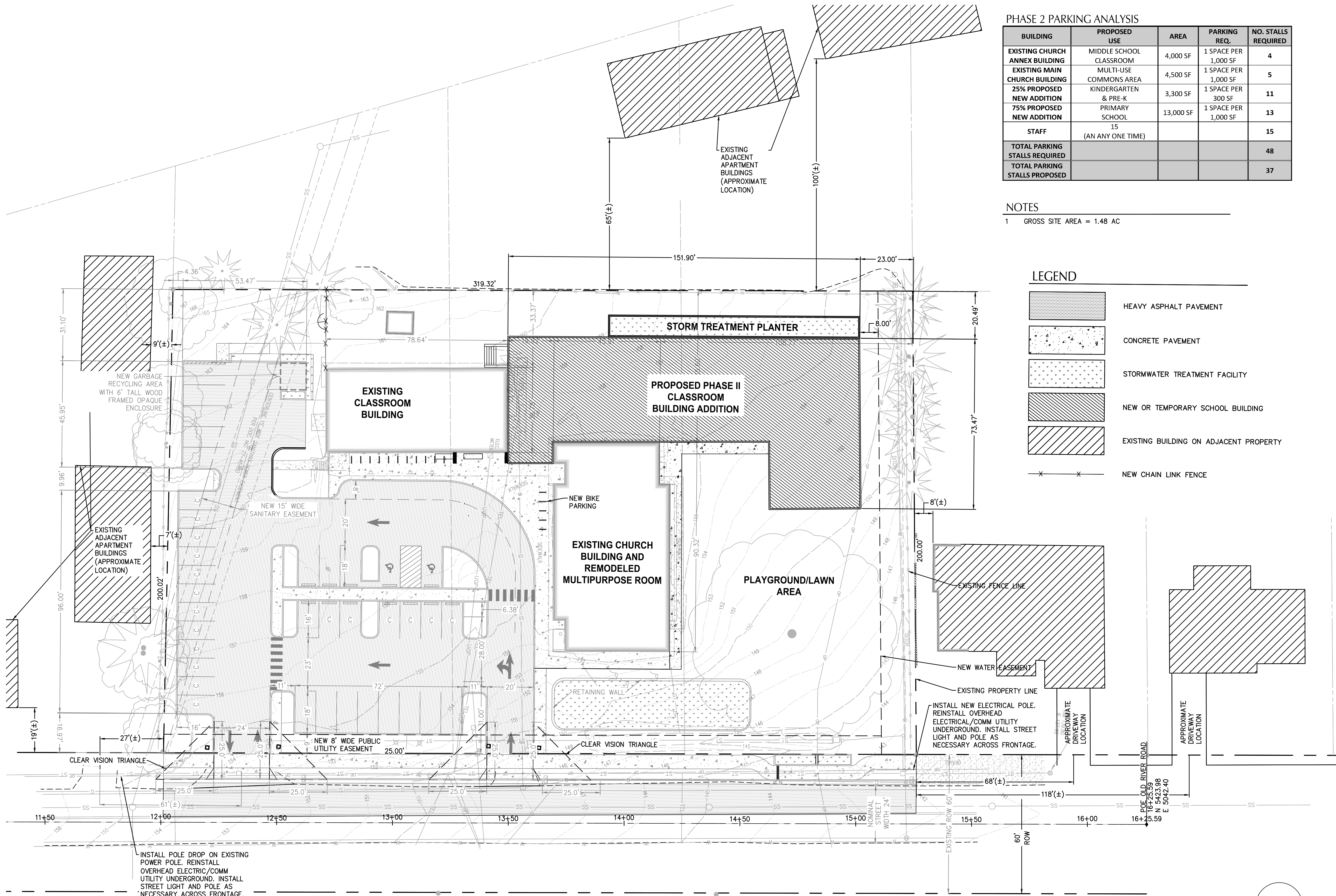
LEGEND

- HEAVY ASPHALT PAVEMENT
- CONCRETE PAVEMENT
- STORMWATER TREATMENT FACILITY
- NEW OR TEMPORARY SCHOOL BUILDING
- EXISTING BUILDING ON ADJACENT PROPERTY
- NEW CHAIN LINK FENCE

THE MARYLHURST SCHOOL

NEW LIFE CHURCH SITE  
19915 Old Lower River Road  
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LAND USE APPLICATION



| Issue                | Revision | Date     |
|----------------------|----------|----------|
| LAND USE APPLICATION |          | 10.30.18 |

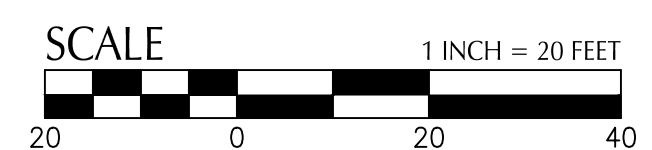
SITE PLAN - PHASE II

Scale AS INDICATED

Date 08.10.18

Sheet No.

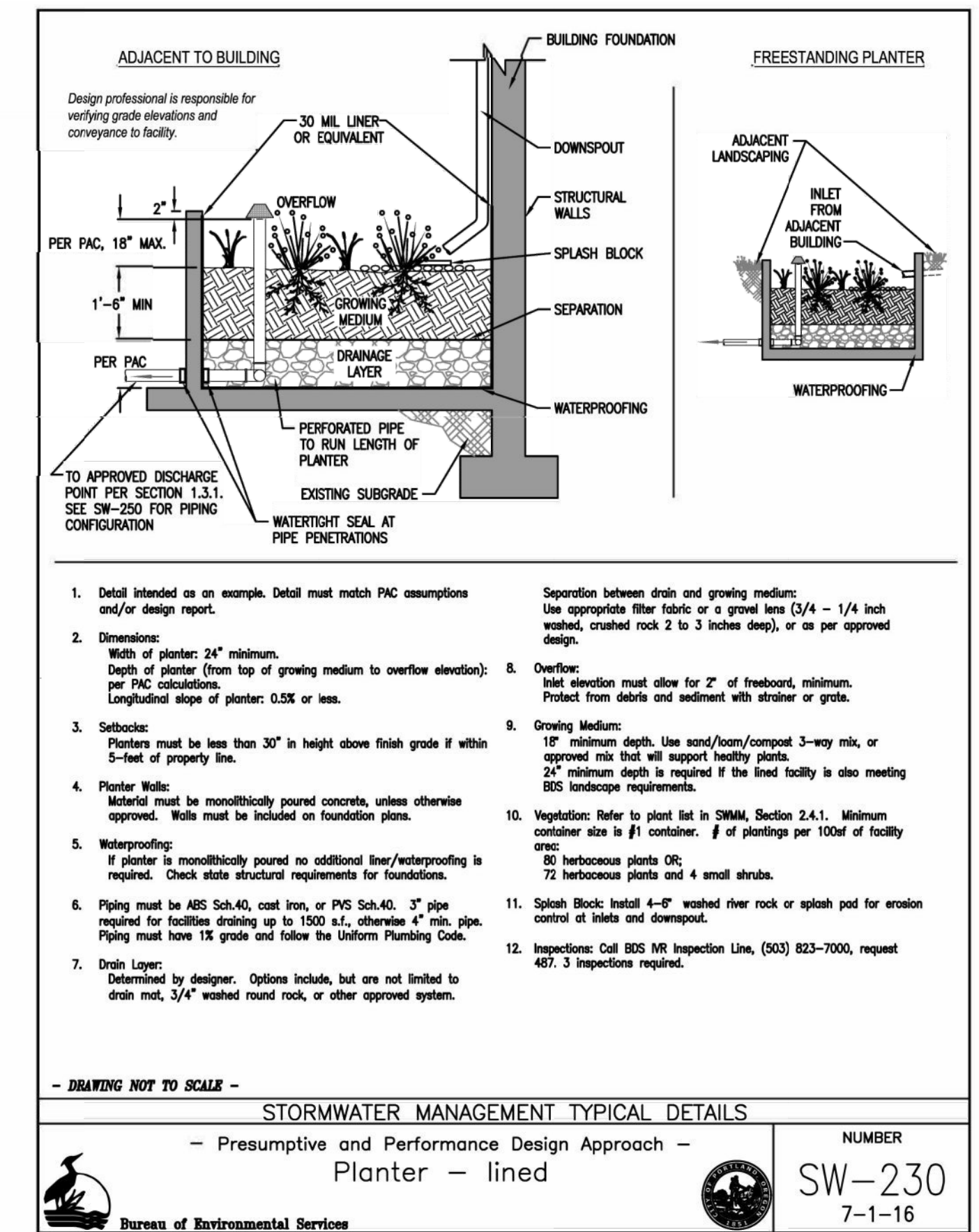
C1.1



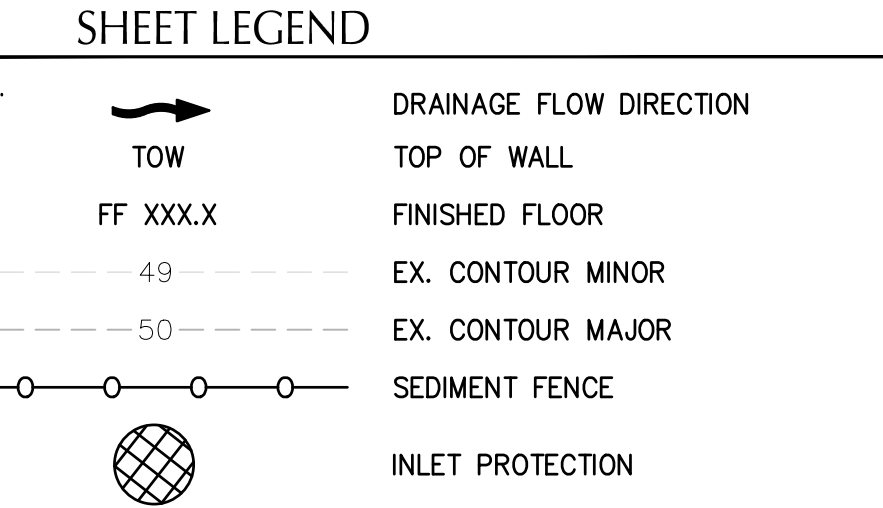
**THE MARYLHURST SCHOOL**

NEW LIFE CHURCH SITE  
 19915 Old Lower River Road  
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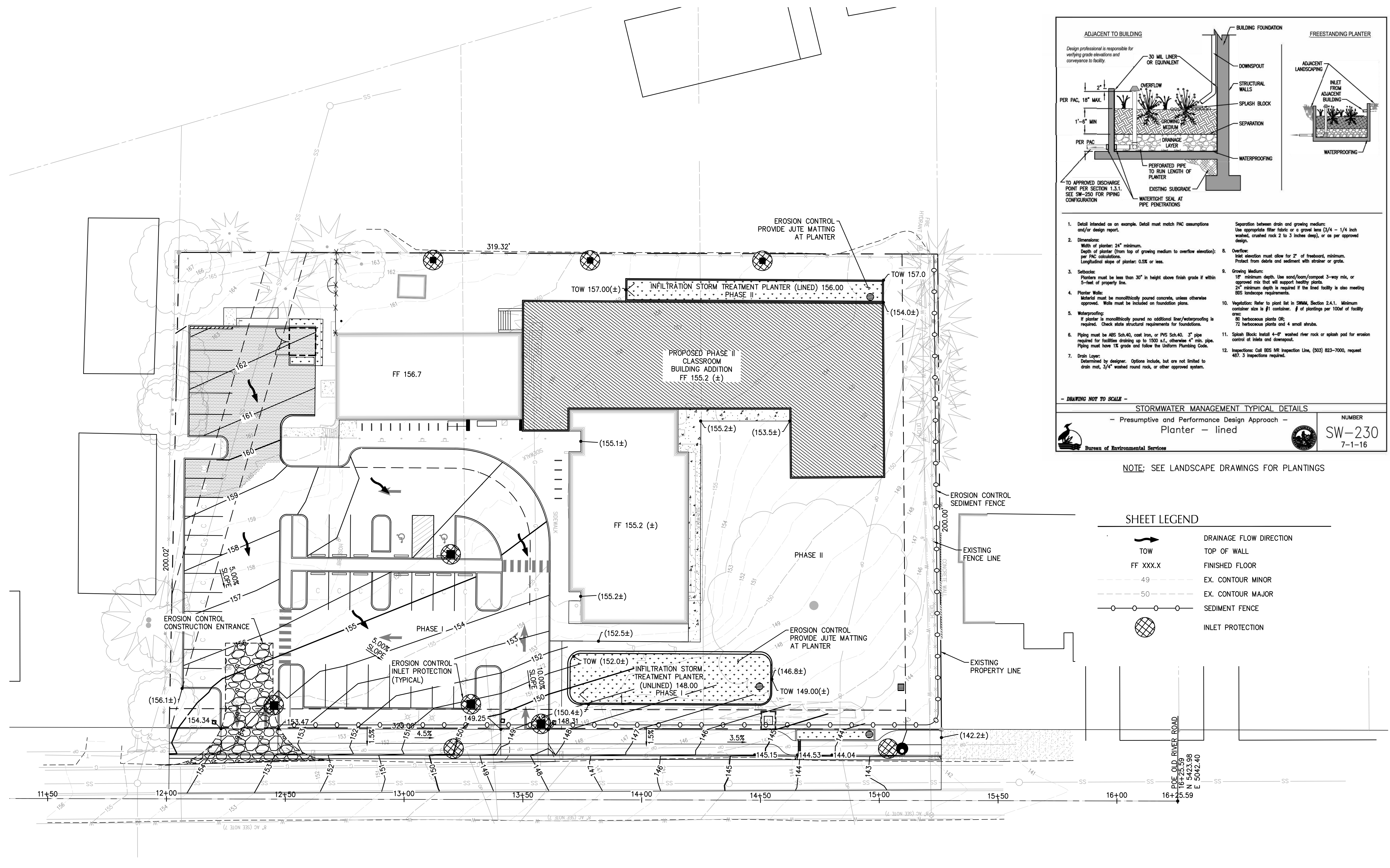
**LAND USE APPLICATION**



NOTE: SEE LANDSCAPE DRAWINGS FOR PLANTINGS



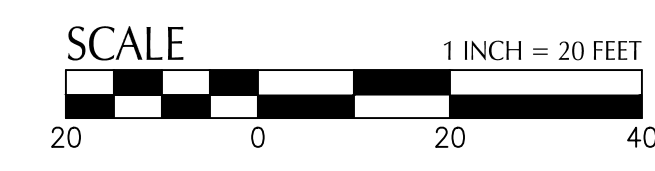
| Issue                | Revision | Date     |
|----------------------|----------|----------|
| LAND USE APPLICATION |          | 10.30.18 |



**GRADING PLAN**

Scale AS INDICATED  
 Date 08.10.18  
 Sheet No.

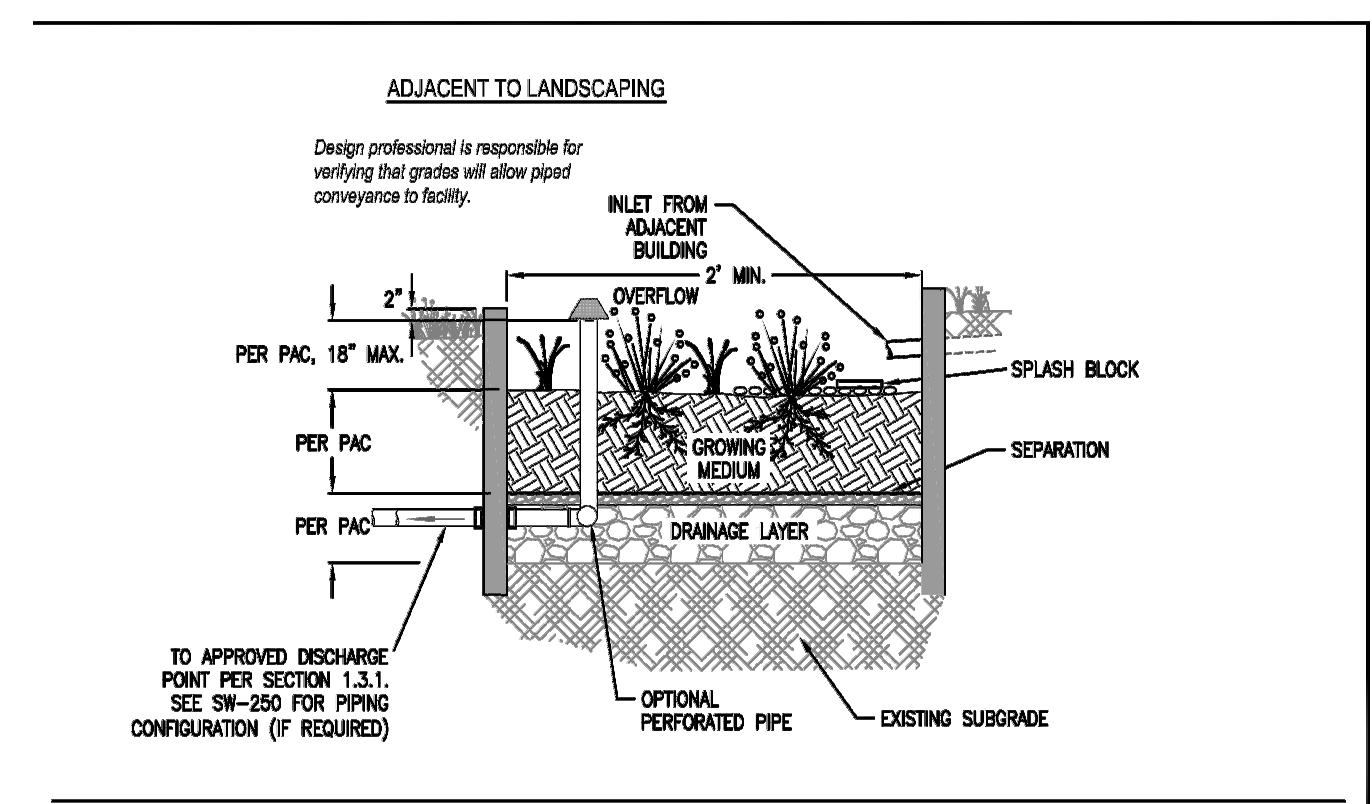
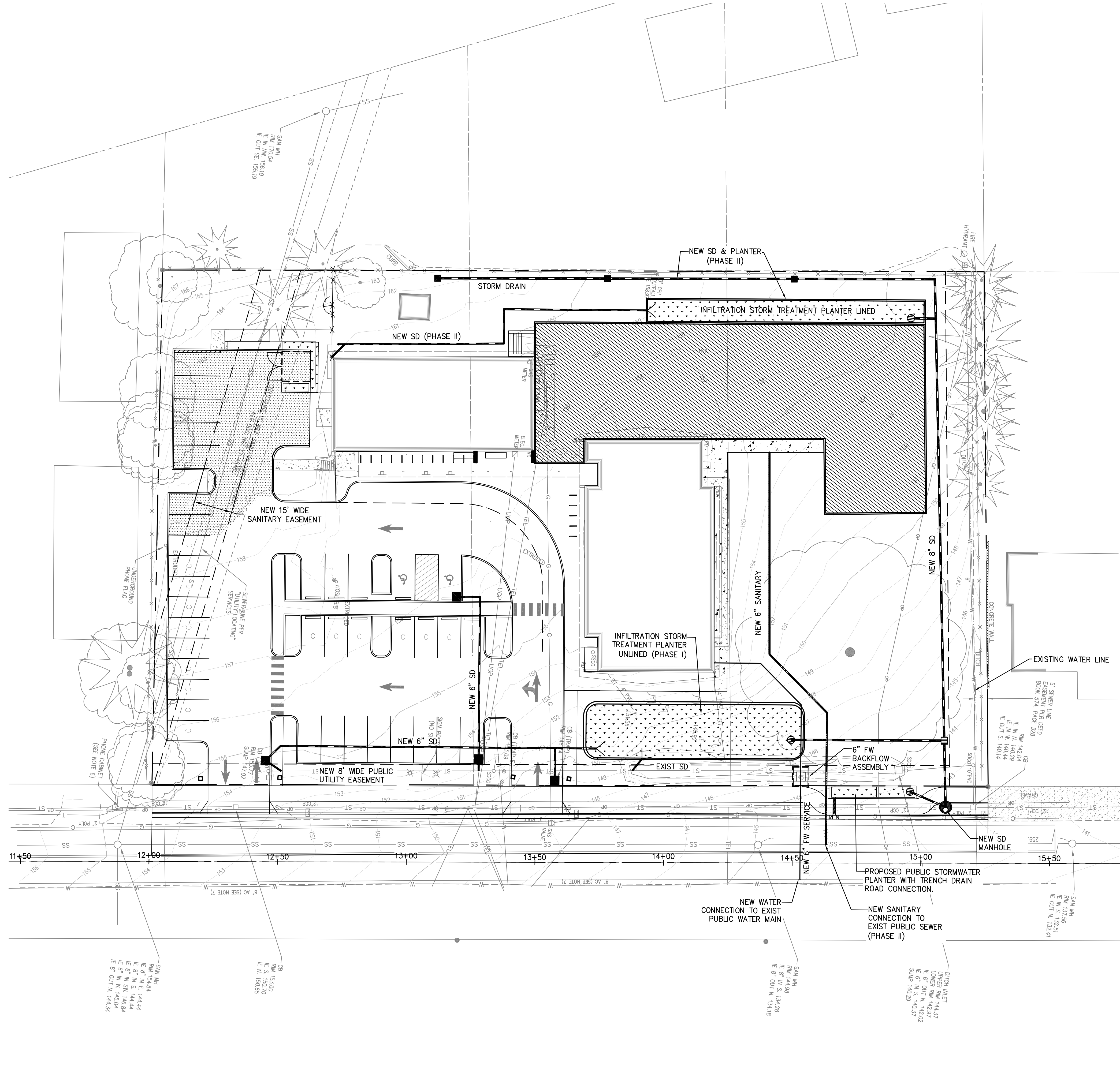
**C2.0**



THE MARYLHURST SCHOOL

NEW LIFE CHURCH SITE  
19915 Old Lower River Road  
West Linn, Oregon, 97068

LAND USE APPLICATION



- Detail intended as an example. Detail must match PAC assumptions and/or design report.
- Provide protection from all vehicle traffic, equipment staging, and foot traffic in proposed infiltration areas prior to, during, and after construction.
- Dimensions:  
Width of planter: 24" minimum.  
Depth of planter (from top of growing medium to overflow elevation) per PAC calculations.  
Longitudinal slope of planter: 0.5% or less.
- Setbacks:  
Planters must be 5-feet from property line and 10-feet from building foundations.
- Planter Walls:  
Material must be concrete, unless otherwise approved.
- Piping must be ABS Sdn.40, cast iron, or PVC Sdn.40. 3" pipe required for facilities existing up to 1500 s.f., otherwise 4" min. pipe. Piping must have 1% grade and follow the Uniform Plumbing Code.
- Drain Layer:  
Per PAC calculations. Options include, but are not limited to: drain rock, 3/4" washed rock, or other approved system.  
Separation between drain and growing medium:  
Use appropriate filter fabric or a gravel layer (3/4" - 1/4 inch washed, crushed rock 2 to 3 inches deep), or as per approved design.
- Overflow:  
Inlet elevation must allow for 2" of freeboard, minimum. Protect from debris and sediment with strainer or grate.
- Growing Medium:  
Use sand/soil/compost 3-way mix, or approved mix that will support healthy plants. 18" minimum depth if there is a drainage layer. If soils are well draining and there is not a drainage layer depth may be reduced as approved.
- Vegetation: Refer to plant list in SWM, Section 2.4.1. Minimum container size is #1 container. # of plantings per 100sf of facility area:  
50 herbaceous plants OR  
72 herbaceous plants and 4 small shrubs.
- Splash Block: Install 4'-6" washed river rock or splash pad for erosion control at inlets and downspout.
- Inspection: Call BDS I/R Inspection Line, (503) 823-7000, request 457. 3 Inspections required.

- DRAWING NOT TO SCALE -

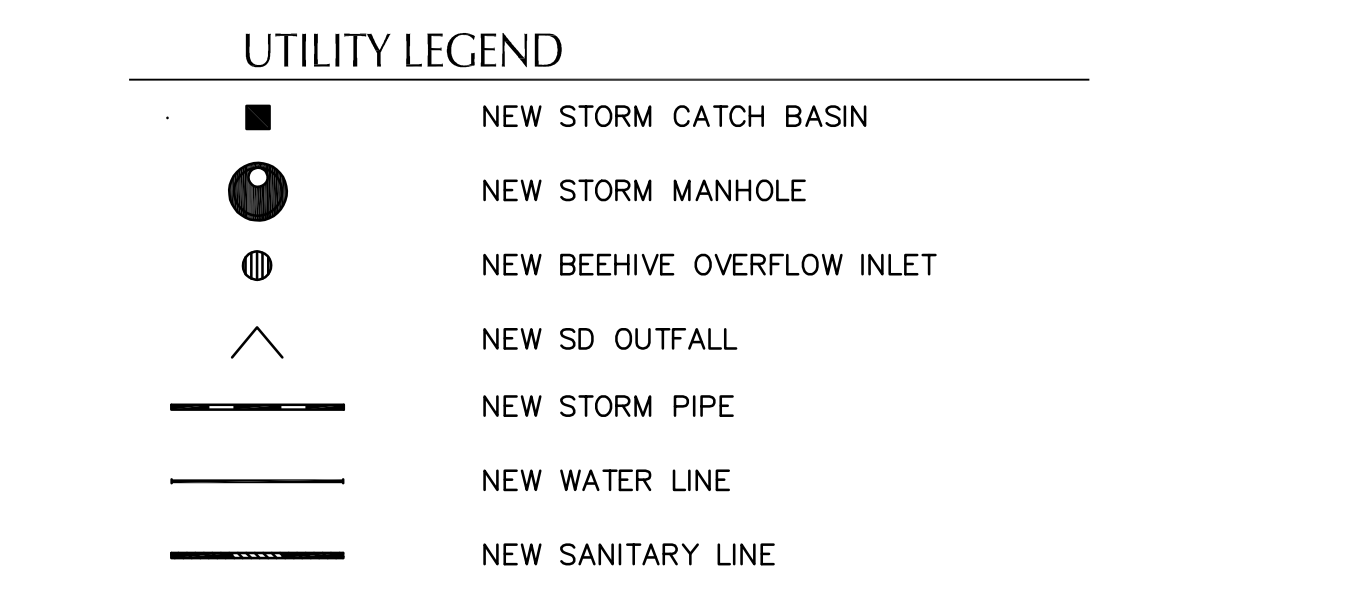
STORMWATER MANAGEMENT TYPICAL DETAILS

- Presumptive and Performance Design Approach -

Planter - unlined

NUMBER SW-231  
7-1-16

NOTE: SEE LANDSCAPE DRAWINGS FOR PLANTINGS

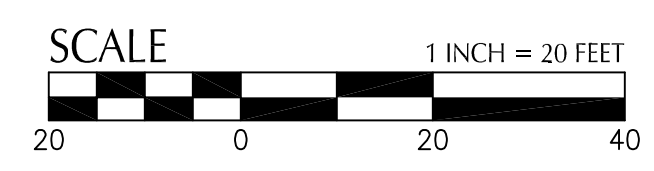


| Issue                | Revision | Date     |
|----------------------|----------|----------|
| LAND USE APPLICATION |          | 10.30.18 |

UTILITY PLAN

Scale AS INDICATED  
Date 08.10.18  
Sheet No.

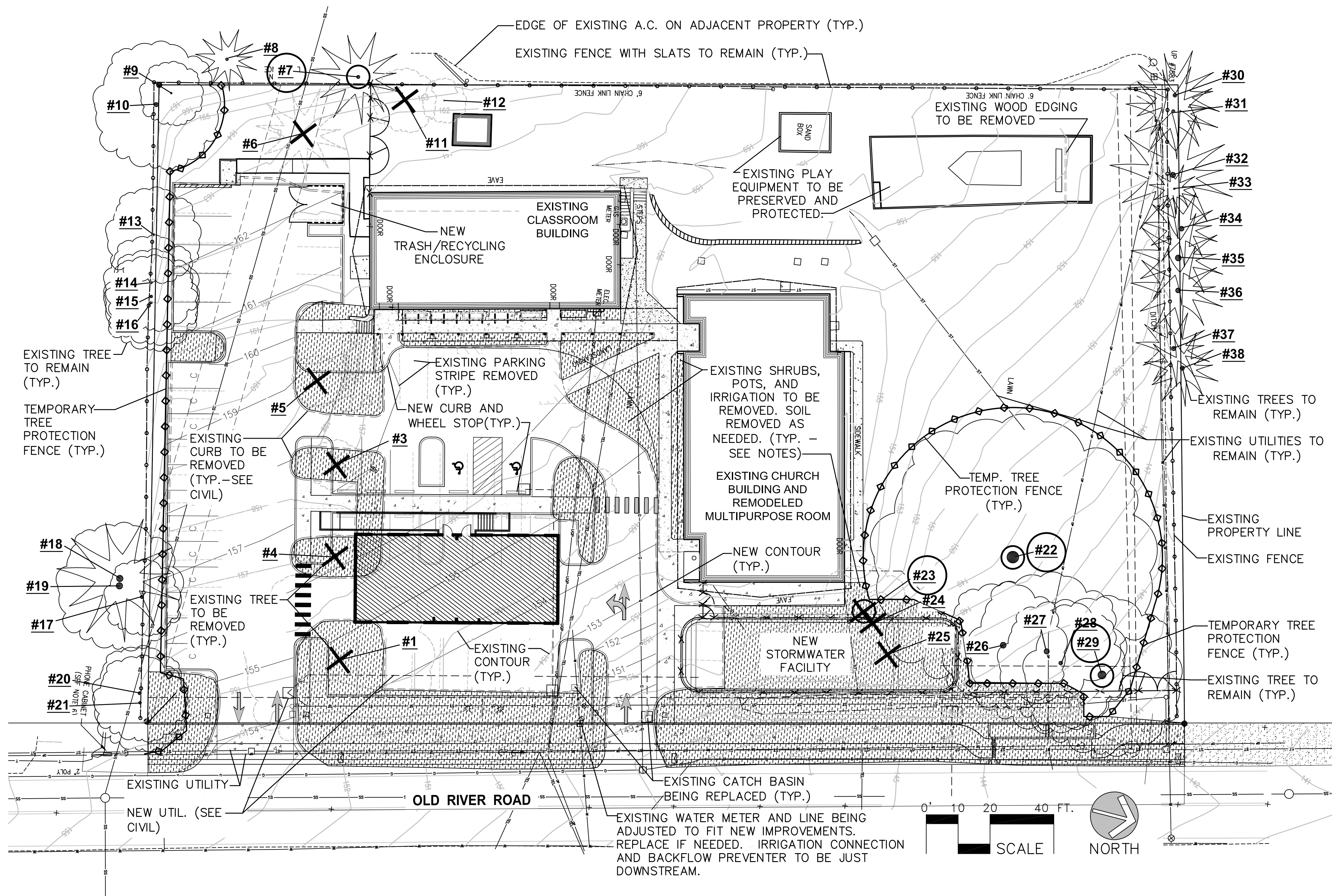
C3.0







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**NOTES:**

1. SEE ARBORIST REPORT FOR NAME, SIZE, AND COMMENTS REGARDING EXISTING TREES.
2. CONFIRM TREATMENT OF EACH TREE ON SITE.
3. ALL WORK TO CONFORM WITH CITY OF WEST LINN "TREE TECHNICAL MANUAL".
4. AREA BETWEEN THE EXISTING TREES AND THE TREE PROTECTION FENCE SHALL BE KEPT FREE OF ALL EROSION MATERIALS, TRAFFIC, CONSTRUCTION, CONSTRUCTION TRAFFIC, MATERIALS, AND EXCAVATION. THIS AREA IS TO BE CONSIDERED THE "TREE PROTECTION AREA".
5. TREE PROTECTION FENCE SHALL REMAIN IN PLACE, IN GOOD CONDITION, AND PLUMB UNTIL APPROVED SUBSTANTIAL COMPLETION. AT THAT TIME IT SHALL BE REMOVED AND TAKEN OFF SITE.
6. ANY MOVEMENT OF THE TREE PROTECTION FENCE AND ANY WORK WITHIN THE "TREE PROTECTION AREA" SHALL BE AS APPROVED BY THE CITY'S AND OWNER'S ARBORIST (CERTIFIED). ANY SUCH WORK SHALL BE DONE IN ACCORDANCE WITH THESE ARBORISTS' APPROVAL..
7. THERE SHALL BE NO WORK, STORAGE, TRANSIT, AND DEMOLITION DONE IN THE AREAS BEHIND THE EXISTING BUILDINGS.
8. 'TREE PROTECTION FENCE' SHALL BE A 6' TALL CHAIN LINK FENCE, WITH METAL POLES SET INTO GROUND AT 10 FEET ON CENTER (MAX.) AND AT CHANGES IN DIRECTION, AND AT CORNERS. IT SHALL BE KEPT IN GOOD CONDITION, PLUMB, AND IN-PLACE.
9. INSTALL UTILITIES AND CONDUCT ALL WORK TO MINIMIZE TREE IMPACTS.
10. EXISTING TREES DESIGNATED AS "SIGNIFICANT" BY THE CITY ARBORIST (11/1/2018) HAVE BEEN INDICATED BY CIRCLING (○) THE TREE CENTER AND TREE #. TREES #7, #22, AND #29 HAVE BEEN SO INDICATED AND ARE TO BE PRESERVED AND PROTECTED. TREE #23 HAS SO BEEN INDICATED, BUT BECAUSE IT IS LOCATED TOO NEAR THE WATER QUALITY FACILITY IT SHALL BE REMOVED, AND IS SO INDICATED. THUS, (1) OF THE (4) "SIGNIFICANT" TREES (25%) ARE TO BE REMOVED.

**THE MARYLHURST SCHOOL**

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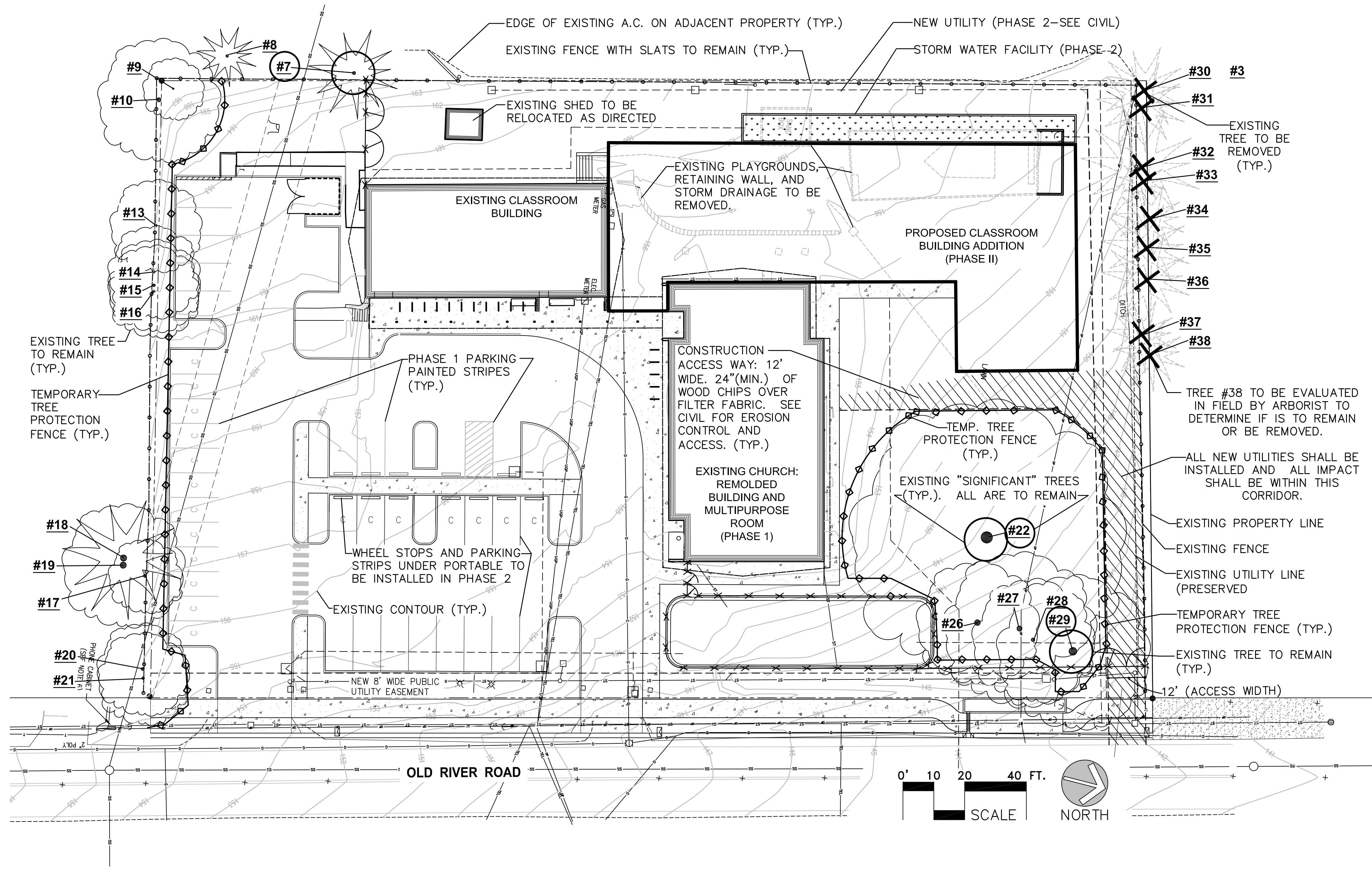
**LAND USE APPLICATION**

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|-------------------------|----------|------------|
| LAND USE APPLICATION    |          | 09/06/2018 |
| DESIGN REVIEW REVISIONS |          | 11/05/2018 |
| DESIGN REVIEW REVISIONS |          | 12/03/2018 |

**EXISTING TREE AND SHRUBS LANDSCAPE DEMO. - PHASE 1**

Scale AS INDICATED  
Date 09/06/2018

Sheet No.  
**L0.01**



**NOTES:**

1. SEE ARBORIST REPORT FOR NAME, SIZE, AND COMMENTS REGARDING EXISTING TREES.
2. CONFIRM TREATMENT OF EACH TREE ON SITE.
3. ALL WORK TO CONFORM WITH CITY OF WEST LINN "TREE TECHNICAL MANUAL".
4. AREA BETWEEN THE EXISTING TREES AND THE TREE PROTECTION FENCE SHALL BE KEPT FREE OF ALL EROSION MATERIALS, TRAFFIC, CONSTRUCTION, CONSTRUCTION TRAFFIC, MATERIALS, AND EXCAVATION. THIS AREA IS TO BE CONSIDERED THE "TREE PROTECTION AREA".
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6. ANY MOVEMENT OF THE TREE PROTECTION FENCE AND ANY WORK WITHIN THE "TREE PROTECTION AREA" SHALL BE AS APPROVED BY THE CITY'S AND OWNER'S ARBORIST (CERTIFIED). ANY SUCH WORK SHALL BE DONE IN ACCORDANCE WITH THESE ARBORISTS' APPROVAL..
7. THERE SHALL BE NO WORK, STORAGE, TRANSIT, AND DEMOLITION DONE IN THE AREAS BEHIND THE EXISTING BUILDINGS.
8. 'TREE PROTECTION FENCE' SHALL BE A 6' TALL CHAIN LINK FENCE, WITH METAL POLES SET INTO GROUND AT 10 FEET ON CENTER (MAX.) AND AT CHANGES IN DIRECTION, AND AT CORNERS. IT SHALL BE KEPT IN GOOD CONDITION, PLUMB, AND IN-PLACE.
9. SEE L0.1 (PHASE 1) FOR MISSING TREE NUMBERS.
10. UTILITIES SHOWN ON NORTH END OF PROPERTY SHALL BE INSTALLED IN THE TEMPORARY ACCESS CORRIDOR AND ALL WORK SHALL BE AS APPROVED BY OWNERS PROJECT ARBORIST PRIOR TO WORK BEGINNING. (LOCATIONS SHOWN HEREON ARE SCHEMATIC).



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**THE MARYLHURST  
SCHOOL**

NEW LIFE CHURCH SITE  
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| LAND USE APPLICATION |          | 09/06/2018 |
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| DESIGN REVIEW REV.   |          | 12/03/2018 |

**EXISTING TREE  
AND ACCESS-  
PHASE 2**

Scale AS INDICATED

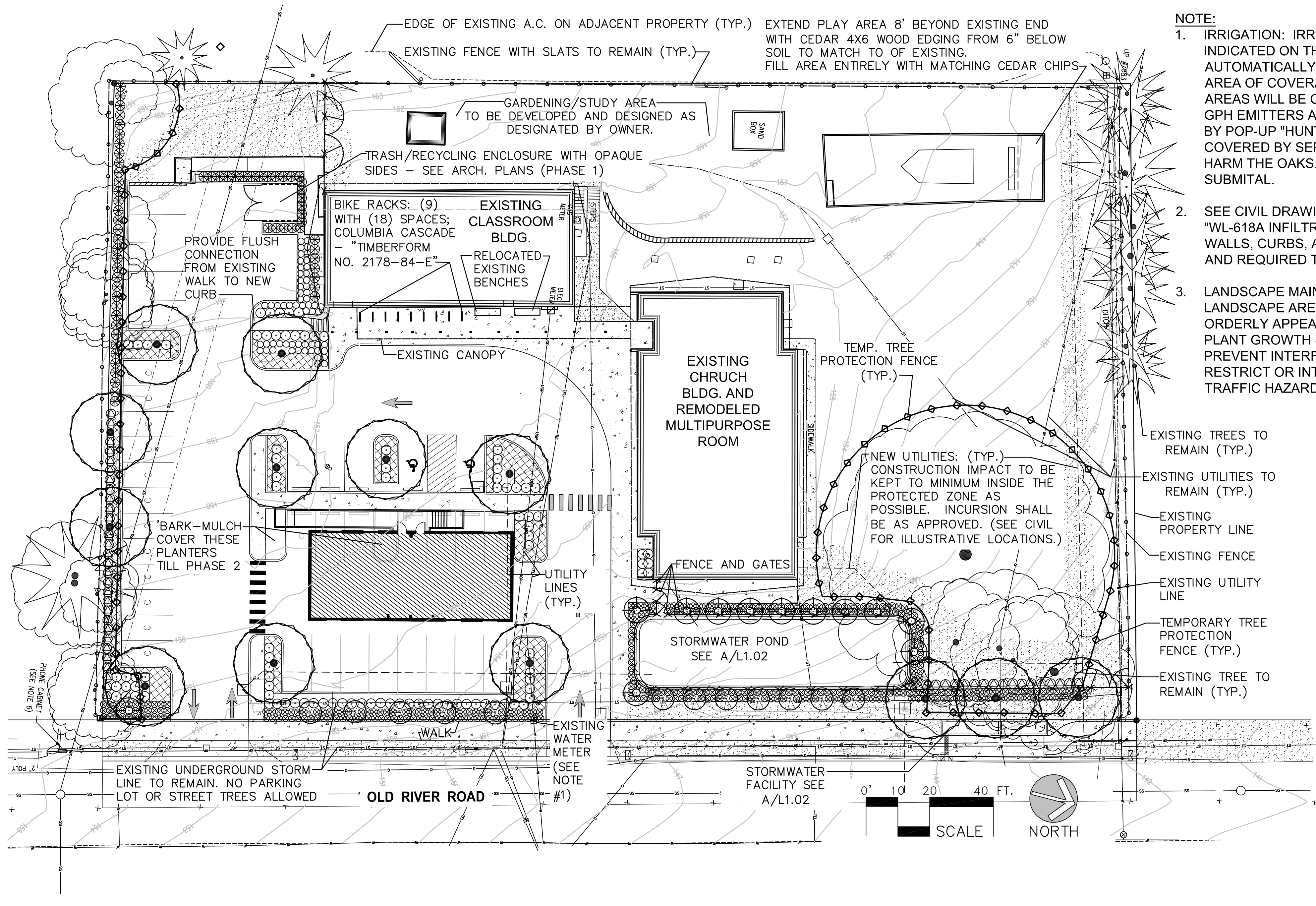
Date 09/06/2018

Sheet No.

**L0.02**



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- NOTE:**
- IRRIGATION: IRRIGATION SHALL BE SERVED FROM THE RELOCATED WATER LINE AND METER INDICATED ON THIS DRAWING. ALL NEW PLANTINGS SHALL BE SERVED BY AN AUTOMATICALLY CONTROLLED NEW IRRIGATION SYSTEM. THE SYSTEM SHALL BE ZONED TO AREA OF COVERAGE WITH SIMILAR IRRIGATION NEEDS. NEW SHRUB AND GROUND COVER AREAS WILL BE COVERED BY SUB-TERRANIAN DRIPPER LINE ("NETAFIM CV" LINE, WITH 0.6 GPH EMITTERS AT 12" ON CENTER). STORMWATER QUALITY PLANTER SHALL BE COVERED BY POP-UP "HUNTER MPR" SPRINKLERS. AREAS OF LAWN BELOW EXISTING OAKS WILL BE COVERED BY SEPARATE TEMPORARY ZONES AS NEEDED TO ESTABLISH LAWNS BUT NOT HARM THE OAKS. A FULL IRRIGATION PLAN WILL BE PROVIDED WITH THE BUILDING PERMIT SUBMITAL.
  - SEE CIVIL DRAWINGS FOR WATER QUALITY AREA S' SECTION AND MATERIALS (City Detail "WL-618A INFILTRATION RAIN GARDEN TYPE 2") AND FOR FURTHER INFORMATION ON PAVING, WALLS, CURBS, AND UTILITIES. ALTER ROUTES AND LOCATION OF UTILITIES AS APPROVED AND REQUIRED TO LIMIT IMPACTS ON EXISTING TREES TO REMAIN.
  - LANDSCAPE MAINTENANCE: THE OWNER, AFTER FINAL ACCEPTANCE, SHALL KEEP ALL LANDSCAPE AREAS IN GOOD CONDITIONS, SO AS TO PROVIDE A HEALTHY, NEAT, AND ORDERLY APPEARANCE AND SHALL KEEP THEM FREE OF ALL WEEDS, REFUSE, AND DEBRIS. PLANT GROWTH SHALL BE CONTROLLED BY TRIMMING, PRUNING, AND OTHERWISE TO: PREVENT INTERFERENCE WITH MAINTENANCE AND REPAIR OF UTILITIES; SO IT WILL NOT RESTRICT OR INTERFERE WITH PEDESTRIAN AND VEHICLES; AND SHALL NOT CONSTITUTE A TRAFFIC HAZARD OR REDUCE VISIBILITY.

**LEGEND:**  
 symbol - plant name: size, comments, and (quantity)

- OREGON WHITE OAK / QUERCUS GARYANA: 3" CALIPER; SINGLE TRUNK, STRAIGHT AND WELL FORMED; (13)
- ARBORVITAE / THUJA OCCIDENTALIS 'EMERALD': 6 FT. HT.; DENSE AND STRAIGHT; (65)
- CHINESE WITH HAZEL / HAMAMELIS MOLLIS (YELLOW): 15 GALLON; MULTI-STEMED; (12)
- DWARF NANDINA / NANDINA DOM. 'COMPACTA': 2 GALLON; (57)
- DWARF OREGON GRAPE / MAHONIA REPENS: 2 GALLON; (365)
- MOCK ORANGE / PHILADELPHUS LEWISII: 15 GALLON; (12)
- OTTO LUYKEN LAUREL / PRUNUS L. 'OTTO LUYKEN': 24"X24"; (95)
- RUGOSA ROSE / ROSA RUGOSA: 5 GALLON; (46)
- CREEPING BRAMBLE / RUBUS PENTALOBUS: 4" POTS AT 18" ON CENTER (TRINGULAR SPACING)
- LAWN: SOD OR SEEDED (FULLY ESTABLISHED)

**THE MARYLHURST SCHOOL**  
 NEW LIFE CHURCH SITE  
 19915 Old Lower River Road  
 West Linn, Oregon, 97068

**LAND USE APPLICATION**

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|----------------------|------------|------------|
| LAND USE APPLICATION |            | 09/06/2018 |
| DESIGN REVIEW REV.   | 11/05/2018 |            |
| DESIGN REVIEW REV.   | 12/03/2018 |            |

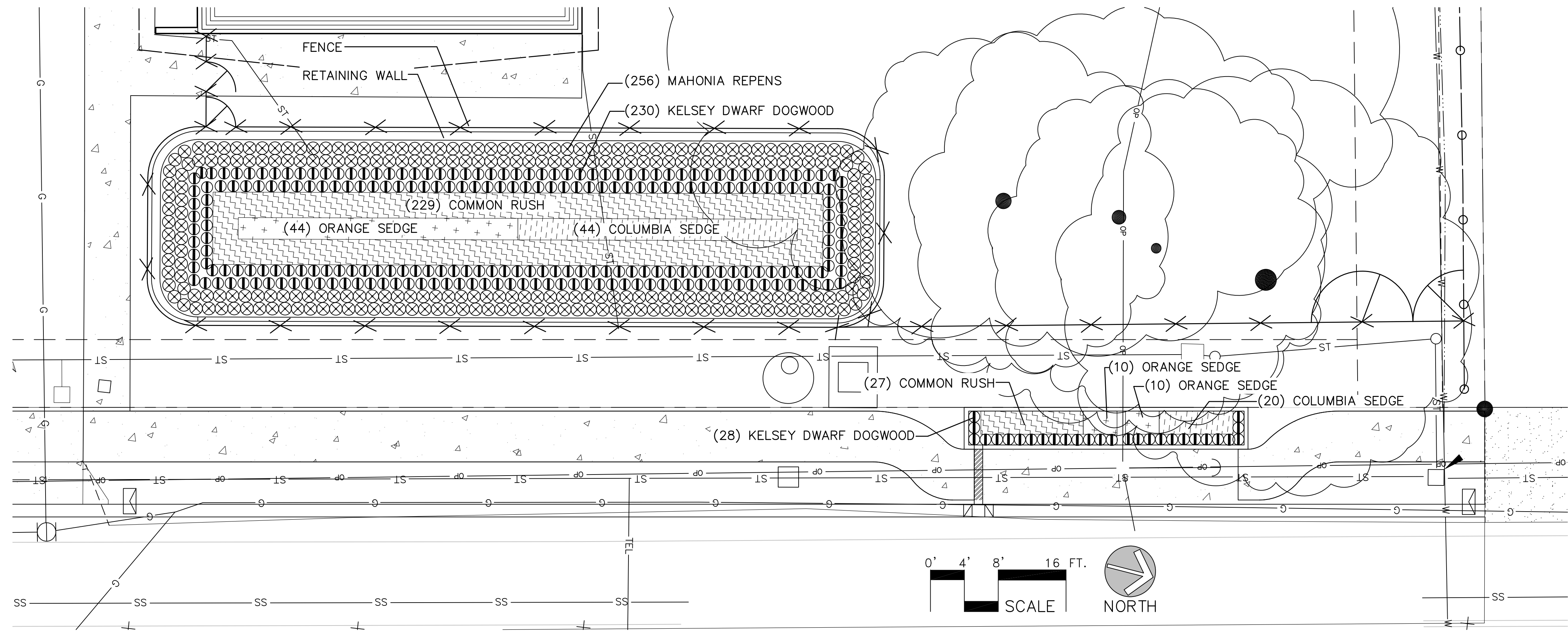
**PLANTING PLAN - PARKING LOT AND SITE - PHASE 1**

Scale AS INDICATED  
 Date 09/06/2018

Sheet No.  
**L1.01**



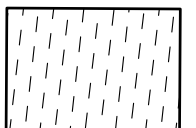
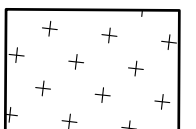
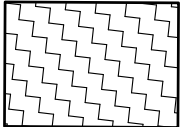
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**THE MARYLHURST  
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 NEW LIFE CHURCH SITE  
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LAND USE APPLICATION

**A STORMWATER QUALITY FACILITIES**

- LEGEND:**  
 symbol - plant name: size, comments, and (total quantity)
- PLANTS INSIDE STORMWATER FACILITY  
 (ALL PLANTS TO BE PLANTED AT 18" ON CENTER / 1 PLANT/1.95 S.F.)
- ⊗ DWARF OREGON GRAPE / MAHONIA REPENS:  
2 GALLON; (256)
  - ① KELSEY (DWARF) DOGWOOD/ CORNUS SERICEA 'KELSEY': 1 GALLON; (258)
  -  COLUMBIA SEDGE/ CARES APERTA: 1 GALLON; (64)
  -  NEW ZEALAND ORANGE SEDGE/ CAREX TESTACAE: 1 GALLON; (64)
  -  COMMON RUSH/ JUNCUS EFFUSUS: 1 GALLON: (71)
- IN ADDITION AND IN-BETWEEN THE ABOVE PLANTS PROVIDE (150) 1 GALLON COMMON CAMAS/ CAMASSIA QUAMASH (BLUE).

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| DESIGN REVIEW REV.   |          | 12/03/2018 |

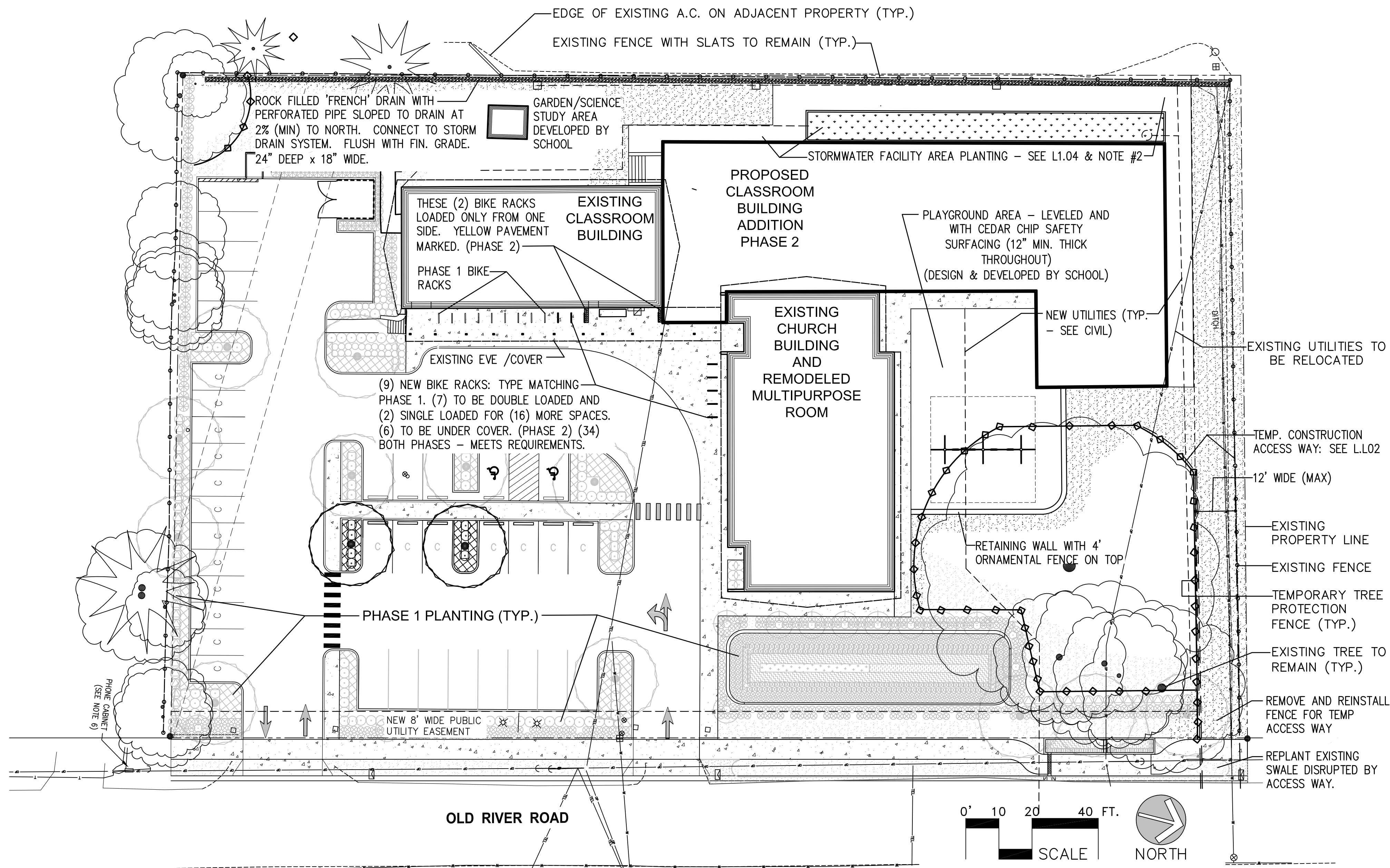
**PLANTING PLAN -  
 STORMWATER  
 FACILITIES -  
 PHASE 1**

Scale AS INDICATED  
 Date 09.06.18

Sheet No.  
**L1.02**



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**NOTE:**

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- SEE CIVIL DRAWINGS FOR WATER QUALITY AREA SECTION AND MATERIALS (City Detail "WL-618A INFILTRATION RAIN GARDEN TYPE 2") AND FOR FURTHER INFORMATION ON PAVING, WALLS, CURBS, AND UTILITIES. ALTER ROUTES AND LOCATION OF UTILITIES AS APPROVED AND REQUIRED TO LIMIT IMPACTS ON EXISTING TREES TO REMAIN.
- SEE L1.01 FOR MAINTENANCE REQUIREMENTS

**LEGEND:**

- symbol - plant name and comments
- LAWN: SEEDED AND FULLY ESTABLISHED
  - OREGON WHITE OAK / QUERCUS GARYANA: 3" CALIPER; SINGLE TRUNK, STRAIGHT AND WELL FORMED; (2)
  - OTTO LUYKEN LAUREL/PRUNUS L. 'OTTO LUYKEN'; 5 GALLON; (7)
  - CREEPING BRAMBLE / RUBUS PENTALOBUS: 4" POTS AT 18" ON CENTER (TRIANGULAR SPACING)

**THE MARYLHURST SCHOOL**

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**LAND USE APPLICATION**

**(A) SITE PLANTING PLAN**

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| DESIGN REVIEW REV.   |          | 12/03/2018 |

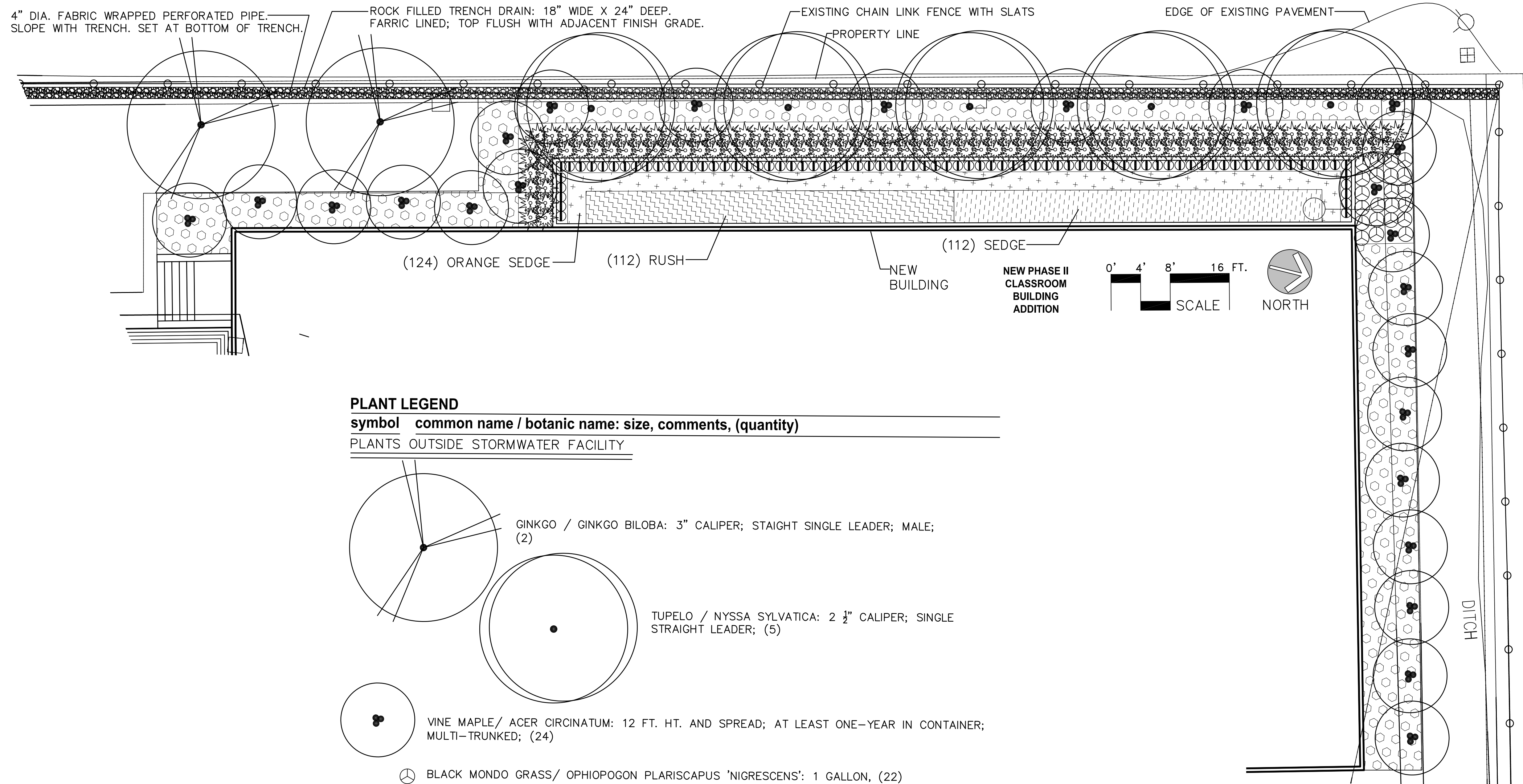
**PLANTING PLAN - PHASE 2**

Scale AS INDICATED  
 Date 09.06.18

Sheet No.  
**L1.03**



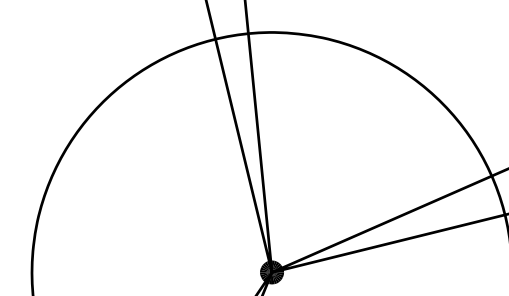
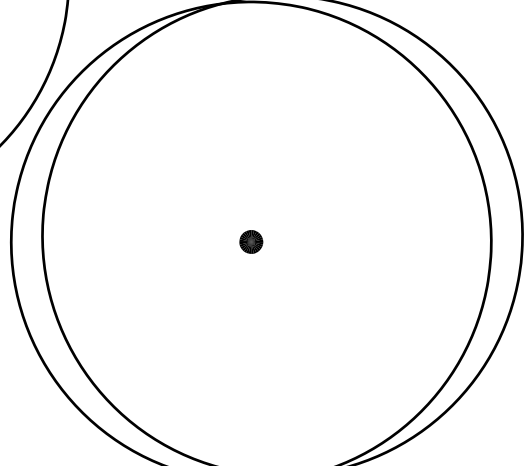
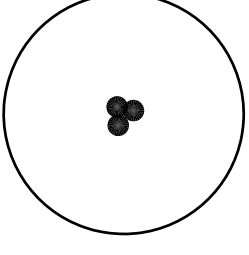


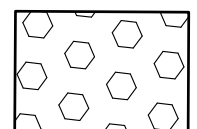
PERCIVAL  
LANDSCAPE  
ARCHITECTURE  
P.O. BOX 14676  
PORTLAND,  
OREGON  
97293  
perlandscape@gmail.com  
503-939-3547



**PLANT LEGEND**


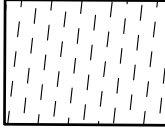
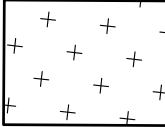
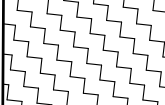
**symbol common name / botanic name: size, comments, (quantity)**

PLANTS OUTSIDE STORMWATER FACILITY

-  GINKGO / GINKGO BILOBA: 3" CALIPER; STRAIGHT SINGLE LEADER; MALE; (2)
-  TUPELO / NYSSA SYLVATICA: 2 1/2" CALIPER; SINGLE STRAIGHT LEADER; (5)
-  VINE MAPLE / ACER CIRCINATUM: 12 FT. HT. AND SPREAD; AT LEAST ONE-YEAR IN CONTAINER; MULTI-TRUNKED; (24)
-  BLACK MONDO GRASS / OPHIOPOGON PLARISCAPUS 'NIGRESCENS': 1 GALLON, (22)
-  SWORD FERN / POLYSTICUM MUNITUM: 1 GALLON; 18" MIN. SPREAD; (127)
-  SALAL / GAULTHERIA SHALLON: 1 GALLON @ 24" O.C.; (384)

PLANTS INSIDE STORMWATER FACILITY

(ALL PLANTS TO BE PLANTED AT 18" ON CENTER)

-  KELSEY (DWARF) DOGWOOD / CORNUS SERICEA 'KELSEY': 1 GALLON; (82)
-  COLUMBIA SEDGE / CARES APERTA: 1 GALLON; (112)
-  NEW ZEALAND ORANGE SEDGE / CAREX TESTACAE: 1 GALLON; (124)
-  COMMON RUSH / JUNCUS EFFUSUS: 1 GALLON; (112)

IN ADDITION AND IN-BETWEEN THE ABOVE PLANTS PROVIDE (100) 1 GALLON COMMON CAMAS / CAMASSIA QUAMASH (BLUE).

**THE MARYLHURST SCHOOL**

NEW LIFE CHURCH SITE  
19915 Old Lower River Road  
West Linn, Oregon, 97068

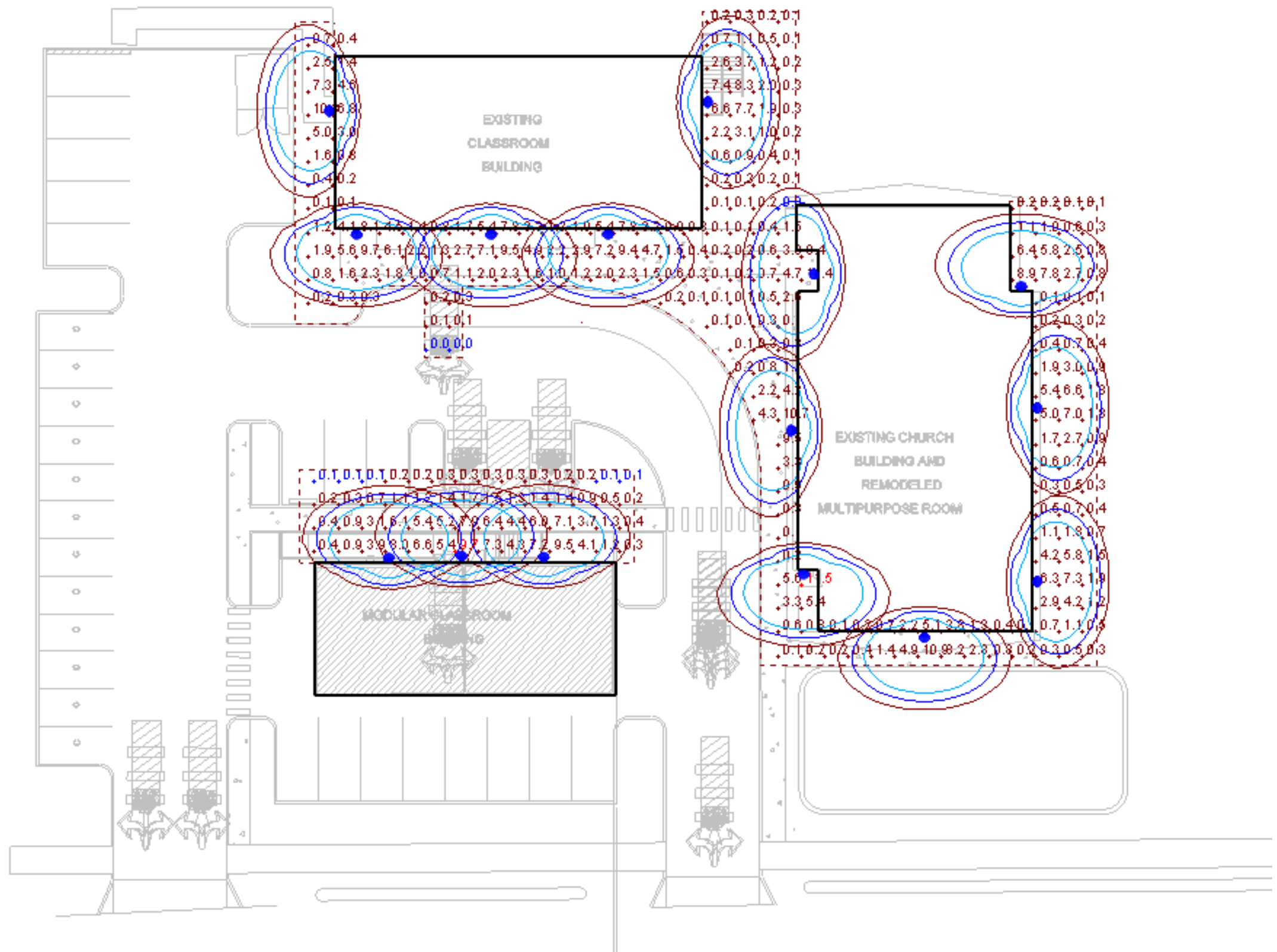
LAND USE APPLICATION

| Issue                | Revision | Date       |
|----------------------|----------|------------|
| LAND USE APPLICATION |          | 08.10.18   |
| DESIGN REVIEW REV.   |          | 11/05/2018 |

**PLANTING PLAN  
(NORTH AND EAST  
OF NEW BUILDING)**

Scale AS INDICATED  
Date 09.06.18

Sheet No.  
**L1.04**

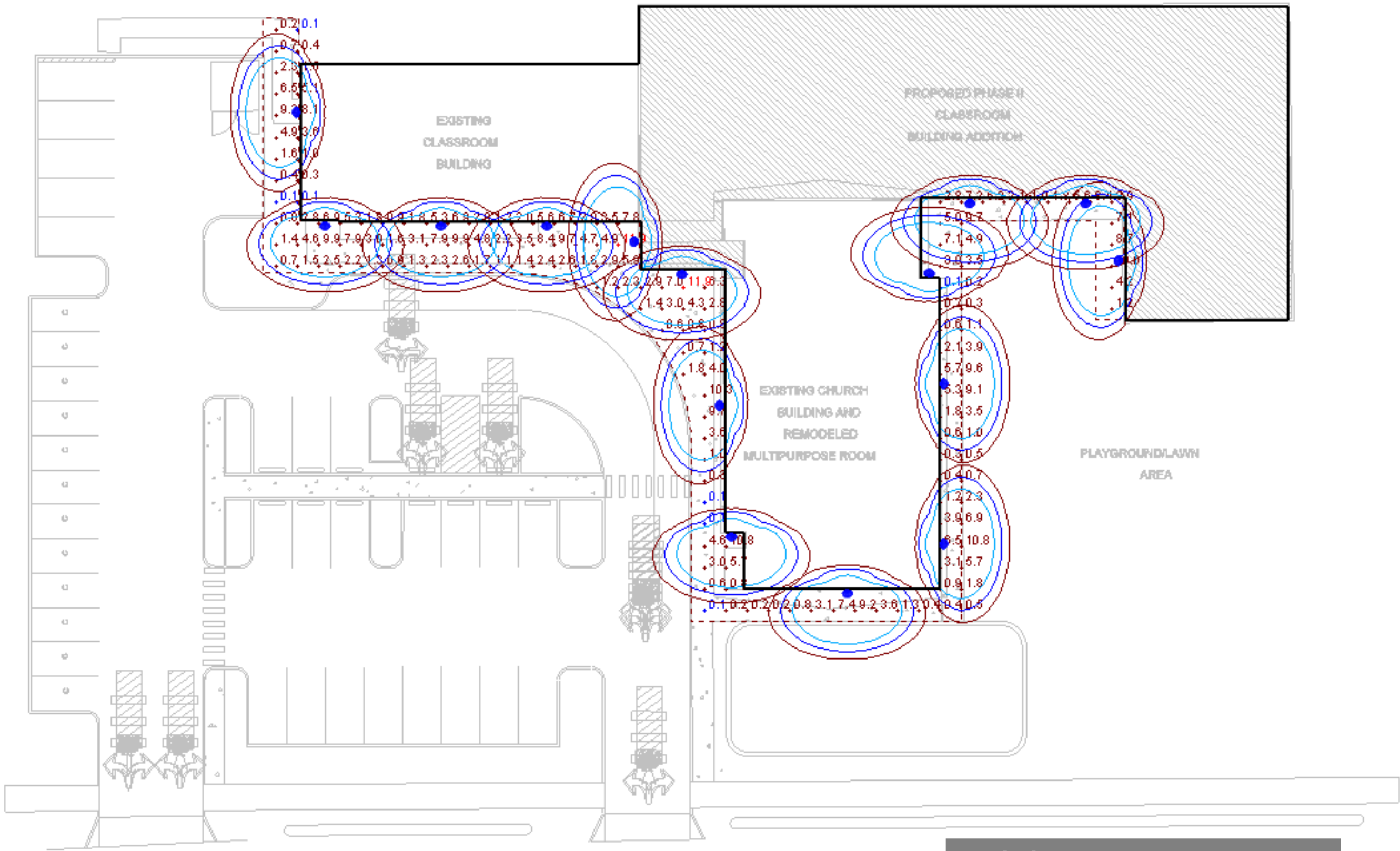


| Symbol | Label | Manufacturer      | Catalog Number          | Description                                                        | Lumens Per Lamp | Wattage |
|--------|-------|-------------------|-------------------------|--------------------------------------------------------------------|-----------------|---------|
|        | A     | Lithonia Lighting | WST LED P1 30K VW MVOLT | WST LED, Performance package 1, 3000 K, visual comfort wide, MVOLT | 1548            | 12      |

| Statistics   |        |        |         |        |
|--------------|--------|--------|---------|--------|
| Description  | Symbol | Avg    | Max     | Min    |
| Calc Zone #6 | +      | 2.6 fc | 9.7 fc  | 0.1 fc |
| Calc Zone #7 | +      | 3.6 fc | 11.9 fc | 0.1 fc |
| Calc Zone #8 | +      | 2.2 fc | 11.5 fc | 0.0 fc |

**Designer**  
 Rob Kenagy  
**Date**  
 10/30/2018  
**Scale**  
 Not to Scale  
**Drawing No.**  
  
**Summary**

Maryhurst School Phase II (recommended)



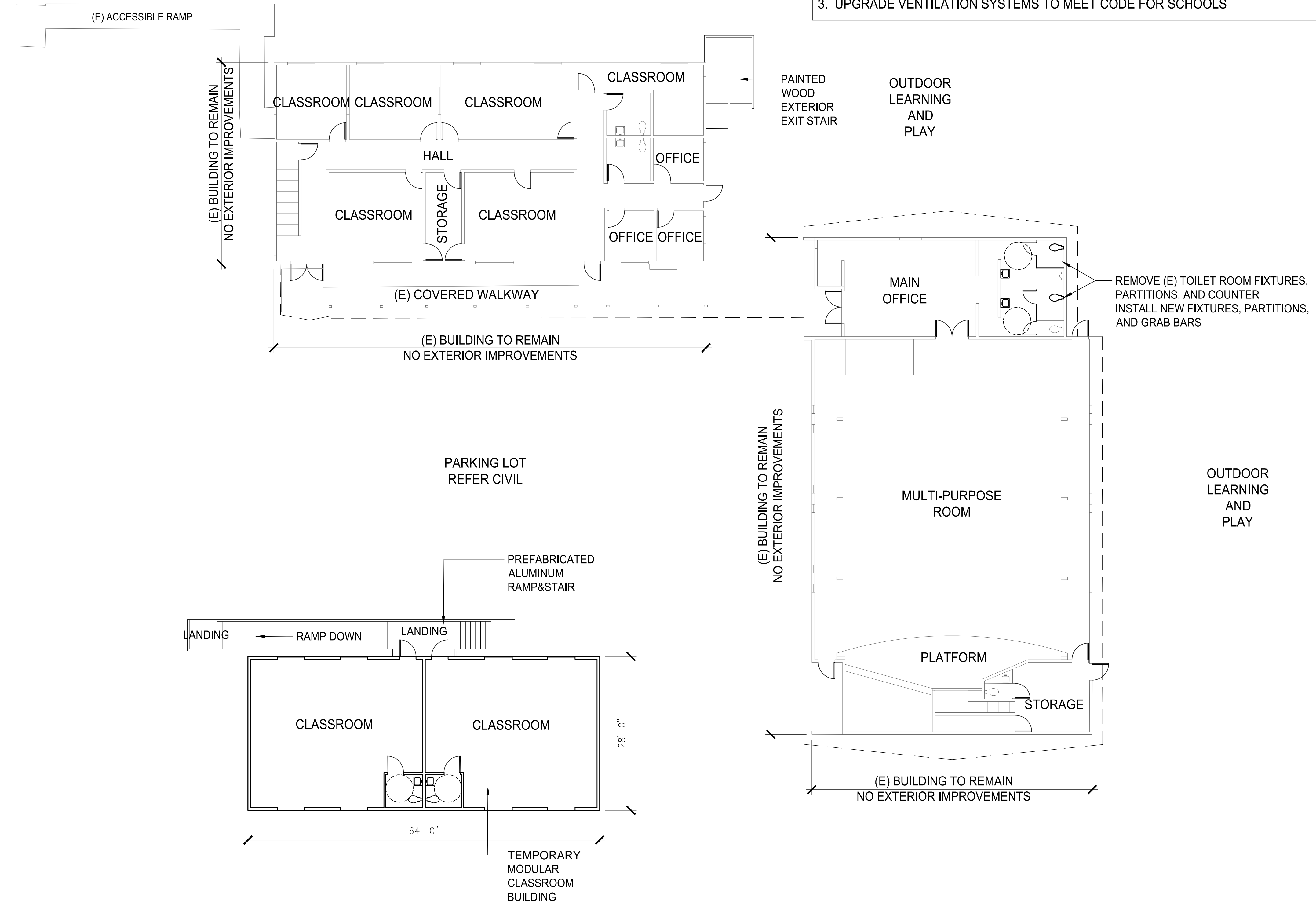
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| Symbol   | Label | Manufacturer      | Catalog Number          | Description                                                        | Lumens Per Lamp | Wattage |
|          | A     | Lithonia Lighting | WST LED P1 30K VW MVOLT | WST LED, Performance package 1, 3000 K, visual comfort wide, MVOLT | 1548            | 12      |

| Statistics   |        |        |         |        |
|--------------|--------|--------|---------|--------|
| Description  | Symbol | Avg    | Max     | Min    |
| Calc Zone #6 | +      | 2.6 fc | 9.7 fc  | 0.1 fc |
| Calc Zone #7 | +      | 3.6 fc | 11.9 fc | 0.1 fc |
| Calc Zone #8 | +      | 2.2 fc | 11.5 fc | 0.0 fc |

|                    |              |
|--------------------|--------------|
| <b>Designer</b>    | Rob Kenagy   |
| <b>Date</b>        | 10/30/2018   |
| <b>Scale</b>       | Not to Scale |
| <b>Drawing No.</b> |              |
| <b>Summary</b>     |              |



**EXISTING BUILDING IMPROVEMENTS:**  
 1. UPGRADE FIRE ALARM SYSTEM TO MEET CODE FOR SCHOOLS  
 2. UPGRADE LIGHT FIXTURES TO MEET EMERGENCY LIGHTING CODE FOR SCHOOLS  
 3. UPGRADE VENTILATION SYSTEMS TO MEET CODE FOR SCHOOLS



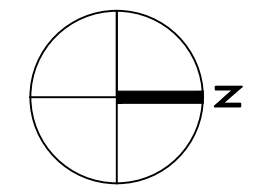
**THE MARYLHURST SCHOOL**  
 NEW LIFE CHURCH SITE  
 19915 Old Lower River Road  
 West Linn, Oregon, 97068

**LAND USE APPLICATION**

| Issue    | Revision  | Date     |
|----------|-----------|----------|
| LAND USE | SUBMITTAL | 09.04.18 |

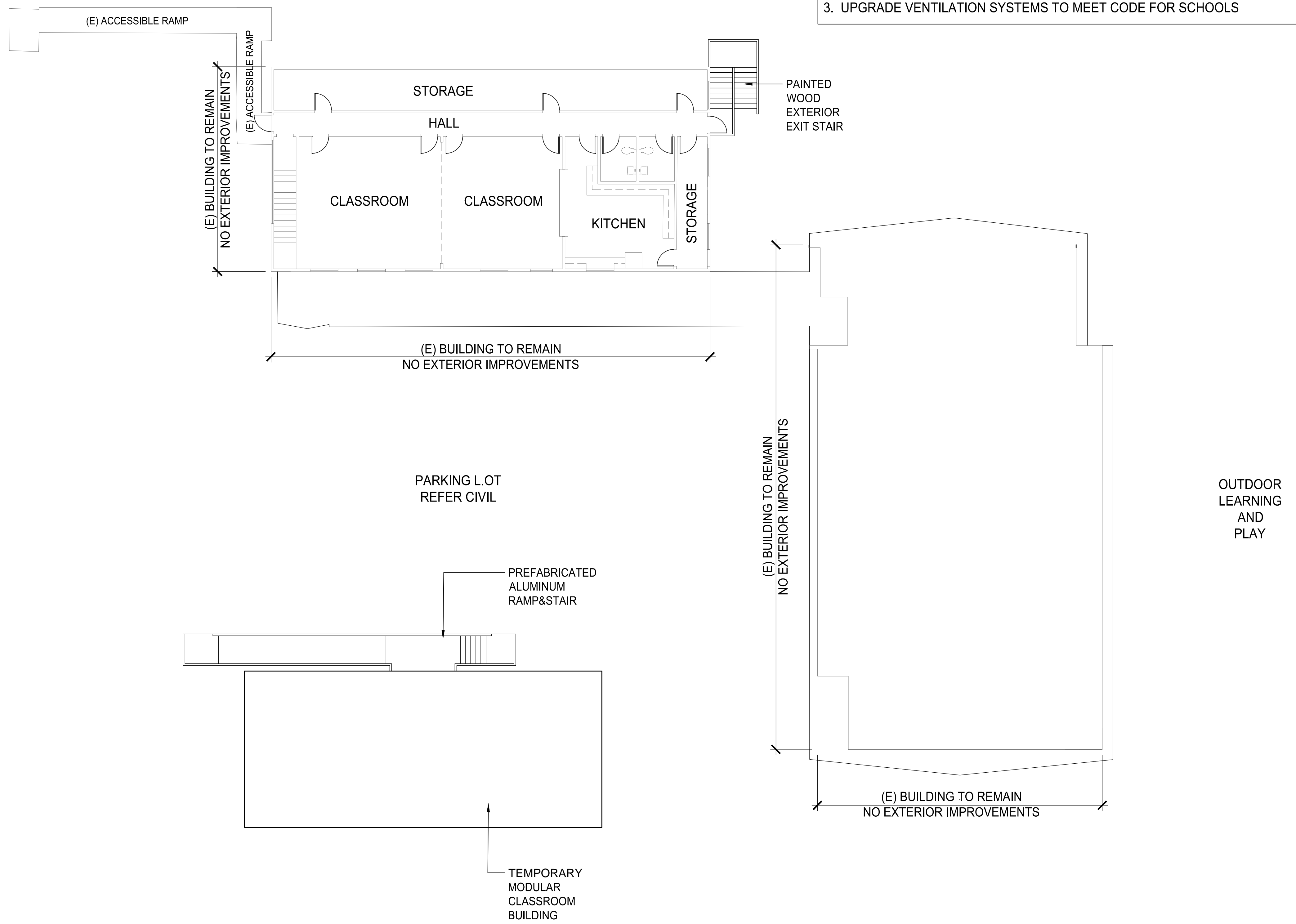
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**FIRST FLOOR PLAN**  
 Scale AS INDICATED  
 Date 11.02.18  
 Sheet No.

**1** PHASE I - FIRST FLOOR PLAN  
 SCALE: 3/32" = 1'-0"



**A101**

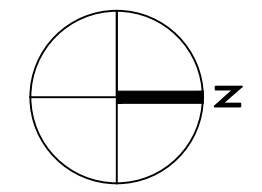
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 1. UPGRADE FIRE ALARM SYSTEM TO MEET CODE FOR SCHOOLS  
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 3. UPGRADE VENTILATION SYSTEMS TO MEET CODE FOR SCHOOLS



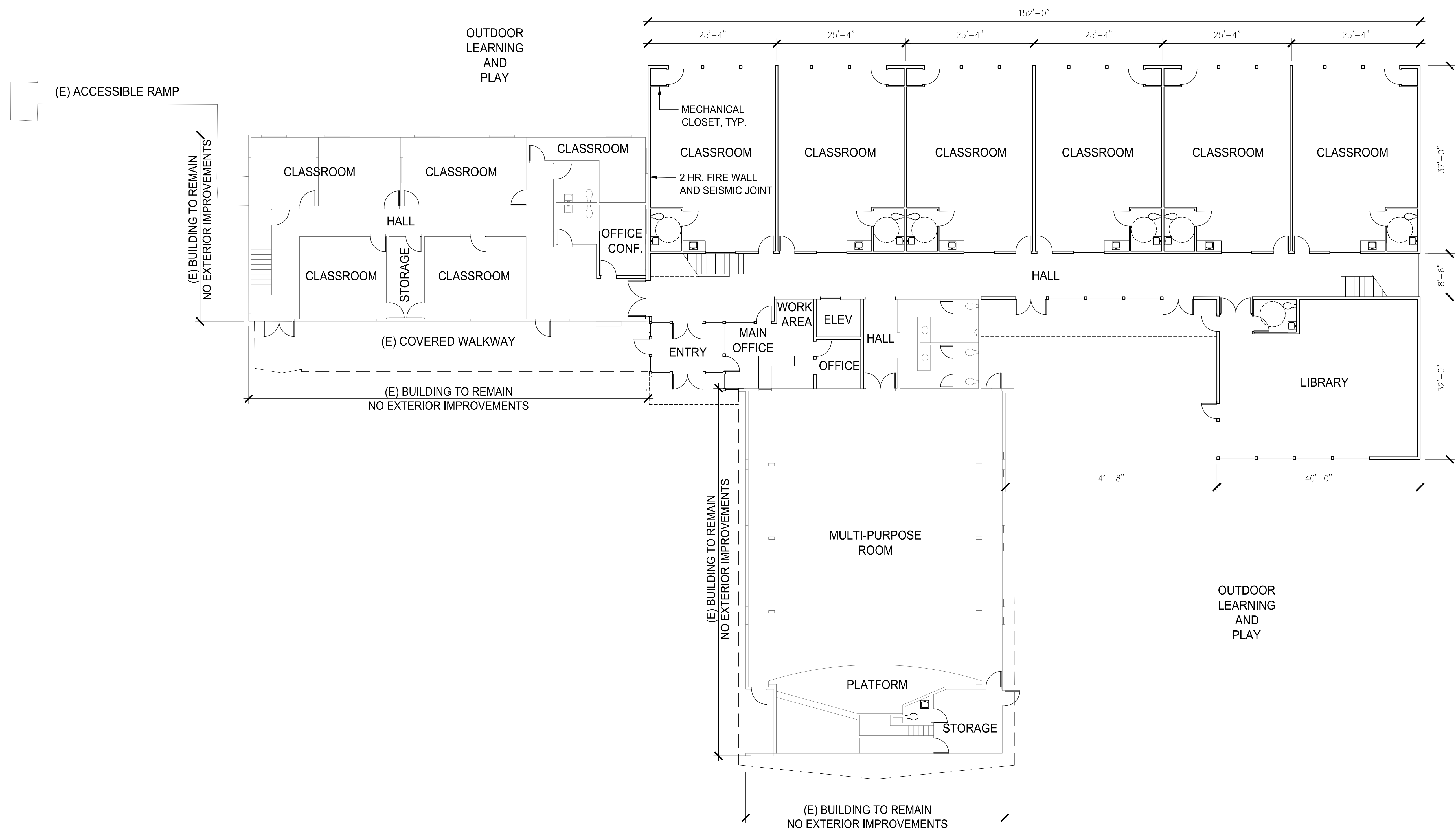
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|----------|-----------|----------|
| LAND USE | SUBMITTAL | 09.04.18 |

**PHASE I**  
**SECOND FLOOR PLAN**  
 Scale AS INDICATED  
 Date 11.02.18  
 Sheet No.

**1** PHASE I - SECOND FLOOR PLAN  
 SCALE: 3/32" = 1'-0"



**A102**

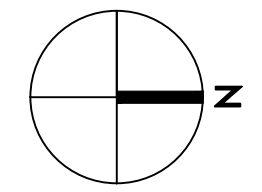


| Issue    | Revision  | Date     |
|----------|-----------|----------|
| LAND USE | SUBMITTAL | 09.04.18 |

**PHASE II**  
**FIRST FLOOR PLAN**

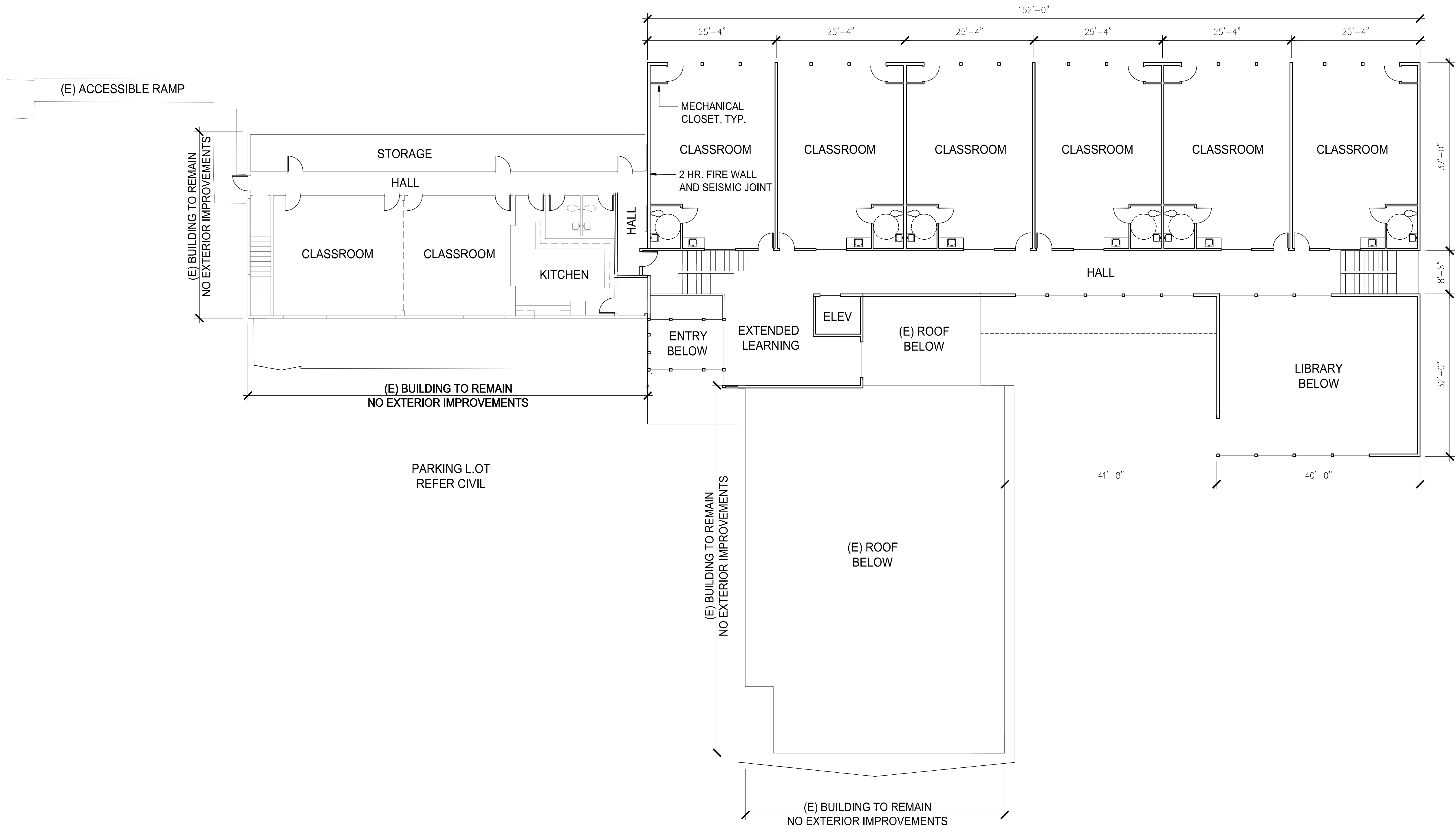
Scale AS INDICATED  
 Date 11.02.18

Sheet No.



**A201**

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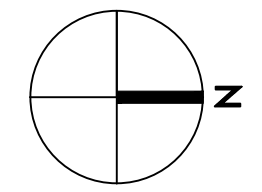


| Issue              | Revision | Date     |
|--------------------|----------|----------|
| LAND USE SUBMITTAL |          | 09.04.18 |

**1** PHASE II - SECOND FLOOR PLAN  
 SCALE: 3/32" = 1'-0"

**PHASE II**  
**SECOND FLOOR PLAN**  
 Scale AS INDICATED  
 Date 11.02.18

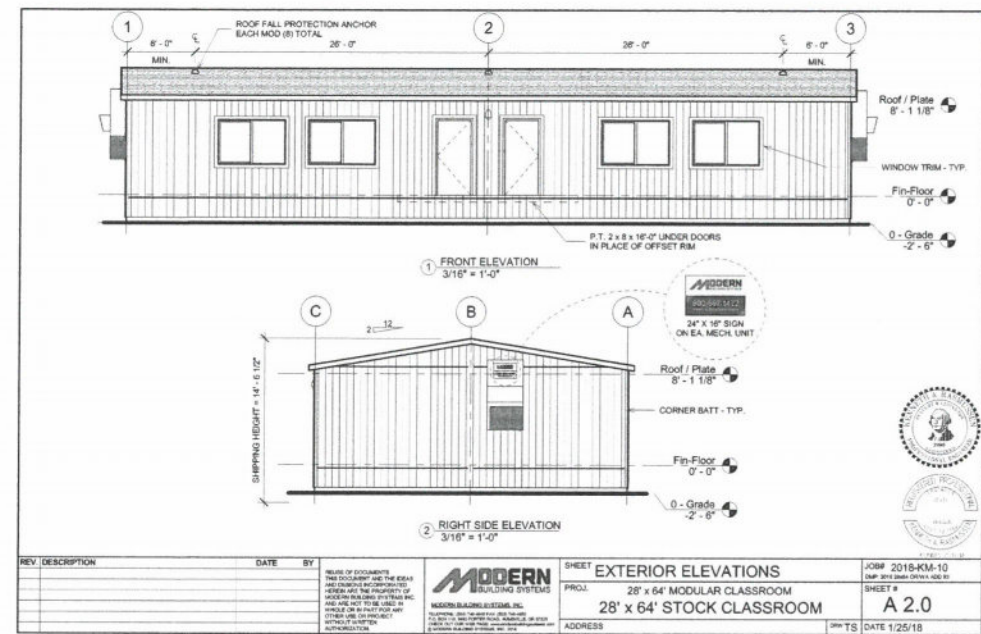
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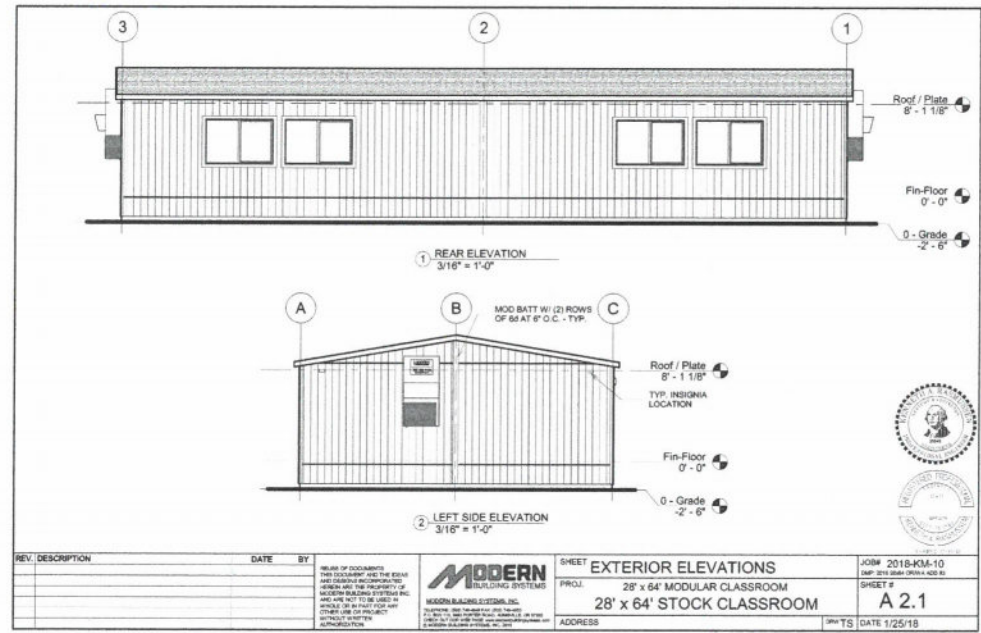
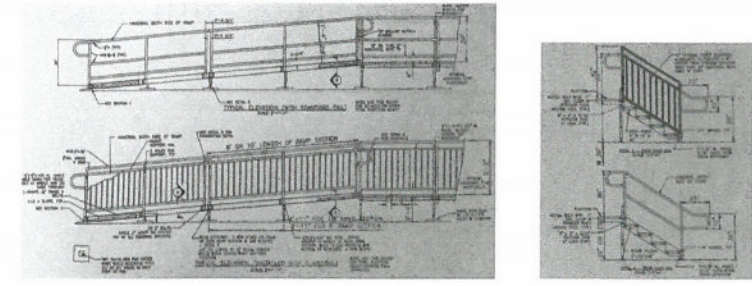
THE MARYLHURST SCHOOL

NEW LIFE CHURCH SITE  
19915 Old Lower River Road  
West Linn, Oregon, 97068

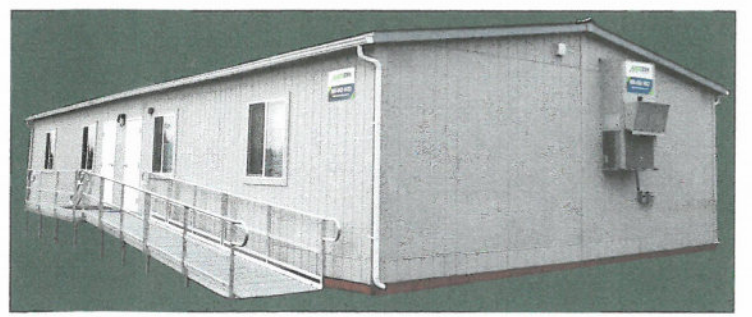
LAND USE APPLICATION



3 PHASE I - STAIR/RAMP SYSTEM EXTERIOR ELEVATIONS  
SCALE: NONE



2 PHASE I - MODULAR CLASSROOM BUILDING AND STAIR/RAMP SYSTEM COLORS  
SCALE: NONE  
2/6/2019 PC Meeting  
Page 161



NOTE: EXTERIOR ELEVATIONS ARE REPRESENTATIVE. ACTUAL MODULAR CLASSROOM BUILDING AND STAIR/RAMP SYSTEM MANUFACTURER, ELEVATIONS, AND COLOR MAY VARY.

1 PHASE I - MODULAR CLASSROOM BUILDING EXTERIOR ELEVATIONS  
SCALE: NONE

| Issue                | Revision | Date     |
|----------------------|----------|----------|
| LAND USE APPLICATION |          | 09.04.18 |

PHASE I  
EXTERIOR ELEVATIONS

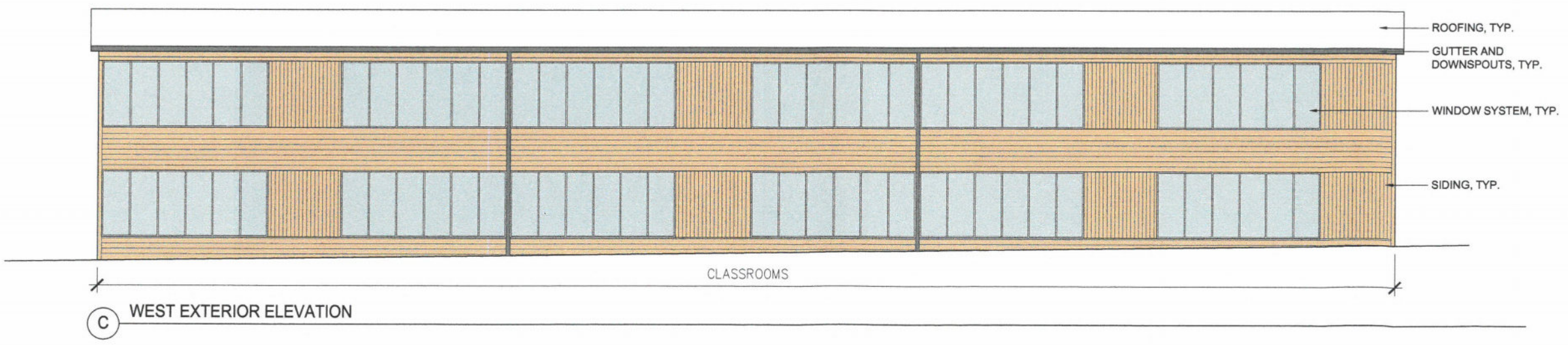
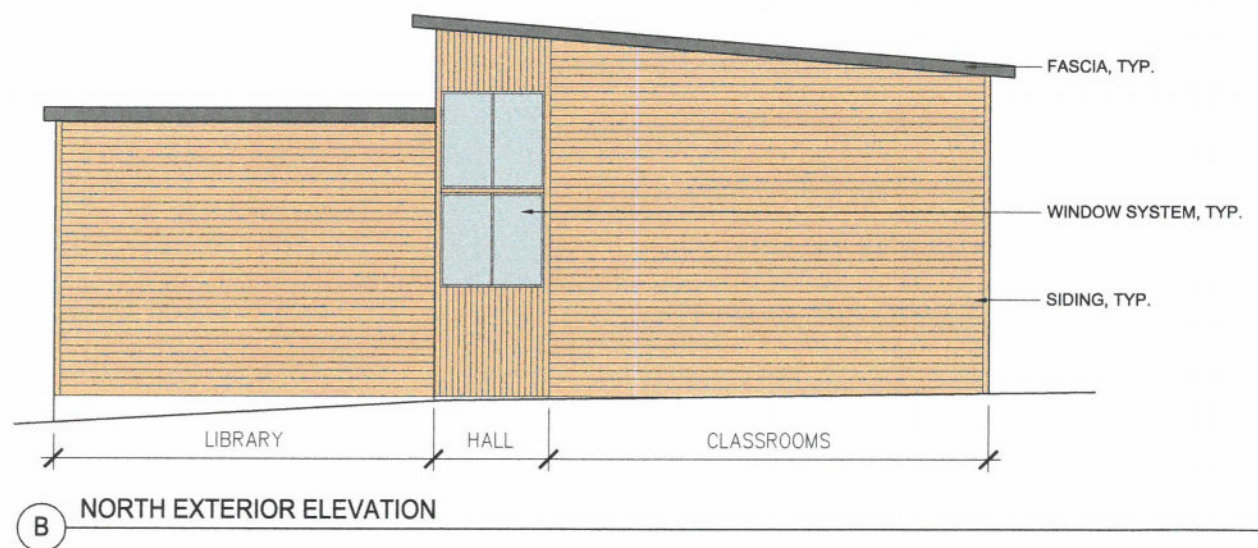
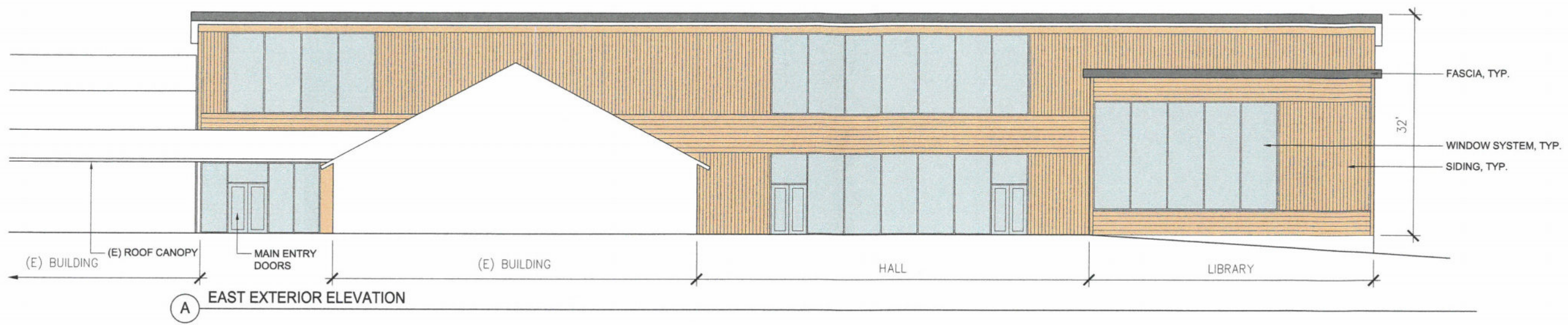
Scale AS INDICATED  
Date 09.04.18

Sheet No.

THE MARYLHURST SCHOOL

NEW LIFE CHURCH SITE  
19915 Old Lower River Road  
West Linn, Oregon, 97068

LAND USE APPLICATION



| Issue                | Revision | Date     |
|----------------------|----------|----------|
| LAND USE APPLICATION |          | 09.04.18 |

PHASE II  
EXTERIOR ELEVATIONS

Scale AS INDICATED  
Date 09.04.18

Sheet No.

**1** PHASE II - NEW CLASSROOM BUILDING EXTERIOR ELEVATIONS  
SCALE: 1/8" = 1'-0"

# Memorandum

Page 1 of 4



**DATE:** December 3, 2018

**PROJECT:** 1700622-The Marylhurst School

**SUBJECT:** Land Use Application – Stormwater Management Report

**TO:** The City of West Linn – Planning

**FROM:** Mark Wharry, PE

**PHONE:** 503-742-6060

KPFF Consulting Engineers

**PHONE:** 503-542-3860

**EMAIL:** Mark.Wharry@kpff.com

The proposed Marylhurst School project will include a new two-story building addition that will connect the existing school and church buildings on-site. The project will also involve parking lot improvements and right-of-way frontage upgrades. The sites impervious areas are collected via roof drains and catch basins and routed to an existing on-site storm drainage swale at the northeast corner of the property along Old River Road. The site drains from the southwest to the northeast at a slope of approximately 6.7%. The proposed project plans to maintain the basic drainage patterns that exist today.

This project is located in West Linn, Oregon and has met the City’s design standards outlined in the City of West Linn Public Works Design Standards Section 2 – Storm Drain Requirements. As modified by the City, West Linn follows the stormwater requirements and methods in the City of Portland Stormwater Manual. Since this development creates over 500 sf of new impervious area, water quality treatment is required in addition to flow control. The proposed stormwater management plan is to use a combination of conveyance piping and stormwater treatment planters to meet the water quality and quantity requirements.

## Water Quality

For water quality the site has been split into two basins. Basin 1 contains the new/redeveloped impervious parking lot and Basin 2 is the area of the new building. See Exhibit 1 in the attachments for more information on Basins 1 and 2. The facility for Basin 1 is to be an unlined/walled planter and the Basin 2 facility is to be a lined/walled planter next to the new school building. The water quality rain event for Clackamas County is a 1-year intensity storm with a total rainfall amount of 0.83 inches. The preliminary calculations are conservative in that the model does not account for additional drain rock storage or infiltration into the ground. In order to size the stormwater treatment planters and detention piping, Autodesk Storm and Sanitary Analysis 2018 computer program was used to model and size water quality treatment planters. Table 1 outlines the water quality calculations for Basin 1 and Basin 2. A public stormwater planter is also proposed to treat the proposed frontage improvements.

**Table 1:** Water Quality Summary

| Basin  | Treated Area (sf) | Facility Area (sf) | Storage Depth to Overflow | Peak Volume WQ Storm (cf) |
|--------|-------------------|--------------------|---------------------------|---------------------------|
| 1      | 29244             | 1950               | 10 inches                 | 1521                      |
| 2      | 7070              | 900                | 10 inches                 | 369                       |
| Public | 6612              | 130                | 6 inches                  | 65                        |

# Memorandum

Page 2 of 4  
December 3, 2018



## **Water Quantity**

Surface Detention is not feasible due to site restrictions including required setbacks, easements, and poor soils. Per correspondence with the City of West Linn Planning department, detention requirements may be waived if downstream analysis results show a conveyance capacity in the existing public storm system. Table 2 shows the basin area and percent impervious areas for both existing conditions and proposed conditions. Table 3 shows preliminary discharge rates for existing conditions and post developed.

**Table 2:** Existing Conditions and Post Developed Basin Areas

| Basin      | Total Basin Area | Percent Impervious Existing Conditions | Percent Impervious (new/redeveloped) Post-Developed |
|------------|------------------|----------------------------------------|-----------------------------------------------------|
| Total Site | 64429 sf         | 40%                                    | 56%                                                 |

**Table 3:** Existing Conditions and Post Developed Runoff

| Design Storm      | Existing Conditions | Post-Developed |
|-------------------|---------------------|----------------|
| 2 (2.5" in 24hr)  | 0.31 cfs            | 0.36 Cfs       |
| 5 (3.0" in 24hr)  | 0.41 cfs            | 0.46 cfs       |
| 10 (3.4" in 24hr) | 0.50 cfs            | 0.55 cfs       |
| 25 (3.9" in 24hr) | 0.61 cfs            | 0.66 cfs       |

## **Downstream Analysis**

A downstream analysis has been performed to analyze the downstream capacity of the existing storm system. Due to the site constraints and correspondence with City Planning, this analysis is being performed to potentially waive detention and flow control requirements for the proposed project. The downstream analysis is to be evaluated till the project site contributes less than 15% of the upstream drainage basin area or 1,500 feet downstream of the stormwater point of discharge. Per the overall basin map provided, the upstream area is approximately 10.5 acres with 60% of this area impervious. The project site is already less than 15% of the upstream basin, and the storm system outfalls into a creek that is less than 1,500 feet. The analysis extends to the creek outfall approximately 400 feet to the north of the proposed site. The creek continues east through a 36" culvert that goes under Old River Road. From here, KPFF was unable to continue its analysis due to unsuitable conditions.

## **Review of Resources**

KPFF reviewed the existing survey of the site provided by Compass Land Surveyors. The survey has provided rim and pipe inverts for existing storm structures in Old River Road along the site frontage. Since the upstream basin and downstream analysis consists of an area larger than what was surveyed, GIS maps from the City of West Linn were reviewed to generate the extents of the upstream basin area. SCS Soil Maps were also reviewed in this area to get an approximate hydrologic soil group. The City of West Linn planning staff noted that they had surveyed the City staff and the public for any drainage problems of which there were none. After reviewing these resources, an "Inspection of the Affected Area" was performed.



## ***Inspection of the Affected Area***

The engineer of record performed a visual inspection the site and upstream basin area that was readily accessible to view. More specifically, the inspection verified the drainage areas and existing stormwater infrastructure. The visual inspection of the drainage area appeared to match what was found in the GIS maps. After inspecting the upstream basin, an inspection of the downstream conveyance system was performed. The downstream conveyance system analyzed begins at a catch basin near the north property line of the project site. Pipe size and depth from rim noted in the survey were verified for this rectangular catch basin in field. Approximately 62 feet to the north, a circular area drain was found and confirmed pipe size and depth from rim, as noted in the survey. GIS Maps show a drain approximately 68 feet to the north of the circular drain, but was not found. Approximately 121 feet to the north of the circular area drain, a rectangular area drain that was not within the extents of the survey was found. The rim of the area drain was approximately 6-inches below surrounding pavement grade and the 12 inch concrete pipe invert was approximately 16 inches below the rim.

GIS Maps noted one more area drain before out falling into the creek, but could not be found during inspection. During visual inspection of the creek, culvert and headwalls, the 12 inch concrete outfall was found within the concrete headwall. This outfall was approximately 30 inches above concrete floor. A height of approximately 14 feet from road pavement to the bottom of 36 inch concrete culvert was measured. From this point, there was vegetation overgrowth that made it unsuitable to continue the analysis. Based on visual observation, there appeared to not be any existing or potential problems to the drainage system. Pictures of the inspection can be found in the attachments.

## ***Analysis of Downstream Effects***

Utilizing the survey, GIS maps, proposed site plan and notes from the site inspection, a stormwater model was developed to analyze the downstream conveyance capacity for the existing 12 inch concrete storm main in Old River Road. Per the City of West Linn Storm Drain Design Standards, the stormwater system must be able to convey the 10-year design storm (Section 2.0013C.6). The stormwater model utilizes the Santa Barbara Unit Hydrograph (SBUH) method to develop peak flows for design storms. A time of concentration of 56 minutes was calculated using engineering standard of practice equations for calculating time of concentration. The peak flow generated from the model was compared to the allowable flow in the 12 inch concrete pipes. The modeled 10-year peak flow for the 10.5 acre upstream basin is 3.53 cubic feet per second (cfs). If the existing pipe with the shallowest slope can convey the design storm, then the other pipes can convey it as well. From our analysis, the lowest slope is a 2.44%, 12 inch concrete pipe. The design flow capacity for this pipe is 5.57 cfs showing that the downstream conveyance system will be able to convey the design storm with the developed site and no detention.

As an additional check on the conveyance system, the model ran the 100 year design storm and resulted in a peak flow of 5.03 cfs which is still less than the design flow capacity listed above. Due to the outfalls height above the bottom of culvert and a design flow capacity of approximately 58 cfs through the 36 inch culvert (at 1% assumed), there is no concern for major drainage impacts in the conveyance system. The downstream analysis follows the requirements as laid out by City of Gresham and City of West Linn design standards and shows that there is pipe capacity to convey the 10-year and 100-year design storm event with the proposed development and no detention/flow control facility. Drainage from the proposed site will be captured in a stormwater treatment facility and then piped to the existing public storm main. The proposed project will not create drainage problems downstream.

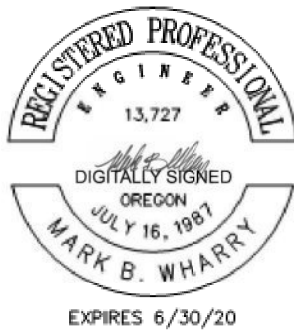
# Memorandum

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December 3, 2018



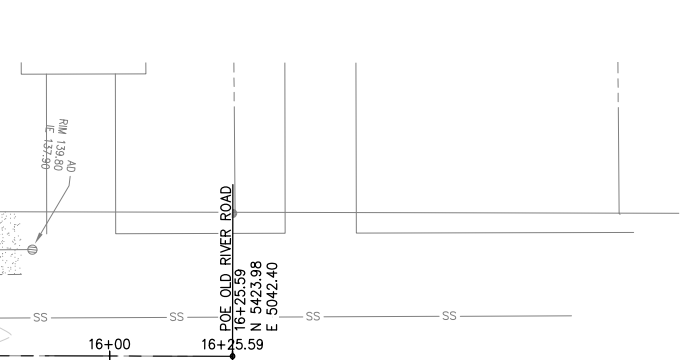
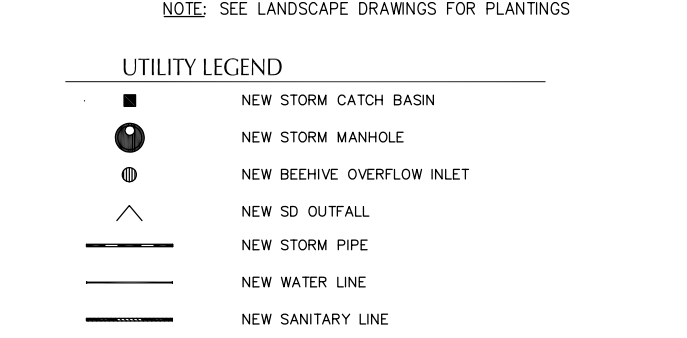
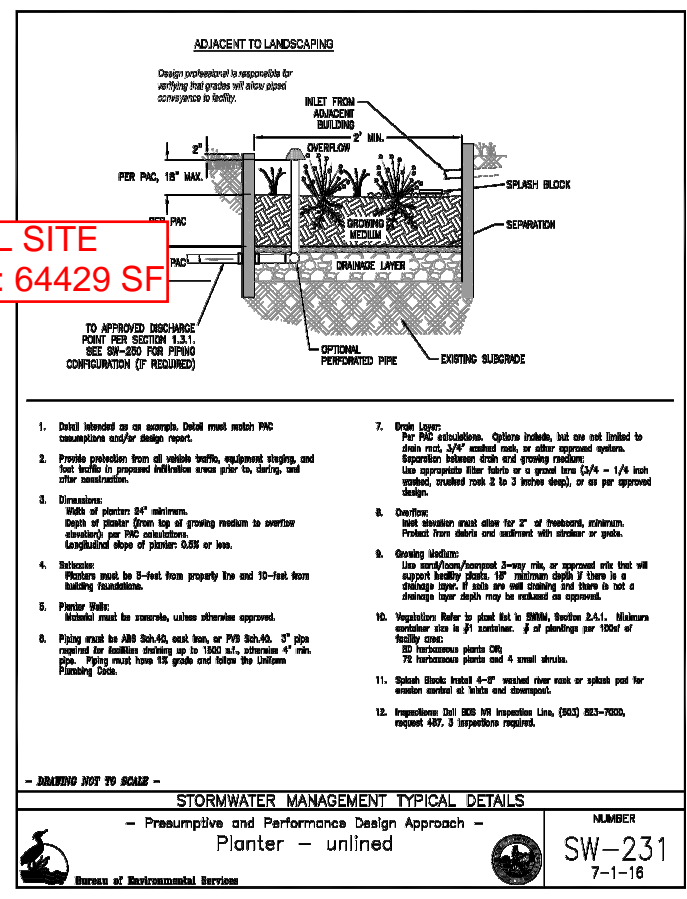
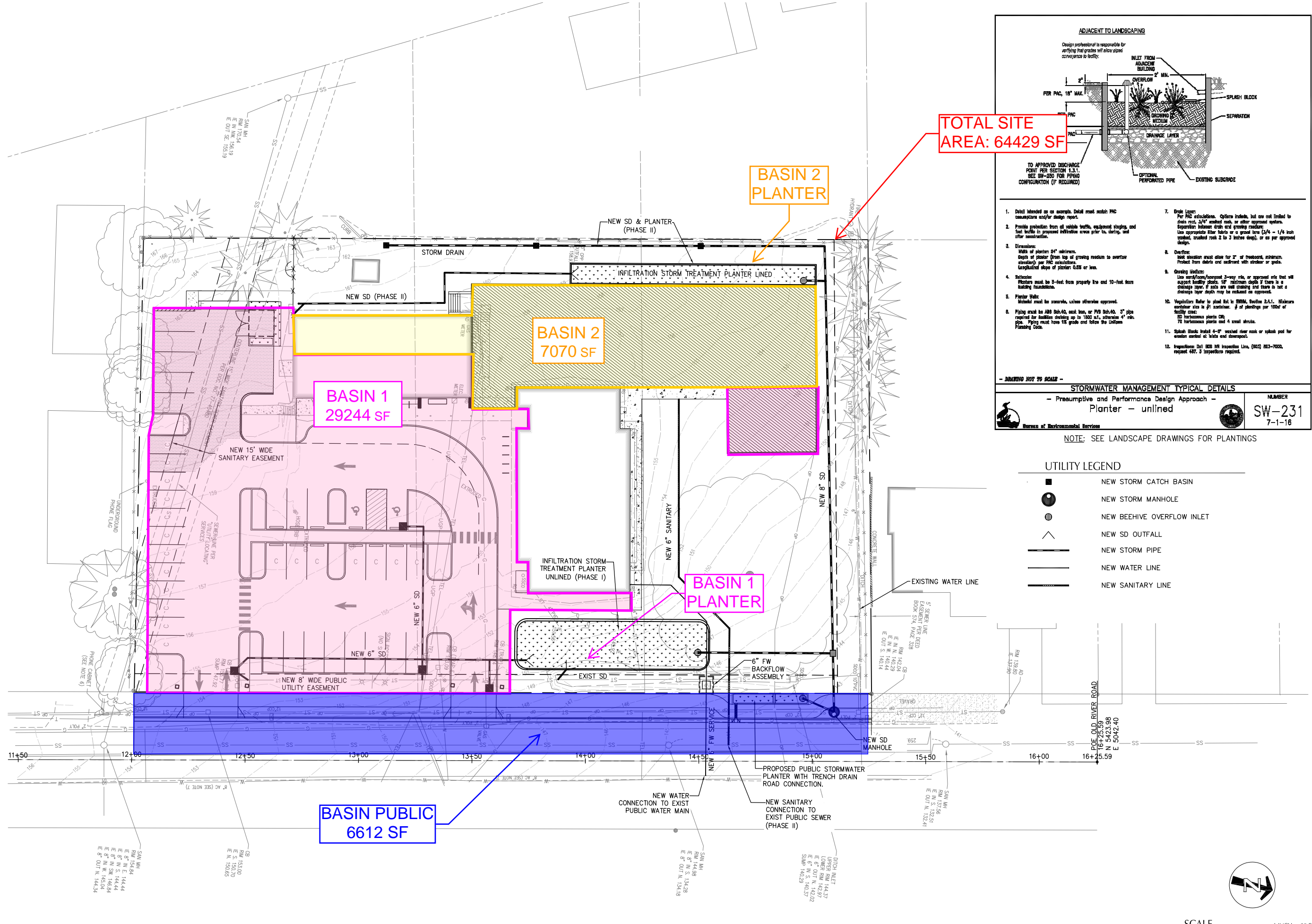
## Conveyance

On-site pipes will be sized to convey the 100-year design storm (4.5-inches in 24hr) per City of West Linn design standards. A preliminary conveyance analysis has been performed to analyze the 12-inch concrete public storm only pipe that continues north in Old River Road. Per survey information, the 12-inch concrete pipe is sloped at approximately 2.5%. Per Exhibit 3, the larger contributing basin is approximately 10 acres. With an approximate impervious area of 40% and time of concentration of approximately 30 minutes, a resulting peak flow of 5.41 cfs is calculated from the model. This 5.41 cfs does not factor detention and flow control from the proposed site or any other site within the 10-acre basin. A 12-inch concrete pipe at 2.5% with a manning's n value of 0.013 has a maximum capacity of 5.63 cfs.



Mark Wharry, PE  
KPFF Consulting Engineers

*Attachments: Basin Map, Utility Plan, Larger Contributing Basin Map, Downstream Analysis Pictures  
10101700622-pm*



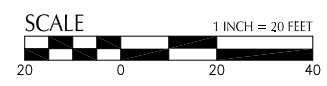
| Issue                | Revision | Date     |
|----------------------|----------|----------|
| LAND USE APPLICATION |          | 10.30.18 |

UTILITY PLAN

Scale AS INDICATED  
Date 08.10.18

Sheet No.

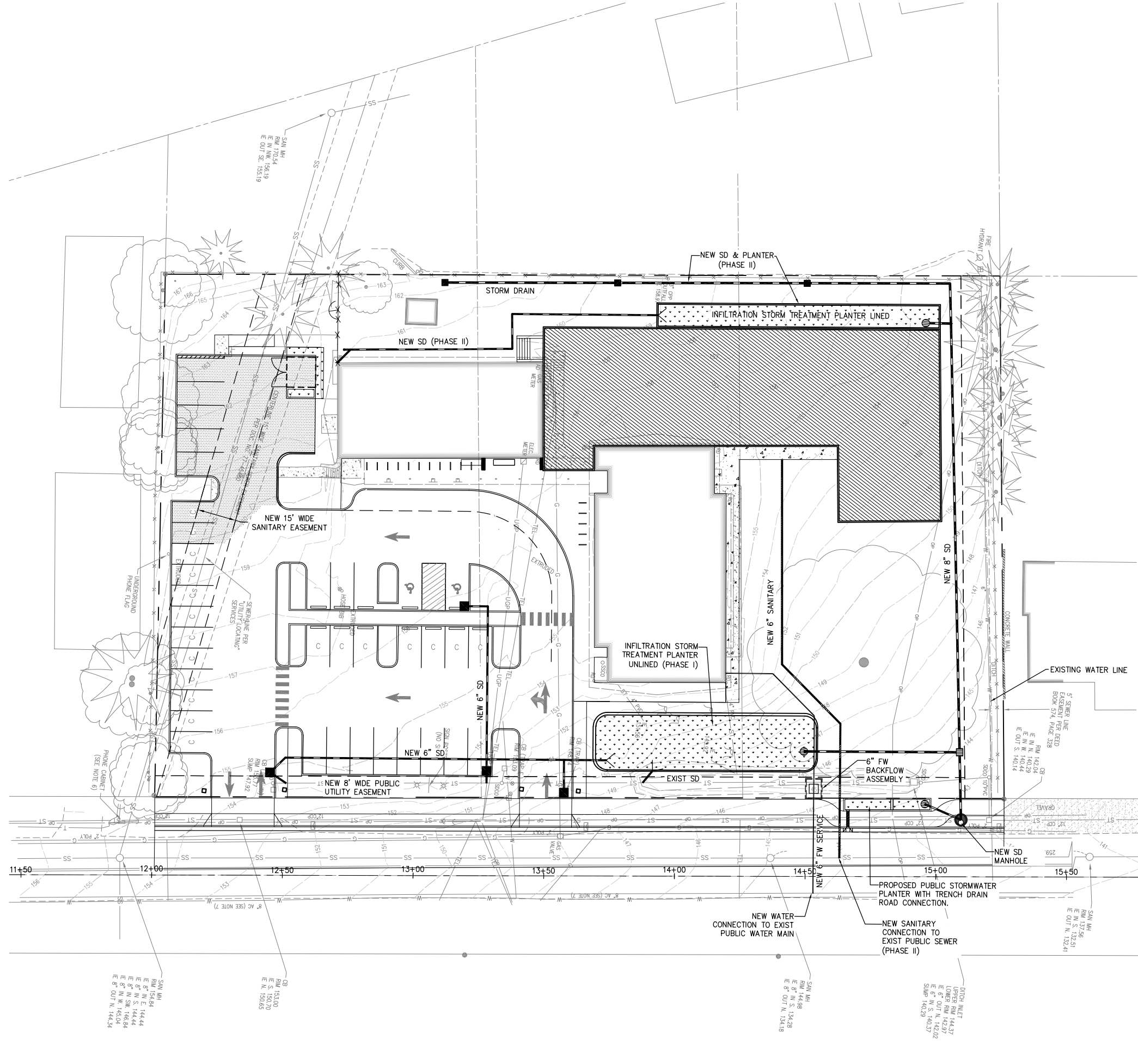
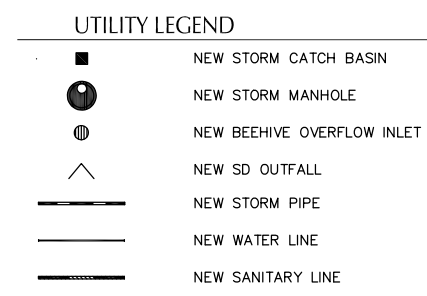
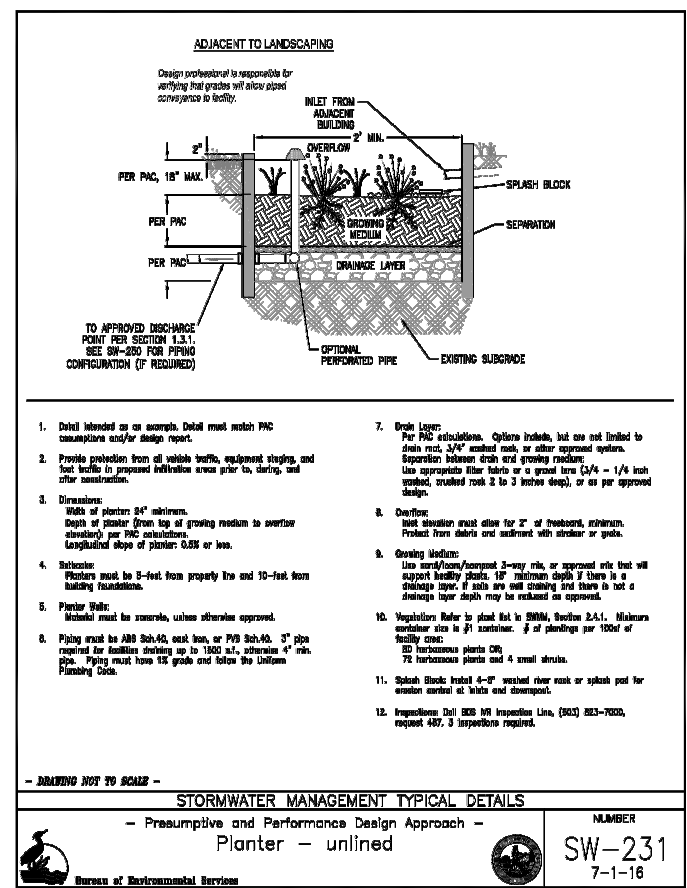
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**THE MARYLHURST SCHOOL**

NEW LIFE CHURCH SITE  
1915 Old Lower River Road  
West Linn, Oregon, 97068

**LAND USE APPLICATION**



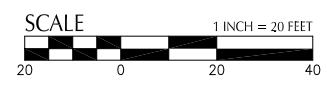
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|----------------------|----------|----------|
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**UTILITY PLAN**

Scale AS INDICATED  
Date 08.10.18

Sheet No.

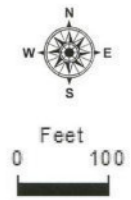
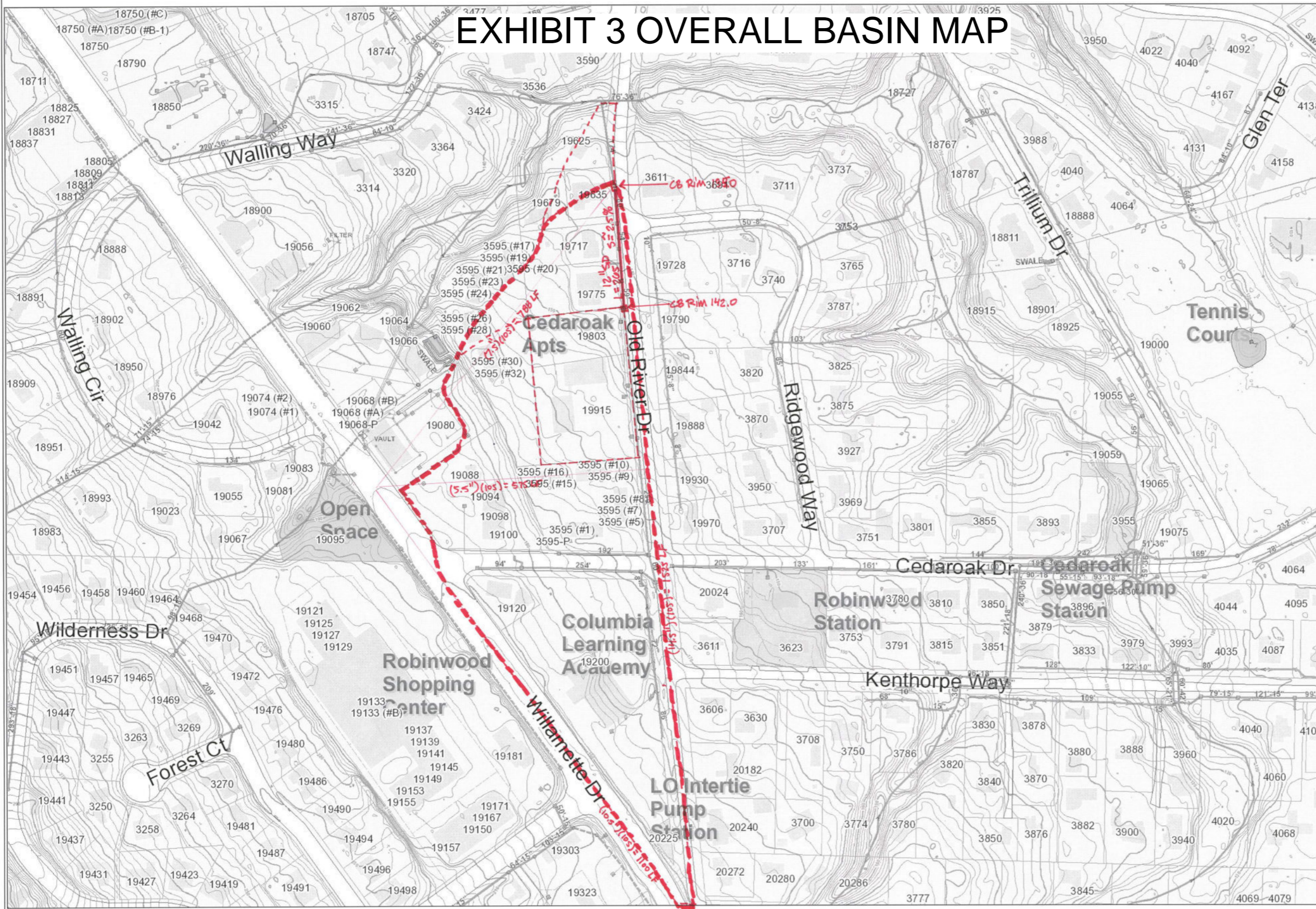
**C3.0**



# EXHIBIT 3 OVERALL BASIN MAP



BASIN AREA  $\approx \frac{(1525)(573)}{2} = 438,138 \text{ SF}$   
 $= 10.0 \text{ AC.}$   
 OVERLAND FLOW LENGTH = 1525 LF

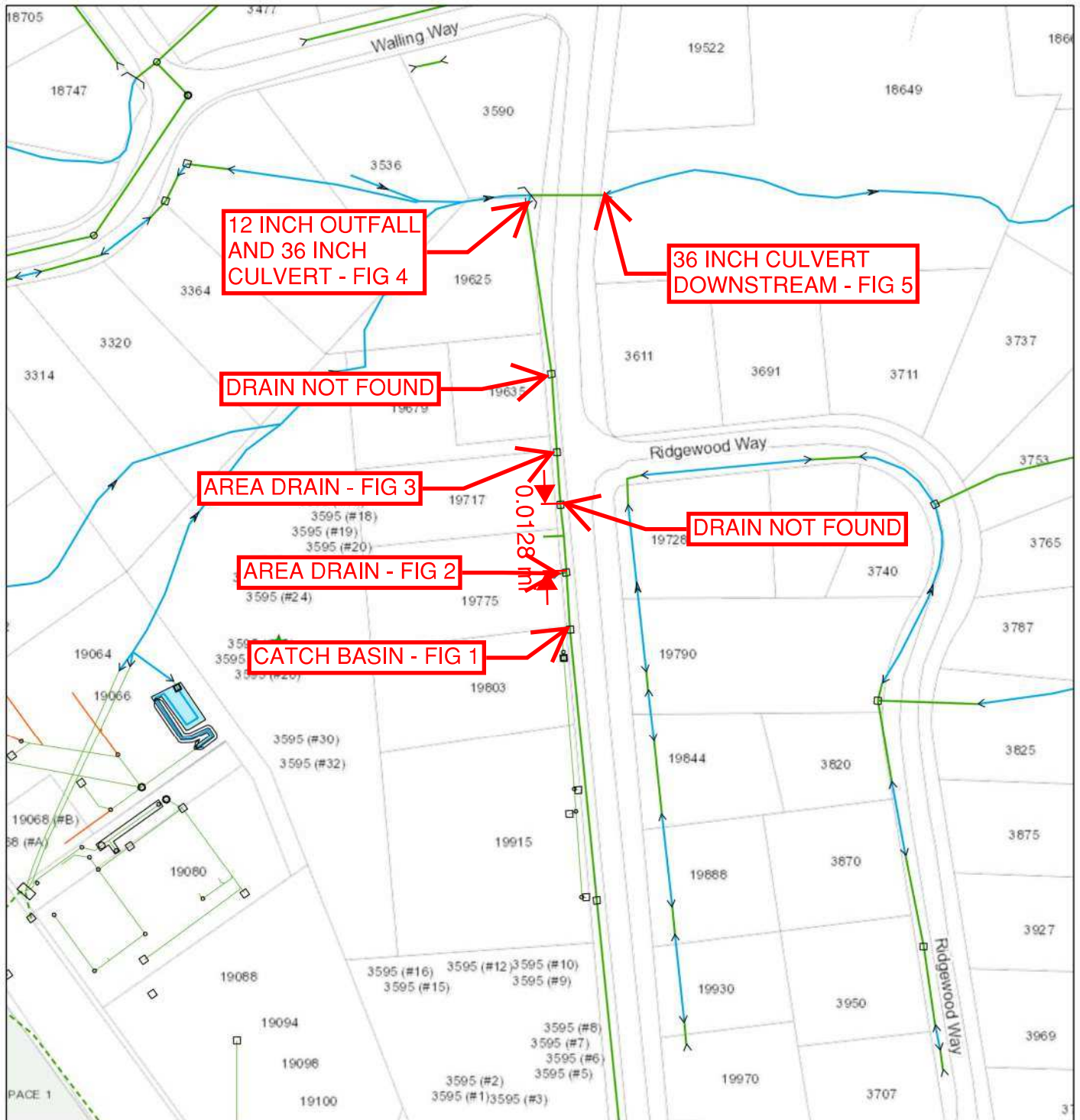


Map created by: public  
 Date Created: 18-Oct-18 01:26 PM

**WEST LINN GIS**

DISCLAIMER: This product is for informational purposes and may not have been prepared for, or be suitable for legal, engineering, or surveying purposes. Users of this information should review or consult the primary data and information sources to ascertain the usability of the information. Map scale is approximate. Source: West Linn GIS (Geographic Information System) MapOptix.

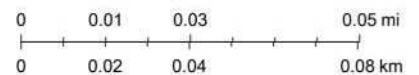
# West Linn GIS Stormwater Map



11/27/2018, 8:55:00 AM

- Storm Map Symbols
- Storm Lines
  - Storm Pipes
  - - - Storm Pipes ODOT
  - Ditches and Creeks
  - Private Pipes
  - Storm Laterals
  - Ponds
  - ★ West Linn Places (ID)

1:1,200



City of West Linn GIS



FIG-1 Existing catch basin in Old River Road downstream of proposed Marylhurst School site. 12 inch concrete pipe conveys to the North toward creek. 12 inch concrete pipe 1.9 feet below rim.



FIG-2 Existing circular area drain in Old River Road. Approximately 62 LF from catch basin. 12 inch concrete pipe found 1.9 feet below rim. Note that GIS maps note a drain where the mail boxes are. No drain was found. Road slopes vary between 2-5% per survey data.



FIG-3 Existing rectangular area drain found approximately 121 LF from circular area drain. Note that rim is approximately 6 inches below adjacent pavement grade. 12 inch concrete pipe found 16 inches below rim grade. Pipe continues North. GIS Map shows an additional area drain/catch basin, but was not found during the site visit.



FIG-4A Old River Road facing South approximately 14 feet above existing 36 inch culvert. Steep road slopes of approximately 4-8% toward the culvert crossing based on GIS maps.



FIG-4B 36 inch culvert and headwalls on the West side of Old River Road. 12 inch outfall is in the headwall to the right of the culvert. Though there are many leaves in the culvert, there does not appear to be any sign of drainage issues at the outfall or culvert. Dense brush and thorned bushes prevent continuation of the analysis downstream of culvert.



FIG-5 Dense bush creek facing East from Old River Road.



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**Report of  
Geotechnical Investigation  
Maryhurst School New Life Church Site Expansion  
19915 Old River Drive  
West Linn, Oregon**

**CGT Project Number G1804863**

Prepared for

Ms. Sheila Walker  
The Maryhurst School  
1232 Linn Avenue  
Oregon City, Oregon 97045

July 19, 2018

# Carlson Geotechnical

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July 19, 2018

Ms. Sheila Walker  
The Marylhurst School  
1232 Linn Avenue  
Oregon City, Oregon 97045

**Report of  
Geotechnical Investigation  
Marylhurst School New Life Church Site Expansion  
19915 Old River Drive  
West Linn, Oregon**

**CGT Project Number G1804863**

Dear Ms. Walker

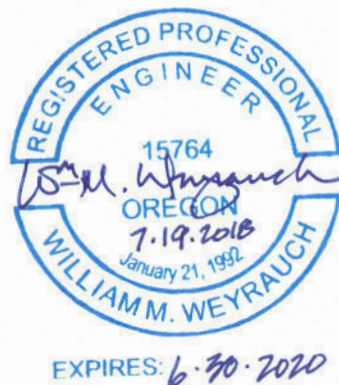
Carlson Geotechnical (CGT), a division of Carlson Testing, Inc. (CTI), is pleased to submit this report summarizing the results of our geotechnical investigation and infiltration testing services for the proposed Marylhurst School New Life Church Site Expansion project. The site is located at 19915 Old River Drive in West Linn, Oregon. We performed our work in general accordance with CGT Proposal GP7920, dated February 13, 2018. Written authorization for our services was received on May 23, 2018.

We appreciate the opportunity to work with you on this project. Please contact us at 503.601.8250 if you have any questions regarding this report.

Respectfully Submitted,  
**CARLSON GEOTECHNICAL**



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## 1.0 INTRODUCTION

Carlson Geotechnical (CGT), a division of Carlson Testing, Inc. (CTI), is pleased to submit this report summarizing the results of our geotechnical investigation and infiltration testing services for the proposed Marylhurst School New Life Church Site Expansion project. The site is located at 19915 Old River Drive in West Linn, Oregon, as shown on the attached Site Location map, Figure 1.

### 1.1 Project Information

CGT developed an understanding of the proposed project based information provided by Ms. Debra Pierson, AIA and Mr. Mark Wharry, P.E., of KPFF. Based on the information provided, we understand the project will include:

- Construction of a two-story, 12,000 square foot, classroom building north of the existing buildings at the site. We understand the building will be constructed out of shipping containers and will be supported on conventional shallow strip footings. No below-grade levels (basements) are anticipated. No structural loading has been provided; however, we anticipate maximum structural loading for the strip footing foundations will be on the order of 4 kips per lineal foot (klf). Per our correspondence, we understand the building occupant load will be less than 250, and therefore will not be classified as Risk Category III or IV per Table 1604.5 of the current Oregon Structural Specialty Code (OSSC).
- Rehabilitation and reconfiguration of the existing asphalt-paved parking lot and drive lanes to the south of the existing buildings. Subject to geotechnical and civil engineering review, the pavement rehabilitation may include complete removal and replacement (R&R), installation of an overlay, and/or surface treatments. Geotechnical assessment of the existing pavements has been requested to help develop plans for pavement rehabilitation.
- Although no grading plans were provided, we anticipate permanent grade changes at the site will include maximum cuts and fills on the order of 3 feet in depth.
- We understand that stormwater from new impervious areas of the site will be collected and diverted to an on-site stormwater facility located along Old River Road. Design of the stormwater facilities will rest with others.

### 1.2 Scope of Services

Our scope of work included the following:

- Contact the Oregon Utilities Notification Center to mark the locations of public utilities within a 20-foot radius of our explorations at the site. CGT also subcontracted a private utility locator service to mark the locations of detectable private utilities within the same radius.
- Conduct subsurface exploration of the site, as detailed in Appendix A.
- Conduct infiltration testing in two locations, as detailed in Appendix B.
- Perform a structural capacity evaluation for onsite pavements, as detailed in Appendix C.
- Perform a liquefaction analysis as detailed in Appendix D.
- Provide a technical narrative describing surface and subsurface materials, based on the results of our explorations, previous experience, and published geologic mapping.
- Provide recommendations for the Seismic Site Class, mapped maximum considered earthquake spectral response accelerations, and site seismic coefficients.

- Provide a qualitative evaluation of seismic hazards at the site, including earthquake-induced liquefaction, landsliding, and surface rupture due to faulting or lateral spread.
- Provide geotechnical recommendations for site preparation and earthwork.
- Provide geotechnical engineering recommendations for use in design and construction of shallow foundations, floor slabs, and pavements.
- Provide this written report summarizing the results of our geotechnical investigation and recommendations for the project.

## **2.0 SITE DESCRIPTION**

### **2.1 Site Geology**

Based on available geologic mapping of the area, the site is underlain by Pleistocene catastrophic flood deposits<sup>1,2,3</sup> originating from glacial outburst floods of Lake Missoula. The flood deposits were produced by the periodic failure of glacial ice dams that impounded Lake Missoula in present day Montana between 21,000 and 12,000 years ago. Floodwaters raged through Idaho, eastern Washington, and through the Columbia River Gorge. Near Rainier, Oregon, the river channel was restricted, causing floodwaters to back up the Willamette Valley as far south as Eugene. Floodwaters in the Portland area were as much as 400 feet deep, leaving only the tops of the tallest hills dry. The flood deposits are typically split into three different facies: the coarse-grained facies, the fine-grained facies, and the channel facies. Fine-grained Missoula flood deposits (Mff) are mapped in the vicinity of the site, which typically consist of silt, clay, and fine-grained sand. Beds are generally poorly defined and thin (less than 3 feet thick).

### **2.2 Site Surface Conditions**

The site was bordered by an existing single family residence to the north, apartment complexes to the west and south, and Old River Road to the east. The site was occupied by two existing buildings (to remain), the existing parking lot, grass lawns, and a children's play area. The area of the proposed building was gently descending to the north, vegetated with a grass lawn and several deciduous trees. Site layout and surface conditions at the time of our field investigation are shown on the attached Site Plan (Figure 2) and Site Photographs (Figure 3).

### **2.3 Subsurface Conditions**

#### **2.3.1 Subsurface Investigation & Laboratory Testing**

Our subsurface investigation consisted of three deep borings in the building area, two shallower borings for infiltration testing, and four shallow hand auger borings for the pavement investigation, completed in June 2018. The approximate exploration locations are shown on the Site Plan, attached as Figure 2. Details regarding the subsurface investigation, logs of the explorations, and results of laboratory testing

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<sup>1</sup> Ma, Madin, Duplantis, and Williams, 2012, Lidar-based Surficial Geologic Map and Database of the Greater Portland, Oregon, Area, Clackamas, Columbia, Marion, Multnomah, Washington, and Yamhill Counties, Oregon, and Clark County, Washington Oregon Department of Geology and Mineral Industries Open-File Report O-12-02.

<sup>2</sup> Madin, I.P., 2004. Geologic mapping and database for the Portland area fault studies: Final report, Clackamas, Multnomah, and Washington Counties, Oregon: Oregon Department of Geology and Mineral Industries, Open-File Report O-04-02, scale 1:100,000.

<sup>3</sup> Beeson, M.H., Tolan, T.L., and Madin, I.P., 1991. Geologic map of the Portland quadrangle, Multnomah and Washington counties, Oregon, and Clark County, Washington: Oregon Department of Geology and Mineral Industries, Geological Map Series 75, scale 1:24,000.

are presented in Appendix A. Subsurface conditions encountered during our investigation are summarized below.

### 2.3.2 Subsurface Materials

We encountered the following subsurface materials within our explorations at the site.

#### 2.3.2.1 Building Area

Borings B-1 through B-3 and IT-1 and IT-2 were advanced in the area of the proposed building and are detailed below.

Silt Fill (ML Fill): At the surface if IT-1 we encountered brown, dry to moist, low plasticity, silt fill with varying amounts of angular gravel. The fill extended to a depth of approximately 3 feet bgs.

Silt (ML): At the surface of B-1 through B-3, IT-2, and below the fill in IT-1, we encountered native, medium stiff to stiff, mottled brown, low plasticity silt. The silt generally became sandier with depth and included very soft to soft zones in B-1 (between 7 and 12 feet bgs), B-2 (between 12 and 20 feet bgs), and B-3 (between 7 and 15 feet bgs). The silt extended the full depth explored in B-1, 21½ feet bgs, and to depths of approximately 10 and 17 feet bgs in B-2 and B-3, respectively.

Poorly Graded Sand, Silty Sand, and Sandy Silt (SP, SM, ML): Beneath the silt in B-2 we encountered very loose to loose, brown, fine grained, poorly graded sand to silty sand with an intermediate layer of medium stiff, brown, low plasticity, sandy silt. These soils extended to a depth of approximately 20 feet bgs.

Fat Clay (CH): Beneath the sandy soils in B-2 and beneath the sandy silt in B-1, we encountered stiff to very stiff, gray, medium to high plasticity, fat clay. The fat clay extended the full depth explored in B-2, approximately 21½ feet bgs. The fat clay extended to a depth of approximately 35 feet bgs in B-3, below which depth it continued but was interbedded with dense to very dense, fine to medium grained, silty sand and clayey sand. These soils extended the full depth explored in B-3, approximately 61½ feet bgs.

#### 2.3.2.2 Pavement Areas

Explorations C-1 through C-4 were advanced within the existing pavement areas and are detailed below.

##### Asphalt Concrete Pavement

Asphalt concrete (AC) was encountered at the surface of explorations C-1 through C-4 and was approximately 3 to 4 inches thick.

##### Poorly Graded Gravel Fill (GP Fill)

Beneath the AC we encountered poorly graded gravel fill (base rock) that was approximately 9 to 14 inches thick. The gravel fill was generally dense, gray, angular to subangular, up to approximately 1 inch in size, and became silty in the lower few inches.

Silt (ML): Beneath the gravel fill in C-1 through C-3, we encountered native, mottled brown, moist, low plasticity silt that extended the full depth explored, approximately 2 to 2¼ feet bgs.

Fat Clay (CH): Beneath the gravel fill in C-4, we encountered mottled gray and brown, moist, medium to high plasticity, fat clay that extended the full depth explored, approximately 2½ feet bgs.

### 2.3.3 Groundwater

Groundwater was encountered at depths of 11½ and 19 feet bgs in B-1 and B-2, respectively, on June 14, 2018. Boring B-3 was advanced using the mud rotary (wet) drilling method, which precluded direct observation of groundwater during drilling, but groundwater was inferred at a depth of approximately 15 feet bgs based on examination of the soil samples.

We researched well logs available on the Oregon Water Resources Department (OWRD)<sup>4</sup> website for wells located nearby. Our review indicated that groundwater levels reported in the area generally ranged from about 13 to 25 feet bgs. It should be noted groundwater levels vary with local topography. In addition, the groundwater levels reported on the OWRD logs often reflect the purpose of the well, so water well logs may only report deeper, confined groundwater, while geotechnical or environmental borings will often report any groundwater encountered, including shallow, unconfined groundwater. Therefore, the levels reported on the OWRD well logs referenced above are considered generally indicative of local water levels and may not reflect actual groundwater levels at the project site.

The depth to groundwater map for the Portland area<sup>5</sup> indicates groundwater is present at depths of 60 to 70 feet bgs in the vicinity of the site. It should be noted that the levels reported by the referenced map are average values for a given location and incorporate a degree of uncertainty. For this location the uncertainty is described as “moderate.”

Recognizing the wide variability in observed and reported groundwater levels at and in the vicinity of the site, we conclude the groundwater observed within our borings is likely perched and reflects the variable permeability of the site soils. Perched groundwater is often discontinuous, both laterally and vertically, and can vary significantly through time (e.g. seasonally and annually). In general, we anticipate that groundwater levels will fluctuate due to seasonal and annual variations in precipitation, changes in site utilization, or other factors.

## 3.0 SEISMIC CONSIDERATIONS

### 3.1 Seismic Design

Section 1613.3.2 of the 2014 OSSC requires that the determination of the seismic site class be based on subsurface data in accordance with Chapter 20 of the ASCE 7-10. Recognizing the presence of liquefiable soils (discussed below), the site was initially assigned as Site Class F based on Section 1613.3.2 of the 2014 OSSC and Table 20.3-1 of ASCE 07-10. Designation as Site Class F typically requires a site-specific evaluation of ground response and spectral accelerations. However, ASCE 07-10 includes an exception to this in Section 20.3.1 of that manual. When the sole reason for classifying a site as Site Class F is due to the presence of liquefiable soils and the proposed structure(s) have a fundamental period of vibration equal to or less than 0.5 seconds (as anticipated for this project), a site

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<sup>4</sup> Oregon Water Resources Department, 2018. Well Log Records, accessed July 2018, from OWRD web site: [http://apps.wrd.state.or.us/apps/gw/well\\_log/](http://apps.wrd.state.or.us/apps/gw/well_log/).

<sup>5</sup> Snyder, D.T., 2008, Estimated depth to ground water and configuration of the water table in the Portland, Oregon area: U.S. Geological Survey, Scientific Investigations Report SIR-2008-5059, scale 1:60,000.

class is permitted to be determined based on standard penetration resistance, undrained shear strength, or shear wave velocity, in accordance with Section 20.3 of that manual.

Based on the results of the explorations, SPTs performed as part of our investigations, and review of geologic mapping, we have assigned the site as Site Class E for the subsurface conditions encountered. Earthquake ground motion parameters for the site were obtained based on the United States Geological Survey (USGS) Seismic Design Values for Buildings - Ground Motion Parameter Web Application<sup>6</sup>. The site Latitude 45.35547° North and Longitude 122.64021° West were input as the site location. The following table shows the recommended seismic design parameters for the site.

**Table 1 Seismic Ground Motion Values**

|                                              | Parameter                                              | Value  |
|----------------------------------------------|--------------------------------------------------------|--------|
| Mapped Acceleration Parameters               | Spectral Acceleration, 0.2 second ( $S_s$ )            | 0.965g |
|                                              | Spectral Acceleration, 1.0 second ( $S_1$ )            | 0.413g |
| Coefficients<br>(Site Class E)               | Site Coefficient, 0.2 sec. ( $F_A$ )                   | 0.942  |
|                                              | Site Coefficient, 1.0 sec. ( $F_V$ )                   | 2.400  |
| Adjusted MCE Spectral<br>Response Parameters | MCE Spectral Acceleration, 0.2 sec. ( $S_{MS}$ )       | 0.909g |
|                                              | MCE Spectral Acceleration, 1.0 sec. ( $S_{M1}$ )       | 0.992g |
| Design Spectral Response Accelerations       | Design Spectral Acceleration, 0.2 seconds ( $S_{DS}$ ) | 0.606g |
|                                              | Design Spectral Acceleration, 1.0 second ( $S_{D1}$ )  | 0.661g |
| Seismic Design Category                      |                                                        | D      |

### 3.2 Seismic Hazards

#### 3.2.1 Liquefaction

In general, liquefaction occurs when deposits of loose/soft, saturated, cohesionless soils, generally sands and silts, are subjected to strong earthquake shaking. If these deposits cannot drain quickly enough, pore water pressures can increase, approaching the value of the overburden pressure. The shear strength of a cohesionless soil is directly proportional to the effective stress, which is equal to the difference between the overburden pressure and the pore water pressure. When the pore water pressure increases to the value of the overburden pressure, the shear strength of the soil approaches zero, and the soil can liquefy. The liquefied soils can undergo rapid consolidation or, if unconfined, can flow as a liquid. Structures supported by the liquefied soils can experience rapid, excessive settlement, shearing, or even catastrophic failure. The Oregon Department of Geology and Mineral Industries' Oregon Statewide Geohazards Viewer<sup>7</sup> shows a high hazard for liquefaction for the site and immediate vicinity.

As discussed in Appendix D, the near-surface silt (ML), sandy silt (ML), silty sand (SM), and poorly graded sand (SP) are judged susceptible to liquefaction when adequately saturated and subjected to

<sup>6</sup> United States Geological Survey, 2018. Seismic Design Parameters determined using: "U.S. Seismic Design Maps Web Application," accessed July 2018, from the USGS website <http://earthquake.usgs.gov>.

<sup>7</sup> Oregon Department of Geology and Mineral Industries, 2018. Oregon Statewide Geohazards Viewer, accessed July 2018, from DOGAMI web site: <http://www.oregongeology.org/sub/hazvu/index.htm>.



design level earthquake shaking. These soils extended to depths of approximately 16 to 20 feet below ground surface (bgs) in the borings advanced in the location of the proposed building. Below these depths, the soils are considered non-liquefiable.

We performed quantitative liquefaction triggering and settlement analyses, the results of which are detailed in Appendix D. To reflect variable groundwater levels at the site, we modeled two groundwater scenarios; one reflecting the depth to groundwater observed in B-3 (15 feet bgs) and one reflecting a hypothetical, seasonal high (10 feet bgs). Our analyses predicted total, liquefaction-induced settlements of ½ to 1½ inches, increasing with shallower groundwater. In our opinion, these estimates effectively bracket the anticipated, liquefaction-induced settlements, which vary as a function of groundwater level. Shallow subsurface conditions encountered in the other borings advanced at the site were relatively uniform and we anticipate similarly liquefiable soil conditions. With regard to differential settlements, we recommend liquefaction-induced differential settlement be considered as one half of total settlement, or up to about ¾ inch. We recommend the differential settlement be assumed across the short axis of the structure.

### 3.2.2 Slope Instability

Due to the relatively minimal planned changes in site grade and relatively level topography at the site, the risk of slope instability at the site is considered low.

### 3.2.3 Surface Rupture

#### 3.2.3.1 Faulting

Although the site is situated in a region of the country with known active faults and historic seismic activity, no known faults exist on or immediately adjacent to the site. Therefore, the risk of surface rupture at the site due to faulting is considered low.

#### 3.2.3.2 Lateral Spread

Surface rupture due to lateral spread can occur on sites underlain by liquefiable soils that are located on or immediately adjacent to slopes steeper than about 3 degrees (20H:1V), and/or adjacent to a free face, such as a stream bank or the shore of an open body of water. During lateral spread, the materials overlying the liquefied soils are subject to lateral movement downslope or toward the free face. The topography across and immediately surrounding the site descends to the northeast at gradients of approximately 16H:1V. Considering the gently sloping topography, lack of local free faces, depth of 10 feet or more to perched groundwater (and therefore to liquefiable soils), and the anticipated discontinuous nature of saturated (and therefore liquefiable) soils, we conclude the risk of surface rupture due to lateral spread is considered very low.

## 4.0 CONCLUSIONS

Based on the results of our field explorations and analyses, the site may be developed as described in Section 1.1, provided the recommendations presented in this report are incorporated into the design and development. The principal geotechnical considerations for this project include:

- The presence of near-surface, moisture-sensitive soils that are susceptible to disturbance during wet weather.

- The presence of liquefiable soils in the site subsurface. As discussed above, our analyses indicate up to 1½ inches of total, liquefaction-induced settlement, with up to ¾ inch of associated differential settlement. We anticipate these settlements will be manageable using conventional shallow foundations (i.e. no mitigation will be required). We recommend the project structural engineer review these settlements and evaluate whether mitigation is warranted.
- The presence of very soft to soft and very loose to loose, compressible soils in the subsurface. These soils were encountered between depths of approximately 5 and 15 feet bgs in our borings. Soils above these depths were generally medium stiff to stiff. We recommend shallow foundations be established within 2 feet of existing site grades and be founded on medium stiff to stiff, native silt to sandy silt (ML). If foundations are to be established at lower elevations, remedial measures may be warranted. CGT should review finalized foundation and grading plans to determine if further analysis and supplemental recommendations are warranted.

## 5.0 RECOMMENDATIONS

The recommendations presented in this report are based on the information provided to us, results of our field investigation and analyses, laboratory data, and professional judgment. CGT has observed only a small portion of the pertinent subsurface conditions. The recommendations are based on the assumptions that the subsurface conditions do not deviate appreciably from those found during the field investigation. CGT should be consulted for further recommendations if the design of the proposed development changes or variations or undesirable geotechnical conditions are encountered during site development.

### 5.1 Site Preparation

#### 5.1.1 Stripping

Existing vegetation, rooted soils, and undocumented fill soils (if encountered) should be removed from within, and for a minimum 5-foot margin around, proposed building pad and exterior hardscaping areas. Based on the results of our field explorations, topsoil stripping depths are anticipated to be less than ½ foot bgs. Undocumented fill was encountered only in IT-1 to a depth of approximately 3 feet bgs, on the eastern margin of the building area, and was not encountered in borings B-1 through B-3, located within the proposed footprint of the building. These materials may be deeper or shallower at locations away from the completed explorations. The geotechnical engineer or his representative should provide recommendations for actual stripping depths based on observations during site stripping. Stripped surface vegetation and rooted soils should be transported off-site for disposal, or stockpiled for later use in landscaped areas. Stripped, inorganic fill materials should be transported off-site for disposal, or may be stockpiled for later use as structural fill as described in Section 5.4.1 of this report.

#### 5.1.2 Grubbing

Grubbing of trees should include the removal of the root mass and roots greater than ½-inch in diameter. Grubbed materials should be transported off-site for disposal. Root masses from larger trees may extend several feet bgs. Where root masses are removed, the resulting excavation should be properly backfilled with structural fill in conformance with Section 5.4.2 of this report.

### 5.1.3 Existing Utilities & Below-Grade Structures

All existing utilities at the site should be identified prior to excavation. Abandoned utility lines beneath the new building, pavements, and hardscaping features should be completely removed or grouted full. Soft, loose, or otherwise unsuitable soils encountered in utility trench excavations should be removed and replaced with structural fill in conformance with Section 5.4 this report. Buried structures (i.e. footings, foundation walls, retaining walls, slabs-on-grade, tanks, etc.), if encountered during site development, should be completely removed and replaced with structural fill in conformance with Section 5.4 of this report.

### 5.1.4 Subgrade Preparation

After site preparation as recommended above, but prior to placement of structural fill and/or aggregate base, the geotechnical engineer or their representative should observe the exposed subgrade soils in order to identify areas of excessive yielding through either proof rolling or probing. Proof rolling of subgrade soils is typically conducted during dry weather using a fully-loaded, 10- to 12-cubic-yard, tandem-axle, tire-mounted, dump truck or equivalent weighted water truck. Areas of limited access or that appear too soft or wet to support proof rolling equipment should be evaluated by probing. During wet weather, subgrade preparation should be performed in general accordance with the recommendations presented in Section 5.3 of this report. If areas of soft soil or excessive yielding are identified, the affected material should be over-excavated to firm, stable subgrade, and replaced with imported granular structural fill in conformance with Section 5.4.2 of this report.

### 5.1.5 Erosion Control

Erosion and sedimentation control measures should be employed in accordance with applicable City, County, and State regulations.

## **5.2 Temporary Excavations**

### 5.2.1 Overview

Conventional earthmoving equipment in proper working condition should be capable of making necessary excavations for the anticipated site cuts as described earlier in this report. All excavations should be in accordance with applicable OSHA and state regulations. It is the contractor's responsibility to select the excavation methods, to monitor site excavations for safety, and to provide any shoring required to protect personnel and adjacent improvements. A "competent person", as defined by OR-OSHA, should be on-site during construction in accordance with regulations presented by OR-OSHA. CGT's current role on the project does not include review or oversight of excavation safety.

### 5.2.2 OSHA Soil Type

For use in the planning and construction of temporary excavations up to 10 feet in depth, an OSHA soil type "C" should be used for the silt and sandy silt (ML) soils encountered near the surface of the site.

### 5.2.3 Utility Trenches

Temporary trench cuts should stand near vertical to depths of approximately 4 feet in the native, silt and sandy silt (ML) soils encountered near the surface of the site. If groundwater seepage undermines the stability of the trench, or if sidewall caving is observed during excavation, the sidewalls should be

flattened or shored. Depending on the time of year trench excavations occur, trench dewatering may be required in order to maintain dry working conditions. Pumping from sumps located within the trench will likely be effective in removing water resulting from seepage. If groundwater is encountered which can not be removed by dewatering, we recommend placing trench stabilization material at the base of the excavations. Trench stabilization material should be in conformance with Section 5.4.4.

#### 5.2.4 Excavations Near Foundations

Excavations near footings should not extend within a 1½H:1V (horizontal:vertical) plane projected out and down from the outside, bottom edge of the footings. In the event excavation needs to extend below the referenced plane, temporary shoring of the excavation and/or underpinning of the subject footing may be required. The geotechnical engineer should be consulted to review proposed excavation plans for this design case to provide specific recommendations.

### 5.3 **Wet Weather Considerations**

For planning purposes, the wet season should be considered to extend from late September to late June. It is our experience that dry weather working conditions should prevail between early July and mid-September. Notwithstanding the above, soil conditions should be evaluated in the field by the geotechnical engineer or their representative at the initial stage of site preparation to determine whether the recommendations within this section should be incorporated into construction.

#### 5.3.1 Overview

The near-surface silt and sandy silt (ML) soils are susceptible to disturbance during wet weather. Trafficability of these soils may be difficult, and significant damage to subgrade soils could occur, if earthwork is undertaken without proper precautions at times when the exposed soils are more than a few percentage points above optimum moisture content. For wet weather construction, site preparation activities may need to be accomplished using track-mounted equipment, loading removed material onto trucks supported on granular haul roads, or other methods to limit soil disturbance. The geotechnical engineer or their representative should evaluate the subgrade during excavation by probing rather than proof rolling. Soils that have been disturbed during site preparation activities, or soft or loose areas identified during probing, should be over-excavated to firm, stable subgrade, and replaced with imported granular structural fill in conformance with Section 5.4.2.

#### 5.3.2 Geotextile Separation Fabric

We recommend a geotextile separation fabric be placed to serve as a barrier between the prepared subgrade and granular fill/base rock in areas of repeated or heavy construction traffic. The geotextile fabric should meet the requirements presented in the current Oregon Department of Transportation (ODOT) Standard Specification for Construction, Section 02320.

#### 5.3.3 Granular Working Surfaces (Haul Roads & Staging Areas)

Haul roads subjected to repeated heavy, tire-mounted, construction traffic (e.g. dump trucks, concrete trucks, etc.) will require a minimum of 18 inches of imported granular material. For light staging areas, 12 inches of imported granular material is typically sufficient. Additional granular material or geo-grid reinforcement may be recommended based on site conditions and/or loading at the time of construction. The imported granular material should be in conformance with Section 5.4.2 and have less than 5 percent material passing the U.S. Standard No. 200 Sieve. The prepared subgrade should be covered with

geotextile fabric (Section 5.3.2) prior to placement of the imported granular material. The imported granular material should be placed in a single lift (up to 24 inches deep) and compacted using a smooth-drum, non-vibratory roller until well-keyed.

#### 5.3.4 Footing Subgrade Protection

We recommend a minimum of 3 inches of imported granular material to protect fine-grained foundation subgrades from foot traffic during inclement weather. The imported granular material should be in conformance with Section 5.4.2. The maximum particle size should be limited to 1 inch. The imported granular material should be placed in one lift over the prepared, undisturbed subgrade, and compacted using non-vibratory equipment until well keyed.

### 5.4 **Structural Fill**

The geotechnical engineer should be provided the opportunity to review all materials considered for use as structural fill (prior to placement). Samples of the proposed fill materials should be submitted to the geotechnical engineer a minimum of 5 business days prior their use on site<sup>8</sup>. The geotechnical engineer or their representative should be contacted to evaluate compaction of structural fill as the material is being placed. Evaluation of compaction may take the form of in-place density tests and/or proof roll tests with suitable equipment. Structural fill should be evaluated at intervals not exceeding every 2 vertical feet as the fill is being placed.

#### 5.4.1 On-Site Soils – General Use

##### 5.4.1.1 *Silt and Sandy Silt (ML)*

Re-use of these soils as structural fill may be difficult because these soils are sensitive to small changes in moisture content and are difficult, if not impossible, to adequately compact during wet weather. We anticipate the moisture content of these soils will be higher than the optimum moisture content for satisfactory compaction. Therefore, moisture conditioning (drying) should be expected in order to achieve adequate compaction. If used as structural fill, these soils should be free of organic matter, debris, and particles larger than 4 inches. When used as structural fill, these soils should be placed in lifts with a maximum pre-compaction thickness of about 8 inches at moisture contents within –1 and +3 percent of optimum, and compacted to not less than 92 percent of the material's maximum dry density, as determined in general accordance with ASTM D1557 (Modified Proctor).

If the on-site materials cannot be properly moisture-conditioned and/or processed, we recommend using imported granular material for structural fill.

#### 5.4.2 Imported Granular Structural Fill – General Use

Imported granular structural fill should consist of angular pit or quarry run rock, crushed rock, or crushed gravel that is fairly well graded between coarse and fine particle sizes. The granular fill should contain no organic matter, debris, or particles larger than 4 inches, and have less than 5 percent material passing the U.S. Standard No. 200 Sieve. For fine-grading purposes, the maximum particle size should be limited to 1½ inches. The percentage of fines can be increased to 12 percent of the material passing the U.S. Standard No. 200 Sieve if placed during dry weather, and provided the fill material is moisture-conditioned, as necessary, for proper compaction. Imported granular fill material should be compacted to

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<sup>8</sup> Laboratory testing for moisture density relationship (Proctor) is required. Tests for gradation may be required.

not less than 95 percent of the material's maximum dry density, as determined in general accordance with ASTM D1557 (Modified Proctor). Proper moisture conditioning and the use of vibratory equipment will facilitate compaction of these materials.

Granular fill materials with high percentages of particle sizes in excess of 1½ inches are considered non-moisture-density testable materials. As an alternative to conventional density testing, compaction of these materials should be evaluated by proof roll test observation (deflection tests), where accepted by the geotechnical engineer.

#### 5.4.3 Floor Slab Base Rock

Floor slab base rock should consist of well-graded granular material (crushed rock) containing no organic matter or debris, have a maximum particle size of ¾ inch, and have less than 5 percent material passing the U.S. Standard No. 200 Sieve. Floor slab base rock should be placed in one lift and compacted to not less than 95 percent of the material's maximum dry density as determined in general accordance with ASTM D1557 (Modified Proctor). We recommend "choking" the surface of the base rock with sand just prior to concrete placement. Choking means the voids between the largest aggregate particles are filled with sand, but does not provide a layer of sand above the base rock. Choking the base rock surface reduces the lateral restraint on the bottom of the concrete during curing.

#### 5.4.4 Trench Base Stabilization Material

If groundwater is present at the base of utility excavations, trench base stabilization material should be placed. Trench base stabilization material should consist of a minimum of 1 foot of well-graded granular material with a maximum particle size of 4 inches and less than 5 percent material passing the U.S. Standard No. 4 Sieve. The material should be free of organic matter and other deleterious material, placed in one lift (up to 24 inches thick), and compacted until well-keyed.

#### 5.4.5 Trench Backfill Material

Trench backfill for the utility pipe base and pipe zone should consist of granular material as recommended by the utility pipe manufacturer. Trench backfill above the pipe zone should consist of well-graded granular material containing no organic matter or debris, have a maximum particle size of ¾ inch, and have less than 8 percent material passing the U.S. Standard No. 200 Sieve. As a guideline, trench backfill should be placed in maximum 12-inch-thick lifts. The earthwork contractor may elect to use alternative lift thicknesses based on their experience with specific equipment and fill material conditions during construction in order to achieve the required compaction. The following table presents recommended relative compaction percentages for utility trench backfill.

**Table 2 Utility Trench Backfill Compaction Recommendations**

| Backfill Zone                    | Recommended <u>Minimum</u> Relative Compaction       |                                                      |
|----------------------------------|------------------------------------------------------|------------------------------------------------------|
|                                  | Structural Areas <sup>1</sup>                        | Landscaping Areas                                    |
| Pipe Base and Within Pipe Zone   | 90% ASTM D1557 or pipe manufacturer's recommendation | 88% ASTM D1557 or pipe manufacturer's recommendation |
| Above Pipe Zone                  | 92% ASTM D1557                                       | 90% ASTM D1557                                       |
| Within 3 Feet of Design Subgrade | 95% ASTM D1557                                       | 90% ASTM D1557                                       |

<sup>1</sup>Includes proposed building, pavement areas, structural fill areas, exterior hardscaping, etc.

#### 5.4.6 Controlled Low-Strength Material (CLSM)

CLSM is a self-compacting, cementitious material that is typically considered when backfilling localized areas. CLSM is sometimes referred to as “controlled density fill” or CDF. Due to its flowable characteristics, CLSM typically can be placed in restricted-access excavations where placing and compacting fill is difficult. If chosen for use at this site, we recommend the CLSM be in conformance with Section 00442 of the most recent, State of Oregon, Standard Specifications for Highway Construction. The geotechnical engineer’s representative should observe placement of the CLSM and obtain samples for compression testing in accordance with ASTM D4832. As a guideline, for each day’s placement, two compressive strength specimens from the same CLSM sample should be tested. The results of the two individual compressive strength tests should be averaged to obtain the reported 28-day compressive strength. If CLSM is considered for use on this site, please contact the geotechnical engineer for site-specific and application-specific recommendations.

### 5.5 **Shallow Foundations**

The recommendations presented in this section assume new foundations are established within 2 feet of existing (at the time of our investigation) site grades, as discussed in Section 4.0 above. For foundations established at depths greater than 2 feet, CGT should be consulted for further evaluation and supplemental recommendations, if warranted.

#### 5.5.1 Subgrade Preparation

Satisfactory subgrade support for shallow foundations can be obtained from the native, medium stiff to stiff silt (ML) anticipated at these depths, or on structural fill placed on these soils. The geotechnical engineer or their representative should be contacted to observe subgrade conditions prior to placement of forms, reinforcement steel, or structural fill (if required). If soft, loose, or otherwise unsuitable soils are encountered, they should be over-excavated as recommended by the geotechnical representative at the time of construction. The resulting over-excavation should be brought back to grade with imported granular structural fill in conformance with Section 5.4.2. The maximum particle size of over-excavation backfill should be limited to 1½ inches. All granular pads for footings should be constructed a minimum of 6 inches wider on each side of the footing for every vertical foot of over-excavation.

#### 5.5.2 Minimum Footing Width & Embedment

Minimum footing widths should be in conformance with the current OSSC. As a guideline, CGT recommends individual spread footings have a minimum width of 24 inches. We recommend continuous

wall footings have a minimum width of 18 inches. All footings should be founded at least 18 inches below the lowest, permanent adjacent grade to develop lateral capacity and for frost protection.

#### 5.5.3 Bearing Pressure & Settlement

Footings founded as recommended above should be proportioned for a maximum allowable soil bearing pressure of 2,000 pounds per square foot (psf). This bearing pressure is a net bearing pressure, applies to the total of dead and long-term live loads, and may be increased by one-third when considering seismic or wind loads. For foundations founded as recommended above, total settlement of foundations is anticipated to be less than 1 inch. Differential settlements between adjacent columns and/or bearing walls should not exceed ½-inch. If an increased allowable soil bearing pressure is desired, the geotechnical engineer should be consulted.

#### 5.5.4 Lateral Capacity

A maximum passive (equivalent fluid) earth pressure of 150 pounds per cubic foot (pcf) is recommended for design of footings cast neat into excavations in suitable native soil or confined by the recommended imported granular structural fill that is properly placed and compacted during construction. The recommended earth pressure was computed using a factor of safety of 1½, which is appropriate due to the amount of movement required to develop full passive resistance. In order to develop the above capacity, the following should be understood:

1. Concrete must be poured neat in excavations or the foundations must be backfilled with imported granular structural fill,
2. The adjacent grade must be level,
3. The static ground water level must remain below the base of the footings throughout the year.
4. Adjacent floor slabs, pavements, or the upper 12-inch depth of adjacent, unpaved areas should not be considered when calculating passive resistance.

An ultimate coefficient of friction equal to 0.35 may be used when calculating resistance to sliding for footings founded on the native soils described above. An ultimate coefficient of friction equal to 0.45 may be used when calculating resistance to sliding for footings founded on a minimum of 6 inches of imported granular structural fill (crushed rock) that is properly placed and compacted during construction.

#### 5.5.5 Subsurface Drainage

We recommend placing foundation drains at the exterior, base elevations of perimeter continuous wall footings. Foundation drains should consist of a minimum 4-inch diameter, perforated, PVC drainpipe wrapped with a non-woven geotextile filter fabric. The drains should be backfilled with a minimum of 2 cubic feet of open graded drain rock per lineal foot of pipe. The drain rock should also be encased in a geotextile fabric in order to provide separation from the surrounding fine-grained soils. Foundation drains should be positively sloped and should outlet to a suitable discharge point. The geotechnical engineer or their representative should observe the drains prior to backfilling. Roof drains should not be tied into foundation drains.



## **5.6 Floor Slabs**

### **5.6.1 Subgrade Preparation**

Satisfactory subgrade support for slabs constructed on grade, supporting up to 150 psf area loading, can be obtained from the native, medium stiff to stiff silt (ML) or new structural fill that is properly placed and compacted on this soil during construction. The geotechnical engineer or their representative should observe floor slab subgrade soils to evaluate surface consistencies. If soft, loose, or otherwise unsuitable soils are encountered, they should be over-excavated as recommended by the CGT geotechnical representative at the time of construction. The resulting over-excavation should be brought back to grade with imported granular structural fill as described in Section 5.4.2.

### **5.6.2 Crushed Rock Base**

Concrete floor slabs should be supported on a minimum 6-inch-thick layer of crushed rock (base rock) in conformance with Section 5.4.3. The surface of the base rock should be choked with sand just prior to vapor barrier membrane or concrete placement. Choking means the voids between the largest aggregate particles are filled with sand, but does not provide a layer of sand above the base rock. Choking the base rock surface reduces the lateral restraint on the bottom of the concrete during curing and helps reduce punctures in vapor barrier membranes<sup>9</sup> due to foot traffic.

### **5.6.3 Design Considerations**

For floor slabs constructed with a 6-inch base rock layer as recommended, an effective modulus of subgrade reaction of 75 pounds per cubic inch (pci) is recommended for the design of the floor slab. A higher effective modulus of subgrade reaction can be obtained by increasing the base rock thickness. Please contact the geotechnical engineer for additional recommendations if a higher modulus is desired. Floor slabs constructed as recommended will likely settle less than ½-inch. For general floor slab construction, slabs should be jointed around columns and walls to permit slabs and foundations to settle differentially.

### **5.6.4 Subgrade Moisture Considerations**

Liquid moisture and moisture vapor should be expected at the subgrade surface. The recommended crushed rock base is anticipated to provide protection against liquid moisture. Where moisture vapor emission through the slab must be minimized, e.g. impervious floor coverings, storage of moisture sensitive materials directly on the slab surface, etc., a vapor retarding membrane or vapor barrier below the slab should be considered. Factors such as cost, special considerations for construction, floor coverings, and end use suggest that the decision regarding a vapor retarding membrane or vapor barrier be made by the architect and owner.

If a vapor retarder or vapor barrier is placed below the slab, its location should be based on current American Concrete Institute (ACI) guidelines, ACI 302 Guide for Concrete Floor and Slab Construction. In some cases, this indicates placement of concrete directly on the vapor retarder or barrier. Please note that the placement of concrete directly on impervious membranes increases the risk of plastic shrinkage cracking and slab curling in the concrete. Construction practices to reduce or eliminate such risk, as described in ACI 302, should be employed during concrete placement.

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<sup>9</sup> Not typically required for polyolefin membranes due to their puncture resistance.

## **5.7 Pavements**

CGT performed a quantitative structural capacity evaluation of the existing pavements at the site, the results of which are presented in Appendix C.

## **5.8 Additional Considerations**

### **5.8.1 Drainage**

Subsurface drains should be connected to the nearest storm drain, on-site infiltration system (to be designed by others) or other suitable discharge point. Paved surfaces and grading near or adjacent to the building should be sloped to drain away from the building. Surface water from paved surfaces and open spaces should be collected and routed to a suitable discharge point. Surface water should not be directed into foundation drains.

### **5.8.2 Expansive Potential**

The near surface native soils consist of low plasticity silts (ML). These soils are not considered to be susceptible to appreciable movements from changes in moisture content. Accordingly, no special considerations are required to mitigate expansive potential of the near surface soils at the site.

## **6.0 RECOMMENDED ADDITIONAL SERVICES**

### **6.1 Design Review**

Geotechnical design review is of paramount importance. We recommend the geotechnical design review take place prior to releasing bid packets to contractors.

### **6.2 Observation of Construction**

Satisfactory earthwork, foundation, floor slab, and pavement performance depends to a large degree on the quality of construction. Sufficient observation of the contractor's activities is a key part of determining that the work is completed in accordance with the construction drawings and specifications. Subsurface conditions observed during construction should be compared with those encountered during subsurface explorations, and recognition of changed conditions often requires experience. We recommend that qualified personnel visit the site with sufficient frequency to detect whether subsurface conditions change significantly from those observed to date and anticipated in this report. We recommend the geotechnical engineer or their representative attend a pre-construction meeting coordinated by the contractor and/or developer. The project geotechnical engineer or their representative should provide observations and/or testing of at least the following earthwork elements during construction:

- Site Stripping & Grubbing
- Subgrade Preparation for Shallow Foundations, Structural Fills, Floor Slabs, and Pavements
- Compaction of Structural Fill and Utility Trench Backfill
- Compaction of Base Rock for Floor Slabs & Pavements
- Compaction of HMAC for Pavements

It is imperative that the owner and/or contractor request earthwork observations and testing at a frequency sufficient to allow the geotechnical engineer to provide a final letter of compliance for the earthwork activities.

## 7.0 LIMITATIONS

We have prepared this report for use by the owner/developer and other members of the design and construction team for the proposed development. The opinions and recommendations contained within this report are forwarded to assist in the planning and design process and are not intended to be, nor should they be construed as, a warranty of subsurface conditions.

We have made observations based on our explorations that indicate the soil conditions at only those specific locations and only to the depths penetrated. These observations do not necessarily reflect soil types, strata thickness, or water level variations that may exist between or away from our explorations. If subsurface conditions vary from those encountered in our site explorations, CGT should be alerted to the change in conditions so that we may provide additional geotechnical recommendations, if necessary. Observation by experienced geotechnical personnel should be considered an integral part of the construction process.

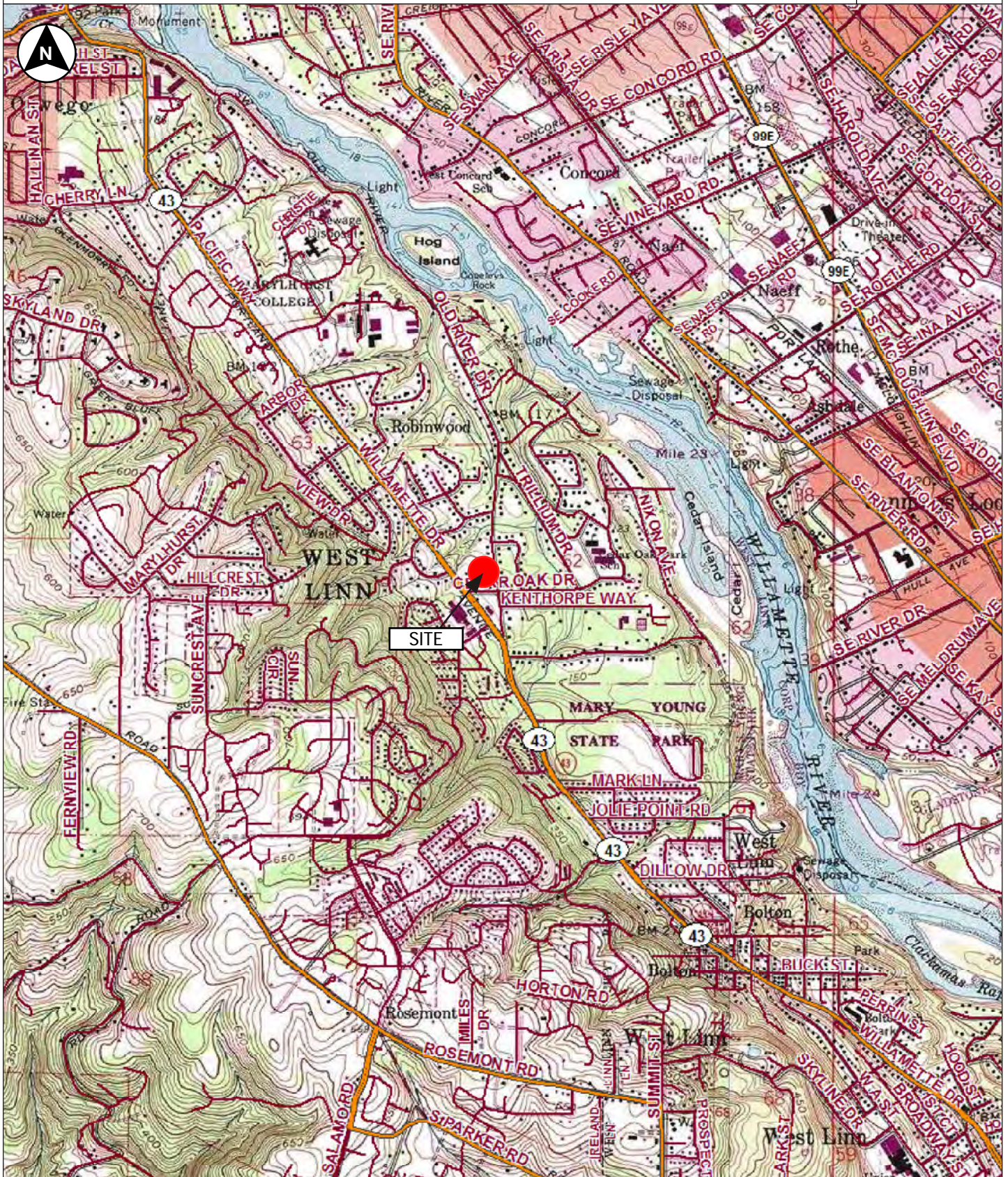
The owner/developer is responsible for ensuring that the project designers and contractors implement our recommendations. When the design has been finalized, prior to releasing bid packets to contractors, we recommend that the design drawings and specifications be reviewed by our firm to see that our recommendations have been interpreted and implemented as intended. If design changes are made, we request that we be retained to review our conclusions and recommendations and to provide a written modification or verification. Design review and construction phase testing and observation services are beyond the scope of our current assignment, but will be provided for an additional fee.

The scope of our services does not include services related to construction safety precautions, and our recommendations are not intended to direct the contractor's methods, techniques, sequences, or procedures, except as specifically described in our report for consideration in design.

Geotechnical engineering and the geologic sciences are characterized by a degree of uncertainty. Professional judgments presented in this report are based on our understanding of the proposed construction, familiarity with similar projects in the area, and on general experience. Within the limitations of scope, schedule, and budget, our services have been executed in accordance with the generally accepted practices in this area at the time this report was prepared; no warranty, expressed or implied, is made. This report is subject to review and should not be relied upon after a period of three years.

**MARYLHURST SCHOOL NEW LIFE CHURCH SITE EXPANSION - WEST LINN, OREGON**  
**Project Number G1804863**

**FIGURE 1**  
**Site Location**



Map created with TOPO!™, © 2006 National Geographic Holdings  
 USGS 7.5 Minute Topographic Map Series, Lake Oswego,  
 Oregon Quadrangle, 1984.  
 Township 2 South, Range 1 East, Section 24 Willamette Meridian

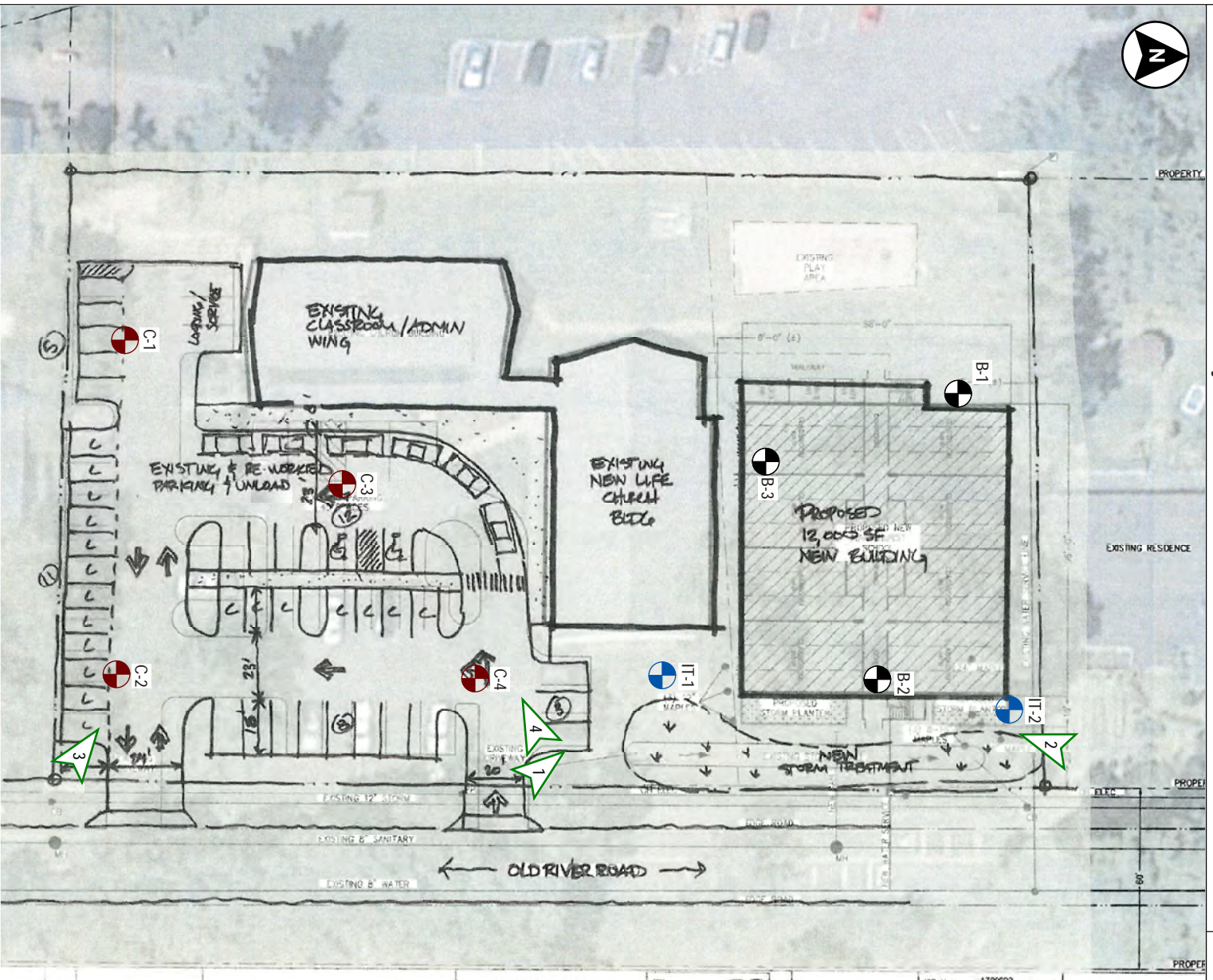
Latitude: 45.388012° North  
 Longitude: 122.640171° West

1 Inch = 2,000 feet



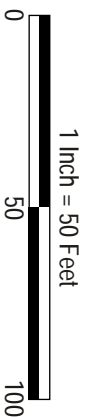
MARYLHURST SCHOOL NEW LIFE CHURCH SITE EXPANSION - WEST LINN, OREGON  
 Project Number G1804863

Figure 2  
 Site Plan



- LEGEND**
- B-1  Hollow-stem auger or mud rotary boring
  - IT-1  Infiltration test boring
  - C-1  Pavement core, DCP test, and hand auger boring
  - 1  Site photograph shown on Figure 3

NOTES: Drawing based on Preliminary Site Plan (Sheet C-1.0, dated January 3, 2018) prepared by KPFF, modified by CGT. Locations noted are approximate.





Photograph 1: Photograph taken in the central northeastern section of the site. Shows the locations of IT-1 and IT-2.



Photograph 2: Photograph of the northeastern portion of the site. Shows the locations of IT-1 and IT-2.



Photograph 3: Photograph taken in southeastern portion of the site. Shows the location of C-2.



Photograph 4: Photograph taken in the central northeastern section of the site. Shows the location of C-4.



Drafted by: CRH

See Figure 2 for approximate photograph locations and directions. Photographs were taken at the time of our fieldwork.

# Carlson Geotechnical

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## Appendix A: Subsurface Investigation and Laboratory Testing

Marylhurst School New Life Church Site Expansion  
19915 Old River Drive  
West Linn, Oregon

**CGT Project Number G1804863**

July 19, 2018

*Prepared For:*

Ms. Sheila Walker  
The Marylhurst School  
1232 Linn Avenue  
Oregon City, Oregon 97045

*Prepared by*  
**Carlson Geotechnical**

|                          |                  |
|--------------------------|------------------|
| Exploration Key.....     | Figure A1        |
| Soil Classification..... | Figure A2        |
| Exploration Logs .....   | Figures A3 – A11 |

## **A.1.0 SUBSURFACE INVESTIGATION**

Our field investigation consisted of nine borings completed in June 2018. The approximate exploration locations are shown on the Site Plan, attached to the geotechnical report as Figure 2. The exploration locations shown therein were determined based on measurements from existing site features (buildings, etc.) and are approximate. Surface elevations indicated on the logs were estimated based on 2-foot topographic contours available from Metro's Regional Land Information System (RLIS)<sup>1</sup> and are approximate.

### **A.1.1 Drilled Borings**

Three borings (B-1 through B-3) were advanced at the site on June 14, 2018, to depths ranging from about 21½ to 61½ feet bgs, using a CME 75 track-mounted drill rig provided and operated by our subcontractor, Western States Soil Conservation of Hubbard, Oregon. The borings were advanced using the hollow-stem auger and mud rotary drilling techniques. Upon completion, the borings were backfilled with granular bentonite. Drilling wastes (cuttings and drilling fluids) were left onsite.

Standard Penetration Tests (SPTs) were conducted within the borings using a split-spoon sampler in general accordance with American Society for Testing and Materials (ASTM) D1586. The SPTs were conducted at 2½- to 5-foot intervals to the termination depths of the borings. The SPT is described on the attached Exploration Key, Figure A1.

CGT also advanced two borings (IT-1 and IT-2) for the purposes of infiltration testing. The borings were advanced to depths of approximately 3½ to 4 feet bgs using a solid-stem auger powered by a walk-behind, Toro track-mounted rig provided and operated by CGT.

### **A.1.2 Pavement Cores and Hand Auger Borings**

CGT advanced four pavement cores (C-1 through C-4) within the onsite parking lot on June 29, 2018. Three-inch diameter hand auger borings were advanced through the core holes to depths of 2 to 2½ feet bgs in order to characterize pavement subgrade soils. The hand auger borings were loosely backfilled with the excavated materials and the pavement surface patched with cold mix asphalt upon completion.

### **A.1.3 Dynamic Cone Penetrometer Tests**

In conjunction with the hand auger borings, we performed dynamic cone penetrometer tests to depths of up to 3 feet bgs. The DCP tests were performed using a Dynamic Cone Penetrometer (DCP) provided and operated by CGT. The DCP test is described on the attached Exploration Key, Figure A1, and the results are shown on the respective exploration log.

### **A.1.4 Material Classification & Sampling**

Soil samples were obtained at selected intervals in the borings using the referenced split-spoon (SPT) sampler and thin-walled, steel (Shelby) tube samplers, detailed on Figure A1. Representative grab samples were obtained at select intervals within the solid stem auger and hand auger borings. A qualified member of CGT's staff collected the samples and logged the soils in general accordance with the Visual-Manual Procedure (ASTM 2488). An explanation of this classification system is attached as Figure A2. The SPT and grab samples were stored in sealable plastic bags and the Shelby tube samples were sealed with caps

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<sup>1</sup> Metro Regional Government, 2018. MetroMap Regional Land Information System (RLIS) data, accessed July 2018, from Metro website: <http://gis.oregonmetro.gov/metromap/>.



and tape and transported to our soils laboratory for further examination and testing. Our geotechnical staff visually examined all samples in order to refine the initial field classifications.

#### **A.1.5 Subsurface Conditions**

Subsurface conditions are summarized in Section 2.3 of the geotechnical report. Detailed logs of the explorations are attached as Figures A3 through A11. A key for symbols and in-situ test methods shown on the logs is attached as Figure A1.

#### **A.2.0 LABORATORY TESTING**

Laboratory testing was performed on samples collected in the field to refine our initial field classifications and determine in-situ parameters. Laboratory testing included the following:

- Twenty-two moisture content determinations (ASTM D2216).
- Three Atterberg limits (plasticity) tests (ASTM D4318).
- Two percentage passing the U.S. Standard No. 200 Sieve tests (ASTM D1140).
- One shelby tube unit weight test (weight-volume measurement).

Results of the laboratory tests are shown on the exploration logs.

GEOTECHNICAL LABORATORY TESTING



Atterberg limits (plasticity) test results (ASTM D4318): PL = Plastic Limit, LL = Liquid Limit, and MC= Moisture Content (ASTM D2216)

□ FINES CONTENT (%) Percentage passing the U.S. Standard No. 200 Sieve (ASTM D1140)

SAMPLING



GRAB Grab sample



SPT

**Standard Penetration Test (SPT)** consists of driving a 2-inch, outside-diameter, split-spoon sampler into the undisturbed formation with repeated blows of a 140-pound, hammer falling a vertical distance of 30 inches (ASTM D1586). The number of blows (N-value) required to drive the sampler the last 12 inches of an 18-inch sample interval is used to characterize the soil consistency or relative density. The drill rig was equipped with an cat-head or automatic hammer to conduct the SPTs. The observed N-values, hammer efficiency, and  $N_{60}$  are noted on the boring logs.



MC

**Modified California** sampling consists of 3-inch, outside-diameter, split-spoon sampler (ASTM G3550) driven similarly to the SPT sampling method described above. A sampler diameter correction factor of 0.44 is applied to calculate the equivalent SPT  $N_{60}$  value per Lacroix and Horn, 1973.



CORE Rock Coring interval



SH

**Shelby Tube** is a 3-inch, inner-diameter, thin-walled, steel tube push sampler (ASTM D1587) used to collect relatively undisturbed samples of fine-grained soils.

WDCP

**Wildcat Dynamic Cone Penetrometer (WDCP)** test consists of driving 1.1-inch diameter, steel rods with a 1.4-inch diameter, cone tip into the ground using a 35-pound drop hammer with a 15-inch free-fall height. The number of blows required to drive the steel rods is recorded for each 10 centimeters (3.94 inches) of penetration. The blow count for each interval is then converted to the corresponding SPT  $N_{60}$  values.

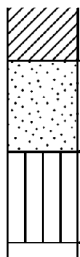
DCP

**Dynamic Cone Penetrometer (DCP)** test consists of driving a 20-millimeter diameter, hardened steel cone on 16-millimeter diameter steel rods into the ground using a 10-kilogram drop hammer with a 460-millimeter free-fall height. The depth of penetration in millimeters is recorded for each drop of the hammer.

POCKET PEN. (tsf)

**Pocket Penetrometer** test is a hand-held instrument that provides an approximation of the unconfined compressive strength in tons per square foot (tsf) of cohesive, fine-grained soils.

CONTACTS



Observed (measured) contact between soil or rock units.

Inferred (approximate) contact between soil or rock units.

Transitional (gradational) contact between soil or rock units.

ADDITIONAL NOTATIONS

*Italics* Notes drilling action or digging effort

{ Braces } Interpretation of material origin/geologic formation (e.g. { Base Rock } or { Columbia River Basalt })

*All measurements are approximate.*

**MARYLHURST SCHOOL NEW LIFE CHURCH SITE EXPANSION - WEST LINN, OREGON**  
**Project Number G1804863**

**FIGURE A2**  
**Soil Classification**

| Classification of Terms and Content                                                                                                                                                                                                                   | USCS Grain Size |                                                                        |                       |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------|------------------------------------------------------------------------|-----------------------|
| NAME: Group Name and Symbol<br>Relative Density or Consistency<br>Color<br>Moisture Content<br>Plasticity<br>Other Constituents<br>Other: Grain Shape, Approximate Gradation<br>Organics, Cement, Structure, Odor, etc.<br>Geologic Name or Formation | Fines           |                                                                        | <#200 (0.075 mm)      |
|                                                                                                                                                                                                                                                       | Sand            | Fine                                                                   | #200 - #40 (0.425 mm) |
|                                                                                                                                                                                                                                                       |                 | Medium                                                                 | #40 - #10 (2 mm)      |
|                                                                                                                                                                                                                                                       |                 | Coarse                                                                 | #10 - #4 (4.75)       |
|                                                                                                                                                                                                                                                       | Gravel          | Fine                                                                   | #4 - 0.75 inch        |
|                                                                                                                                                                                                                                                       |                 | Coarse                                                                 | 0.75 inch - 3 inches  |
| Cobbles                                                                                                                                                                                                                                               |                 | 3 to 12 inches;<br>scattered <15% estimated<br>numerous >15% estimated |                       |
| Boulders                                                                                                                                                                                                                                              |                 | > 12 inches                                                            |                       |

| Relative Density or Consistency |              |                                   |                            |                           |              |                                   |
|---------------------------------|--------------|-----------------------------------|----------------------------|---------------------------|--------------|-----------------------------------|
| Granular Material               |              | Fine-Grained (cohesive) Materials |                            |                           |              |                                   |
| SPT N-Value                     | Density      | SPT N-Value                       | Torvane tsf Shear Strength | Pocket Pen tsf Unconfined | Consistency  | Manual Penetration Test           |
|                                 |              | <2                                | <0.13                      | <0.25                     | Very Soft    | Thumb penetrates more than 1 inch |
| 0 - 4                           | Very Loose   | 2 - 4                             | 0.13 - 0.25                | 0.25 - 0.50               | Soft         | Thumb penetrates about 1 inch     |
| 4 - 10                          | Loose        | 4 - 8                             | 0.25 - 0.50                | 0.50 - 1.00               | Medium Stiff | Thumb penetrates about ¼ inch     |
| 10 - 30                         | Medium Dense | 8 - 15                            | 0.50 - 1.00                | 1.00 - 2.00               | Stiff        | Thumb penetrates less than ¼ inch |
| 30 - 50                         | Dense        | 15 - 30                           | 1.00 - 2.00                | 2.00 - 4.00               | Very Stiff   | Readily indented by thumbnail     |
| >50                             | Very Dense   | >30                               | >2.00                      | >4.00                     | Hard         | Difficult to indent by thumbnail  |

| Moisture Content                                                                                                                                                                                    |                |                   |               |                 | Structure                                                                                                                                                                                                                                                                                                                                                                                                                                                   |  |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------|-------------------|---------------|-----------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--|
| Dry: Absence of moisture, dusty, dry to the touch<br>Damp: Some moisture but leaves no moisture on hand<br>Moist: Leaves moisture on hand<br>Wet: Visible free water, likely from below water table |                |                   |               |                 | Stratified: Alternating layers of material or color >6 mm thick<br>Laminated: Alternating layers < 6 mm thick<br>Fissured: Breaks along definite fracture planes<br>Slicksided: Striated, polished, or glossy fracture planes<br>Blocky: Cohesive soil that can be broken down into small angular lumps which resist further breakdown<br>Lenses: Has small pockets of different soils, note thickness<br>Homogeneous: Same color and appearance throughout |  |
|                                                                                                                                                                                                     | Plasticity     | Dry Strength      | Dilatancy     | Toughness       |                                                                                                                                                                                                                                                                                                                                                                                                                                                             |  |
| ML                                                                                                                                                                                                  | Non to Low     | Non to Low        | Slow to Rapid | Low, can't roll |                                                                                                                                                                                                                                                                                                                                                                                                                                                             |  |
| CL                                                                                                                                                                                                  | Low to Medium  | Medium to High    | None to Slow  | Medium          |                                                                                                                                                                                                                                                                                                                                                                                                                                                             |  |
| MH                                                                                                                                                                                                  | Medium to High | Low to Medium     | None to Slow  | Low to Medium   |                                                                                                                                                                                                                                                                                                                                                                                                                                                             |  |
| CH                                                                                                                                                                                                  | Medium to High | High to Very High | None          | High            |                                                                                                                                                                                                                                                                                                                                                                                                                                                             |  |

| Visual-Manual Classification                                     |                                                  |                    |               |                                                                                      |  |  |
|------------------------------------------------------------------|--------------------------------------------------|--------------------|---------------|--------------------------------------------------------------------------------------|--|--|
| Major Divisions                                                  |                                                  |                    | Group Symbols | Typical Names                                                                        |  |  |
| Coarse Grained Soils:<br>More than 50% retained on No. 200 sieve | Gravels: 50% or more retained on the No. 4 sieve | Clean Gravels      | GW            | Well-graded gravels and gravel/sand mixtures, little or no fines                     |  |  |
|                                                                  |                                                  |                    | GP            | Poorly-graded gravels and gravel/sand mixtures, little or no fines                   |  |  |
|                                                                  |                                                  | Gravels with Fines | GM            | Silty gravels, gravel/sand/silt mixtures                                             |  |  |
|                                                                  |                                                  |                    | GC            | Clayey gravels, gravel/sand/clay mixtures                                            |  |  |
|                                                                  | Sands: More than 50% passing the No. 4 sieve     | Clean Sands        | SW            | Well-graded sands and gravelly sands, little or no fines                             |  |  |
|                                                                  |                                                  |                    | SP            | Poorly-graded sands and gravelly sands, little or no fines                           |  |  |
|                                                                  |                                                  | Sands with Fines   | SM            | Silty sands, sand/silt mixtures                                                      |  |  |
|                                                                  |                                                  |                    | SC            | Clayey sands, sand/clay mixtures                                                     |  |  |
| Fine-Grained Soils:<br>50% or more Passes No. 200 Sieve          | Silt and Clays<br>Low Plasticity Fines           |                    | ML            | Inorganic silts, rock flour, clayey silts                                            |  |  |
|                                                                  |                                                  |                    | CL            | Inorganic clays of low to medium plasticity, gravelly clays, sandy clays, lean clays |  |  |
|                                                                  |                                                  |                    | OL            | Organic silt and organic silty clays of low plasticity                               |  |  |
|                                                                  | Silt and Clays<br>High Plasticity Fines          |                    | MH            | Inorganic silts, clayey silts                                                        |  |  |
|                                                                  |                                                  |                    | CH            | Inorganic clays of high plasticity, fat clays                                        |  |  |
|                                                                  |                                                  |                    | OH            | Organic clays of medium to high plasticity                                           |  |  |
| Highly Organic Soils                                             |                                                  |                    | PT            | Peat, muck, and other highly organic soils                                           |  |  |



ASTM D2488 Standard Practice for Description and Identification of Soils (Visual-Manual Procedure)



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# FIGURE A3

## Boring B-1

PAGE 1 OF 1

|                                                             |                                                                        |
|-------------------------------------------------------------|------------------------------------------------------------------------|
| <b>CLIENT</b> The Marylhurst School - Ms. Sheila Walker     | <b>PROJECT NAME</b> Marylhurst School - New Life Church Site Expansion |
| <b>PROJECT NUMBER</b> G1804863                              | <b>PROJECT LOCATION</b> 19915 Old River Drive, West Linn, OR           |
| <b>DATE STARTED</b> 6/14/18 <b>GROUND ELEVATION</b> 151 ft  | <b>ELEVATION DATUM</b> Feet MSL                                        |
| <b>WEATHER</b> Cloudy ~65 degrees <b>SURFACE</b> Grass      | <b>LOGGED BY</b> MLL <b>REVIEWED BY</b> JAJ                            |
| <b>DRILLING CONTRACTOR</b> Western States Soil Conservation | <b>SEEPAGE</b> 11.5 ft / El. 139.5 ft                                  |
| <b>EQUIPMENT</b> CME 75 Truck                               | <b>GROUNDWATER AT END</b> ---                                          |
| <b>DRILLING METHOD</b> 6-inch (OD) Hollow Stem Auger        | <b>GROUNDWATER 0.5 HOURS AFTER DRILLING</b> 13.5 ft / El. 137.5 ft     |

| ELEVATION (ft) | GRAPHIC LOG | GROUP SYMBOL | MATERIAL DESCRIPTION                                                                                             | GROUNDWATER DEPTH (ft) | SAMPLE TYPE NUMBER | RECOVERY % (RQD) | BLOW COUNTS (N <sub>SPT</sub> VALUE) | N <sub>60</sub> VALUE<br>ETR <sub>Hammer</sub> = 75.1% | DRY UNIT WT. (pcf) | ▲ SPT N <sub>60</sub> VALUE ▲ |    |    |    |    |     |
|----------------|-------------|--------------|------------------------------------------------------------------------------------------------------------------|------------------------|--------------------|------------------|--------------------------------------|--------------------------------------------------------|--------------------|-------------------------------|----|----|----|----|-----|
|                |             |              |                                                                                                                  |                        |                    |                  |                                      |                                                        |                    | PL                            | LL |    |    |    |     |
|                |             |              |                                                                                                                  |                        |                    |                  |                                      |                                                        |                    | □ FINES CONTENT (%) □         |    |    |    |    |     |
|                |             |              |                                                                                                                  |                        |                    |                  |                                      |                                                        |                    | 0                             | 20 | 40 | 60 | 80 | 100 |
| 150            | XXXX        | OL           | <b>SOD:</b> Upper 4 inches<br><b>SILT:</b> Stiff, brown to gray-brown, moist, low plasticity.                    |                        |                    |                  |                                      |                                                        |                    |                               |    |    |    |    |     |
|                |             | ML           |                                                                                                                  |                        | SPT 1              | 78               | 4-4-6 (10)                           | 9                                                      |                    |                               |    |    |    |    | 26  |
| 145            |             |              | <b>SANDY SILT:</b> Soft, brown, moist, low to no plasticity, fine grained sand.                                  |                        | SPT 2              | 100              | 1-2-1 (3)                            | 3                                                      |                    |                               |    |    |    |    | 36  |
|                |             |              |                                                                                                                  |                        | SPT 3              | 100              | 0-1-1 (2)                            | 2                                                      |                    |                               |    |    |    |    |     |
| 140            |             |              | Very soft at 10 feet bgs.<br>Wet below 11½ feet bgs.<br>Driller notes stiffer soil based on pushing Shelby tube. |                        | SPT 4              | 100              | 0-0-0 (0)                            |                                                        |                    |                               |    |    |    |    | 38  |
|                |             | ML           |                                                                                                                  |                        | SH 5               | 17               |                                      |                                                        |                    |                               |    |    |    |    |     |
| 135            |             |              | Stiff, blue-gray below 15 feet bgs.                                                                              |                        | SPT 6              | 100              | 2-3-5 (8)                            | 9                                                      |                    |                               |    |    |    |    | 32  |
| 130            |             |              | Very stiff at 20 feet bgs.                                                                                       |                        | SPT 7              | 100              | 2-6-8 (14)                           | 17                                                     |                    |                               |    |    |    |    |     |

- Boring terminated at 21½ feet bgs.
- No caving observed.
- Groundwater encountered at 11½ feet bgs.
- Boring backfilled with bentonite.

CGT BOREHOLE G1804863.GPJ 7/19/18 DRAFTED BY: MLL



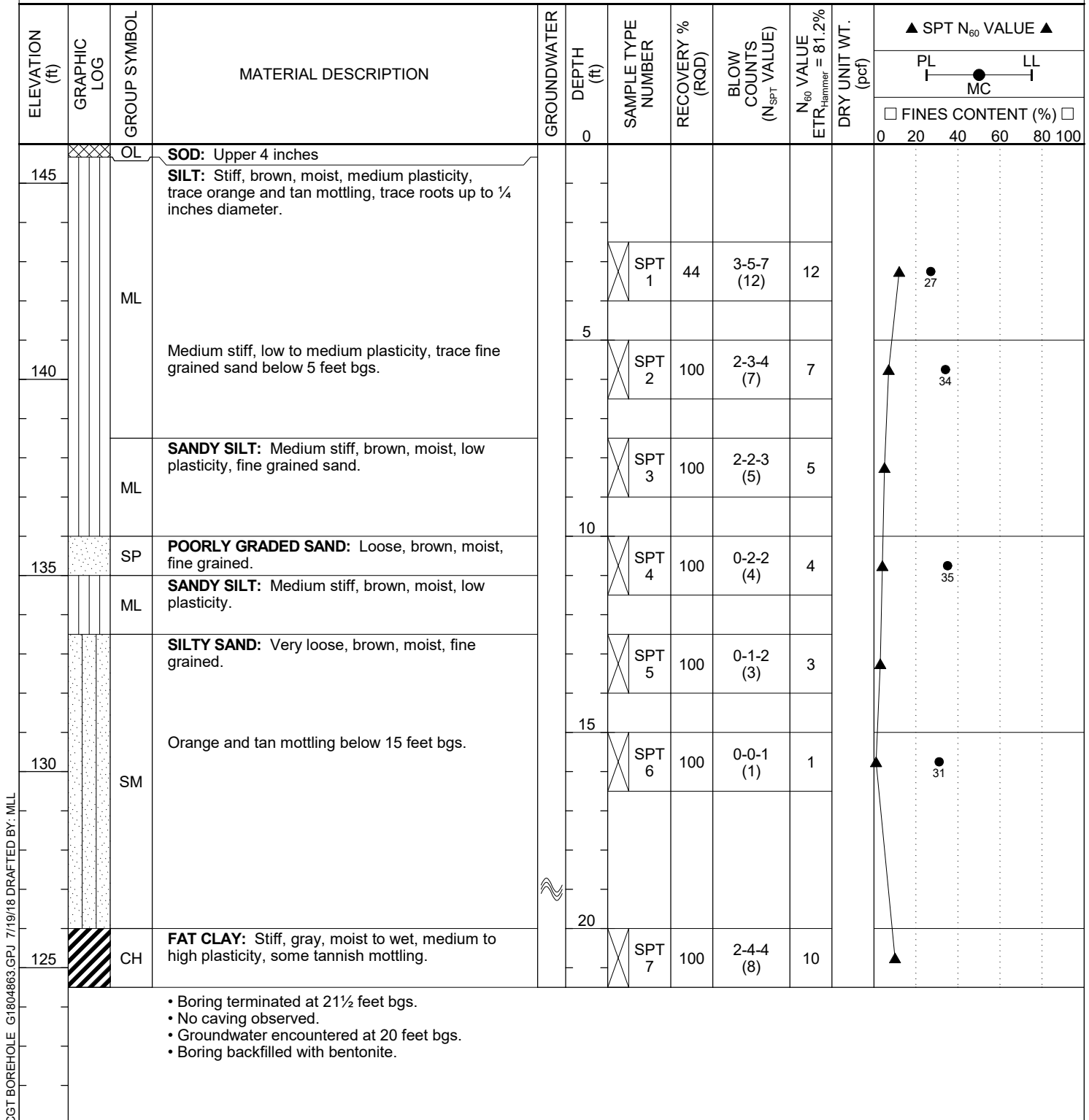
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# FIGURE A4

## Boring B-2

PAGE 1 OF 1

|                                                             |                                                                        |
|-------------------------------------------------------------|------------------------------------------------------------------------|
| <b>CLIENT</b> The Marylhurst School - Ms. Sheila Walker     | <b>PROJECT NAME</b> Marylhurst School - New Life Church Site Expansion |
| <b>PROJECT NUMBER</b> G1804863                              | <b>PROJECT LOCATION</b> 19915 Old River Drive, West Linn, OR           |
| <b>DATE STARTED</b> 6/14/18 <b>GROUND ELEVATION</b> 146 ft  | <b>ELEVATION DATUM</b> Feet MSL                                        |
| <b>WEATHER</b> Cloudy ~65 degrees <b>SURFACE</b> Grass      | <b>LOGGED BY</b> MLL <b>REVIEWED BY</b> JAJ                            |
| <b>DRILLING CONTRACTOR</b> Western States Soil Conservation | <b>SEEPAGE</b> 19.0 ft / El. 127.0 ft                                  |
| <b>EQUIPMENT</b> CME 55 Track                               | <b>GROUNDWATER AT END</b> ---                                          |
| <b>DRILLING METHOD</b> 6-inch (OD) Hollow Stem Auger        | <b>GROUNDWATER AFTER DRILLING</b> ---                                  |





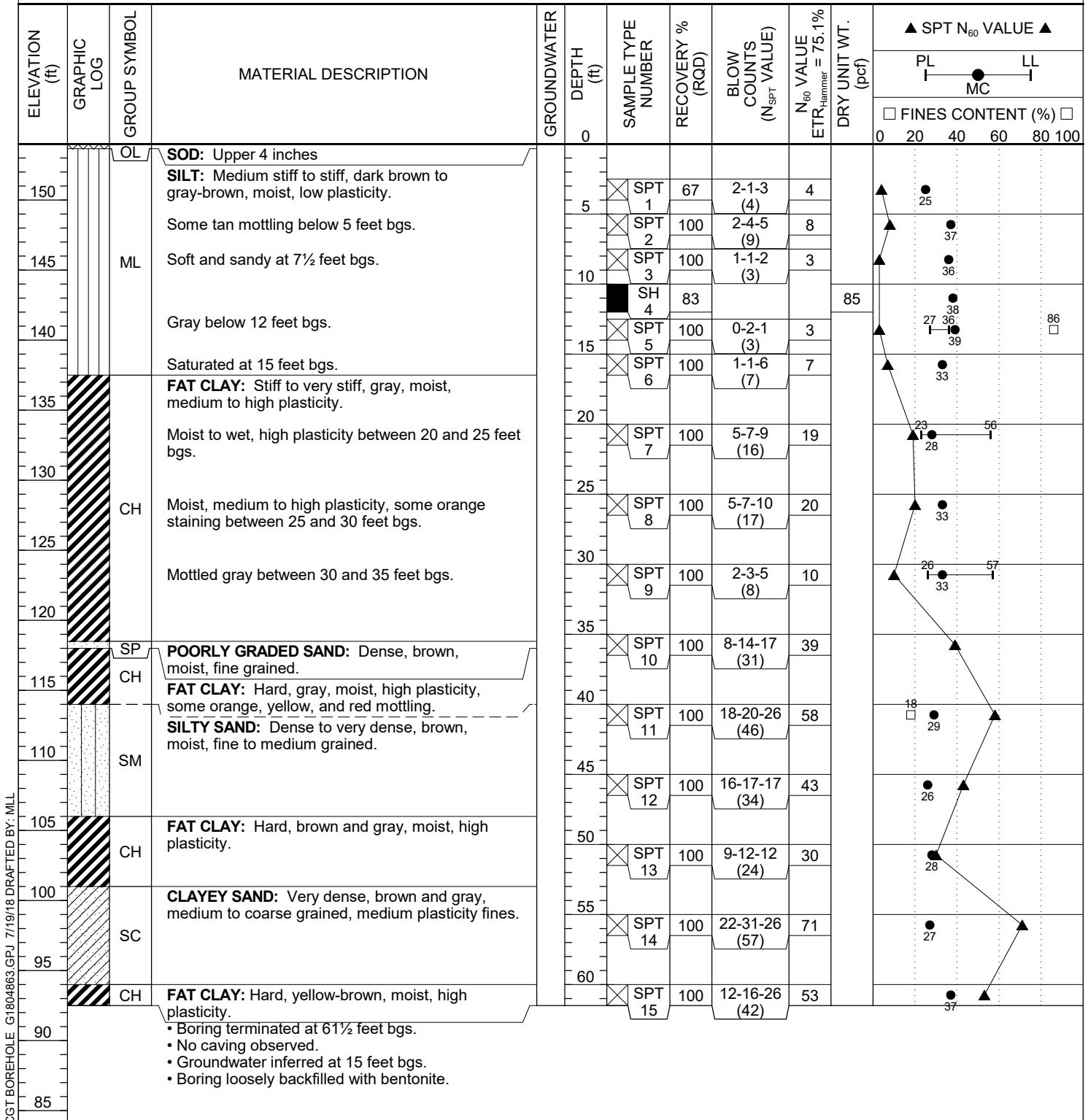
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# FIGURE A5

## Boring B-3

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|                                                             |                                                                        |
|-------------------------------------------------------------|------------------------------------------------------------------------|
| <b>CLIENT</b> The Marylhurst School - Ms. Sheila Walker     | <b>PROJECT NAME</b> Marylhurst School - New Life Church Site Expansion |
| <b>PROJECT NUMBER</b> G1804863                              | <b>PROJECT LOCATION</b> 19915 Old River Drive, West Linn, OR           |
| <b>DATE STARTED</b> 6/14/18 <b>GROUND ELEVATION</b> 154 ft  | <b>ELEVATION DATUM</b> Feet MSL                                        |
| <b>WEATHER</b> Cloudy ~65 degrees <b>SURFACE</b> Grass      | <b>LOGGED BY</b> MLL <b>REVIEWED BY</b> JAJ                            |
| <b>DRILLING CONTRACTOR</b> Western States Soil Conservation | <b>SEEPAGE</b> ---                                                     |
| <b>EQUIPMENT</b> CME 75 Truck                               | <b>GROUNDWATER AT END</b> ---                                          |
| <b>DRILLING METHOD</b> Mud Rotary                           | <b>GROUNDWATER AFTER DRILLING</b> ---                                  |





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# FIGURE A6

## Boring IT-1

PAGE 1 OF 1

|                                                            |                                                                        |
|------------------------------------------------------------|------------------------------------------------------------------------|
| <b>CLIENT</b> The Marylhurst School - Ms. Sheila Walker    | <b>PROJECT NAME</b> Marylhurst School - New Life Church Site Expansion |
| <b>PROJECT NUMBER</b> G1804863                             | <b>PROJECT LOCATION</b> 19915 Old River Drive, West Linn, OR           |
| <b>DATE STARTED</b> 6/13/18 <b>GROUND ELEVATION</b> 146 ft | <b>ELEVATION DATUM</b> Feet MSL                                        |
| <b>WEATHER</b> Cloudy ~55 degrees <b>SURFACE</b> Grass     | <b>LOGGED BY</b> CRH <b>REVIEWED BY</b> JAJ                            |
| <b>DRILLING CONTRACTOR</b> CGT                             | <b>SEEPAGE</b> ---                                                     |
| <b>EQUIPMENT</b> Toro Dual Tracked Auger                   | <b>GROUNDWATER AT END</b> ---                                          |
| <b>DRILLING METHOD</b> Test Pit & Infiltration Test        | <b>GROUNDWATER AFTER DRILLING</b> ---                                  |

| ELEVATION (ft) | GRAPHIC LOG | GROUP SYMBOL | MATERIAL DESCRIPTION                                                                                                                                                                                              | GROUNDWATER DEPTH (ft) | SAMPLE TYPE NUMBER | RECOVERY % (RQD) | BLOW COUNTS (N <sub>SPT</sub> VALUE) | N <sub>60</sub> VALUE | DRY UNIT WT. (pcf) | ▲ SPT N <sub>SPT</sub> VALUE ▲ |                   |
|----------------|-------------|--------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------|--------------------|------------------|--------------------------------------|-----------------------|--------------------|--------------------------------|-------------------|
|                |             |              |                                                                                                                                                                                                                   |                        |                    |                  |                                      |                       |                    | PL                             | LL                |
|                |             |              |                                                                                                                                                                                                                   | 0                      |                    |                  |                                      |                       |                    |                                | MC                |
|                |             |              |                                                                                                                                                                                                                   |                        |                    |                  |                                      |                       |                    |                                | 0 20 40 60 80 100 |
| 145            |             |              | <b>SILT FILL:</b> Brown, dry, low plasticity, some angular gravel up to ½ inches in diameter, some gray mottling, some rootlets and trace roots up to ¼ inch in diameter.                                         |                        |                    |                  |                                      |                       |                    |                                |                   |
| 144            |             | ML           | Gray-brown, increased gravel between 1 and 3 feet bgs.<br><br>Some roots ½ inches in diameter between 1½ and 2 feet bgs.                                                                                          | 1                      |                    |                  |                                      |                       |                    |                                |                   |
| 143            |             |              |                                                                                                                                                                                                                   | 2                      |                    |                  |                                      |                       |                    |                                |                   |
| 142            |             | ML           | <b>SILT:</b> Brown with gray and orange mottling, moist, low plasticity.                                                                                                                                          | 3                      |                    |                  |                                      |                       |                    |                                |                   |
|                |             |              |                                                                                                                                                                                                                   | 4                      | GRAB 1             | 100              |                                      |                       |                    |                                |                   |
| 141            |             |              | <ul style="list-style-type: none"> <li>Boring terminated at 4 feet bgs.</li> <li>No caving observed.</li> <li>No groundwater encountered.</li> <li>Boring loosely backfilled with excavated materials.</li> </ul> |                        |                    |                  |                                      |                       |                    |                                |                   |

CGT BOREHOLE G1804863.GPJ 7/19/18 DRAFTED BY: CRH



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# FIGURE A7

## Boring IT-2

PAGE 1 OF 1

|                                                         |                                                                        |
|---------------------------------------------------------|------------------------------------------------------------------------|
| <b>CLIENT</b> The Marylhurst School - Ms. Sheila Walker | <b>PROJECT NAME</b> Marylhurst School - New Life Church Site Expansion |
| <b>PROJECT NUMBER</b> G1804863                          | <b>PROJECT LOCATION</b> 19915 Old River Drive, West Linn, OR           |
| <b>DATE STARTED</b> 6/13/18                             | <b>GROUND ELEVATION</b> 144 ft                                         |
| <b>WEATHER</b> Cloudy ~55 degrees                       | <b>ELEVATION DATUM</b> Feet MSL                                        |
| <b>SURFACE</b> Dirt                                     | <b>LOGGED BY</b> CRH                                                   |
| <b>DRILLING CONTRACTOR</b> CGT                          | <b>REVIEWED BY</b> JAJ                                                 |
| <b>EQUIPMENT</b> Toro Dual Tracked Auger                | <b>SEEPAGE</b> ---                                                     |
| <b>DRILLING METHOD</b> Test Pit & Infiltration Test     | <b>GROUNDWATER AT END</b> ---                                          |
|                                                         | <b>GROUNDWATER AFTER DRILLING</b> ---                                  |

| ELEVATION (ft) | GRAPHIC LOG | GROUP SYMBOL | MATERIAL DESCRIPTION                                                                                                                                                                                               | GROUNDWATER DEPTH (ft) | SAMPLE TYPE NUMBER | RECOVERY % (RQD) | BLOW COUNTS (N <sub>SPT</sub> VALUE) | N <sub>60</sub> VALUE | DRY UNIT WT. (pcf) | ▲ SPT N <sub>SPT</sub> VALUE ▲ |                       |
|----------------|-------------|--------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------|--------------------|------------------|--------------------------------------|-----------------------|--------------------|--------------------------------|-----------------------|
|                |             |              |                                                                                                                                                                                                                    |                        |                    |                  |                                      |                       |                    | PL                             | LL                    |
|                |             |              |                                                                                                                                                                                                                    | 0                      |                    |                  |                                      |                       |                    |                                | MC                    |
|                |             |              |                                                                                                                                                                                                                    |                        |                    |                  |                                      |                       |                    |                                | □ FINES CONTENT (%) □ |
|                |             |              |                                                                                                                                                                                                                    |                        |                    |                  |                                      |                       |                    |                                | 0 20 40 60 80 100     |
| 143            |             |              | <b>SILT:</b> Brown to gray-brown, moist, low plasticity, rootlets in upper 6 inches, some roots up to ½ inch in diameter, and trace subrounded gravel up to ¾ inch in diameter.                                    | 1                      |                    |                  |                                      |                       |                    |                                |                       |
| 142            |             | ML           | Some gray and orange mottling below 2 feet bgs.<br><br>Orange-brown below 2½ feet bgs.                                                                                                                             | 2                      |                    |                  |                                      |                       |                    |                                |                       |
| 141            |             |              | Brown below 3 feet bgs.                                                                                                                                                                                            | 3                      |                    |                  |                                      |                       |                    |                                |                       |
|                |             |              |                                                                                                                                                                                                                    |                        | GRAB 1             | 100              |                                      |                       |                    |                                |                       |
| 140            |             |              | <ul style="list-style-type: none"> <li>Boring terminated at 3½ feet bgs.</li> <li>No caving observed.</li> <li>No groundwater encountered.</li> <li>Boring loosely backfilled with excavated materials.</li> </ul> |                        |                    |                  |                                      |                       |                    |                                |                       |
| 139            |             |              |                                                                                                                                                                                                                    |                        |                    |                  |                                      |                       |                    |                                |                       |

CGT BOREHOLE G1804863.GPJ 7/19/18 DRAFTED BY: CRH





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# FIGURE A8

## Boring C-1

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|                                                         |                                                                        |
|---------------------------------------------------------|------------------------------------------------------------------------|
| <b>CLIENT</b> The Marylhurst School - Ms. Sheila Walker | <b>PROJECT NAME</b> Marylhurst School - New Life Church Site Expansion |
| <b>PROJECT NUMBER</b> G1804863                          | <b>PROJECT LOCATION</b> 19915 Old River Drive, West Linn, OR           |
| <b>DATE STARTED</b> 6/29/18                             | <b>GROUND ELEVATION</b> 164 ft                                         |
| <b>WEATHER</b> Sunny~68 degrees                         | <b>ELEVATION DATUM</b> Feet MSL                                        |
| <b>SURFACE</b> Asphalt Concrete                         | <b>LOGGED BY</b> CRH                                                   |
| <b>DRILLING CONTRACTOR</b> CGT                          | <b>REVIEWED BY</b> JAJ                                                 |
| <b>EQUIPMENT</b> Asphalt Core, Hand Auger, & DCP        | <b>SEEPAGE</b> ---                                                     |
| <b>DRILLING METHOD</b> Asphalt Core, Hand Auger, & DCP  | <b>GROUNDWATER AT END</b> ---                                          |
|                                                         | <b>GROUNDWATER AFTER DRILLING</b> ---                                  |

| ELEVATION (ft) | GRAPHIC LOG | GROUP SYMBOL | MATERIAL DESCRIPTION                                                                                                                                                                                                                                     | DEPTH (ft) | SAMPLE TYPE | POCKET PEN. (tsf) | MOISTURE CONTENT (%) | FINES CONTENT (%) | DCP (Inches Per Blow) |
|----------------|-------------|--------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------|-------------|-------------------|----------------------|-------------------|-----------------------|
|                |             |              | <b>ASPHALT CONCRETE:</b> 1 lift, 3 inches thick.                                                                                                                                                                                                         | 0          |             |                   |                      |                   |                       |
| 163            |             | GP           | <b>POORLY GRADED GRAVEL FILL:</b> Dense, gray, wet (water added during coring), angular to subangular, up to 1 inch in size, silty in lower few inches.                                                                                                  | 1          |             |                   |                      |                   |                       |
| 162            |             | ML           | <b>SILT:</b> Brown with some gray-brown and orange-brown, moist, medium plasticity.                                                                                                                                                                      | 2          | GRAB 1      |                   |                      |                   |                       |
| 161            |             |              |                                                                                                                                                                                                                                                          | 3          |             |                   |                      |                   |                       |
| 160            |             |              | <ul style="list-style-type: none"> <li>Boring terminated at 2 feet bgs.</li> <li>No caving observed.</li> <li>No groundwater encountered.</li> <li>Boring loosely backfilled with excavated materials and surface patched cold patch asphalt.</li> </ul> |            |             |                   |                      |                   |                       |
| 159            |             |              |                                                                                                                                                                                                                                                          |            |             |                   |                      |                   |                       |

CGT EXPLORATION WITH DCP G1804863.GPJ 7/19/18 DRAFTED BY: CRH



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# FIGURE A9

## Boring C-2

PAGE 1 OF 1

**CLIENT** The Marylhurst School - Ms. Sheila Walker      **PROJECT NAME** Marylhurst School - New Life Church Site Expansion  
**PROJECT NUMBER** G1804863      **PROJECT LOCATION** 19915 Old River Drive, West Linn, OR  
**DATE STARTED** 6/29/18      **GROUND ELEVATION** 157 ft      **ELEVATION DATUM** Feet MSL  
**WEATHER** Sunny~68 degrees      **SURFACE** Asphalt Concrete      **LOGGED BY** CRH      **REVIEWED BY** JAJ  
**DRILLING CONTRACTOR** CGT      **SEEPAGE** ---  
**EQUIPMENT** Asphalt Core, Hand Auger, & DCP      **GROUNDWATER AT END** ---  
**DRILLING METHOD** Asphalt Core, Hand Auger, & DCP      **GROUNDWATER AFTER DRILLING** ---

| ELEVATION (ft) | GRAPHIC LOG | GROUP SYMBOL | MATERIAL DESCRIPTION                                                                                                                                                                                                                                      | DEPTH (ft) | SAMPLE TYPE | POCKET PEN. (tsf) | MOISTURE CONTENT (%) | FINES CONTENT (%) | DCP (Inches Per Blow) |
|----------------|-------------|--------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------|-------------|-------------------|----------------------|-------------------|-----------------------|
| 156            |             |              | <b>ASPHALT CONCRETE:</b> 1 lift, 4 inches thick.                                                                                                                                                                                                          | 0          |             |                   |                      |                   |                       |
| 155            |             | GP           | <b>POORLY GRADED GRAVEL FILL:</b> Dense, gray, wet (water added during coring), angular to subangular, up to 1 inch in size, silty in lower few inches.                                                                                                   | 1          |             |                   |                      |                   |                       |
| 154            |             | ML           | <b>SILT:</b> Gray with orange-red mottling, moist, medium plasticity, trace rootlets, trace black concretions, and some tan concretions.                                                                                                                  | 2          | GRAB 1      |                   |                      |                   |                       |
| 153            |             |              | <ul style="list-style-type: none"> <li>Boring terminated at 2¼ feet bgs.</li> <li>No caving observed.</li> <li>No groundwater encountered.</li> <li>Boring loosely backfilled with excavated materials and surface patched cold patch asphalt.</li> </ul> | 3          |             |                   |                      |                   |                       |
| 152            |             |              |                                                                                                                                                                                                                                                           |            |             |                   |                      |                   |                       |

CGT EXPLORATION WITH DCP G1804863.GPJ 7/19/18 DRAFTED BY: CRH



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# FIGURE A10

## Boring C-3

PAGE 1 OF 1

**CLIENT** The Marylhurst School - Ms. Sheila Walker      **PROJECT NAME** Marylhurst School - New Life Church Site Expansion  
**PROJECT NUMBER** G1804863      **PROJECT LOCATION** 19915 Old River Drive, West Linn, OR  
**DATE STARTED** 6/29/18      **GROUND ELEVATION** 157 ft      **ELEVATION DATUM** Feet MSL  
**WEATHER** Sunny~68 degrees      **SURFACE** Asphalt Concrete      **LOGGED BY** CRH      **REVIEWED BY** JAJ  
**DRILLING CONTRACTOR** CGT      **SEEPAGE** ---  
**EQUIPMENT** Asphalt Core, Hand Auger, & DCP      **GROUNDWATER AT END** ---  
**DRILLING METHOD** Asphalt Core, Hand Auger, & DCP      **GROUNDWATER AFTER DRILLING** ---

| ELEVATION (ft) | GRAPHIC LOG | GROUP SYMBOL | MATERIAL DESCRIPTION                                                                                                                                    | DEPTH (ft) | SAMPLE TYPE | POCKET PEN. (tsf) | MOISTURE CONTENT (%) | FINES CONTENT (%) | DCP (Inches Per Blow) |
|----------------|-------------|--------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|------------|-------------|-------------------|----------------------|-------------------|-----------------------|
|                |             |              | <b>ASPHALT CONCRETE:</b> 1 lift, 3½ inches thick.                                                                                                       | 0          |             |                   |                      |                   |                       |
| 156            |             | GP           | <b>POORLY GRADED GRAVEL FILL:</b> Dense, gray, wet (water added during coring), angular to subangular, up to 1 inch in size, silty in lower few inches. | 1          |             |                   |                      |                   |                       |
| 155            |             | ML           | <b>SILT:</b> Gray and brown, moist, medium plasticity, some red-orange mottling, trace fine grained, micaceous sand.<br><br>Brown below 1½ feet bgs.    | 2          | GRAB 1      |                   |                      |                   |                       |
| 154            |             |              |                                                                                                                                                         | 3          |             |                   |                      |                   |                       |

- Boring terminated at 2 feet bgs.
- No caving observed.
- No groundwater encountered.
- Boring loosely backfilled with excavated materials and surface patched cold patch asphalt.

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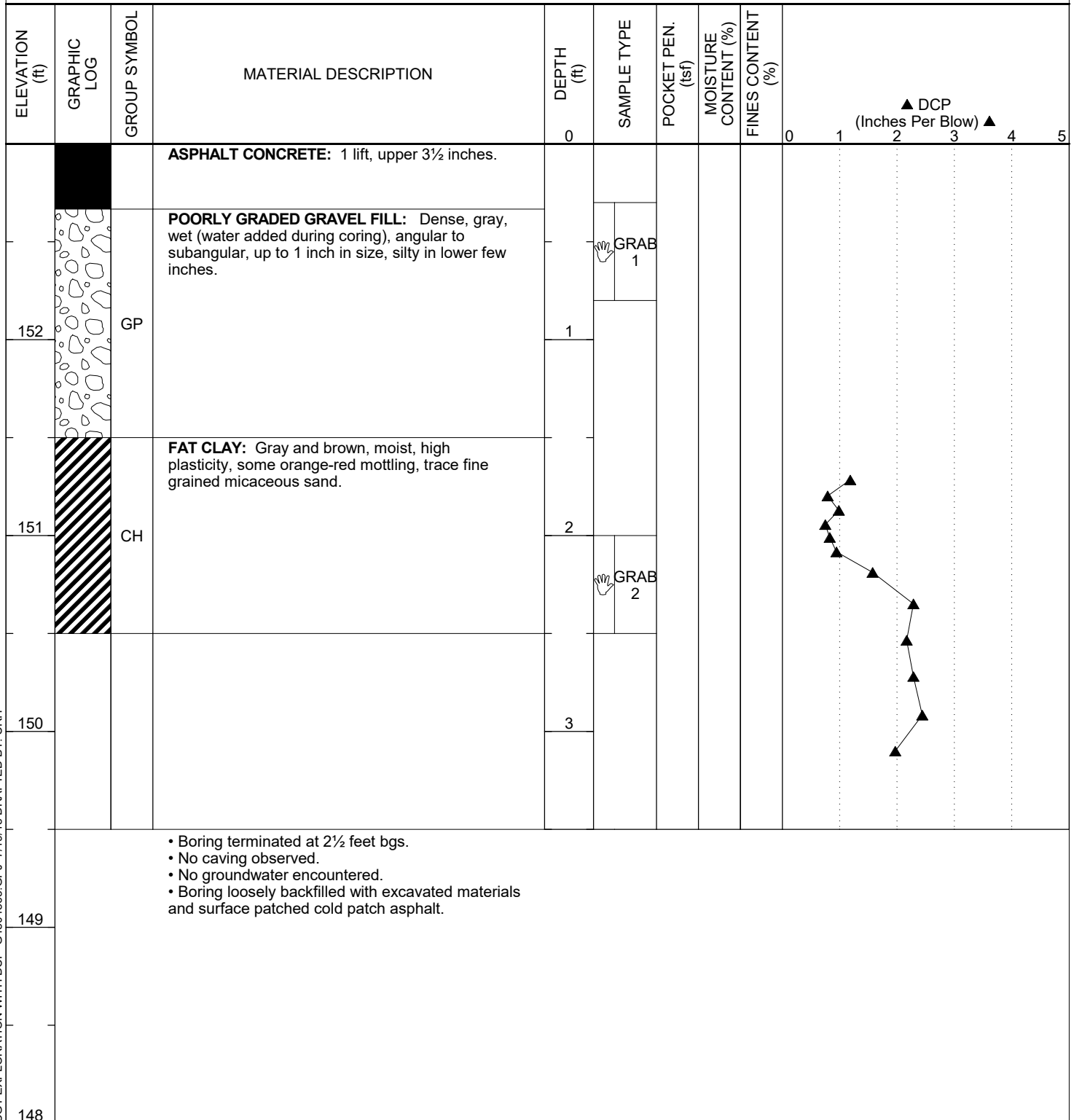
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# FIGURE A11

## Boring C-4

PAGE 1 OF 1

|                                                         |                                                                        |
|---------------------------------------------------------|------------------------------------------------------------------------|
| <b>CLIENT</b> The Marylhurst School - Ms. Sheila Walker | <b>PROJECT NAME</b> Marylhurst School - New Life Church Site Expansion |
| <b>PROJECT NUMBER</b> G1804863                          | <b>PROJECT LOCATION</b> 19915 Old River Drive, West Linn, OR           |
| <b>DATE STARTED</b> 6/29/18                             | <b>GROUND ELEVATION</b> 153 ft                                         |
| <b>WEATHER</b> Sunny~68 degrees                         | <b>ELEVATION DATUM</b> Feet MSL                                        |
| <b>SURFACE</b> Asphalt Concrete                         | <b>LOGGED BY</b> CRH                                                   |
| <b>DRILLING CONTRACTOR</b> CGT                          | <b>REVIEWED BY</b> JAJ                                                 |
| <b>EQUIPMENT</b> Asphalt Core, Hand Auger, & DCP        | <b>SEEPAGE</b> ---                                                     |
| <b>DRILLING METHOD</b> Asphalt Core, Hand Auger, & DCP  | <b>GROUNDWATER AT END</b> ---                                          |
|                                                         | <b>GROUNDWATER AFTER DRILLING</b> ---                                  |



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## Appendix B: Results of Infiltration Testing

**Marylhurst School New Life Church Site Expansion  
19915 Old River Drive  
West Linn, Oregon**

**CGT Project Number G1804863**

July 19, 2018

*Prepared For:*

Ms. Sheila Walker  
The Marylhurst School  
1232 Linn Avenue  
Oregon City, Oregon 97045

*Prepared by*  
**Carlson Geotechnical**

**B.1.0 INTRODUCTION**

CGT performed two infiltration tests at the project site on June 14, 2018. The tests were performed adjacent to the existing ditch along Old River Drive, as shown on the site plan attached to the geotechnical report. The test locations are shown on the Site Plan (Figure 2) attached to the geotechnical report. The testing is detailed in the following sections.

**B.2.0 TEST PROCEDURE**

CGT advanced two borings (IT-1 and IT-2) for the purposes of infiltration testing. The borings were advanced to depths of approximately 3½ to 4 feet bgs using a solid-stem auger powered by a walk-behind, Toro track-mounted rig provided and operated by CGT.

The infiltration tests were performed in general accordance with the Encased Falling Head test method described in Appendix E of the Stormwater Standards, Clackamas County Service District No. 1, dated July 1, 2013. The borings were advanced to the test depths and a 6-inch-inner-diameter PVC pipe was inserted into each of the auger holes. The subsurface soils at the base of the pipes were soaked for at least four hours, in accordance with the referenced test method by pouring about 12 inches of water (measured vertically) into the test pipes. After the soaking period, testing was initiated by recording the drop in water level of an approximate 12-inch column of water at 10- minute intervals.

**B.3.0 TEST RESULTS**

The following tables present the details, raw data, and calculated infiltration rates observed during testing. Please note that the calculated infiltration rates do not include any safety or correction factors.

**Table B1 Results of Infiltration Test IT-1**

| Test Depth: 4 feet bgs  |                               | Soil Type: Silt (ML)                    |
|-------------------------|-------------------------------|-----------------------------------------|
| Time Interval (minutes) | Drop in Water Level (inches)* | Raw Infiltration Rate (inches per hour) |
| 10                      | 0                             |                                         |
| 10                      | 1                             |                                         |
| 10                      | 0.25                          |                                         |
| 10                      | 0.25                          |                                         |
| 10                      | 0.5                           | --                                      |
| 10                      | 0.5                           |                                         |
| 10                      | 0.25                          |                                         |
| 10                      | 0.25                          |                                         |
| 10                      | 0.25                          |                                         |
| 10                      | 0.25                          | 1.50                                    |

\* Water level measurements taken in inches, measured to the nearest one-sixteenth inch, reported in decimal equivalents.

\*\*Unfactored rate – does not include any safety or correction factors.

**Table B2 Results of Infiltration Test TP-3**

| Test Depth: 3½ feet bgs    |                                  | Soil Type: Silt (ML)                         |
|----------------------------|----------------------------------|----------------------------------------------|
| Time Interval<br>(minutes) | Drop in Water Level<br>(inches)* | Raw Infiltration Rate<br>(inches per hour)** |
| 10                         | 0                                |                                              |
| 10                         | 0                                |                                              |
| 10                         | 0                                |                                              |
| 10                         | 0                                |                                              |
| 10                         | 0                                | --                                           |
| 10                         | 0                                |                                              |
| 10                         | 0                                |                                              |
| 10                         | 0                                |                                              |
| 10                         | 0                                |                                              |
| 10                         | 0                                | 0                                            |

\* Water level measurements taken in inches, measured to the nearest one-sixteenth inch, reported in decimal equivalents.  
 \*\*Unfactored rate – does not include any safety or correction factors.

**B.4.0 DISCUSSION**

As detailed above, stabilized, raw infiltration rates varied between zero and 1½ inches per hour. Note that these infiltration rates do not include any safety or correction factors. We recommend the stormwater infiltration system designer consult the appropriate design manual in order to assign appropriate safety/correction factors to calculate the design infiltration rate for the proposed infiltration system(s). Once the design is completed, we recommend the infiltration system design (provided by others) and location be reviewed by the geotechnical engineer. If the location and/or depth of the system(s) change from what was indicated at the time of our fieldwork, additional testing may be recommended.

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## **Appendix C: Pavement Structural Capacity Evaluation Marylhurst School New Life Church Site Expansion**

**Marylhurst School New Life Church Site Expansion  
19915 Old River Drive  
West Linn, Oregon**

**CGT Project Number G1804863**

July 19, 2018

*Prepared For:*  
*Ms. Sheila Walker*  
*The Marylhurst School*  
*1232 Linn Avenue*  
*Oregon City, Oregon 97045*

*Prepared by*  
**Carlson Geotechnical**



### **C.1.0 BACKGROUND**

CGT performed a quantitative evaluation of the structural capacity of the existing pavements for the Marylhurst School New Life Church Site Expansion project. The purpose of our evaluation was to estimate if the existing pavement met the structural requirements for an assumed pavement life and determine if structural enhancements are required to help maintain serviceability. We performed the structural capacity evaluation based on visual survey and materials investigation/testing in general accordance with Sections 5.3 and 5.4 of the AASHTO Guide for Design of Pavement Structures, 1993 (AASHTO), and Section 2 of ODOT's 2018 Pavement Data Collection Manual. The following sections summarize the results of the visual condition survey, the results of our structural capacity analyses, and conclusions for the pavement structure.

### **C.2.0 PAVEMENT MATERIALS INVESTIGATION**

As indicated in the geotechnical report, CGT advanced four shallow subsurface explorations (pavement cores C-1 through C-4) within the existing pavement on June 29, 2018. The Pavement Site Plan, Figure C1, shows the approximate locations of the pavement cores and subsurface explorations. The overall results of our completed field investigation for the entire project are detailed in Section 2.3 of the report. The results of the pavement cores are briefly summarized below.

- The asphaltic concrete (AC) section observed in the pavement cores was 3 to 4 inches thick.
- The aggregate base section below the AC pavement varied from about 9 to 14 inches thick.
- The subgrade underlying the aggregate base consisted of native, medium plasticity silt or medium to high plasticity, fat clay.

### **C.3.0 STRUCTURAL CAPACITY EVALUATION**

#### **C.3.1 Visual Survey**

##### C.3.1.1 Overview

CGT engineering staff observed surface conditions within the subject pavements during coring on June 29, 2018. The pavements were also observed by a Senior Geotechnical Engineer on July 17, 2018. The purpose of our observations was to identify the type, amount, severity, and location of observed surface distress (deficiencies) in the existing pavement in accordance with AASHTO procedures and ODOT's 2018 Pavement Data Collection Manual<sup>1</sup>. Representative photographs taken during our site visits are shown on the attached Figure C2. The photograph locations are shown on Figure C1.

The following table presents a checklist of typical surface deficiencies in flexible (asphalt) pavement. This table also includes our observations of the presence (or absence) of the surface deficiencies within the parking lot and drive lanes.

---

<sup>1</sup> Previously known as the 2010 Oregon Department of Transportation Distress Survey Manual (ODOT DSM)

**Table C1 Typical Asphalt Pavement Surface Deficiencies**

| Distress Type                    | Typical Cause(s)                                                                                                                                                                                                                                                      | Observed at Site?                               |
|----------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------|
| Rutting in the wheel paths       | Ruts typically develop from consolidation or lateral movement under traffic.                                                                                                                                                                                          | None observed                                   |
| Fatigue (alligator) cracking     | Typically caused by excessive deflection of the surface over unstable subgrade or lower courses of pavement. The unstable support usually is often the result of saturated granular base or subgrade but may be attributed to thin asphalt sections or other factors. | Yes, see Section C.3.1.2 for discussion         |
| Longitudinal/transverse cracking | Typically due to poorly constructed paving joints, shrinkage of asphalt layer, daily temperature cycling, etc.                                                                                                                                                        | Yes, see Section C.3.1.3 for discussion         |
| Patching                         | Typically used where the original pavement surface is removed and replaced, or additional material is applied to the pavement surface after original construction.                                                                                                    | One patch observed, see Photograph 2, Figure C2 |
| Disintegration (potholes)        | Typically caused by weakness in the pavement resulting from insufficient asphalt, failure of base, and/or poor drainage.                                                                                                                                              | None observed                                   |
| Disintegration (raveling)        | Typically caused by lack of compaction and/or improper mix proportions.                                                                                                                                                                                               | Yes, see Section C.3.1.4 for discussion         |
| Localized Subsidence             | Typically caused by poor quality subgrade materials susceptible to consolidation                                                                                                                                                                                      | None observed                                   |
| Edge cracking                    | Typically due to lack of lateral (shoulder) support. Another cause of edge cracking can be settlement or yielding of subgrade or granular base.                                                                                                                       | None observed                                   |
| Edge joint (seam) cracking       | Typically due to poor drainage due to a shoulder being higher than the main pavement.                                                                                                                                                                                 | None observed                                   |
| Corrugations (washboarding)      | This form of distress typically occurs in asphalt layers that lack stability due to less than favorable mix proportions.                                                                                                                                              | None observed                                   |
| Upheaval                         | Typically caused by expansive soils and/or tree roots.                                                                                                                                                                                                                | None observed                                   |

**C.3.1.2 Fatigue (alligator) Cracking and Recommended Mitigation**

We observed only two locations where limited fatigue cracking was present. In both cases, the cracks were interconnected. The cracks were generally ¼-inch to ½-inch in width and exhibited little to no spalling. The most severe example of fatigue cracking is shown in Photograph 4, on the attached Figure C2. The severity of fatigue cracking was characterized as “low” in accordance with guidelines presented in the ODOT DSM. In general, the recommended treatment for this distress is full depth replacement of the asphalt pavement and underlying base rock. Limited subgrade correction may also be required in such locations.

Where fatigue cracking is repaired by removal and replacement, we recommend that the new pavement section in the repair area consist of at least 3½ inches of asphalt concrete over 14 inches of compacted base rock. The asphalt concrete should be compacted to a minimum of 91 percent of the Rice Density (ASTM D 2041). Base rock should be compacted to a minimum of 95 percent of the modified proctor density (ASTM D 1557).

**C.3.1.3 Longitudinal/Transverse Cracking and Recommended Mitigation**

We observed several longitudinal and transverse cracks within the parking lot and drive lanes. An example of longitudinal and transverse cracking is shown as Photograph 1 on the attached Figure C2. The cracks were generally ¼-inch to ½-inch in width. In general, the most substantial longitudinal cracks appeared to follow what would have been seams or laydown patterns associated with the original placement of the pavement.

The degree of longitudinal and transverse cracking was characterized as “low” in accordance with guidelines presented in the ODOT DSM.

Cracks should be cleaned and filled in order to limit intrusion of water and fines which lead to further deterioration. Crack filling is recommended as a standard maintenance practice and prior to any overlay, if place.

#### C.3.1.4 Disintegration (Raveling) and Recommended Mitigation

We observed negligible to low severity raveling within the parking areas and drive lanes. The most evident raveling was noted along what was likely a laydown pattern associated with the original pavement installation and is shown as Photograph 3 on the attached Figure C2. The raveling observed at this location appears to be a result low laydown temperature or lack of compaction when installed. The vast majority of the parking areas and drive lanes did not show raveling and where observed would be characterized as negligible to “low” in accordance with guidelines presented in the ODOT DSM.

Measures to address raveling vary from surface coating to asphalt overlay. Surface treatments, such as seal coating or chip sealing, can extend the service life of raveled surfaces.

### **C.3.2 Structural Capacity Evaluation**

#### C.3.2.1 Methodology

We evaluated the structural capacity of the existing pavement structure using the results of the pavement materials investigation and visual survey in general accordance with Section 5.4.5 of AASHTO. The purpose of this evaluation was to determine whether structural enhancement (such as an overlay) was required to help manage anticipated design vehicular traffic. The methodology presented by AASHTO incorporates the use of structural numbers (SN) as follows:

- $SN_{eff}$  = Effective structural number of the existing pavement structure, determined from the visual condition survey and investigation of the existing pavement.
- $SN_f$  = Required structural number for future traffic.
- $SN_{oi}$  = Required overlay structural number. This value is equal to  $SN_f - SN_{eff}$ . The methodology indicates that, in the event that  $SN_{eff}$  is greater than  $S_f$ , and no functional deficiencies are observed in the existing pavement, an overlay is not required. Similarly, in the event that  $SN_{eff}$  is less than  $SN_f$ , an overlay is required to maintain the desired level of serviceability over the indicated design period.

#### C.3.2.2 Design Input Parameters

For the purposes of calculating the structural numbers, a number of parameters were estimated based on the results of the visual survey and pavement investigation. Input parameters related to future traffic and level of serviceability were based on guidelines presented in the Asphalt Pavement Association of Oregon (APAO) Asphalt Pavement Design Guide (APDG). The estimated single-axle loads (ESALs) are based on the APAO APDG Table 3.1 Level II. Other pavement design input parameters are based on AASHTO and APAO guidelines. The parameters used in the evaluation are shown in the following table.

**Table C2 Design Input Parameters**

| Structural Number            | Required Input Parameter                                                      | Value Used in Evaluation |
|------------------------------|-------------------------------------------------------------------------------|--------------------------|
| SN <sub>eff</sub>            | a <sub>1</sub> = Structural layer coefficient, AC layer                       | 0.35                     |
|                              | a <sub>2</sub> = Structural layer coefficient, base layer                     | 0.10                     |
|                              | a <sub>3</sub> = Structural layer coefficient, subbase layer                  | N/A                      |
|                              | D <sub>1</sub> = Thickness of existing pavement, surface layer                | Varied <sup>1</sup>      |
|                              | D <sub>2</sub> = Thickness of existing pavement, base layer                   | Varied <sup>1</sup>      |
|                              | D <sub>3</sub> = Thickness of existing pavement, subbase layer                | N/A                      |
|                              | M <sub>2</sub> = Drainage coefficient for granular base                       | 0.8                      |
|                              | M <sub>3</sub> = Drainage coefficient for granular subbase                    | N/A                      |
| SN <sub>r</sub> <sup>2</sup> | N <sub>r</sub> = Design period <sup>2</sup>                                   | 20 years                 |
|                              | ESAL <sub>r</sub> = Design 18-kip ESAL over design period <sup>3</sup>        | 50,000                   |
|                              | M <sub>R</sub> = Design resilient modulus <sup>4</sup>                        | 4,000 psi                |
|                              | Design Serviceability (PSI) Loss (Initial = 4.2, Terminal = 2.5) <sup>2</sup> | 1.7                      |
|                              | R = Design Reliability <sup>2</sup>                                           | 75 percent               |
|                              | S <sub>o</sub> = Design Standard Deviation                                    | 0.49                     |

<sup>1</sup>Layer thicknesses were evaluated on a case-by-case basis based on the results of the explorations. See Section C.3.2.3 for details.

<sup>2</sup>Values based on AASHTO and APAO guidelines for most pavements of this type.

<sup>3</sup>ESAL value based on APAO APDG Table 3.1 Level II.

<sup>4</sup>Value based on silt and clay subgrade soils encountered in the explorations and correlated to Figure 3.11 of APAO APDG.

The following summarizes additional comments on the values presented in Table C2:

- Layer coefficients (a<sub>1</sub>, a<sub>2</sub>, and a<sub>3</sub>) were determined based on results of visual condition survey discussed in Section C.3.1 above and Table 5.2 of AASHTO.
- Layer thicknesses (D<sub>1</sub>, D<sub>2</sub>, and D<sub>3</sub>) were based on results of our pavement materials investigation.
- A design period of 20 years and design serviceability values of 4.2 (initial) and 2.5 (terminal) were assigned in accordance AASHTO and APAO guidelines.
- The value used for drainage coefficients (m<sub>n</sub>) was selected in accordance with Table 2.4 of the referenced AASHTO manual, based on “good” drainage characteristics of the base and subgrade materials. This quality of drainage was selected based on the unsaturated nature of the pavement materials during our investigation in June 2018.
- The value used for standard deviation (S<sub>o</sub>) was selected in accordance with Section 5.3 of the referenced ODOT Pavement Design Guide (August 2011).

### C.3.2.3 Results of Analyses

Using the above inputs and procedures presented by AASHTO, we calculated the structural numbers for the parking areas and drive lanes as illustrated by the core results and design analysis. The following table summarizes the results of our analyses:

**Table C3      Calculated Structural Numbers**

| Area of Interest <sup>1</sup> | Pavement Exploration | Existing Pavement Section (inches) |                          | Calculated Structural Number |                 |                  |
|-------------------------------|----------------------|------------------------------------|--------------------------|------------------------------|-----------------|------------------|
|                               |                      | AC Thickness                       | Aggregate Base Thickness | SN <sub>eff</sub>            | SN <sub>f</sub> | SN <sub>ol</sub> |
| Parking Areas and Drive Lanes | C-1                  | 3                                  | 9                        | 1.95                         | 2.60            | 0.65             |
|                               | C-2                  | 4                                  | 14                       | 2.80                         | 2.60            | NA               |
|                               | C-3                  | 3½                                 | 9                        | 2.13                         | 2.60            | 0.47             |
|                               | C-4                  | 3½                                 | 14                       | 2.63                         | 2.60            | NA               |

**C.4.0 REVIEW & DISCUSSION**

We completed a pavement condition survey and structural capacity evaluation of the existing pavement within the parking areas and drive lanes for the Marylhurst School New Life Church Site Expansion project to determine whether structural enhancement was required to help manage anticipated future vehicular traffic. Although some longitudinal and transverse cracks, limited raveling, and isolated fatigue cracks were observed, we found the pavements to be in good overall condition.

As indicated above, we observed limited fatigue cracking of low severity at a couple of locations. Fatigue cracking is typically attributed to inadequate structural support of the pavement section, and once the “alligator” pattern has developed on the pavement surface, surficial repairs (i.e., crack sealants, etc.) are no longer effective. We recommend that removal and replacement of the existing AC section in these areas is warranted.

As the results in Table C3 indicate, two of the four core locations indicate a sufficient structural number to meet the long term needs of the assumed traffic loading. However, results for two core locations suggest that additional structural capacity in the pavement section will be needed. We offer the following principal recommendations for your consideration in order to extend the service life of the existing pavements on the order of 20 years.

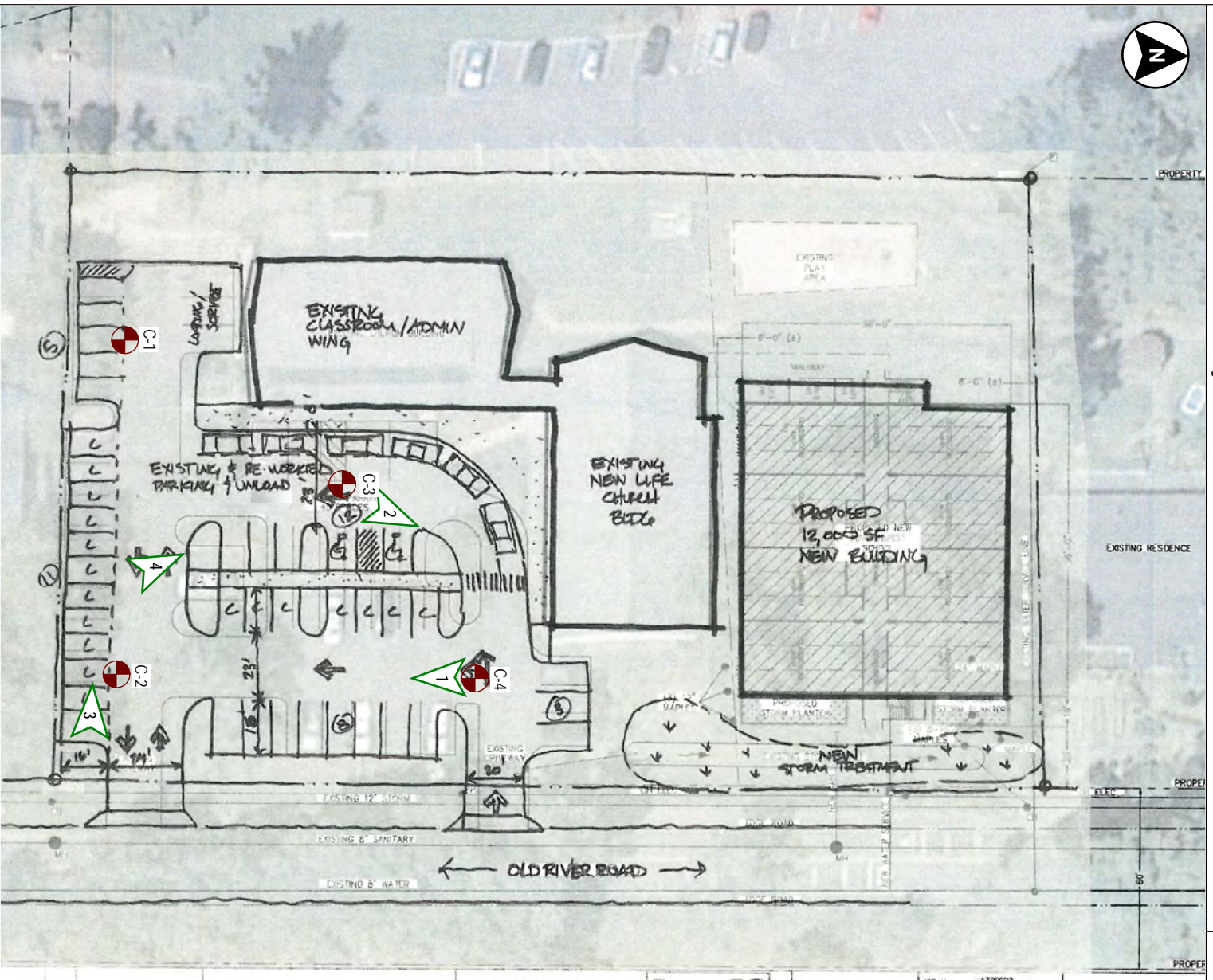
- Repair the fatigue cracked locations.
- Clean and fill all transverse and longitudinal cracks.
- Sealcoat or preferably chip seal the pavement surface to address raveling and provide a new “wearing surface”, particularly important above repaired areas.
- Monitor the pavement performance annually.
- Make minor repairs and perform minor maintenance as needed.
- Anticipate and budget for a minimum 1½-inch overlay within 5 years.



Attachments: Pavement Site Plan, Figure C1  
 Site Photographs, Figure C2

MARYLHURST SCHOOL NEW LIFE CHURCH SITE EXPANSION - WEST LINN, OREGON  
 Project Number G1804863

Figure C1  
 Pavement Site Plan



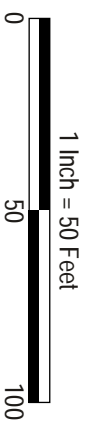
LEGEND

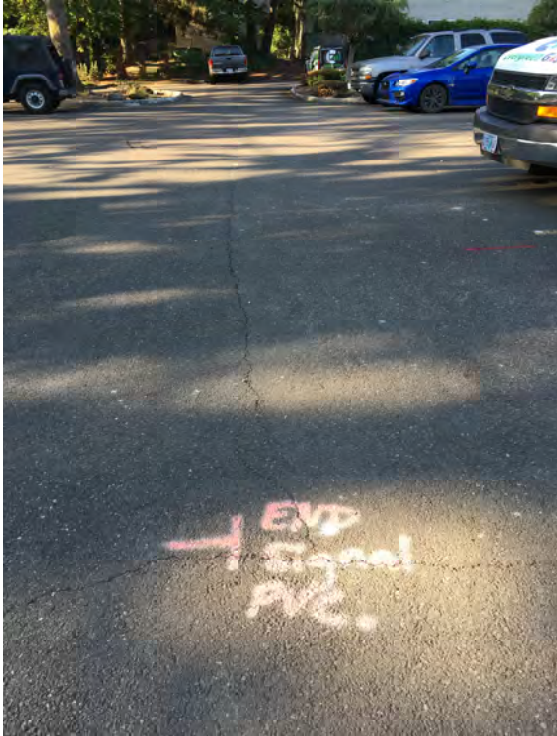
- C-1  Pavement core, DCP test, and hand auger boring
-  Site photograph shown on Figure C2



Drafted by: AS

NOTES: Drawing based on Preliminary Site Plan (Sheet C-1.0, dated January 3, 2018) prepared by KPFF, modified by CGT. Locations noted are approximate.





Photograph 1: Longitudinal and Transverse Cracking



Photograph 2: Patch in Drive Lane



Photograph 3: Surface Raveling



Photograph 4: Fatigue Cracking



Drafted by: ALS

See Figure C2 for approximate photograph locations and directions. Photographs were taken at the time of our fieldwork.

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## Appendix D: Liquefaction Analyses

**Marylhurst School New Life Church Site Expansion  
19915 Old River Drive  
West Linn, Oregon**

**CGT Project Number G1804863**

July 19, 2018

*Prepared For:*

Ms. Sheila Walker  
The Marylhurst School  
1232 Linn Avenue  
Oregon City, Oregon 97045

*Prepared by*  
**Carlson Geotechnical**



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**D.4.0 REVIEW OF ESTIMATED SETTLEMENTS..... 6**

**ATTACHMENTS:** Liquefaction Analyses Results

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### **D.1.0 INTRODUCTION**

In general, liquefaction occurs when deposits of loose/soft, saturated, cohesionless soils, generally sands and silts, are subjected to strong earthquake shaking. If these deposits cannot drain quickly enough, pore water pressures can increase, approaching the value of the overburden pressure. The shear strength of a cohesionless soil is directly proportional to the effective stress, which is equal to the difference between the overburden pressure and the pore water pressure. When the pore water pressure increases to the value of the overburden pressure, the shear strength of the soil reduces to zero, and the soil deposit can liquefy. The liquefied soils can undergo rapid consolidation or, if unconfined, can flow as a liquid. Structures supported by the liquefied soils can experience rapid, excessive settlement, shearing, or even catastrophic failure. The Oregon Department of Geology and Mineral Industries' Oregon Statewide Geohazards Viewer<sup>1</sup> shows a high hazard for liquefaction for the site and immediate vicinity.

### **D.2.0 QUALITATIVE ASSESSMENT**

For fine-grained soils, susceptibility to liquefaction is evaluated based on penetration resistance and plasticity, among other characteristics. Criteria for identifying non-liquefiable, fine-grained soils are constantly evolving. Current practice to identify non-liquefiable, fine-grained soils is based on moisture content and plasticity characteristics of the soils<sup>2,3</sup>. The susceptibility of sands, gravels, and sand-gravel mixtures to liquefaction is typically assessed based on penetration resistance, as measured using SPTs, CPTs, or Becker Hammer Penetration tests (BPTs).

Subsurface conditions encountered at the site are described in Section 2.3 of the geotechnical report. We assessed the liquefaction susceptibility of the soils encountered using the criteria referenced above for fine-grained soils. Based on their low plasticity, very soft to medium stiff consistency, and very loose to loose relative density, the near-surface silt (ML), sandy silt (ML), silty sand (SM), and poorly graded sand (SP) are judged susceptible to liquefaction when adequately saturated and subjected to design level earthquake shaking. These soils extended to depths of approximately 16 to 20 feet below ground surface (bgs) in the borings advanced in the location of the proposed building. Below these depths, the soils consisted of high plasticity, stiff to hard, fat clays (CH) with layers of sand (SP), silty sand (SM), and clayey sand (SC) that were generally dense to very dense and are therefore considered non-liquefiable.

### **D.3.0 QUANTITATIVE ANALYSIS**

We performed quantitative liquefaction triggering and settlement analysis for the site using industry standard procedures detailed in the following sections.

#### **D.3.1 Soil and Groundwater**

Soil and groundwater parameters were based on the results of the geotechnical investigation performed as part of this assignment, summarized in Section 2.3 of the geotechnical report. Our analyses relied on

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<sup>1</sup> Oregon Department of Geology and Mineral Industries, 2018. Oregon Statewide Geohazards Viewer, accessed July 2018, from DOGAMI web site: <http://www.oregongeology.org/sub/hazvu/index.htm>.

<sup>2</sup> Seed, R.B. et al., 2003. Recent Advances in Soil Liquefaction Engineering: A Unified and Consistent Framework. Earthquake Engineering Research Center Report No. EERC 2003-06.

<sup>3</sup> Bray, Jonathan D., Sancio, Rodolfo B., et al., 2006. Liquefaction Susceptibility of Fine-Grained Soils, Journal of Geotechnical and Geoenvironmental Engineering, Volume 132, Issue 9, September 2006.

soil type and SPT data obtained from boring B-3, which was advanced to a depth of approximately 61½ feet bgs.

As discussed in Section 2.3.3 of the geotechnical report, the depth to groundwater was variable between the borings, ranging from approximately 11½ to 19 feet bgs in June of 2018. Our research of groundwater levels in the vicinity of the site indicated similarly variable groundwater levels. We conclude the groundwater observed within our borings is likely perched and reflects the variable permeability of the site soils. Perched groundwater is often discontinuous, both laterally and vertically, and can vary significantly through time (e.g. seasonally and annually). Accordingly, we modeled two groundwater scenarios, one reflecting the depth to groundwater observed in B-3 (15 feet bgs) and one reflecting a hypothetical, seasonal high (10 feet bgs).

### D.3.2 Seismic Scaling Factors

Seismic scaling factors required for quantitative liquefaction analysis include earthquake magnitude (M) and ground surface peak ground acceleration (PGA). In accordance with the 2014 Oregon Structural Specialty Code (OSSC) and ASCE 7-10, we evaluated liquefaction potential for the “aggregate” seismic event, which is a design-level event that is calculated considering the cumulative effect from all seismic sources in the region for the indicated probability of exceedance (2 percent in 50 years).

Section 11.8.3 of ASCE 7-10 provides guidance for selecting the aggregate “bedrock” (Site Class B) PGA, site coefficient to account for site soil effects, and ground surface PGA for use in liquefaction analysis. No guidance is provided for selection of a corresponding earthquake magnitude (M). Recognizing the ground surface PGA was derived using aggregated (composite) probabilistic data for design-level earthquakes, we assigned the earthquake magnitude for use in our analyses by taking the mean value from the de-aggregated seismic hazard data available at the USGS Unified Hazard Tool website<sup>4</sup>. The parameters for the aggregate seismic source are presented in Table D1.

| Parameter                                                                                   | Value  | Source                              |
|---------------------------------------------------------------------------------------------|--------|-------------------------------------|
| Site Classification                                                                         | E      | Section 3.1 of main report          |
| Mapped MCE <sub>G</sub> “Bedrock” Peak Ground Acceleration, PGA                             | 0.417g | Figure 22-7 of ASCE 7-10            |
| Site Coefficient, F <sub>PGA</sub>                                                          | 0.900  | Table 11.8-1 of ASCE 7-10           |
| MCE <sub>G</sub> Peak Ground Acceleration Adjusted for Site Class Effects, PGA <sub>M</sub> | 0.38g  | Equation 11.8-1 of ASCE 7-10        |
| Aggregate Earthquake Magnitude                                                              | M7.3   | Mean value from de-aggregation data |

Note: MCE = Maximum Considered Earthquake

<sup>4</sup> United States Geological Survey, 2018. NSHMP PSHA Unified Hazard Tool, accessed July 2018, from the USGS website <https://earthquake.usgs.gov/hazards/interactive/index.php>.

### D.3.3 Liquefaction Triggering and Settlement Analysis

Our liquefaction triggering and settlement analyses were performed using methods detailed in Idriss and Boulanger (2014)<sup>5</sup>. We utilized the commercially available software program LiqSVs (version 1.2.1.1) produced by Geologismiki to perform the SPT-based liquefaction analysis. With the exception of the non-liquefiable zones referenced above, all soil types were considered in the evaluation of liquefaction potential. The triggering analysis showed the near-surface, silt (ML), sandy silt (ML), silty sand (SM), and poorly graded sand (SP) are liquefiable to a depth of approximately 16 feet bgs. Below those depths, the factors of safety against liquefaction were generally greater than 2.0 and the soils are considered non-liquefiable. Detailed results of the triggering and settlement analyses are attached.

We took the incremental settlement estimates produced by the software and applied depth weighting factors, as outlined in Cetin, *et al.* (2009)<sup>6</sup>. The results of our calculations are presented in the following tables.

**Table D2 Factored Settlement using Cetin Depth Factor Approach, B-3, Groundwater at 10 feet bgs**

| Mid-Layer Depth | Layer Thickness | Unfactored Incremental Settlement | Unfactored Accumulated Settlement | Cetin Depth Factor | Factored Incremental Settlement | Factored Accumulated Settlement |
|-----------------|-----------------|-----------------------------------|-----------------------------------|--------------------|---------------------------------|---------------------------------|
| feet            | feet            | inches                            | inches                            | dimensionless      | inches                          | inches                          |
| 2.50            | 5.00            | 0                                 | 1.66                              | 0.96               | 0.00                            | 1.35                            |
| 6.25            | 2.50            | 0                                 | 1.66                              | 0.90               | 0.00                            | 1.35                            |
| 10.00           | 5.00            | 1.12                              | 1.66                              | 0.83               | 0.93                            | 1.35                            |
| 13.75           | 2.50            | 0.54                              | 0.54                              | 0.77               | 0.42                            | 0.42                            |
| 15.75           | 1.50            | 0                                 | 0.00                              | 0.74               | 0.00                            | 0.00                            |
| 20.75           | 8.50            | 0                                 | 0.00                              | 0.65               | 0.00                            | 0.00                            |
| 27.50           | 5.00            | 0                                 | 0.00                              | 0.54               | 0.00                            | 0.00                            |
| 32.50           | 5.00            | 0                                 | 0.00                              | 0.46               | 0.00                            | 0.00                            |
| 35.50           | 1.00            | 0                                 | 0.00                              | 0.41               | 0.00                            | 0.00                            |
| 38.00           | 4.00            | 0                                 | 0.00                              | 0.37               | 0.00                            | 0.00                            |
| 44.00           | 8.00            | 0                                 | 0.00                              | 0.27               | 0.00                            | 0.00                            |
| 50.50           | 5.00            | 0                                 | 0.00                              | 0.16               | 0.00                            | 0.00                            |
| 56.50           | 7.00            | 0                                 | 0.00                              | 0.06               | 0.00                            | 0.00                            |
| 60.75           | 1.50            | 0                                 | 0.00                              | 0.00               | 0.00                            | 0.00                            |

5 Idriss, I.M., Boulanger, R.W., 2014. CPT and SPT Based Liquefaction Triggering Procedures, Center for Geotechnical Modeling Report No. UCD/CGM-14/01.

6 Cetin, K.O., Bilge, H.T., Wu, J., Kammerer, A.M., and Seed, R.B., 2009. Probabilistic Model for the Assessment of Cyclically Induced Reconsolidation (Volumetric) Settlements, Journal of Geotechnical and Geoenvironmental Engineering, ASCE 135(3), 387-398.

**Table D3 Factored Settlement using Cetin Depth Factor Approach, B-3, Groundwater at 15 feet bgs**

| Mid-Layer Depth | Layer Thickness | Unfactored Incremental Settlement | Unfactored Accumulated Settlement | Cetin Depth Factor | Factored Incremental Settlement | Factored Accumulated Settlement |
|-----------------|-----------------|-----------------------------------|-----------------------------------|--------------------|---------------------------------|---------------------------------|
| feet            | feet            | inches                            | inches                            | dimensionless      | inches                          | inches                          |
| 2.50            | 5.00            | 0                                 | 0.54                              | 0.96               | 0.00                            | 0.42                            |
| 6.25            | 2.50            | 0                                 | 0.54                              | 0.90               | 0.00                            | 0.42                            |
| 10.00           | 5.00            | 0                                 | 0.54                              | 0.83               | 0.00                            | 0.42                            |
| 13.75           | 2.50            | 0.54                              | 0.54                              | 0.77               | 0.42                            | 0.42                            |
| 15.75           | 1.50            | 0                                 | 0.00                              | 0.74               | 0.00                            | 0.00                            |
| 20.75           | 8.50            | 0                                 | 0.00                              | 0.65               | 0.00                            | 0.00                            |
| 27.50           | 5.00            | 0                                 | 0.00                              | 0.54               | 0.00                            | 0.00                            |
| 32.50           | 5.00            | 0                                 | 0.00                              | 0.46               | 0.00                            | 0.00                            |
| 35.50           | 1.00            | 0                                 | 0.00                              | 0.41               | 0.00                            | 0.00                            |
| 38.00           | 4.00            | 0                                 | 0.00                              | 0.37               | 0.00                            | 0.00                            |
| 44.00           | 8.00            | 0                                 | 0.00                              | 0.27               | 0.00                            | 0.00                            |
| 50.50           | 5.00            | 0                                 | 0.00                              | 0.16               | 0.00                            | 0.00                            |
| 56.50           | 7.00            | 0                                 | 0.00                              | 0.06               | 0.00                            | 0.00                            |
| 60.75           | 1.50            | 0                                 | 0.00                              | 0.00               | 0.00                            | 0.00                            |

#### D.4.0 REVIEW OF ESTIMATED SETTLEMENTS

Based on the factored, incremental settlements detailed above, our analyses indicate approximately ½ and 1½ inches of total, liquefaction-induced settlement for the groundwater conditions modeled. In our opinion, these estimates effectively bracket the anticipated, liquefaction-induced settlements, which vary as a function of groundwater level. Shallow subsurface conditions encountered in the other borings advanced at the site were relatively uniform and we anticipate similarly liquefiable soil conditions. With regard to differential settlements, we recommend that differential settlement be taken as one half of total settlement, or up to about ¾ inch. We recommend the differential settlement be assumed across the short axis of the structure.



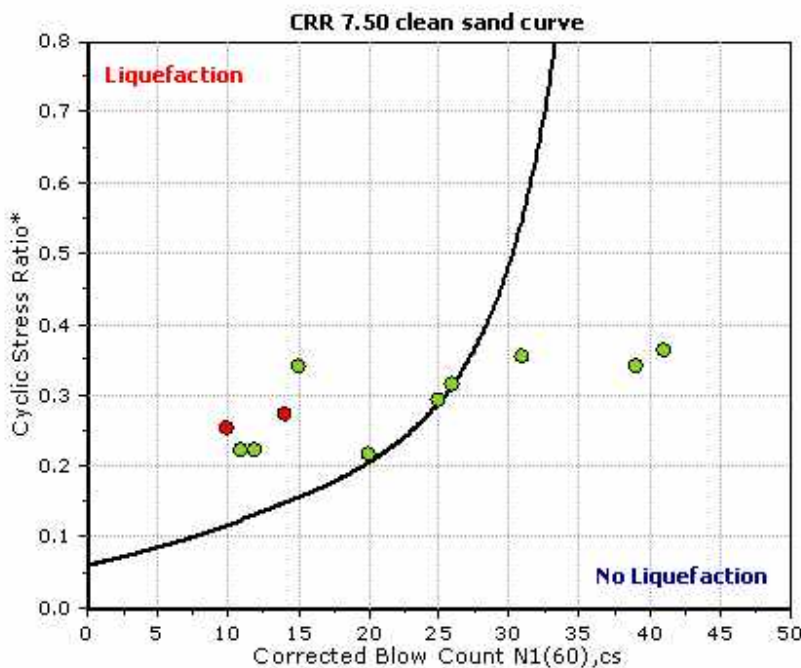
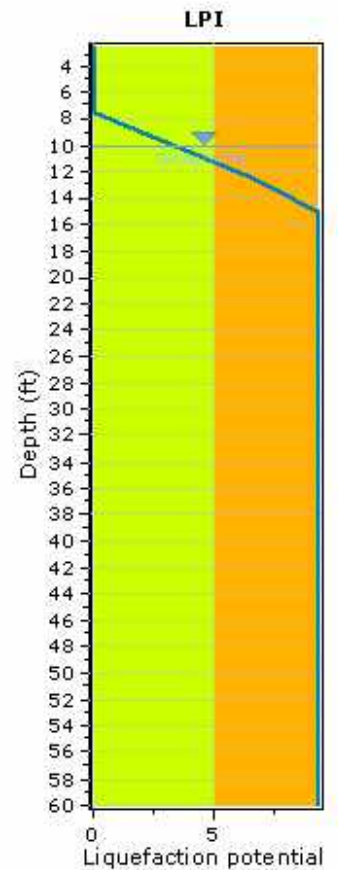
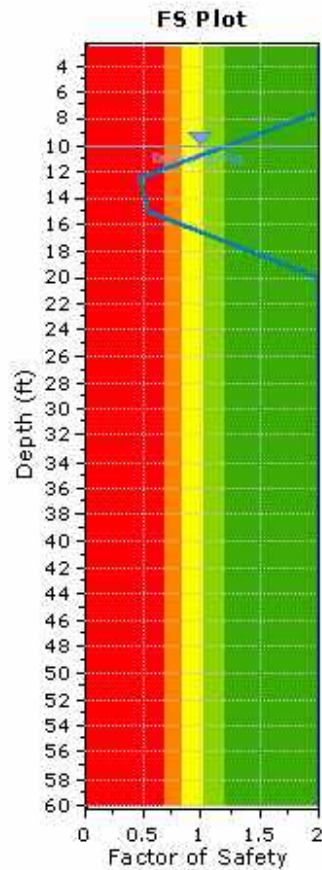
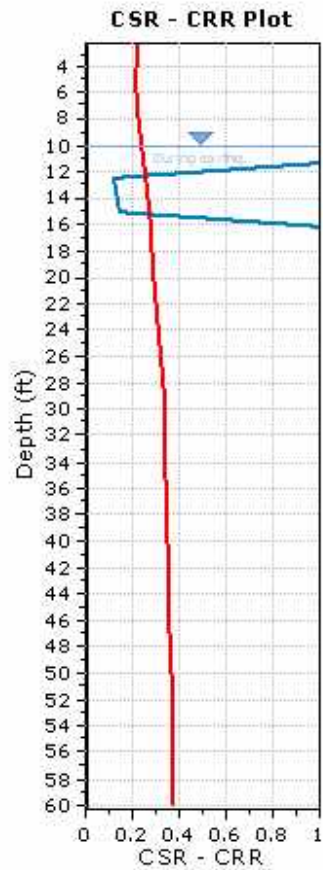
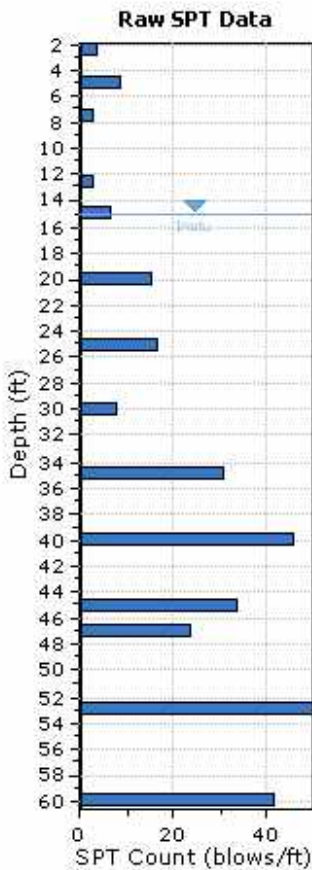
**SPT BASED LIQUEFACTION ANALYSIS REPORT**

**Project title : Marylhurst School New Life Church Site Expansion**  
**Location : 19915 Old River Drive, West Linn, OR**

**SPT Name: B-3**

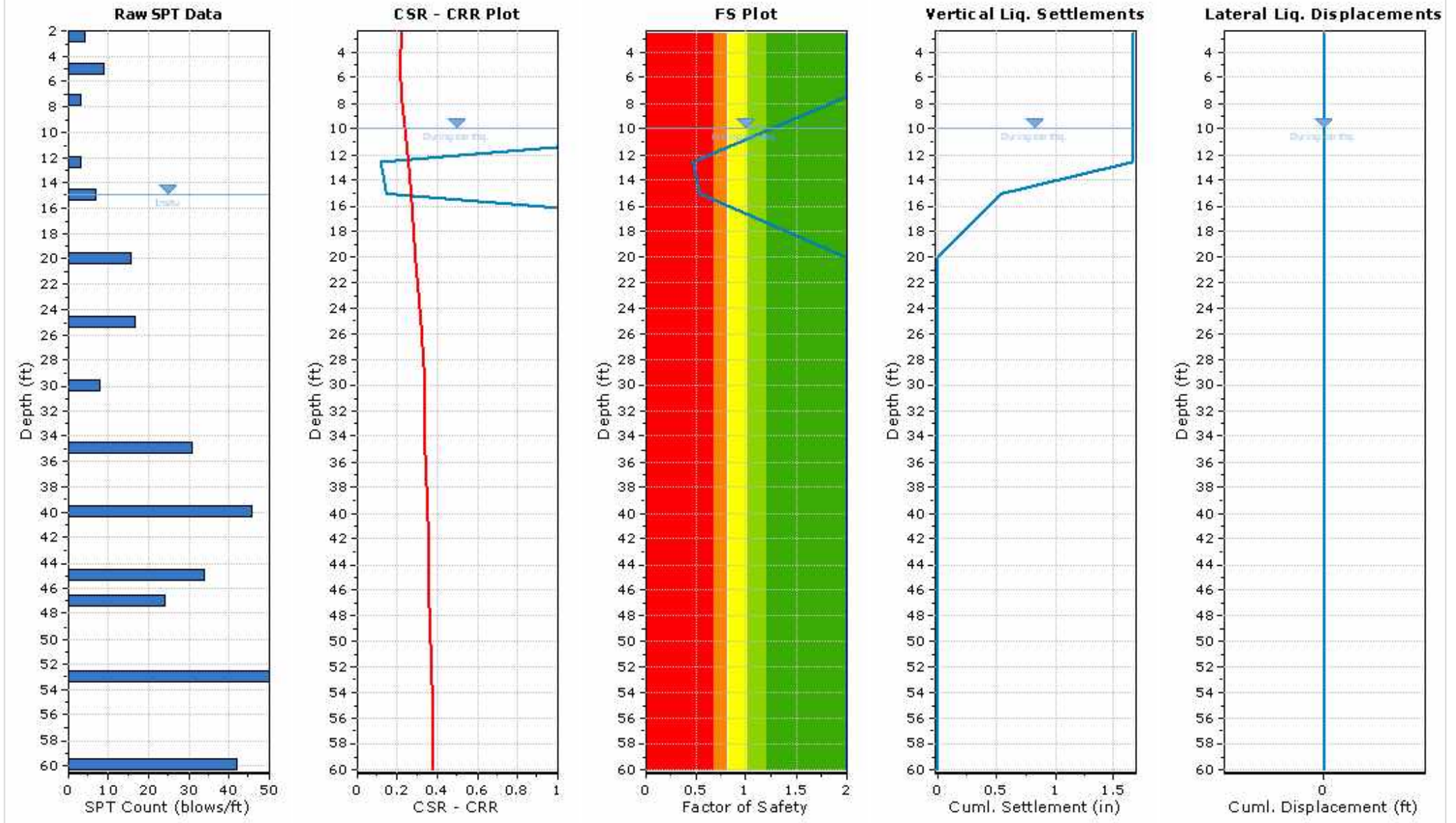
**:: Input parameters and analysis properties ::**

|                          |                          |                              |          |
|--------------------------|--------------------------|------------------------------|----------|
| Analysis method:         | Boulanger & Idriss, 2014 | G.W.T. (in-situ):            | 15.00 ft |
| Fines correction method: | Boulanger & Idriss, 2014 | G.W.T. (earthq.):            | 10.00 ft |
| Sampling method:         | Standard Sampler         | Earthquake magnitude $M_w$ : | 7.30 ft  |
| Borehole diameter:       | 65mm to 115mm            | Peak ground acceleration:    | 0.38 g   |
| Rod length:              | 3.30 ft                  | Eq. external load:           | 0.00 tsf |
| Hammer energy ratio:     | 1.25                     |                              |          |



- F.S. color scheme**
- Red: Almost certain it will liquefy
  - Orange: Very likely to liquefy
  - Yellow: Liquefaction and no liq. are equally likely
  - Green: Unlike to liquefy
  - Dark Green: Almost certain it will not liquefy
- LPI color scheme**
- Red: Very high risk
  - Orange: High risk
  - Yellow: Low risk

**:: Overall Liquefaction Assessment Analysis Plots ::**



| :: Field input data :: |                         |                   |                   |                      |             |
|------------------------|-------------------------|-------------------|-------------------|----------------------|-------------|
| Test Depth (ft)        | SPT Field Value (blows) | Fines Content (%) | Unit Weight (pcf) | Infl. Thickness (ft) | Can Liquefy |
| 2.50                   | 4                       | 86.00             | 115.00            | 5.00                 | Yes         |
| 5.00                   | 9                       | 86.00             | 115.00            | 2.50                 | Yes         |
| 7.50                   | 3                       | 86.00             | 115.00            | 5.00                 | Yes         |
| 12.50                  | 3                       | 86.00             | 115.00            | 2.50                 | Yes         |
| 15.00                  | 7                       | 95.00             | 115.00            | 1.50                 | Yes         |
| 20.00                  | 16                      | 95.00             | 115.00            | 8.50                 | No          |
| 25.00                  | 17                      | 95.00             | 115.00            | 5.00                 | No          |
| 30.00                  | 8                       | 95.00             | 115.00            | 5.00                 | No          |
| 35.00                  | 31                      | 15.00             | 115.00            | 1.00                 | Yes         |
| 40.00                  | 46                      | 18.00             | 115.00            | 4.00                 | Yes         |
| 45.00                  | 34                      | 18.00             | 115.00            | 8.00                 | Yes         |
| 47.00                  | 24                      | 95.00             | 115.00            | 5.00                 | Yes         |
| 53.00                  | 57                      | 30.00             | 115.00            | 7.00                 | Yes         |
| 60.00                  | 42                      | 95.00             | 115.00            | 1.50                 | Yes         |

**Abbreviations**

- Depth: Depth at which test was performed (ft)
- SPT Field Value: Number of blows per foot
- Fines Content: Fines content at test depth (%)
- Unit Weight: Unit weight at test depth (pcf)
- Infl. Thickness: Thickness of the soil layer to be considered in settlements analysis (ft)
- Can Liquefy: User defined switch for excluding/including test depth from the analysis procedure

| :: Cyclic Resistance Ratio (CRR) calculation data :: |                 |                   |                  |             |                      |      |       |       |       |       |       |              |        |                    |                |                    |
|------------------------------------------------------|-----------------|-------------------|------------------|-------------|----------------------|------|-------|-------|-------|-------|-------|--------------|--------|--------------------|----------------|--------------------|
| Depth (ft)                                           | SPT Field Value | Unit Weight (pcf) | $\sigma_v$ (tsf) | $u_o$ (tsf) | $\sigma'_{vo}$ (tsf) | m    | $C_N$ | $C_E$ | $C_B$ | $C_R$ | $C_S$ | $(N_1)_{60}$ | FC (%) | $\Delta(N_1)_{60}$ | $(N_1)_{60cs}$ | CRR <sub>7.5</sub> |
| 2.50                                                 | 4               | 115.00            | 0.14             | 0.00        | 0.14                 | 0.48 | 1.70  | 1.25  | 1.00  | 0.75  | 1.00  | 6            | 86.00  | 5.53               | 12             | 4.000              |
| 5.00                                                 | 9               | 115.00            | 0.29             | 0.00        | 0.29                 | 0.41 | 1.70  | 1.25  | 1.00  | 0.75  | 1.00  | 14           | 86.00  | 5.53               | 20             | 4.000              |
| 7.50                                                 | 3               | 115.00            | 0.43             | 0.00        | 0.43                 | 0.50 | 1.57  | 1.25  | 1.00  | 0.80  | 1.00  | 5            | 86.00  | 5.53               | 11             | 4.000              |
| 12.50                                                | 3               | 115.00            | 0.72             | 0.00        | 0.72                 | 0.53 | 1.23  | 1.25  | 1.00  | 0.85  | 1.00  | 4            | 86.00  | 5.53               | 10             | 0.118              |
| 15.00                                                | 7               | 115.00            | 0.86             | 0.00        | 0.86                 | 0.49 | 1.11  | 1.25  | 1.00  | 0.85  | 1.00  | 8            | 95.00  | 5.50               | 14             | 0.148              |
| 20.00                                                | 16              | 115.00            | 1.15             | 0.16        | 0.99                 | 0.40 | 1.03  | 1.25  | 1.00  | 0.95  | 1.00  | 19           | 95.00  | 5.50               | 25             | 4.000              |
| 25.00                                                | 17              | 115.00            | 1.44             | 0.31        | 1.13                 | 0.40 | 0.98  | 1.25  | 1.00  | 0.95  | 1.00  | 20           | 95.00  | 5.50               | 26             | 4.000              |
| 30.00                                                | 8               | 115.00            | 1.73             | 0.47        | 1.26                 | 0.49 | 0.92  | 1.25  | 1.00  | 1.00  | 1.00  | 9            | 95.00  | 5.50               | 15             | 4.000              |
| 35.00                                                | 31              | 115.00            | 2.01             | 0.62        | 1.39                 | 0.31 | 0.92  | 1.25  | 1.00  | 1.00  | 1.00  | 36           | 15.00  | 3.26               | 39             | 4.000              |
| 40.00                                                | 46              | 115.00            | 2.30             | 0.78        | 1.52                 | 0.20 | 0.93  | 1.25  | 1.00  | 1.00  | 1.00  | 53           | 18.00  | 4.09               | 57             | 4.000              |
| 45.00                                                | 34              | 115.00            | 2.59             | 0.94        | 1.65                 | 0.29 | 0.88  | 1.25  | 1.00  | 1.00  | 1.00  | 37           | 18.00  | 4.09               | 41             | 4.000              |
| 47.00                                                | 24              | 115.00            | 2.70             | 1.00        | 1.70                 | 0.36 | 0.84  | 1.25  | 1.00  | 1.00  | 1.00  | 25           | 95.00  | 5.50               | 31             | 4.000              |
| 53.00                                                | 57              | 115.00            | 3.05             | 1.19        | 1.86                 | 0.14 | 0.93  | 1.25  | 1.00  | 1.00  | 1.00  | 66           | 30.00  | 5.36               | 71             | 4.000              |
| 60.00                                                | 42              | 115.00            | 3.45             | 1.40        | 2.05                 | 0.24 | 0.85  | 1.25  | 1.00  | 1.00  | 1.00  | 45           | 95.00  | 5.50               | 51             | 4.000              |



| :: Cyclic Resistance Ratio (CRR) calculation data :: |                 |                   |                  |             |                      |   |       |       |       |       |       |              |        |                    |                |                    |
|------------------------------------------------------|-----------------|-------------------|------------------|-------------|----------------------|---|-------|-------|-------|-------|-------|--------------|--------|--------------------|----------------|--------------------|
| Depth (ft)                                           | SPT Field Value | Unit Weight (pcf) | $\sigma_v$ (tsf) | $u_o$ (tsf) | $\sigma'_{vo}$ (tsf) | m | $C_N$ | $C_E$ | $C_B$ | $C_R$ | $C_S$ | $(N_1)_{60}$ | FC (%) | $\Delta(N_1)_{60}$ | $(N_1)_{60cs}$ | CRR <sub>7.5</sub> |

**Abbreviations**

- $\sigma_v$ : Total stress during SPT test (tsf)
- $u_o$ : Water pore pressure during SPT test (tsf)
- $\sigma'_{vo}$ : Effective overburden pressure during SPT test (tsf)
- m: Stress exponent normalization factor
- $C_N$ : Overburden correction factor
- $C_E$ : Energy correction factor
- $C_B$ : Borehole diameter correction factor
- $C_R$ : Rod length correction factor
- $C_S$ : Liner correction factor
- $N_{1(60)}$ : Corrected  $N_{SPT}$  to a 60% energy ratio
- $\Delta(N_1)_{60}$ : Equivalent clean sand adjustment
- $N_{1(60)cs}$ : Corrected  $N_{1(60)}$  value for fines content
- CRR<sub>7.5</sub>: Cyclic resistance ratio for M=7.5

| :: Cyclic Stress Ratio calculation (CSR fully adjusted and normalized) :: |                   |                       |                  |                         |       |          |       |                    |                |      |                         |             |       |       |   |
|---------------------------------------------------------------------------|-------------------|-----------------------|------------------|-------------------------|-------|----------|-------|--------------------|----------------|------|-------------------------|-------------|-------|-------|---|
| Depth (ft)                                                                | Unit Weight (pcf) | $\sigma_{v,eq}$ (tsf) | $u_{o,eq}$ (tsf) | $\sigma'_{vo,eq}$ (tsf) | $r_d$ | $\alpha$ | CSR   | MSF <sub>max</sub> | $(N_1)_{60cs}$ | MSF  | CSR <sub>eq,M=7.5</sub> | $K_{sigma}$ | CSR*  | FS    |   |
| 2.50                                                                      | 115.00            | 0.14                  | 0.00             | 0.14                    | 1.00  | 1.00     | 0.247 | 1.24               | 12             | 1.02 | 0.243                   | 1.10        | 0.221 | 2.000 | ● |
| 5.00                                                                      | 115.00            | 0.29                  | 0.00             | 0.29                    | 0.99  | 1.00     | 0.245 | 1.49               | 20             | 1.03 | 0.238                   | 1.10        | 0.216 | 2.000 | ● |
| 7.50                                                                      | 115.00            | 0.43                  | 0.00             | 0.43                    | 0.99  | 1.00     | 0.244 | 1.21               | 11             | 1.01 | 0.240                   | 1.09        | 0.221 | 2.000 | ● |
| 12.50                                                                     | 115.00            | 0.72                  | 0.08             | 0.64                    | 0.97  | 1.00     | 0.269 | 1.19               | 10             | 1.01 | 0.265                   | 1.05        | 0.253 | 0.466 | ● |
| 15.00                                                                     | 115.00            | 0.86                  | 0.16             | 0.71                    | 0.96  | 1.00     | 0.290 | 1.29               | 14             | 1.02 | 0.284                   | 1.04        | 0.272 | 0.543 | ● |
| 20.00                                                                     | 115.00            | 1.15                  | 0.31             | 0.84                    | 0.94  | 1.00     | 0.319 | 1.72               | 25             | 1.05 | 0.304                   | 1.04        | 0.293 | 2.000 | ● |
| 25.00                                                                     | 115.00            | 1.44                  | 0.47             | 0.97                    | 0.92  | 1.00     | 0.337 | 1.77               | 26             | 1.05 | 0.320                   | 1.01        | 0.315 | 2.000 | ● |
| 30.00                                                                     | 115.00            | 1.73                  | 0.62             | 1.10                    | 0.90  | 1.00     | 0.347 | 1.32               | 15             | 1.02 | 0.339                   | 1.00        | 0.341 | 2.000 | ● |
| 35.00                                                                     | 115.00            | 2.01                  | 0.78             | 1.23                    | 0.87  | 1.00     | 0.352 | 2.20               | 39             | 1.08 | 0.325                   | 0.95        | 0.340 | 2.000 | ● |
| 40.00                                                                     | 115.00            | 2.30                  | 0.94             | 1.36                    | 0.85  | 1.00     | 0.353 | 2.20               | 57             | 1.08 | 0.326                   | 0.93        | 0.353 | 2.000 | ● |
| 45.00                                                                     | 115.00            | 2.59                  | 1.09             | 1.50                    | 0.82  | 1.00     | 0.352 | 2.20               | 41             | 1.08 | 0.325                   | 0.90        | 0.362 | 2.000 | ● |
| 47.00                                                                     | 115.00            | 2.70                  | 1.15             | 1.55                    | 0.81  | 1.00     | 0.350 | 2.06               | 31             | 1.07 | 0.327                   | 0.92        | 0.356 | 2.000 | ● |
| 53.00                                                                     | 115.00            | 3.05                  | 1.34             | 1.71                    | 0.78  | 1.00     | 0.346 | 2.20               | 71             | 1.08 | 0.319                   | 0.86        | 0.372 | 2.000 | ● |
| 60.00                                                                     | 115.00            | 3.45                  | 1.56             | 1.89                    | 0.75  | 1.00     | 0.338 | 2.20               | 51             | 1.08 | 0.313                   | 0.83        | 0.377 | 2.000 | ● |

**Abbreviations**

- $\sigma_{v,eq}$ : Total overburden pressure at test point, during earthquake (tsf)
- $u_{o,eq}$ : Water pressure at test point, during earthquake (tsf)
- $\sigma'_{vo,eq}$ : Effective overburden pressure, during earthquake (tsf)
- $r_d$ : Nonlinear shear mass factor
- $\alpha$ : Improvement factor due to stone columns
- CSR : Cyclic Stress Ratio
- MSF : Magnitude Scaling Factor
- CSR<sub>eq,M=7.5</sub>: CSR adjusted for M=7.5
- $K_{sigma}$ : Effective overburden stress factor
- CSR\*: CSR fully adjusted
- FS: Calculated factor of safety against soil liquefaction

| :: Liquefaction potential according to Iwasaki :: |       |      |      |                |       |
|---------------------------------------------------|-------|------|------|----------------|-------|
| Depth (ft)                                        | FS    | F    | wz   | Thickness (ft) | $I_L$ |
| 2.50                                              | 2.000 | 0.00 | 9.62 | 2.50           | 0.00  |
| 5.00                                              | 2.000 | 0.00 | 9.24 | 2.50           | 0.00  |
| 7.50                                              | 2.000 | 0.00 | 8.86 | 2.50           | 0.00  |
| 12.50                                             | 0.466 | 0.53 | 8.10 | 5.00           | 6.59  |

| :: Liquefaction potential according to Iwasaki :: |       |      |      |                |                |
|---------------------------------------------------|-------|------|------|----------------|----------------|
| Depth (ft)                                        | FS    | F    | wz   | Thickness (ft) | I <sub>L</sub> |
| 15.00                                             | 0.543 | 0.46 | 7.71 | 2.50           | 2.69           |
| 20.00                                             | 2.000 | 0.00 | 6.95 | 5.00           | 0.00           |
| 25.00                                             | 2.000 | 0.00 | 6.19 | 5.00           | 0.00           |
| 30.00                                             | 2.000 | 0.00 | 5.43 | 5.00           | 0.00           |
| 35.00                                             | 2.000 | 0.00 | 4.67 | 5.00           | 0.00           |
| 40.00                                             | 2.000 | 0.00 | 3.90 | 5.00           | 0.00           |
| 45.00                                             | 2.000 | 0.00 | 3.14 | 5.00           | 0.00           |
| 47.00                                             | 2.000 | 0.00 | 2.84 | 2.00           | 0.00           |
| 53.00                                             | 2.000 | 0.00 | 1.92 | 6.00           | 0.00           |
| 60.00                                             | 2.000 | 0.00 | 0.86 | 7.00           | 0.00           |

Overall potential I<sub>L</sub> : 9.28

I<sub>L</sub> = 0.00 - No liquefaction  
 I<sub>L</sub> between 0.00 and 5 - Liquefaction not probable  
 I<sub>L</sub> between 5 and 15 - Liquefaction probable  
 I<sub>L</sub> > 15 - Liquefaction certain

| :: Vertical settlements estimation for dry sands :: |                                 |                 |      |                        |      |      |      |                 |                |                     |         |         |
|-----------------------------------------------------|---------------------------------|-----------------|------|------------------------|------|------|------|-----------------|----------------|---------------------|---------|---------|
| Depth (ft)                                          | (N <sub>1</sub> ) <sub>60</sub> | τ <sub>av</sub> | p    | G <sub>max</sub> (tsf) | a    | b    | γ    | ε <sub>15</sub> | N <sub>c</sub> | ε <sub>Nc</sub> (%) | Δh (ft) | ΔS (in) |
| 2.50                                                | 6                               | 0.00            | 0.00 | 0.00                   | 0.00 | 0.00 | 0.00 | 0.00            | 0.00           | 0.00                | 5.00    | 0.000   |
| 5.00                                                | 14                              | 0.00            | 0.00 | 0.00                   | 0.00 | 0.00 | 0.00 | 0.00            | 0.00           | 0.00                | 2.50    | 0.000   |
| 7.50                                                | 5                               | 0.00            | 0.00 | 0.00                   | 0.00 | 0.00 | 0.00 | 0.00            | 0.00           | 0.00                | 5.00    | 0.000   |

Cumulative settlements: 0.000

**Abbreviations**

τ<sub>av</sub>: Average cyclic shear stress  
 p: Average stress  
 G<sub>max</sub>: Maximum shear modulus (tsf)  
 a, b: Shear strain formula variables  
 γ: Average shear strain  
 ε<sub>15</sub>: Volumetric strain after 15 cycles  
 N<sub>c</sub>: Number of cycles  
 ε<sub>Nc</sub>: Volumetric strain for number of cycles N<sub>c</sub> (%)  
 Δh: Thickness of soil layer (in)  
 ΔS: Settlement of soil layer (in)

| :: Vertical & Lateral displacements estimation for saturated sands :: |                                   |                      |                |                   |                      |                    |         |                        |          |
|-----------------------------------------------------------------------|-----------------------------------|----------------------|----------------|-------------------|----------------------|--------------------|---------|------------------------|----------|
| Depth (ft)                                                            | (N <sub>1</sub> ) <sub>60cs</sub> | γ <sub>lim</sub> (%) | F <sub>α</sub> | FS <sub>liq</sub> | γ <sub>max</sub> (%) | e <sub>v</sub> (%) | dz (ft) | S <sub>v-1D</sub> (in) | LDI (ft) |
| 12.50                                                                 | 10                                | 47.32                | 0.91           | 0.466             | 47.32                | 3.74               | 2.50    | 1.121                  | 0.00     |
| 15.00                                                                 | 14                                | 30.65                | 0.79           | 0.543             | 30.65                | 3.02               | 1.50    | 0.543                  | 0.00     |
| 20.00                                                                 | 25                                | 0.00                 | 0.00           | 2.000             | 0.00                 | 0.00               | 8.50    | 0.000                  | 0.00     |
| 25.00                                                                 | 26                                | 0.00                 | 0.00           | 2.000             | 0.00                 | 0.00               | 5.00    | 0.000                  | 0.00     |
| 30.00                                                                 | 15                                | 0.00                 | 0.00           | 2.000             | 0.00                 | 0.00               | 5.00    | 0.000                  | 0.00     |
| 35.00                                                                 | 39                                | 1.07                 | -0.73          | 2.000             | 0.00                 | 0.00               | 1.00    | 0.000                  | 0.00     |
| 40.00                                                                 | 57                                | 0.00                 | -2.17          | 2.000             | 0.00                 | 0.00               | 4.00    | 0.000                  | 0.00     |
| 45.00                                                                 | 41                                | 0.70                 | -0.88          | 2.000             | 0.00                 | 0.00               | 8.00    | 0.000                  | 0.00     |
| 47.00                                                                 | 31                                | 4.04                 | -0.16          | 2.000             | 0.00                 | 0.00               | 5.00    | 0.000                  | 0.00     |
| 53.00                                                                 | 71                                | 0.00                 | -3.38          | 2.000             | 0.00                 | 0.00               | 7.00    | 0.000                  | 0.00     |

| <b>:: Vertical &amp; Lateral displacements estimation for saturated sands ::</b> |                                       |                                |                      |                         |                                |                              |                    |                                  |                     |
|----------------------------------------------------------------------------------|---------------------------------------|--------------------------------|----------------------|-------------------------|--------------------------------|------------------------------|--------------------|----------------------------------|---------------------|
| <b>Depth<br/>(ft)</b>                                                            | <b>(N<sub>1</sub>)<sub>60cs</sub></b> | <b>γ<sub>lim</sub><br/>(%)</b> | <b>F<sub>α</sub></b> | <b>FS<sub>liq</sub></b> | <b>γ<sub>max</sub><br/>(%)</b> | <b>e<sub>v</sub><br/>(%)</b> | <b>dz<br/>(ft)</b> | <b>S<sub>v-1D</sub><br/>(in)</b> | <b>LDI<br/>(ft)</b> |
| 60.00                                                                            | 51                                    | 0.02                           | -1.67                | 2.000                   | 0.00                           | 0.00                         | 1.50               | 0.000                            | 0.00                |
| <b>Cumulative settlements:</b>                                                   |                                       |                                |                      |                         |                                |                              |                    | <b>1.664</b>                     | <b>0.00</b>         |

**Abbreviations**

- γ<sub>lim</sub>: Limiting shear strain (%)
- F<sub>α</sub>/N: Maximun shear strain factor
- γ<sub>max</sub>: Maximum shear strain (%)
- e<sub>v</sub>:: Post liquefaction volumetric strain (%)
- S<sub>v-1D</sub>: Estimated vertical settlement (in)
- LDI: Estimated lateral displacement (ft)

## References

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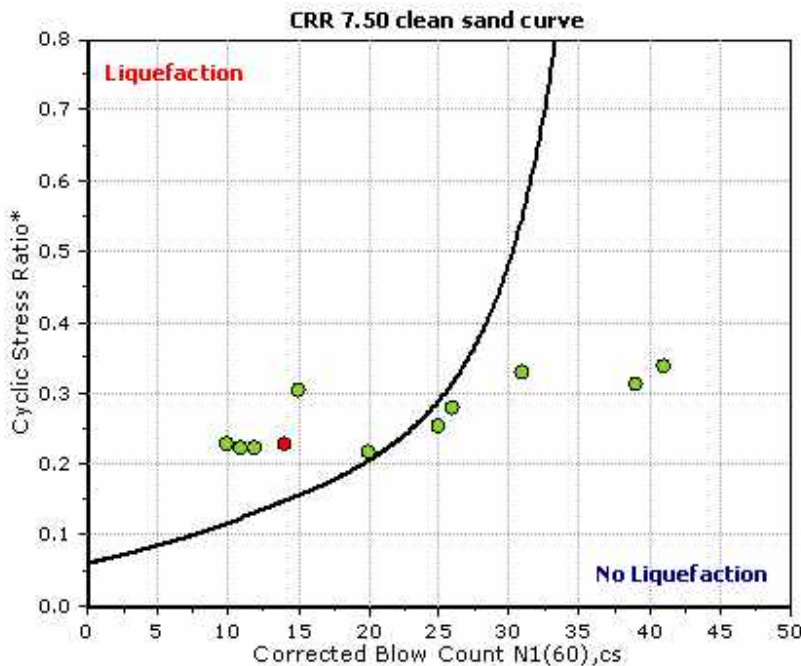
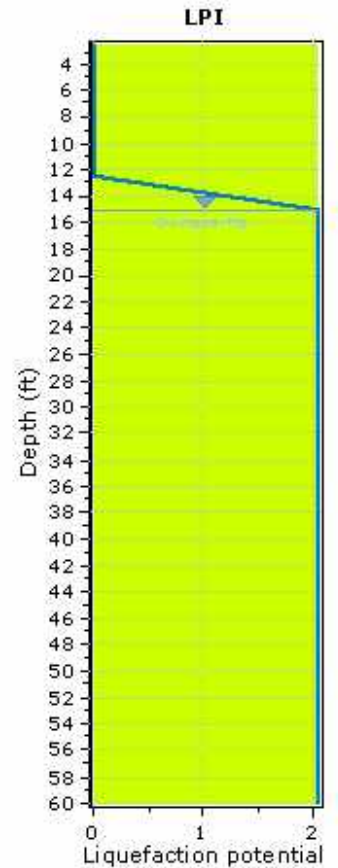
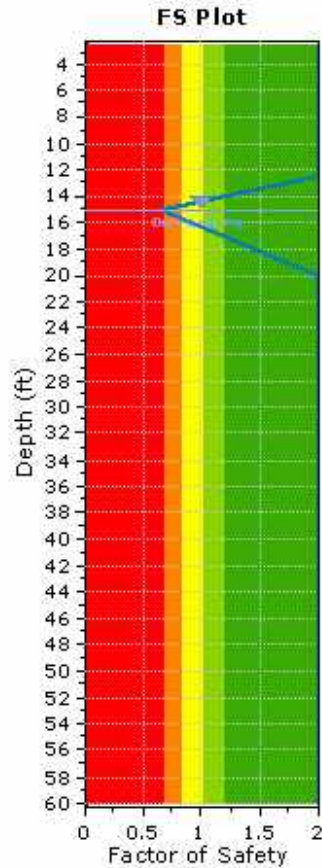
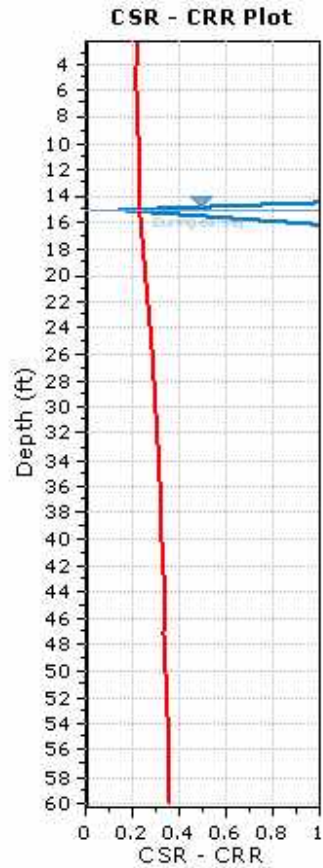
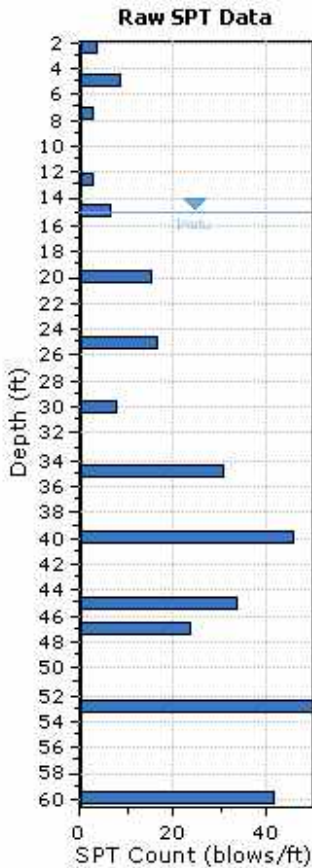
**SPT BASED LIQUEFACTION ANALYSIS REPORT**

**Project title : Marylhurst School New Life Church Site Expansion**  
**Location : 19915 Old River Drive, West Linn, OR**

**SPT Name: B-3**

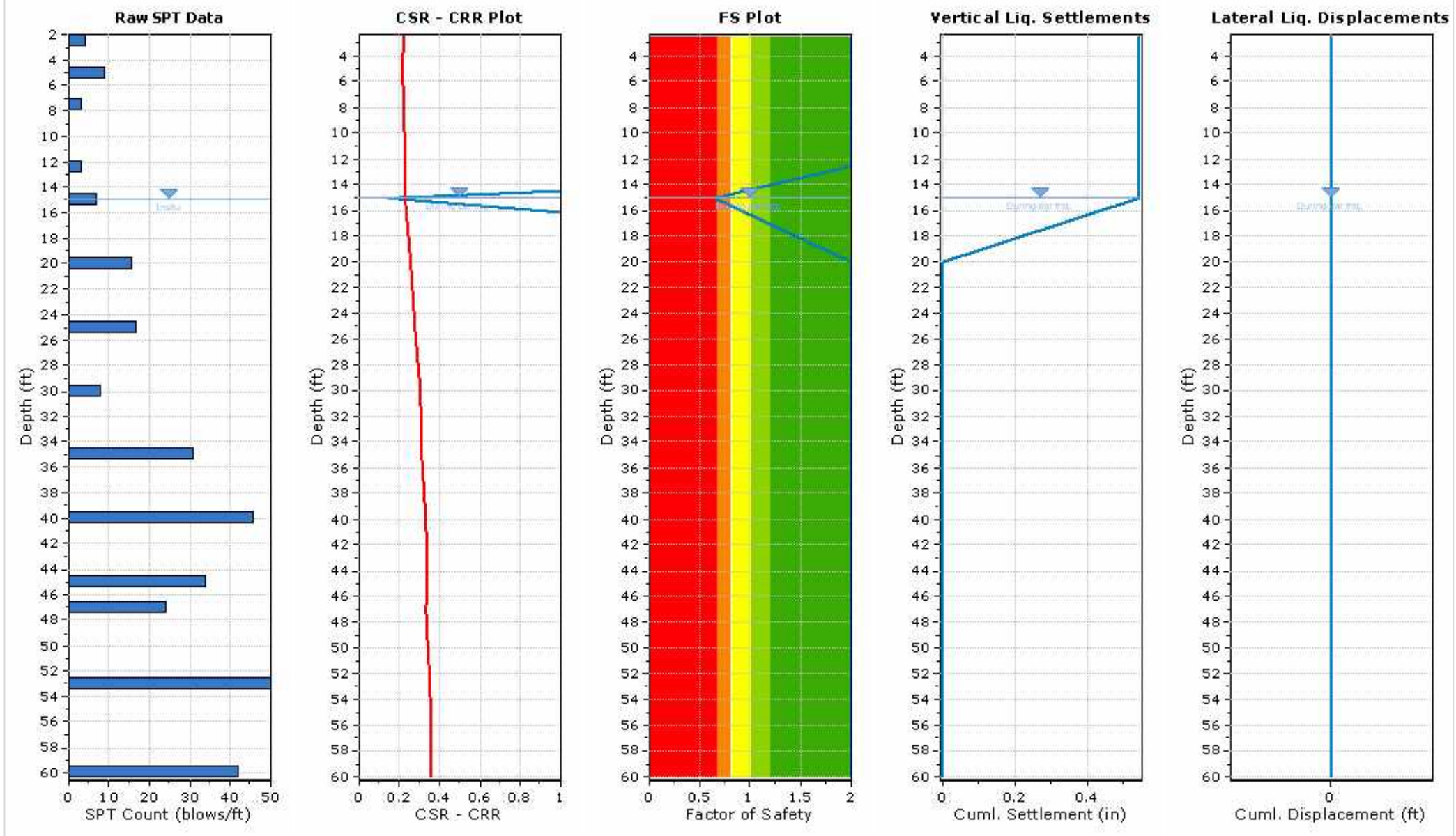
**:: Input parameters and analysis properties ::**

|                          |                          |                              |          |
|--------------------------|--------------------------|------------------------------|----------|
| Analysis method:         | Boulanger & Idriss, 2014 | G.W.T. (in-situ):            | 15.00 ft |
| Fines correction method: | Boulanger & Idriss, 2014 | G.W.T. (earthq.):            | 15.00 ft |
| Sampling method:         | Standard Sampler         | Earthquake magnitude $M_w$ : | 7.30 ft  |
| Borehole diameter:       | 65mm to 115mm            | Peak ground acceleration:    | 0.38 g   |
| Rod length:              | 3.30 ft                  | Eq. external load:           | 0.00 tsf |
| Hammer energy ratio:     | 1.25                     |                              |          |



- F.S. color scheme**
- Red: Almost certain it will liquefy
  - Orange: Very likely to liquefy
  - Yellow: Liquefaction and no liq. are equally likely
  - Green: Unlike to liquefy
  - Dark Green: Almost certain it will not liquefy
- LPI color scheme**
- Red: Very high risk
  - Orange: High risk
  - Green: Low risk

**:: Overall Liquefaction Assessment Analysis Plots ::**



| :: Field input data :: |                         |                   |                   |                      |             |
|------------------------|-------------------------|-------------------|-------------------|----------------------|-------------|
| Test Depth (ft)        | SPT Field Value (blows) | Fines Content (%) | Unit Weight (pcf) | Infl. Thickness (ft) | Can Liquefy |
| 2.50                   | 4                       | 86.00             | 115.00            | 5.00                 | Yes         |
| 5.00                   | 9                       | 86.00             | 115.00            | 2.50                 | Yes         |
| 7.50                   | 3                       | 86.00             | 115.00            | 5.00                 | Yes         |
| 12.50                  | 3                       | 86.00             | 115.00            | 2.50                 | Yes         |
| 15.00                  | 7                       | 95.00             | 115.00            | 1.50                 | Yes         |
| 20.00                  | 16                      | 95.00             | 115.00            | 8.50                 | No          |
| 25.00                  | 17                      | 95.00             | 115.00            | 5.00                 | No          |
| 30.00                  | 8                       | 95.00             | 115.00            | 5.00                 | No          |
| 35.00                  | 31                      | 15.00             | 115.00            | 1.00                 | Yes         |
| 40.00                  | 46                      | 18.00             | 115.00            | 4.00                 | Yes         |
| 45.00                  | 34                      | 18.00             | 115.00            | 8.00                 | Yes         |
| 47.00                  | 24                      | 95.00             | 115.00            | 5.00                 | Yes         |
| 53.00                  | 57                      | 30.00             | 115.00            | 7.00                 | Yes         |
| 60.00                  | 42                      | 95.00             | 115.00            | 1.50                 | Yes         |

**Abbreviations**

- Depth: Depth at which test was performed (ft)
- SPT Field Value: Number of blows per foot
- Fines Content: Fines content at test depth (%)
- Unit Weight: Unit weight at test depth (pcf)
- Infl. Thickness: Thickness of the soil layer to be considered in settlements analysis (ft)
- Can Liquefy: User defined switch for excluding/including test depth from the analysis procedure

| :: Cyclic Resistance Ratio (CRR) calculation data :: |                 |                   |                  |             |                      |      |       |       |       |       |       |              |        |                    |                |                    |
|------------------------------------------------------|-----------------|-------------------|------------------|-------------|----------------------|------|-------|-------|-------|-------|-------|--------------|--------|--------------------|----------------|--------------------|
| Depth (ft)                                           | SPT Field Value | Unit Weight (pcf) | $\sigma_v$ (tsf) | $u_o$ (tsf) | $\sigma'_{vo}$ (tsf) | m    | $C_N$ | $C_E$ | $C_B$ | $C_R$ | $C_S$ | $(N_1)_{60}$ | FC (%) | $\Delta(N_1)_{60}$ | $(N_1)_{60cs}$ | CRR <sub>7.5</sub> |
| 2.50                                                 | 4               | 115.00            | 0.14             | 0.00        | 0.14                 | 0.48 | 1.70  | 1.25  | 1.00  | 0.75  | 1.00  | 6            | 86.00  | 5.53               | 12             | 4.000              |
| 5.00                                                 | 9               | 115.00            | 0.29             | 0.00        | 0.29                 | 0.41 | 1.70  | 1.25  | 1.00  | 0.75  | 1.00  | 14           | 86.00  | 5.53               | 20             | 4.000              |
| 7.50                                                 | 3               | 115.00            | 0.43             | 0.00        | 0.43                 | 0.50 | 1.57  | 1.25  | 1.00  | 0.80  | 1.00  | 5            | 86.00  | 5.53               | 11             | 4.000              |
| 12.50                                                | 3               | 115.00            | 0.72             | 0.00        | 0.72                 | 0.53 | 1.23  | 1.25  | 1.00  | 0.85  | 1.00  | 4            | 86.00  | 5.53               | 10             | 4.000              |
| 15.00                                                | 7               | 115.00            | 0.86             | 0.00        | 0.86                 | 0.49 | 1.11  | 1.25  | 1.00  | 0.85  | 1.00  | 8            | 95.00  | 5.50               | 14             | 0.148              |
| 20.00                                                | 16              | 115.00            | 1.15             | 0.16        | 0.99                 | 0.40 | 1.03  | 1.25  | 1.00  | 0.95  | 1.00  | 19           | 95.00  | 5.50               | 25             | 4.000              |
| 25.00                                                | 17              | 115.00            | 1.44             | 0.31        | 1.13                 | 0.40 | 0.98  | 1.25  | 1.00  | 0.95  | 1.00  | 20           | 95.00  | 5.50               | 26             | 4.000              |
| 30.00                                                | 8               | 115.00            | 1.73             | 0.47        | 1.26                 | 0.49 | 0.92  | 1.25  | 1.00  | 1.00  | 1.00  | 9            | 95.00  | 5.50               | 15             | 4.000              |
| 35.00                                                | 31              | 115.00            | 2.01             | 0.62        | 1.39                 | 0.31 | 0.92  | 1.25  | 1.00  | 1.00  | 1.00  | 36           | 15.00  | 3.26               | 39             | 4.000              |
| 40.00                                                | 46              | 115.00            | 2.30             | 0.78        | 1.52                 | 0.20 | 0.93  | 1.25  | 1.00  | 1.00  | 1.00  | 53           | 18.00  | 4.09               | 57             | 4.000              |
| 45.00                                                | 34              | 115.00            | 2.59             | 0.94        | 1.65                 | 0.29 | 0.88  | 1.25  | 1.00  | 1.00  | 1.00  | 37           | 18.00  | 4.09               | 41             | 4.000              |
| 47.00                                                | 24              | 115.00            | 2.70             | 1.00        | 1.70                 | 0.36 | 0.84  | 1.25  | 1.00  | 1.00  | 1.00  | 25           | 95.00  | 5.50               | 31             | 4.000              |
| 53.00                                                | 57              | 115.00            | 3.05             | 1.19        | 1.86                 | 0.14 | 0.93  | 1.25  | 1.00  | 1.00  | 1.00  | 66           | 30.00  | 5.36               | 71             | 4.000              |
| 60.00                                                | 42              | 115.00            | 3.45             | 1.40        | 2.05                 | 0.24 | 0.85  | 1.25  | 1.00  | 1.00  | 1.00  | 45           | 95.00  | 5.50               | 51             | 4.000              |

| :: Cyclic Resistance Ratio (CRR) calculation data :: |                 |                   |                  |             |                      |   |       |       |       |       |       |              |        |                    |                |                    |
|------------------------------------------------------|-----------------|-------------------|------------------|-------------|----------------------|---|-------|-------|-------|-------|-------|--------------|--------|--------------------|----------------|--------------------|
| Depth (ft)                                           | SPT Field Value | Unit Weight (pcf) | $\sigma_v$ (tsf) | $u_o$ (tsf) | $\sigma'_{vo}$ (tsf) | m | $C_N$ | $C_E$ | $C_B$ | $C_R$ | $C_S$ | $(N_1)_{60}$ | FC (%) | $\Delta(N_1)_{60}$ | $(N_1)_{60cs}$ | CRR <sub>7.5</sub> |

**Abbreviations**

- $\sigma_v$ : Total stress during SPT test (tsf)
- $u_o$ : Water pore pressure during SPT test (tsf)
- $\sigma'_{vo}$ : Effective overburden pressure during SPT test (tsf)
- m: Stress exponent normalization factor
- $C_N$ : Overburden correction factor
- $C_E$ : Energy correction factor
- $C_B$ : Borehole diameter correction factor
- $C_R$ : Rod length correction factor
- $C_S$ : Liner correction factor
- $N_{1(60)}$ : Corrected  $N_{SPT}$  to a 60% energy ratio
- $\Delta(N_1)_{60}$ : Equivalent clean sand adjustment
- $N_{1(60)cs}$ : Corrected  $N_{1(60)}$  value for fines content
- CRR<sub>7.5</sub>: Cyclic resistance ratio for M=7.5

| :: Cyclic Stress Ratio calculation (CSR fully adjusted and normalized) :: |                   |                       |                  |                         |       |          |       |                    |                |      |                         |             |       |       |   |
|---------------------------------------------------------------------------|-------------------|-----------------------|------------------|-------------------------|-------|----------|-------|--------------------|----------------|------|-------------------------|-------------|-------|-------|---|
| Depth (ft)                                                                | Unit Weight (pcf) | $\sigma_{v,eq}$ (tsf) | $u_{o,eq}$ (tsf) | $\sigma'_{vo,eq}$ (tsf) | $r_d$ | $\alpha$ | CSR   | MSF <sub>max</sub> | $(N_1)_{60cs}$ | MSF  | CSR <sub>eq,M=7.5</sub> | $K_{sigma}$ | CSR*  | FS    |   |
| 2.50                                                                      | 115.00            | 0.14                  | 0.00             | 0.14                    | 1.00  | 1.00     | 0.247 | 1.24               | 12             | 1.02 | 0.243                   | 1.10        | 0.221 | 2.000 | ● |
| 5.00                                                                      | 115.00            | 0.29                  | 0.00             | 0.29                    | 0.99  | 1.00     | 0.245 | 1.49               | 20             | 1.03 | 0.238                   | 1.10        | 0.216 | 2.000 | ● |
| 7.50                                                                      | 115.00            | 0.43                  | 0.00             | 0.43                    | 0.99  | 1.00     | 0.244 | 1.21               | 11             | 1.01 | 0.240                   | 1.09        | 0.221 | 2.000 | ● |
| 12.50                                                                     | 115.00            | 0.72                  | 0.00             | 0.72                    | 0.97  | 1.00     | 0.240 | 1.19               | 10             | 1.01 | 0.236                   | 1.04        | 0.228 | 2.000 | ● |
| 15.00                                                                     | 115.00            | 0.86                  | 0.00             | 0.86                    | 0.96  | 1.00     | 0.237 | 1.29               | 14             | 1.02 | 0.233                   | 1.02        | 0.228 | 0.649 | ● |
| 20.00                                                                     | 115.00            | 1.15                  | 0.16             | 0.99                    | 0.94  | 1.00     | 0.269 | 1.72               | 25             | 1.05 | 0.256                   | 1.01        | 0.254 | 2.000 | ● |
| 25.00                                                                     | 115.00            | 1.44                  | 0.31             | 1.13                    | 0.92  | 1.00     | 0.290 | 1.77               | 26             | 1.05 | 0.275                   | 0.99        | 0.278 | 2.000 | ● |
| 30.00                                                                     | 115.00            | 1.73                  | 0.47             | 1.26                    | 0.90  | 1.00     | 0.304 | 1.32               | 15             | 1.02 | 0.297                   | 0.98        | 0.303 | 2.000 | ● |
| 35.00                                                                     | 115.00            | 2.01                  | 0.62             | 1.39                    | 0.87  | 1.00     | 0.312 | 2.20               | 39             | 1.08 | 0.289                   | 0.92        | 0.314 | 2.000 | ● |
| 40.00                                                                     | 115.00            | 2.30                  | 0.78             | 1.52                    | 0.85  | 1.00     | 0.317 | 2.20               | 57             | 1.08 | 0.293                   | 0.89        | 0.328 | 2.000 | ● |
| 45.00                                                                     | 115.00            | 2.59                  | 0.94             | 1.65                    | 0.82  | 1.00     | 0.318 | 2.20               | 41             | 1.08 | 0.294                   | 0.87        | 0.339 | 2.000 | ● |
| 47.00                                                                     | 115.00            | 2.70                  | 1.00             | 1.70                    | 0.81  | 1.00     | 0.318 | 2.06               | 31             | 1.07 | 0.297                   | 0.90        | 0.330 | 2.000 | ● |
| 53.00                                                                     | 115.00            | 3.05                  | 1.19             | 1.86                    | 0.78  | 1.00     | 0.317 | 2.20               | 71             | 1.08 | 0.293                   | 0.83        | 0.351 | 2.000 | ● |
| 60.00                                                                     | 115.00            | 3.45                  | 1.40             | 2.05                    | 0.75  | 1.00     | 0.312 | 2.20               | 51             | 1.08 | 0.289                   | 0.81        | 0.358 | 2.000 | ● |

**Abbreviations**

- $\sigma_{v,eq}$ : Total overburden pressure at test point, during earthquake (tsf)
- $u_{o,eq}$ : Water pressure at test point, during earthquake (tsf)
- $\sigma'_{vo,eq}$ : Effective overburden pressure, during earthquake (tsf)
- $r_d$ : Nonlinear shear mass factor
- $\alpha$ : Improvement factor due to stone columns
- CSR : Cyclic Stress Ratio
- MSF : Magnitude Scaling Factor
- CSR<sub>eq,M=7.5</sub>: CSR adjusted for M=7.5
- $K_{sigma}$ : Effective overburden stress factor
- CSR\*: CSR fully adjusted
- FS: Calculated factor of safety against soil liquefaction

| :: Liquefaction potential according to Iwasaki :: |       |      |      |                |       |
|---------------------------------------------------|-------|------|------|----------------|-------|
| Depth (ft)                                        | FS    | F    | wz   | Thickness (ft) | $I_L$ |
| 2.50                                              | 2.000 | 0.00 | 9.62 | 2.50           | 0.00  |
| 5.00                                              | 2.000 | 0.00 | 9.24 | 2.50           | 0.00  |
| 7.50                                              | 2.000 | 0.00 | 8.86 | 2.50           | 0.00  |
| 12.50                                             | 2.000 | 0.00 | 8.10 | 5.00           | 0.00  |



| <b>:: Liquefaction potential according to Iwasaki ::</b> |       |      |      |                |                |
|----------------------------------------------------------|-------|------|------|----------------|----------------|
| Depth (ft)                                               | FS    | F    | wz   | Thickness (ft) | I <sub>L</sub> |
| 15.00                                                    | 0.649 | 0.35 | 7.71 | 2.50           | 2.06           |
| 20.00                                                    | 2.000 | 0.00 | 6.95 | 5.00           | 0.00           |
| 25.00                                                    | 2.000 | 0.00 | 6.19 | 5.00           | 0.00           |
| 30.00                                                    | 2.000 | 0.00 | 5.43 | 5.00           | 0.00           |
| 35.00                                                    | 2.000 | 0.00 | 4.67 | 5.00           | 0.00           |
| 40.00                                                    | 2.000 | 0.00 | 3.90 | 5.00           | 0.00           |
| 45.00                                                    | 2.000 | 0.00 | 3.14 | 5.00           | 0.00           |
| 47.00                                                    | 2.000 | 0.00 | 2.84 | 2.00           | 0.00           |
| 53.00                                                    | 2.000 | 0.00 | 1.92 | 6.00           | 0.00           |
| 60.00                                                    | 2.000 | 0.00 | 0.86 | 7.00           | 0.00           |

**Overall potential I<sub>L</sub> : 2.06**

I<sub>L</sub> = 0.00 - No liquefaction  
 I<sub>L</sub> between 0.00 and 5 - Liquefaction not probable  
 I<sub>L</sub> between 5 and 15 - Liquefaction probable  
 I<sub>L</sub> > 15 - Liquefaction certain

| <b>:: Vertical settlements estimation for dry sands ::</b> |                                 |                 |      |                        |      |      |      |                 |                |                     |         |         |
|------------------------------------------------------------|---------------------------------|-----------------|------|------------------------|------|------|------|-----------------|----------------|---------------------|---------|---------|
| Depth (ft)                                                 | (N <sub>1</sub> ) <sub>60</sub> | τ <sub>av</sub> | p    | G <sub>max</sub> (tsf) | a    | b    | γ    | ε <sub>15</sub> | N <sub>c</sub> | ε <sub>Nc</sub> (%) | Δh (ft) | ΔS (in) |
| 2.50                                                       | 6                               | 0.00            | 0.00 | 0.00                   | 0.00 | 0.00 | 0.00 | 0.00            | 0.00           | 0.00                | 5.00    | 0.000   |
| 5.00                                                       | 14                              | 0.00            | 0.00 | 0.00                   | 0.00 | 0.00 | 0.00 | 0.00            | 0.00           | 0.00                | 2.50    | 0.000   |
| 7.50                                                       | 5                               | 0.00            | 0.00 | 0.00                   | 0.00 | 0.00 | 0.00 | 0.00            | 0.00           | 0.00                | 5.00    | 0.000   |
| 12.50                                                      | 4                               | 0.00            | 0.00 | 0.00                   | 0.00 | 0.00 | 0.00 | 0.00            | 0.00           | 0.00                | 2.50    | 0.000   |

**Cumulative settlements: 0.000**

**Abbreviations**

τ<sub>av</sub>: Average cyclic shear stress  
 p: Average stress  
 G<sub>max</sub>: Maximum shear modulus (tsf)  
 a, b: Shear strain formula variables  
 γ: Average shear strain  
 ε<sub>15</sub>: Volumetric strain after 15 cycles  
 N<sub>c</sub>: Number of cycles  
 ε<sub>Nc</sub>: Volumetric strain for number of cycles N<sub>c</sub> (%)  
 Δh: Thickness of soil layer (in)  
 ΔS: Settlement of soil layer (in)

| <b>:: Vertical &amp; Lateral displacements estimation for saturated sands ::</b> |                                   |                      |                |                   |                      |                    |         |                        |          |
|----------------------------------------------------------------------------------|-----------------------------------|----------------------|----------------|-------------------|----------------------|--------------------|---------|------------------------|----------|
| Depth (ft)                                                                       | (N <sub>1</sub> ) <sub>60cs</sub> | γ <sub>lim</sub> (%) | F <sub>a</sub> | FS <sub>liq</sub> | γ <sub>max</sub> (%) | e <sub>v</sub> (%) | dz (ft) | S <sub>v-1D</sub> (in) | LDI (ft) |
| 15.00                                                                            | 14                                | 30.65                | 0.79           | 0.649             | 30.65                | 3.02               | 1.50    | 0.543                  | 0.00     |
| 20.00                                                                            | 25                                | 0.00                 | 0.00           | 2.000             | 0.00                 | 0.00               | 8.50    | 0.000                  | 0.00     |
| 25.00                                                                            | 26                                | 0.00                 | 0.00           | 2.000             | 0.00                 | 0.00               | 5.00    | 0.000                  | 0.00     |
| 30.00                                                                            | 15                                | 0.00                 | 0.00           | 2.000             | 0.00                 | 0.00               | 5.00    | 0.000                  | 0.00     |
| 35.00                                                                            | 39                                | 1.07                 | -0.73          | 2.000             | 0.00                 | 0.00               | 1.00    | 0.000                  | 0.00     |
| 40.00                                                                            | 57                                | 0.00                 | -2.17          | 2.000             | 0.00                 | 0.00               | 4.00    | 0.000                  | 0.00     |
| 45.00                                                                            | 41                                | 0.70                 | -0.88          | 2.000             | 0.00                 | 0.00               | 8.00    | 0.000                  | 0.00     |
| 47.00                                                                            | 31                                | 4.04                 | -0.16          | 2.000             | 0.00                 | 0.00               | 5.00    | 0.000                  | 0.00     |
| 53.00                                                                            | 71                                | 0.00                 | -3.38          | 2.000             | 0.00                 | 0.00               | 7.00    | 0.000                  | 0.00     |

| <b>:: Vertical &amp; Lateral displacements estimation for saturated sands ::</b> |                                       |                                |                      |                         |                                |                              |                    |                                  |                     |
|----------------------------------------------------------------------------------|---------------------------------------|--------------------------------|----------------------|-------------------------|--------------------------------|------------------------------|--------------------|----------------------------------|---------------------|
| <b>Depth<br/>(ft)</b>                                                            | <b>(N<sub>1</sub>)<sub>60cs</sub></b> | <b>γ<sub>lim</sub><br/>(%)</b> | <b>F<sub>α</sub></b> | <b>FS<sub>liq</sub></b> | <b>γ<sub>max</sub><br/>(%)</b> | <b>e<sub>v</sub><br/>(%)</b> | <b>dz<br/>(ft)</b> | <b>S<sub>v-1D</sub><br/>(in)</b> | <b>LDI<br/>(ft)</b> |
| 60.00                                                                            | 51                                    | 0.02                           | -1.67                | 2.000                   | 0.00                           | 0.00                         | 1.50               | 0.000                            | 0.00                |
| <b>Cumulative settlements:</b>                                                   |                                       |                                |                      |                         |                                |                              |                    | <b>0.543</b>                     | <b>0.00</b>         |

**Abbreviations**

- γ<sub>lim</sub>: Limiting shear strain (%)
- F<sub>α</sub>/N: Maximun shear strain factor
- γ<sub>max</sub>: Maximum shear strain (%)
- e<sub>v</sub>:: Post liquefaction volumetric strain (%)
- S<sub>v-1D</sub>: Estimated vertical settlement (in)
- LDI: Estimated lateral displacement (ft)

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# The Marylhurst School

Traffic Impact Study  
West Linn, Oregon

**Date:**

January 22, 2019

**Prepared for:**

Sheila Walker

**Prepared by:**

Jessica Hajar  
Todd Mobley, PE



**LANCASTER**  
ENGINEERING



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## ***Executive Summary***

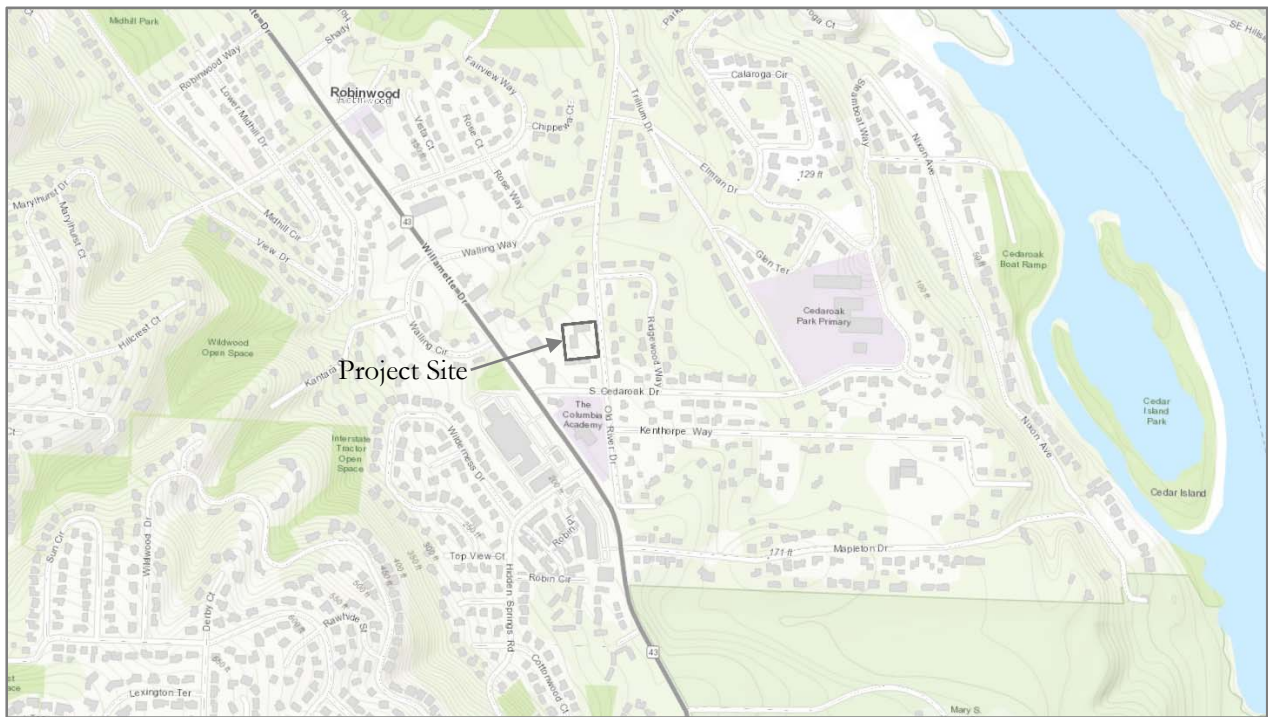
1. The Marylhurst School is proposed for development at 19915 Old River Drive in West Linn, Oregon. The site previously hosted a church with a pre-school program.
2. There are two phases for the school construction, the first is a temporary facility and use of two existing buildings which is planned to support up to 115 students. The second phase is the construction of the new school building. The new construction is anticipated to have a maximum capacity of 194 students.
3. The trip generation calculations show that the proposed development is projected to generate 105 morning peak hour, 71 afternoon peak hour, and 30 evening peak hour site trips for the first phase of development. In the second phase of development, the school is projected to generate 177 morning peak hour, 120 afternoon peak hour, and 50 evening peak hour site trips.
4. All study intersections are projected to operate within the City of West Linn and ODOT standards under all analysis scenarios. No capacity related mitigation is necessary or recommended.
5. Queues for the turning movements at Highway 43 at Cedar Oak Drive do not exceed the available storage length. Queue length at the City intersections do not exceed three vehicles.
6. Due to the low number of crashes and the low severity of collisions, there do not appear to be any significant safety hazards at the nearby transportation facilities. No safety mitigation is recommended.



## Introduction

A new school is proposed at 19915 Old River Drive in West Linn, Oregon. The site previously hosted the New Life Church Robinwood. The project site is located east of Willamette Drive and north of Cedar Oak Drive at 19915 Old River Drive in West Linn, Oregon (see Figure 1). The applicant is proposing to develop the property in two phases. The first phase includes a new temporary building and will utilize two existing buildings. This first phase is anticipated to have a maximum capacity of 115 students. The second phase will be a permanent development which is anticipated to have a maximum capacity of 194 students.

Figure 1: Project Site Location



Access between the site and the greater transportation system will be provided via the two existing driveways onto Old River Road. For drop-off and pick-up, parents enter through the northern driveway, and exit through the southern driveway. General parking can enter/exit through the southern driveway. The proposed site plan is shown in Figure 2 on page 3.



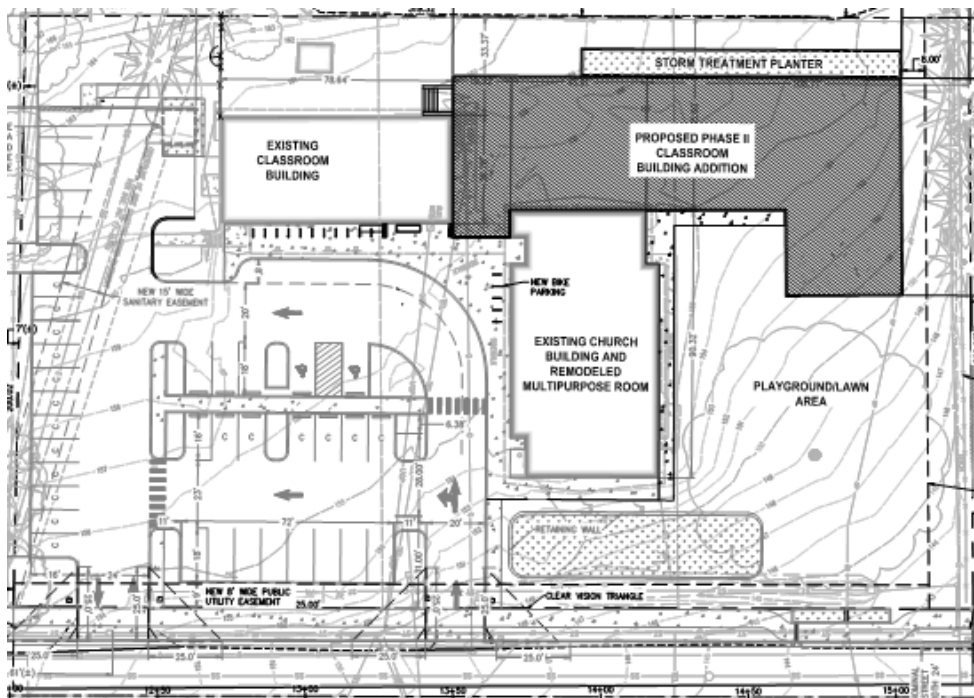


Figure 2 - Proposed Site Plan

### Supporting Transportation Network

The trips associated with the proposed development are anticipated to predominantly use the following three nearby vicinity roadways: Old River Drive, Cedar Oak Drive, and Willamette Drive/Highway 43. Table 1 provides a description of each of the vicinity roadways.

Table 1 – Vicinity Roadway Descriptions

| Roadway                       | Jurisdiction | Functional Classification | Cross-Section | Speed         | On-street Parking | Bicycle Lanes | Curbs              | Sidewalks          |
|-------------------------------|--------------|---------------------------|---------------|---------------|-------------------|---------------|--------------------|--------------------|
| Old River Road                | West Linn    | Neighborhood Route        | 2 Lanes       | 25 mph Posted | Not Permitted     | Both Sides    | Both Sides         | Partial Both Sides |
| Cedar Oak Drive               | West Linn    | Neighborhood Route        | 2 Lanes       | 25 mph Posted | Not Permitted     | None          | Partial Both Sides | Partial Both Sides |
| Willamette Drive / Highway 43 | ODOT         | Major Arterial            | 2 Lanes       | 35 mph Posted | Not Permitted     | None          | Both Sides         | Both Sides         |



## *Study Intersections*

It is anticipated that the majority of traffic traveling to and from the project site will be traveling along Old River Drive to Cedar Oak Drive and then onto Willamette Drive. These assumptions were confirmed with the City of West Linn<sup>1</sup> and ODOT. As a result, the intersections of Old River Drive at Cedar Oak Drive and Willamette Drive at Cedar Oak Drive were evaluated for potential operational and safety impact.

The intersection of Old River Drive at Cedar Oak Drive is a four-legged intersection under City of West Linn Jurisdiction that is stop-controlled for the minor street approaches of Old River Drive. Each approach has one shared lane for all turning movements. There is a striped crosswalk on the southern approach.

The intersection of Willamette Drive/Highway 43 at Cedar Oak Drive is a three-legged signalized intersection under ODOT jurisdiction. The southbound approach has one left-turn lane, two through lanes, and a bicycle lane. The northbound approach has one through / right-turn lane and a bicycle lane. The westbound approach has a left-turn lane and a right-turn lane. Crosswalks are marked across all approaches.

Figure 3 on the following page shows the study intersection configurations and traffic control devices.

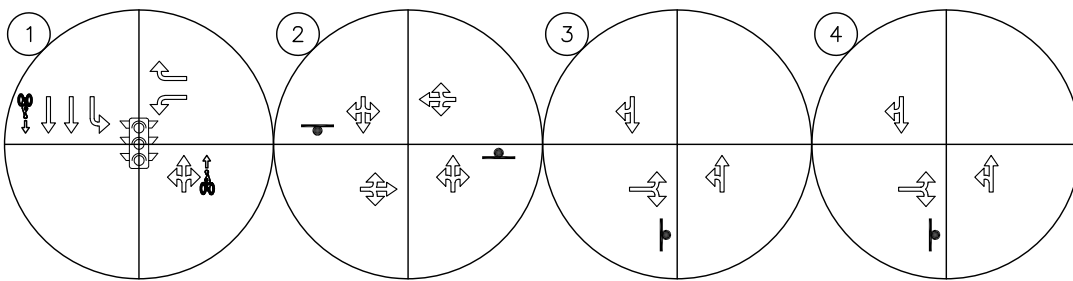
## *Pedestrian & Bicycle Accessibility*

As explained in the following section on trip generation, the Marylhurst School enrolls students from throughout the Portland Metro area, and it is not typical for students to walk or bike to school. Like many private schools, busing is not provided and the large majority of the students arrive via passenger vehicles.

While bike lanes and sidewalks are not in place on Old River Road in the vicinity of the site, the school is not expected to generate trips from people walking or biking to the site. Also, the surrounding West Linn neighborhoods have streets of similar character that commonly do not have dedicated infrastructure for pedestrians and bicycles. However, bike lanes and sidewalks are provided on higher volume roadways near the site, such as Pacific Highway and on portions of Cedar Oak Drive.

---

<sup>1</sup> Scope approval via email on July 30<sup>th</sup> from Amy Pepper



**LEGEND**

- STUDY INTERSECTION (EXISTING)
- STOP SIGN
- TRAFFIC SIGNAL
- BIKE LANE
- PROJECT SITE
- ARTERIAL ROADWAY
- NEIGHBORHOOD ROUTE
- LOCAL ROADWAY



**STUDY INTERSECTION CONFIGURATIONS**

no scale

**FIGURE 3**

**PAGE 5**



## Trip Generation

The proposed Marylhurst School includes two phases of development, the first including space and staff to accommodate 115 students, and the second to accommodate 194 students. To estimate the number of trips that will be generated by the proposed phases, trip rates from the *Trip Generation Manual*<sup>2</sup> were used. Data from land-use code 534, *Private School (K-8)*, was used to estimate the proposed development’s trip generation of the site based on the number of students.

Land-use code 534, *Private School (K-8)*, was used since that precisely describes Marylhurst School. Like many private schools, students come from homes throughout the region, with very few, if any students living in the immediate neighborhood. The school does not provide busing although they do encourage carpooling among parents that pick up and drop off students, which is also a relatively common practice at similar private schools.

The trip generation calculations show that the proposed development is projected to generate 105 morning peak hour, 71 afternoon peak hour, and 30 evening peak hour site trips for the first phase of development. In the second phase of development, the school is projected to generate 177 morning peak hour, 120 afternoon peak hour, and 50 evening peak hour site trips, respectively. The trip generation estimates are summarized in Table 2. Detailed trip generation calculations are included as an attachment to this study.

Table 2 –Trip Generation Summary

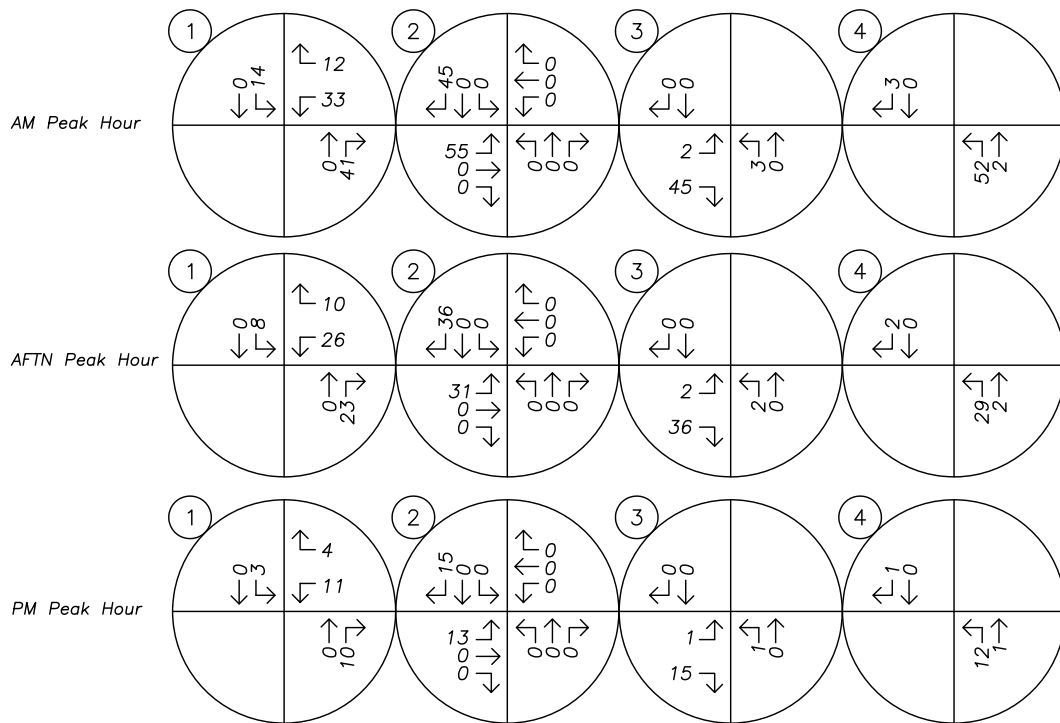
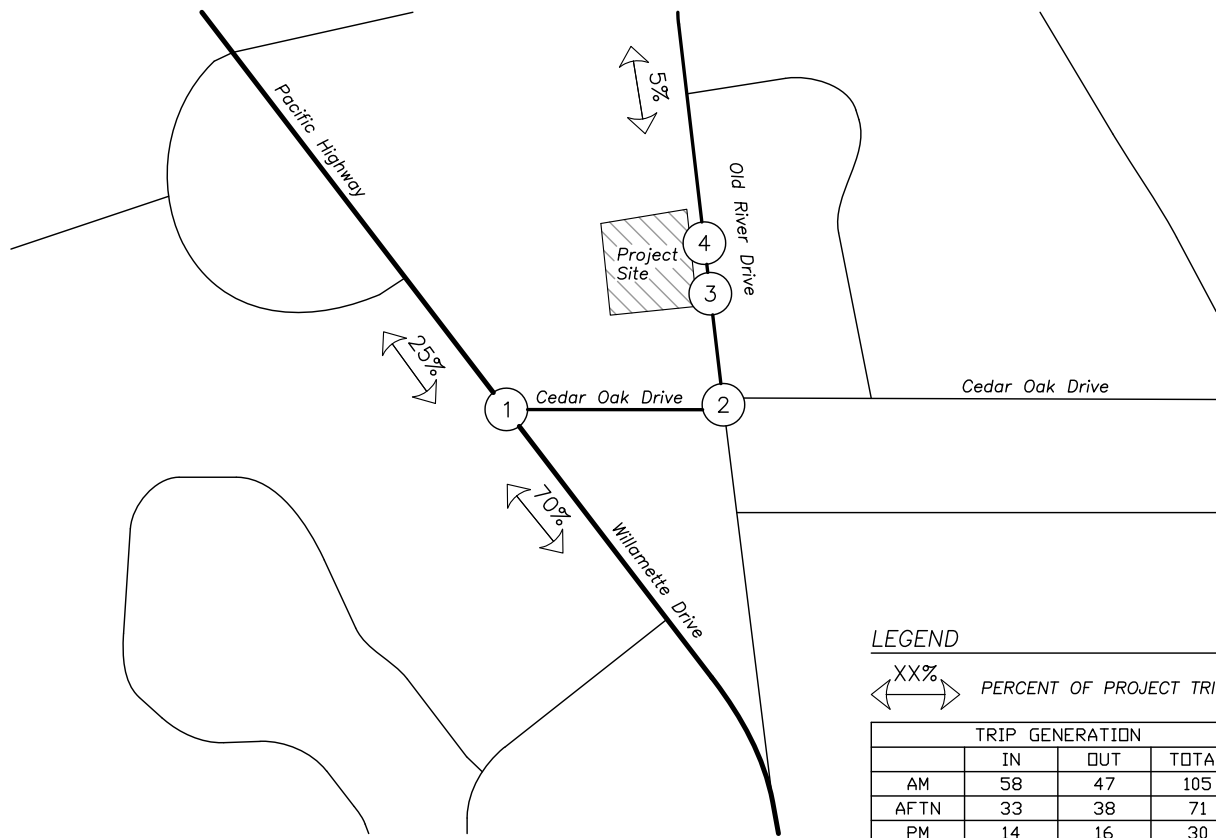
| ITE Code                      | Size<br>(Students) | Morning<br>Peak Hour |      |       | Afternoon<br>Peak Hour |      |       | Evening<br>Peak Hour |      |       |
|-------------------------------|--------------------|----------------------|------|-------|------------------------|------|-------|----------------------|------|-------|
|                               |                    | Enter                | Exit | Total | Enter                  | Exit | Total | Enter                | Exit | Total |
| <b>Phase One - Temporary</b>  |                    |                      |      |       |                        |      |       |                      |      |       |
| Private School (K-8) -<br>534 | 115                | 58                   | 47   | 105   | 33                     | 38   | 71    | 14                   | 16   | 30    |
| <b>Phase Two - Permanent</b>  |                    |                      |      |       |                        |      |       |                      |      |       |
| Private School (K-8) -<br>534 | 194                | 97                   | 80   | 177   | 56                     | 64   | 120   | 23                   | 27   | 50    |

## Trip Distribution

The directional distribution of site trips to and from the proposed development was estimated based on existing traffic patterns as well as the locations of where trips would most likely be coming to and from. Based on the local destinations and the proximity to major transportation facilities, the trip distribution is shown in Figure 4 and Figure 5.

The total site trip assignment for Phase 1 is shown in Figure 4 on page 7. The total site trip assignment for Phase 2 is shown in Figure 5 on page 8.

<sup>2</sup> Institute of Transportation Engineers (ITE), *Trip Generation Manual*, 10<sup>th</sup> Edition, 2017.

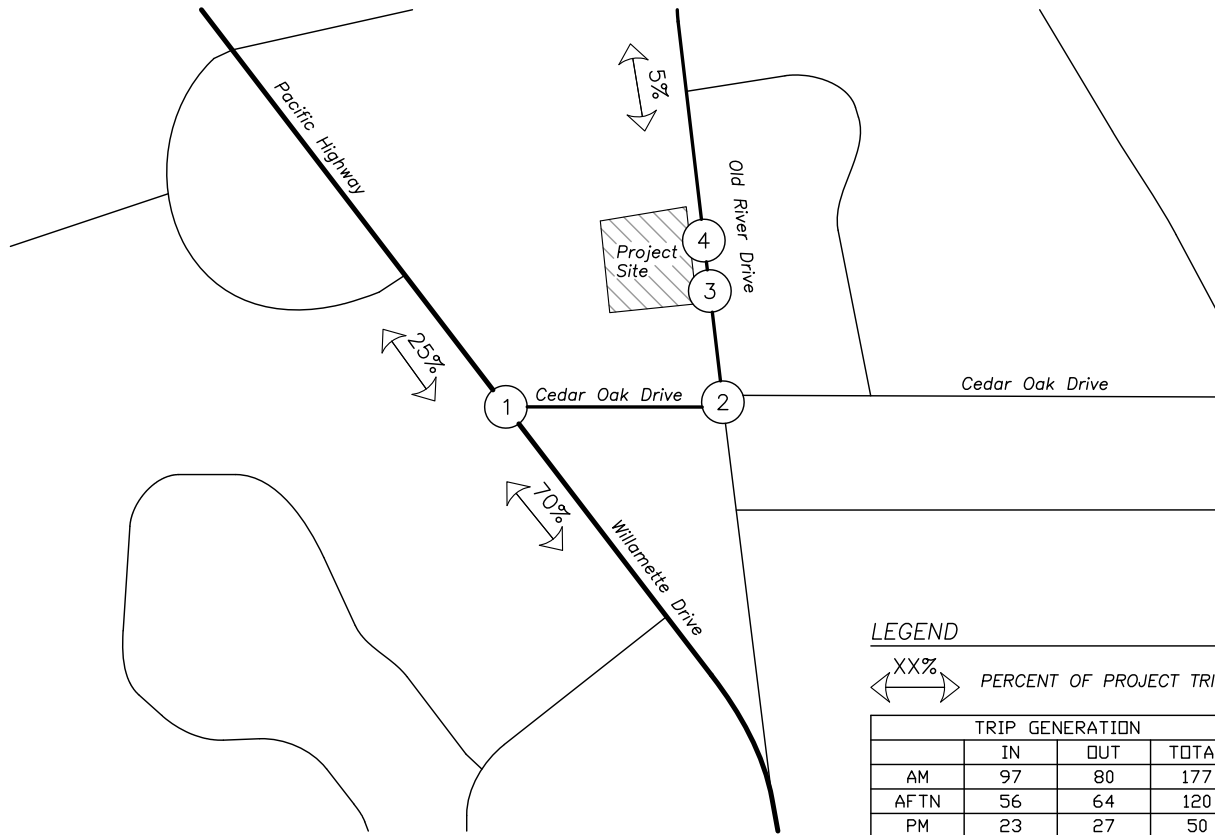


**SITE TRIP DISTRIBUTION & ASSIGNMENT**  
**Proposed Development Plan – Phase 1**  
**AM, AFTN, & PM Peak Hours**



**FIGURE 4**

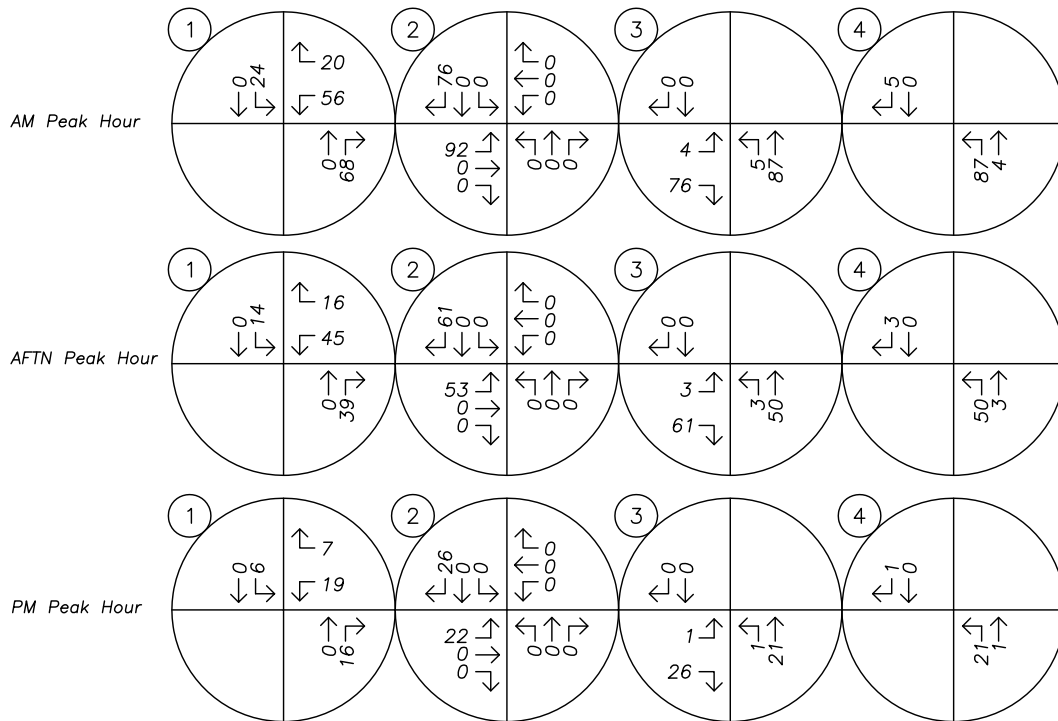
**PAGE 7**



**LEGEND**

XX% PERCENT OF PROJECT TRIPS

|      | TRIP GENERATION |     |       |
|------|-----------------|-----|-------|
|      | IN              | OUT | TOTAL |
| AM   | 97              | 80  | 177   |
| AFTN | 56              | 64  | 120   |
| PM   | 23              | 27  | 50    |



**SITE TRIP DISTRIBUTION & ASSIGNMENT**  
**Proposed Development Plan – Phase 2**  
**AM, AFTN, & PM Peak Hours**



**FIGURE**  
**5**  
**PAGE**  
**8**



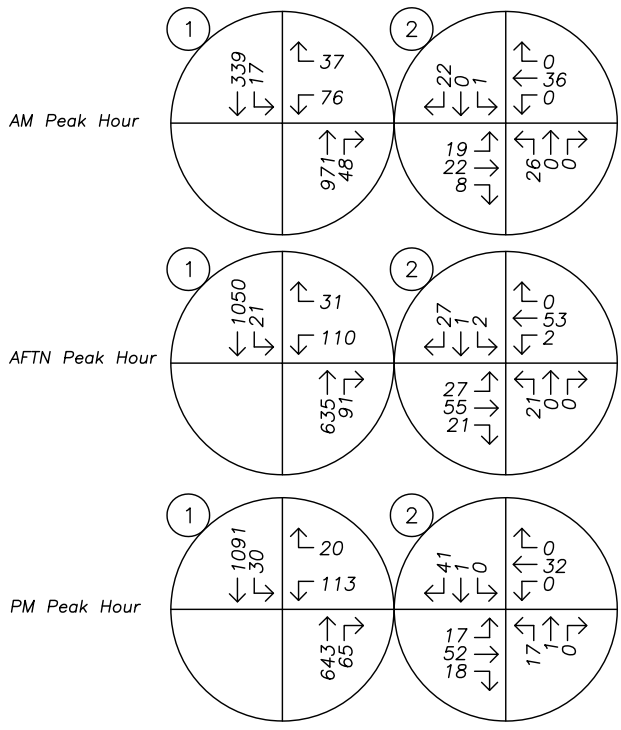
## ***Traffic Volumes***

To determine if the nearby transportation facilities can adequately accommodate future trips to and from the proposed development in addition to the existing uses within the site vicinity, peak hour observations of traffic conditions were conducted. Traffic observations were conducted at the study intersections on Tuesday, July 31<sup>st</sup>, 2018 between 7:00 AM and 9:00 AM, 2:00 PM and 4:00 PM, and 4:00 PM and 6:00 PM, and on Wednesday, August 1<sup>st</sup>, 2018 between 2:00 PM and 4:00 PM, in order to account for the morning, afternoon, and evening peak hours. Data from each intersection's peak hour was used for analysis. The existing volumes are shown in Figure 6 on page 10. Technical data is provided in the technical appendix.

## ***Future Traffic Volumes***

Future traffic volumes along ODOT highways were projected in conformance with the requirements established in ODOT's Analysis Procedures Manual. This includes the determination of the 30<sup>th</sup>-highest hour volumes. Based on seasonal trend variations, an adjustment factor of 1.012 was applied to highway volumes. Additionally, annual growth factors for ODOT facilities were determined based on data from ODOT's Future Volumes Table and estimated to be 0.93% per year. Detailed information is provided in the Appendix. To estimate future traffic volumes along all City roadways, a growth rate of two percent per year was used. These growth rates were applied to estimate background conditions before accounting for trips to be generated from the proposed development.

The anticipated completion of the temporary school is 2019 and the anticipated completion of the permanent school structures is 2021. The 2021 background traffic volumes are shown in Figure 7 on page 11. The year 2021 background conditions with the addition of site trips from Phase Two is shown in Figure 8.



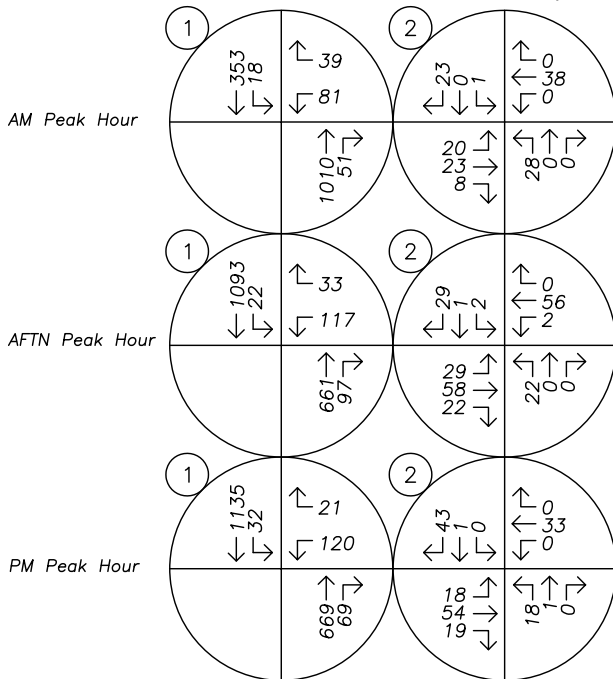
**TRAFFIC VOLUMES**  
 Year 2018 Existing Traffic Volumes  
 AM, AFTN, & PM Peak Hours



**FIGURE 6**

**PAGE 10**





ODOT FACILITY GROWTH RATE: 0.93 PERCENT PER YEAR LINEAR  
 LOCAL FACILITY GROWTH RATE: 2.0 PERCENT PER YEAR COMPOUNDED

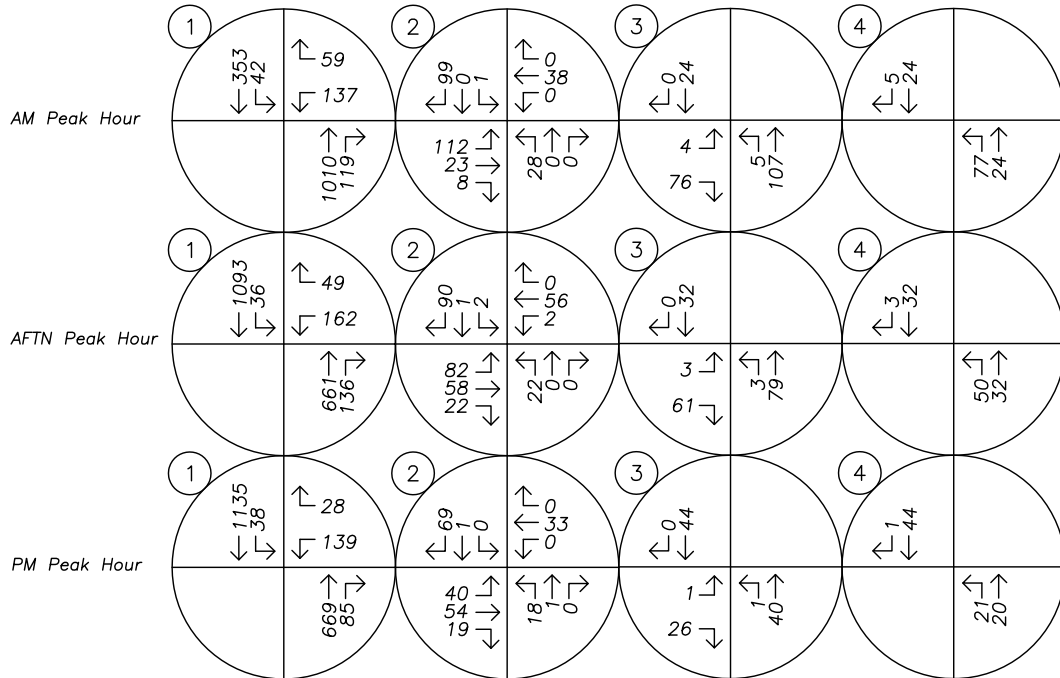
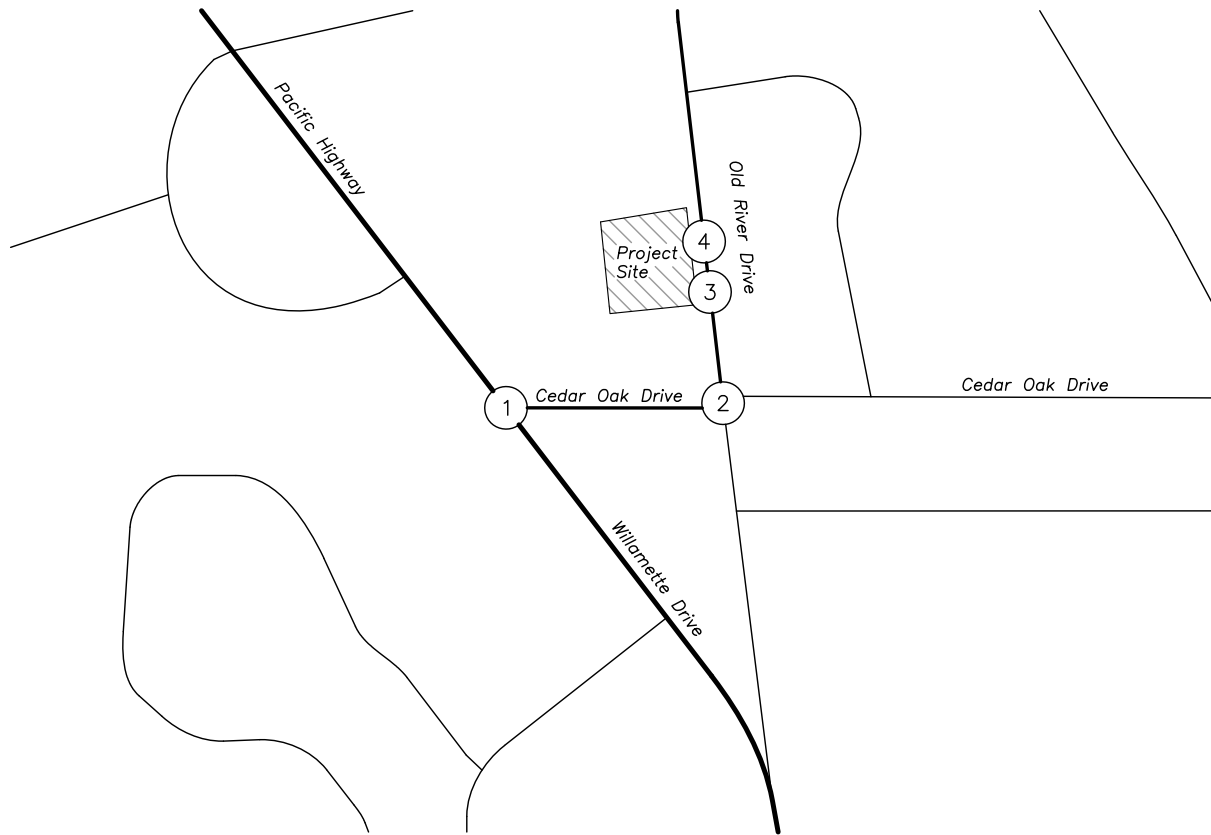


**TRAFFIC VOLUMES**  
 Year 2021 Background Conditions  
 AM, AFTN, & PM Peak Hours



**FIGURE**  
7

**PAGE**  
11



**TRAFFIC VOLUMES**  
 Year 2021 Background Conditions plus Phase Two  
 AM, AFTN, & PM Peak Hours



**FIGURE 8**

**PAGE 12**



## Operational Analysis

To determine the capacity and level-of-service at the study intersections, a capacity analysis was conducted. The analysis was conducted using the intersection analysis methodologies in the Highway Capacity Manual (HCM). The level of service (LOS) of an intersection can range from LOS A, which indicates little or no delay experienced by vehicles, to LOS F, which indicates a high degree of congestion and delay. The minimum operational standard specified in the city of West Linn Comprehensive Plan (April 2006) is LOS D for all facilities except major arterials where the minimum is LOS E. The intersection of Highway 43/Willamette Drive operates under the jurisdiction of the Oregon Department of Transportation and must meet the v/c ratio targets established under the Oregon Highway Plan. For intersections inside the Urban Growth Boundary and within the Portland Metropolitan Region, there is a maximum v/c ratio of 0.99.

All study intersections are projected to operate within the City of West Linn and ODOT standards under all analysis scenarios. The results of the capacity analysis are summarized in the following table. The applicable performance standard is shown in bold for each intersection. No mitigation is necessary or recommended with regard to intersection capacity or operation as part of the proposed development. Detailed data sheets, as well as the year 2019 background plus Phase One analysis results, are attached in the technical appendix.

**Table 3 - Capacity Analysis Summary**

|                                               | Morning Peak |          |             | Afternoon Peak |          |             | Evening Peak |          |             |
|-----------------------------------------------|--------------|----------|-------------|----------------|----------|-------------|--------------|----------|-------------|
|                                               | Delay        | LOS      | V/C         | Delay          | LOS      | V/C         | Delay        | LOS      | V/C         |
| <b>Highway 43 at Cedar Oak Drive</b>          |              |          |             |                |          |             |              |          |             |
| Year 2018 Existing Conditions                 | 14           | B        | <b>0.79</b> | 12             | B        | <b>0.82</b> | 11           | B        | <b>0.79</b> |
| Year 2021 Background Conditions               | 16           | B        | <b>0.83</b> | 14             | B        | <b>0.85</b> | 13           | B        | <b>0.83</b> |
| Year 2021 Background + Phase 2                | 37           | D        | <b>0.94</b> | 19             | B        | <b>0.90</b> | 15           | B        | <b>0.84</b> |
| <b>Cedar Oak Drive at Old River Road</b>      |              |          |             |                |          |             |              |          |             |
| Year 2018 Existing Conditions                 | 10           | <b>A</b> | 0.04        | 10             | <b>B</b> | 0.04        | 10           | <b>B</b> | 0.05        |
| Year 2021 Background Conditions               | 10           | <b>A</b> | 0.04        | 11             | <b>B</b> | 0.04        | 10           | <b>B</b> | 0.05        |
| Year 2021 Background + Phase 2                | 14           | <b>B</b> | 0.09        | 13             | <b>B</b> | 0.11        | 11           | <b>B</b> | 0.09        |
| <b>Old River Road at Southern Site Access</b> |              |          |             |                |          |             |              |          |             |
| Year 2021 Background + Phase 2                | 9            | <b>A</b> | 0.08        | 9              | <b>A</b> | 0.07        | 9            | <b>A</b> | 0.03        |
| <b>Old River Road at Northern Site Access</b> |              |          |             |                |          |             |              |          |             |
| Year 2021 Background + Phase 2                | 7            | <b>A</b> | 0.05        | 7              | <b>A</b> | 0.04        | 7            | <b>A</b> | 0.02        |

## Crash Analysis

Using data obtained from the Oregon Department of Transportation's (ODOT) Crash Analysis and Reporting Unit, a review was performed for the most recent five years of available crash data (January 2012 through December 2016) at the study intersections. Crash rates were calculated under the common assumption that traffic counted during the evening peak hour represents 10 percent of annual average daily traffic (AADT) at the intersection. The crash data was evaluated based on the number of crashes, the type of collisions, and the severity of the collisions at the nearby transportation facilities. Crash rates greater than 1.0



CMEV are generally indicative of a need for further investigation and possible mitigation. Willamette Drive at Cedar Oak Drive is an ODOT intersection. As such, crash data at the intersection was evaluated by comparing the 90<sup>th</sup> percentile crash rates in accordance with the Analysis Procedures Manual.

There were nine crashes reported at the intersection of Willamette Drive at Cedar Oak Drive. Eight of the crashes were rear-end collisions and one crash was an angle-type collision. All of the rear-end collisions occurred between vehicles traveling along Willamette Drive, split evenly in each direction. The crashes resulted in two reports of Injury B – *Non-Incapacitating Injury*, and five reports of Injury C – *Possible Injury or Complaint of Pain*. The crash rate for this intersection was calculated to be 0.249 CMEV, which is less than the 90<sup>th</sup> percentile rate of 0.509 identified by ODOT for three-legged signalized intersections within urban areas.

No other crashes were reported within the immediate site vicinity. Due to the low number of crashes and the low severity of collisions, there do not appear to be any significant safety hazards at the nearby transportation facilities. Accordingly, no safety mitigation is necessary or recommended.

Detailed crash history information is provided in the technical appendix.

### ***Sight Distance***

Intersection sight distance was examined for both existing access driveways. Sight distances were measured and evaluated in accordance with the standards established in *A Policy on Geometric Design of Highways and Streets*<sup>3</sup>. According to AASHTO, the driver's eye is assumed to be 15 feet from the near edge of the nearest lane of the intersecting street and at a height of 3.5 feet. The vehicle driver's eye-height along the major-street approach is assumed to be 3.5 feet above the cross-street pavement.

Based on the posted speed of 25 mph on Old River Road, the minimum recommended intersection sight distance for passenger cars is 280 feet in each direction. Sight distance at both access driveways was measured to be in excess of 400 feet to the north, past the intersection of Ridgewood Way, and in excess of 350 feet to the south, past the intersection of Cedar Oak Drive. Sight distance is met at both site accesses. No mitigation is necessary.

### ***Access Spacing***

The City of West Linn Public Works Design Standards Section 5.0070.D.4 states that there shall be a minimum distance of 30 feet between any two curb cuts on the same lot on a neighborhood route. The two site access driveways are spaced 115 feet apart, measured centerline to centerline. No mitigation is required related to access spacing.

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<sup>3</sup> American Association of State Highway and Transportation Officials (AASHTO), *A Policy on Geometric Design of Highways and Streets*, 7<sup>th</sup> Edition, 2018.



## On-Site Circulation

As stated previously, the northern site access will be one-way enter-only. The southern driveway will have two-way traffic for parking and vehicles exiting the site. Signage will be present at each driveway to indicate the direction of traffic flow within the site. Additionally, a school administrator will be present to assist with directing vehicles during the peak periods associated with school pick up and drop off activities.

It is noted that parents of students in grades kindergarten through eighth grade usually utilize the pick-up/drop-off area. Although parents of pre-school students normally park and come inside, the pick-up/drop-off times for pre-school students do not overlap with those for K-8. Pick-up times for middle school, primary, and pre-school students are staggered at least 45 minutes apart to disperse impacts and traffic congestion on the site. There are also students enrolled in the extended care program which allows kids to be dropped off up to one hour early and stay up to two and a half hours after normal pick-up time. Information regarding pick-up and drop-off times as well as expected parking utilization is included in the attached appendix.

The site plan is shown in Figure 9 on page 15 for context. The site has 37 parking stalls and two drive aisles for pick-up and drop-off.

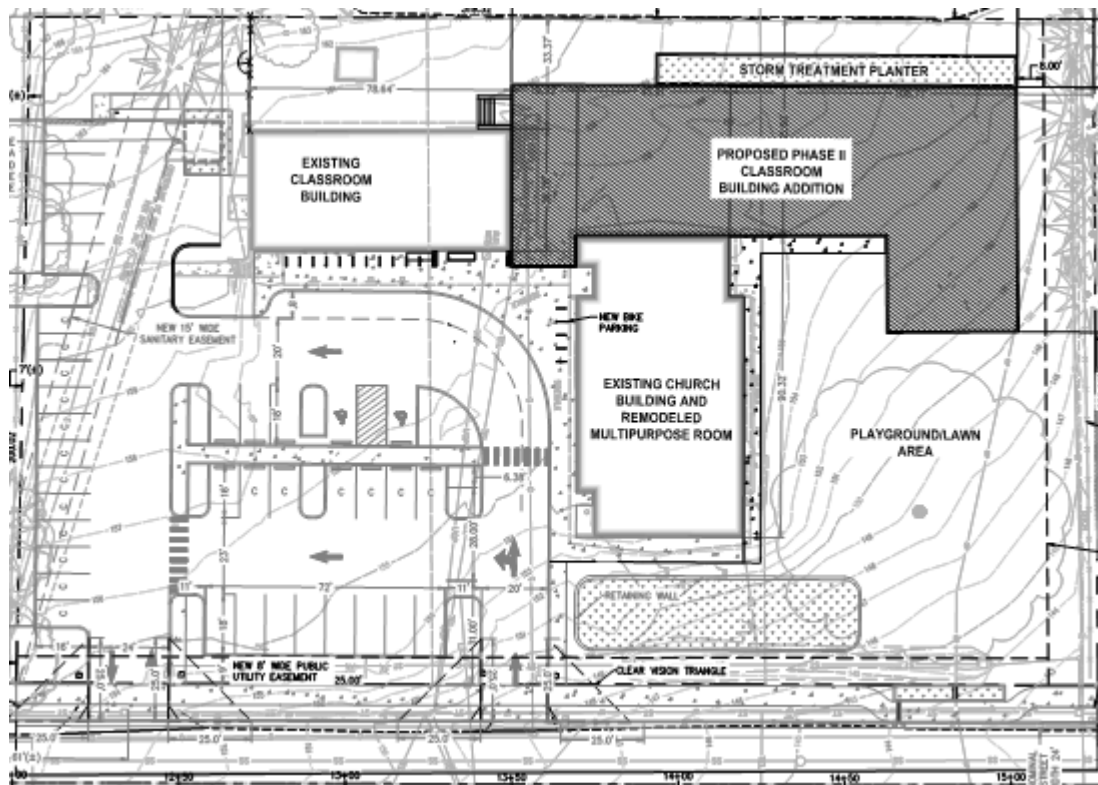


Figure 9 - Site Plan



The Marylhurst School has indicated that correspondence about site circulation during pick-up and drop-off occurs through weekly emails and orientation packets. Additionally, a map will be posted in order to assist parents with carpooling.

### *Queuing Analysis*

Queues were examined for the study intersections under Year 2021 Background plus Phase Two morning peak hour scenario because this peak hour experiences the highest number of site trips and traffic volumes. The queue lengths were calculated using a Synchro/SimTraffic simulation, with the reported values based on the 95<sup>th</sup> percentile queue lengths. This means that during the peak hour, 95 percent of the time the queue lengths will be less than or equal to the reported values. The results show that the calculated 95<sup>th</sup> percentile queues for left-turn movements at the intersection of Highway 43 at Cedar Oak Drive do not exceed the storage length of the turn lanes. The calculated 95<sup>th</sup> percentile queue lengths at the intersections of Cedar Oak Drive at Old River Drive and Old River Drive at the southern site access do not exceed 60 feet, or approximately three vehicles. The northern site access has a 95<sup>th</sup> percentile queue length of approximately one vehicle. Detailed queuing analysis worksheets are provided in the appendix.

The following table shows the pick-up and drop-off data provided by the school.

**Table 4 - Pick-Up and Drop-Off Schedule (Monday - Thursday)**

| # Students | Grades        | Drop-Off | Pick-Up |
|------------|---------------|----------|---------|
| 15         | Middle School | 8:30 AM  | 3:15 PM |
| 72         | Primary       | 8:30 AM  | 2:30 PM |
| 18         | Threes/Fours  | 9:00 AM  | 1:00 PM |

The school has indicated that there are 36 families with multiple students and 11 kids with parents who teach at the school. Conservatively assuming 36 families with 2 children, and 11 teachers with 1 child, the total number of vehicles arriving/departing would be reduced from 105 to 58. Additionally, on average 10 students arrive early and 20 students stay late for the extended car program. Based on these estimates, there would be 48 vehicles arriving during morning drop-off and 38 during afternoon pick-up.

Typically, morning drop-off is expected to operate quickly, with each student taking less than 15 seconds to exit their vehicle. Afternoon drop-off is expected to experience higher queue lengths within the site as parents arrive early, park, and wait for kids to find them. Based on the percentage of students in each grade, reductions for families and teachers with kids who attend the school, and expected attendance of the extended care program, 7 vehicles are expected to arrive at or before 1:00 PM, 26 vehicles are expected to arrive at or before 2:30 PM, and 5 vehicles are expected to arrive at or before 3:15 PM. Peak periods for pick-up often last approximately 15 to 20 minutes, therefore each pick-up period will not overlap.

Based on the measured drive aisle length of 425 feet within the site, the site can accommodate approximately 20 vehicles before queueing begins on the public roadway. Additionally, the existing site has less available parking for parents and queues related to the school's drop-off and pick-up activity do not impact nearby



roadways. Based on the analysis and information provided by the school, queues are not expected to significantly impact the traffic flow along vicinity roadways.

### ***Conclusions***

All study intersections are projected to operate within the City of West Linn and ODOT standards under all analysis scenarios. No mitigation is necessary or recommended with regard to intersection capacity or operation as part of the proposed development.

Queues for the turning movements at Highway 43 at Cedar Oak Drive do not exceed the available storage length. Queue length at the City intersections do not exceed three vehicles.

Due to the low number of crashes and the low severity of collisions, there do not appear to be any significant safety hazards at the nearby transportation facilities. No safety mitigation is recommended.

Le

*Appendix*





## TRIP GENERATION CALCULATIONS

*Land Use:* Private School (K-8)  
*Land Use Code:* 534  
*Setting/Location* General Urban/Suburban  
*Variable:* Students  
*Variable Value:* 115

### AM PEAK HOUR

*Trip Rate:* 0.91

|                          | Enter     | Exit      | Total      |
|--------------------------|-----------|-----------|------------|
| Directional Distribution | 55%       | 45%       |            |
| Trip Ends                | <b>58</b> | <b>47</b> | <b>105</b> |

### PM PEAK HOUR

*Trip Rate:* 0.26

|                          | Enter     | Exit      | Total     |
|--------------------------|-----------|-----------|-----------|
| Directional Distribution | 46%       | 54%       |           |
| Trip Ends                | <b>14</b> | <b>16</b> | <b>30</b> |

### WEEKDAY

*Trip Rate:* 4.11

|                          | Enter      | Exit       | Total      |
|--------------------------|------------|------------|------------|
| Directional Distribution | 50%        | 50%        |            |
| Trip Ends                | <b>237</b> | <b>236</b> | <b>473</b> |

### PM PEAK HOUR OF GENERATOR

*Trip Rate:* 0.62

|                          | Enter     | Exit      | Total     |
|--------------------------|-----------|-----------|-----------|
| Directional Distribution | 47%       | 53%       |           |
| Trip Ends                | <b>33</b> | <b>38</b> | <b>71</b> |

Source: TRIP GENERATION, Tenth Edition



## TRIP GENERATION CALCULATIONS

*Land Use:* Private School (K-8)  
*Land Use Code:* 534  
*Setting/Location* General Urban/Suburban  
*Variable:* Students  
*Variable Value:* 194

### AM PEAK HOUR

*Trip Rate:* 0.91

|                          | Enter     | Exit      | Total      |
|--------------------------|-----------|-----------|------------|
| Directional Distribution | 55%       | 45%       |            |
| Trip Ends                | <b>97</b> | <b>80</b> | <b>177</b> |

### PM PEAK HOUR

*Trip Rate:* 0.26

|                          | Enter     | Exit      | Total     |
|--------------------------|-----------|-----------|-----------|
| Directional Distribution | 46%       | 54%       |           |
| Trip Ends                | <b>23</b> | <b>27</b> | <b>50</b> |

### WEEKDAY

*Trip Rate:* 4.11

|                          | Enter      | Exit       | Total      |
|--------------------------|------------|------------|------------|
| Directional Distribution | 50%        | 50%        |            |
| Trip Ends                | <b>399</b> | <b>398</b> | <b>797</b> |

### PM PEAK HOUR OF GENERATOR

*Trip Rate:* 0.62

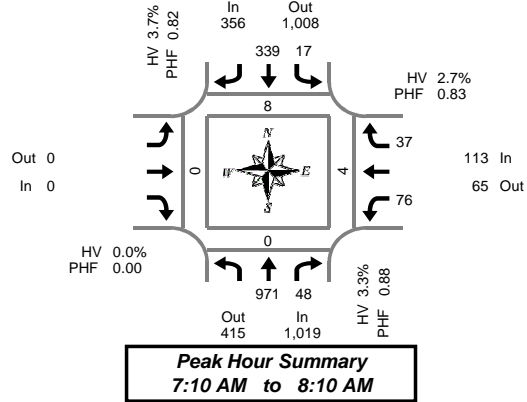
|                          | Enter     | Exit      | Total      |
|--------------------------|-----------|-----------|------------|
| Directional Distribution | 47%       | 53%       |            |
| Trip Ends                | <b>56</b> | <b>64</b> | <b>120</b> |

Source: TRIP GENERATION, Tenth Edition

# Total Vehicle Summary



Clay Carney  
(503) 833-2740



## Hwy 43 & Cedar Oak Dr

Wednesday, August 01, 2018

7:00 AM to 9:00 AM

### 5-Minute Interval Summary

7:00 AM to 9:00 AM

| Interval Start Time | Northbound Hwy 43 |    |       | Southbound Hwy 43 |     |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |    |       | Interval Total | Pedestrians Crosswalk |       |      |      |
|---------------------|-------------------|----|-------|-------------------|-----|-------|------------------------|--|-------|------------------------|----|-------|----------------|-----------------------|-------|------|------|
|                     | T                 | R  | Bikes | L                 | T   | Bikes |                        |  | Bikes | L                      | R  | Bikes |                | North                 | South | East | West |
| 7:00 AM             | 73                | 5  | 0     | 2                 | 25  | 0     |                        |  | 0     | 1                      | 3  | 0     | 109            | 0                     | 0     | 0    | 0    |
| 7:05 AM             | 65                | 3  | 0     | 1                 | 33  | 0     |                        |  | 0     | 3                      | 3  | 0     | 108            | 0                     | 0     | 0    | 0    |
| 7:10 AM             | 89                | 5  | 0     | 0                 | 21  | 0     |                        |  | 0     | 4                      | 4  | 0     | 123            | 0                     | 0     | 0    | 0    |
| 7:15 AM             | 89                | 1  | 0     | 2                 | 23  | 0     |                        |  | 0     | 7                      | 4  | 0     | 126            | 0                     | 0     | 3    | 0    |
| 7:20 AM             | 95                | 7  | 0     | 0                 | 33  | 0     |                        |  | 0     | 6                      | 6  | 0     | 147            | 0                     | 0     | 0    | 0    |
| 7:25 AM             | 91                | 3  | 0     | 2                 | 28  | 0     |                        |  | 0     | 5                      | 1  | 0     | 130            | 1                     | 0     | 0    | 0    |
| 7:30 AM             | 90                | 3  | 0     | 1                 | 19  | 0     |                        |  | 0     | 6                      | 2  | 0     | 121            | 0                     | 0     | 0    | 0    |
| 7:35 AM             | 84                | 3  | 0     | 1                 | 25  | 0     |                        |  | 0     | 12                     | 4  | 0     | 129            | 1                     | 0     | 1    | 0    |
| 7:40 AM             | 86                | 2  | 1     | 1                 | 28  | 0     |                        |  | 0     | 5                      | 2  | 0     | 124            | 0                     | 0     | 0    | 0    |
| 7:45 AM             | 67                | 1  | 0     | 3                 | 39  | 0     |                        |  | 0     | 9                      | 2  | 0     | 121            | 2                     | 0     | 0    | 0    |
| 7:50 AM             | 68                | 5  | 0     | 0                 | 31  | 0     |                        |  | 0     | 4                      | 6  | 0     | 114            | 0                     | 0     | 0    | 0    |
| 7:55 AM             | 73                | 3  | 0     | 2                 | 34  | 0     |                        |  | 0     | 6                      | 4  | 0     | 122            | 3                     | 0     | 0    | 0    |
| 8:00 AM             | 66                | 10 | 0     | 2                 | 29  | 0     |                        |  | 0     | 4                      | 1  | 0     | 112            | 0                     | 0     | 0    | 0    |
| 8:05 AM             | 73                | 5  | 0     | 3                 | 29  | 0     |                        |  | 0     | 8                      | 1  | 0     | 119            | 1                     | 0     | 0    | 0    |
| 8:10 AM             | 63                | 7  | 0     | 0                 | 41  | 0     |                        |  | 0     | 3                      | 2  | 0     | 116            | 1                     | 0     | 2    | 0    |
| 8:15 AM             | 67                | 5  | 0     | 1                 | 29  | 0     |                        |  | 0     | 7                      | 4  | 0     | 113            | 0                     | 0     | 1    | 0    |
| 8:20 AM             | 75                | 3  | 0     | 1                 | 38  | 0     |                        |  | 0     | 8                      | 3  | 1     | 128            | 0                     | 0     | 1    | 0    |
| 8:25 AM             | 78                | 1  | 0     | 4                 | 37  | 0     |                        |  | 0     | 4                      | 8  | 0     | 132            | 1                     | 0     | 0    | 0    |
| 8:30 AM             | 86                | 1  | 0     | 0                 | 34  | 0     |                        |  | 0     | 9                      | 5  | 0     | 135            | 0                     | 0     | 0    | 0    |
| 8:35 AM             | 65                | 2  | 0     | 0                 | 31  | 0     |                        |  | 0     | 3                      | 1  | 0     | 102            | 2                     | 1     | 1    | 0    |
| 8:40 AM             | 87                | 5  | 0     | 3                 | 35  | 0     |                        |  | 0     | 6                      | 4  | 0     | 140            | 0                     | 0     | 0    | 0    |
| 8:45 AM             | 62                | 4  | 0     | 3                 | 43  | 1     |                        |  | 0     | 5                      | 6  | 0     | 123            | 1                     | 0     | 0    | 0    |
| 8:50 AM             | 82                | 1  | 0     | 2                 | 40  | 0     |                        |  | 0     | 5                      | 2  | 0     | 132            | 0                     | 0     | 0    | 0    |
| 8:55 AM             | 69                | 6  | 0     | 0                 | 34  | 0     |                        |  | 0     | 6                      | 1  | 0     | 116            | 2                     | 0     | 0    | 0    |
| Total Survey        | 1,843             | 91 | 1     | 34                | 759 | 1     |                        |  | 0     | 136                    | 79 | 1     | 2,942          | 15                    | 1     | 9    | 0    |

### 15-Minute Interval Summary

7:00 AM to 9:00 AM

| Interval Start Time | Northbound Hwy 43 |    |       | Southbound Hwy 43 |     |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |    |       | Interval Total | Pedestrians Crosswalk |       |      |      |
|---------------------|-------------------|----|-------|-------------------|-----|-------|------------------------|--|-------|------------------------|----|-------|----------------|-----------------------|-------|------|------|
|                     | T                 | R  | Bikes | L                 | T   | Bikes |                        |  | Bikes | L                      | R  | Bikes |                | North                 | South | East | West |
| 7:00 AM             | 227               | 13 | 0     | 3                 | 79  | 0     |                        |  | 0     | 8                      | 10 | 0     | 340            | 0                     | 0     | 0    | 0    |
| 7:15 AM             | 275               | 11 | 0     | 4                 | 84  | 0     |                        |  | 0     | 18                     | 11 | 0     | 403            | 1                     | 0     | 3    | 0    |
| 7:30 AM             | 260               | 8  | 1     | 3                 | 72  | 0     |                        |  | 0     | 23                     | 8  | 0     | 374            | 1                     | 0     | 1    | 0    |
| 7:45 AM             | 208               | 9  | 0     | 5                 | 104 | 0     |                        |  | 0     | 19                     | 12 | 0     | 357            | 5                     | 0     | 0    | 0    |
| 8:00 AM             | 202               | 22 | 0     | 5                 | 99  | 0     |                        |  | 0     | 15                     | 4  | 0     | 347            | 2                     | 0     | 2    | 0    |
| 8:15 AM             | 220               | 9  | 0     | 6                 | 104 | 0     |                        |  | 0     | 19                     | 15 | 1     | 373            | 1                     | 0     | 2    | 0    |
| 8:30 AM             | 238               | 8  | 0     | 3                 | 100 | 0     |                        |  | 0     | 18                     | 10 | 0     | 377            | 2                     | 1     | 1    | 0    |
| 8:45 AM             | 213               | 11 | 0     | 5                 | 117 | 1     |                        |  | 0     | 16                     | 9  | 0     | 371            | 3                     | 0     | 0    | 0    |
| Total Survey        | 1,843             | 91 | 1     | 34                | 759 | 1     |                        |  | 0     | 136                    | 79 | 1     | 2,942          | 15                    | 1     | 9    | 0    |

### Peak Hour Summary

7:10 AM to 8:10 AM

| By Approach | Northbound Hwy 43 |     |       |       | Southbound Hwy 43 |       |       |       | Eastbound Cedar Oak Dr |     |       |       | Westbound Cedar Oak Dr |     |       |       | Total | Pedestrians Crosswalk |       |      |      |
|-------------|-------------------|-----|-------|-------|-------------------|-------|-------|-------|------------------------|-----|-------|-------|------------------------|-----|-------|-------|-------|-----------------------|-------|------|------|
|             | In                | Out | Total | Bikes | In                | Out   | Total | Bikes | In                     | Out | Total | Bikes | In                     | Out | Total | Bikes |       | North                 | South | East | West |
| Volume      | 1,019             | 415 | 1,434 | 1     | 356               | 1,008 | 1,364 | 0     | 0                      | 0   | 0     | 0     | 113                    | 65  | 178   | 0     | 1,488 | 8                     | 0     | 4    | 0    |
| %HV         | 3.3%              |     |       |       | 3.7%              |       |       |       | 0.0%                   |     |       |       | 2.7%                   |     |       |       | 3.4%  |                       |       |      |      |
| PHF         | 0.88              |     |       |       | 0.82              |       |       |       | 0.00                   |     |       |       | 0.83                   |     |       |       | 0.92  |                       |       |      |      |

| By Movement | Northbound Hwy 43 |      |       |       | Southbound Hwy 43 |      |       |       | Eastbound Cedar Oak Dr |      |       |      | Westbound Cedar Oak Dr |       |       |      | Total |
|-------------|-------------------|------|-------|-------|-------------------|------|-------|-------|------------------------|------|-------|------|------------------------|-------|-------|------|-------|
|             | T                 | R    | Total | Bikes | L                 | T    | Total | Bikes |                        |      | Total | L    | R                      | Total | Bikes |      |       |
| Volume      | 971               | 48   | 1,019 | 1     | 17                | 339  | 356   | 0     |                        |      | 0     | 76   | 37                     | 113   | 1,488 |      |       |
| %HV         | NA                | 2.9% | 12.5% | 3.3%  | 0.0%              | 3.8% | NA    | 3.7%  | NA                     | NA   | NA    | 0.0% | 3.9%                   | NA    | 0.0%  | 2.7% | 3.4%  |
| PHF         | 0.88              | 0.67 | 0.88  | 0.61  | 0.81              | 0.82 |       |       | 0.00                   | 0.73 | 0.66  | 0.83 | 0.92                   |       |       |      |       |

### Rolling Hour Summary

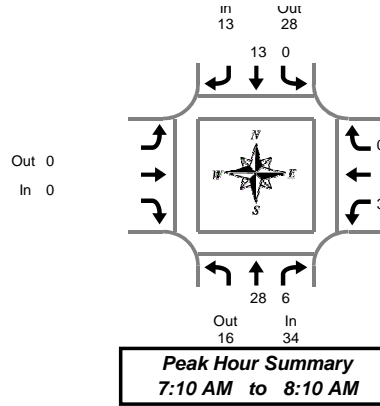
7:00 AM to 9:00 AM

| Interval Start Time | Northbound Hwy 43 |    |       | Southbound Hwy 43 |     |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |    |       | Interval Total | Pedestrians Crosswalk |       |      |      |
|---------------------|-------------------|----|-------|-------------------|-----|-------|------------------------|--|-------|------------------------|----|-------|----------------|-----------------------|-------|------|------|
|                     | T                 | R  | Bikes | L                 | T   | Bikes |                        |  | Bikes | L                      | R  | Bikes |                | North                 | South | East | West |
| 7:00 AM             | 970               | 41 | 1     | 15                | 339 | 0     |                        |  | 0     | 68                     | 41 | 0     | 1,474          | 7                     | 0     | 4    | 0    |
| 7:15 AM             | 945               | 50 | 1     | 17                | 359 | 0     |                        |  | 0     | 75                     | 35 | 0     | 1,481          | 9                     | 0     | 6    | 0    |
| 7:30 AM             | 890               | 48 | 1     | 19                | 379 | 0     |                        |  | 0     | 76                     | 39 | 1     | 1,451          | 9                     | 0     | 5    | 0    |
| 7:45 AM             | 868               | 48 | 0     | 19                | 407 | 0     |                        |  | 0     | 71                     | 41 | 1     | 1,454          | 10                    | 1     | 5    | 0    |
| 8:00 AM             | 873               | 50 | 0     | 19                | 420 | 1     |                        |  | 0     | 68                     | 38 | 1     | 1,468          | 8                     | 1     | 5    | 0    |

# Heavy Vehicle Summary



Clay Carney  
(503) 833-2740



## Hwy 43 & Cedar Oak Dr

Wednesday, August 01, 2018

7:00 AM to 9:00 AM

### Heavy Vehicle 5-Minute Interval Summary 7:00 AM to 9:00 AM

| Interval Start Time | Northbound Hwy 43 |    |       | Southbound Hwy 43 |    |       | Eastbound Cedar Oak Dr |   |   | Westbound Cedar Oak Dr |   |   | Interval Total |       |     |
|---------------------|-------------------|----|-------|-------------------|----|-------|------------------------|---|---|------------------------|---|---|----------------|-------|-----|
|                     | T                 | R  | Total | L                 | T  | Total | Total                  | L | R | Total                  | L | R |                | Total |     |
| 7:00 AM             | 4                 | 0  | 4     | 0                 | 2  | 2     |                        |   |   |                        | 0 | 0 | 0              | 6     |     |
| 7:05 AM             | 0                 | 3  | 3     | 0                 | 0  | 0     |                        |   |   |                        | 0 | 0 | 0              | 3     |     |
| 7:10 AM             | 3                 | 1  | 4     | 0                 | 0  | 0     |                        |   |   |                        | 0 | 0 | 0              | 4     |     |
| 7:15 AM             | 3                 | 0  | 3     | 0                 | 0  | 0     |                        |   |   |                        | 0 | 0 | 0              | 3     |     |
| 7:20 AM             | 2                 | 1  | 3     | 0                 | 2  | 2     |                        |   |   |                        | 0 | 1 | 0              | 6     |     |
| 7:25 AM             | 4                 | 0  | 4     | 0                 | 2  | 2     |                        |   |   |                        | 0 | 0 | 0              | 6     |     |
| 7:30 AM             | 0                 | 1  | 1     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 1 | 0              | 3     |     |
| 7:35 AM             | 3                 | 0  | 3     | 0                 | 2  | 2     |                        |   |   |                        | 0 | 0 | 0              | 5     |     |
| 7:40 AM             | 3                 | 0  | 3     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 1 | 0              | 5     |     |
| 7:45 AM             | 1                 | 0  | 1     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 0 | 0              | 2     |     |
| 7:50 AM             | 0                 | 1  | 1     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 0 | 0              | 2     |     |
| 7:55 AM             | 4                 | 0  | 4     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 0 | 0              | 5     |     |
| 8:00 AM             | 3                 | 1  | 4     | 0                 | 2  | 2     |                        |   |   |                        | 0 | 0 | 0              | 6     |     |
| 8:05 AM             | 2                 | 1  | 3     | 0                 | 0  | 0     |                        |   |   |                        | 0 | 0 | 0              | 3     |     |
| 8:10 AM             | 2                 | 2  | 4     | 0                 | 2  | 2     |                        |   |   |                        | 0 | 0 | 0              | 6     |     |
| 8:15 AM             | 6                 | 0  | 6     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 1 | 1              | 9     |     |
| 8:20 AM             | 4                 | 0  | 4     | 0                 | 0  | 0     |                        |   |   |                        | 0 | 0 | 0              | 4     |     |
| 8:25 AM             | 2                 | 0  | 2     | 0                 | 2  | 2     |                        |   |   |                        | 0 | 1 | 0              | 5     |     |
| 8:30 AM             | 2                 | 0  | 2     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 1 | 1              | 5     |     |
| 8:35 AM             | 0                 | 0  | 0     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 0 | 0              | 1     |     |
| 8:40 AM             | 2                 | 0  | 2     | 0                 | 0  | 0     |                        |   |   |                        | 0 | 0 | 0              | 2     |     |
| 8:45 AM             | 2                 | 0  | 2     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 0 | 0              | 3     |     |
| 8:50 AM             | 2                 | 0  | 2     | 0                 | 1  | 1     |                        |   |   |                        | 0 | 0 | 0              | 3     |     |
| 8:55 AM             | 0                 | 0  | 0     | 0                 | 3  | 3     |                        |   |   |                        | 0 | 0 | 0              | 3     |     |
| Total Survey        | 54                | 11 | 65    | 0                 | 27 | 27    |                        |   |   |                        | 0 | 6 | 2              | 8     | 100 |

### Heavy Vehicle 15-Minute Interval Summary 7:00 AM to 9:00 AM

| Interval Start Time | Northbound Hwy 43 |    |       | Southbound Hwy 43 |    |       | Eastbound Cedar Oak Dr |   |   | Westbound Cedar Oak Dr |   |   | Interval Total |       |     |
|---------------------|-------------------|----|-------|-------------------|----|-------|------------------------|---|---|------------------------|---|---|----------------|-------|-----|
|                     | T                 | R  | Total | L                 | T  | Total | Total                  | L | R | Total                  | L | R |                | Total |     |
| 7:00 AM             | 7                 | 4  | 11    | 0                 | 2  | 2     |                        |   |   |                        | 0 | 0 | 0              | 13    |     |
| 7:15 AM             | 9                 | 1  | 10    | 0                 | 4  | 4     |                        |   |   |                        | 0 | 1 | 0              | 15    |     |
| 7:30 AM             | 6                 | 1  | 7     | 0                 | 4  | 4     |                        |   |   |                        | 0 | 2 | 0              | 13    |     |
| 7:45 AM             | 5                 | 1  | 6     | 0                 | 3  | 3     |                        |   |   |                        | 0 | 0 | 0              | 9     |     |
| 8:00 AM             | 7                 | 4  | 11    | 0                 | 4  | 4     |                        |   |   |                        | 0 | 0 | 0              | 15    |     |
| 8:15 AM             | 12                | 0  | 12    | 0                 | 3  | 3     |                        |   |   |                        | 0 | 2 | 1              | 18    |     |
| 8:30 AM             | 4                 | 0  | 4     | 0                 | 2  | 2     |                        |   |   |                        | 0 | 1 | 1              | 8     |     |
| 8:45 AM             | 4                 | 0  | 4     | 0                 | 5  | 5     |                        |   |   |                        | 0 | 0 | 0              | 9     |     |
| Total Survey        | 54                | 11 | 65    | 0                 | 27 | 27    |                        |   |   |                        | 0 | 6 | 2              | 8     | 100 |

### Heavy Vehicle Peak Hour Summary 7:10 AM to 8:10 AM

| By Approach | Northbound Hwy 43 |     |       | Southbound Hwy 43 |     |       | Eastbound Cedar Oak Dr |     |       | Westbound Cedar Oak Dr |     |       | Total |
|-------------|-------------------|-----|-------|-------------------|-----|-------|------------------------|-----|-------|------------------------|-----|-------|-------|
|             | In                | Out | Total | In                | Out | Total | In                     | Out | Total | In                     | Out | Total |       |
| Volume      | 34                | 16  | 50    | 13                | 28  | 41    | 0                      | 0   | 0     | 3                      | 6   | 9     | 50    |
| PHF         | 0.77              |     |       | 0.65              |     |       | 0.00                   |     |       | 0.38                   |     |       | 0.83  |

| By Movement | Northbound Hwy 43 |      |       | Southbound Hwy 43 |      |       | Eastbound Cedar Oak Dr |   |   | Westbound Cedar Oak Dr |      |      | Total |       |
|-------------|-------------------|------|-------|-------------------|------|-------|------------------------|---|---|------------------------|------|------|-------|-------|
|             | T                 | R    | Total | L                 | T    | Total | Total                  | L | R | Total                  | L    | R    |       | Total |
| Volume      | 28                | 6    | 34    | 0                 | 13   | 13    |                        |   |   | 0                      | 3    | 0    | 3     | 50    |
| PHF         | 0.78              | 0.75 | 0.77  | 0.00              | 0.65 | 0.65  |                        |   |   | 0.00                   | 0.38 | 0.00 | 0.38  | 0.83  |

### Heavy Vehicle Rolling Hour Summary 7:00 AM to 9:00 AM

| Interval Start Time | Northbound Hwy 43 |   |       | Southbound Hwy 43 |    |       | Eastbound Cedar Oak Dr |   |   | Westbound Cedar Oak Dr |   |   | Interval Total |       |
|---------------------|-------------------|---|-------|-------------------|----|-------|------------------------|---|---|------------------------|---|---|----------------|-------|
|                     | T                 | R | Total | L                 | T  | Total | Total                  | L | R | Total                  | L | R |                | Total |
| 7:00 AM             | 27                | 7 | 34    | 0                 | 13 | 13    |                        |   |   | 0                      | 3 | 0 | 3              | 50    |
| 7:15 AM             | 27                | 7 | 34    | 0                 | 15 | 15    |                        |   |   | 0                      | 3 | 0 | 3              | 52    |
| 7:30 AM             | 30                | 6 | 36    | 0                 | 14 | 14    |                        |   |   | 0                      | 4 | 1 | 5              | 55    |
| 7:45 AM             | 28                | 5 | 33    | 0                 | 12 | 12    |                        |   |   | 0                      | 3 | 2 | 5              | 50    |
| 8:00 AM             | 27                | 4 | 31    | 0                 | 14 | 14    |                        |   |   | 0                      | 3 | 2 | 5              | 50    |

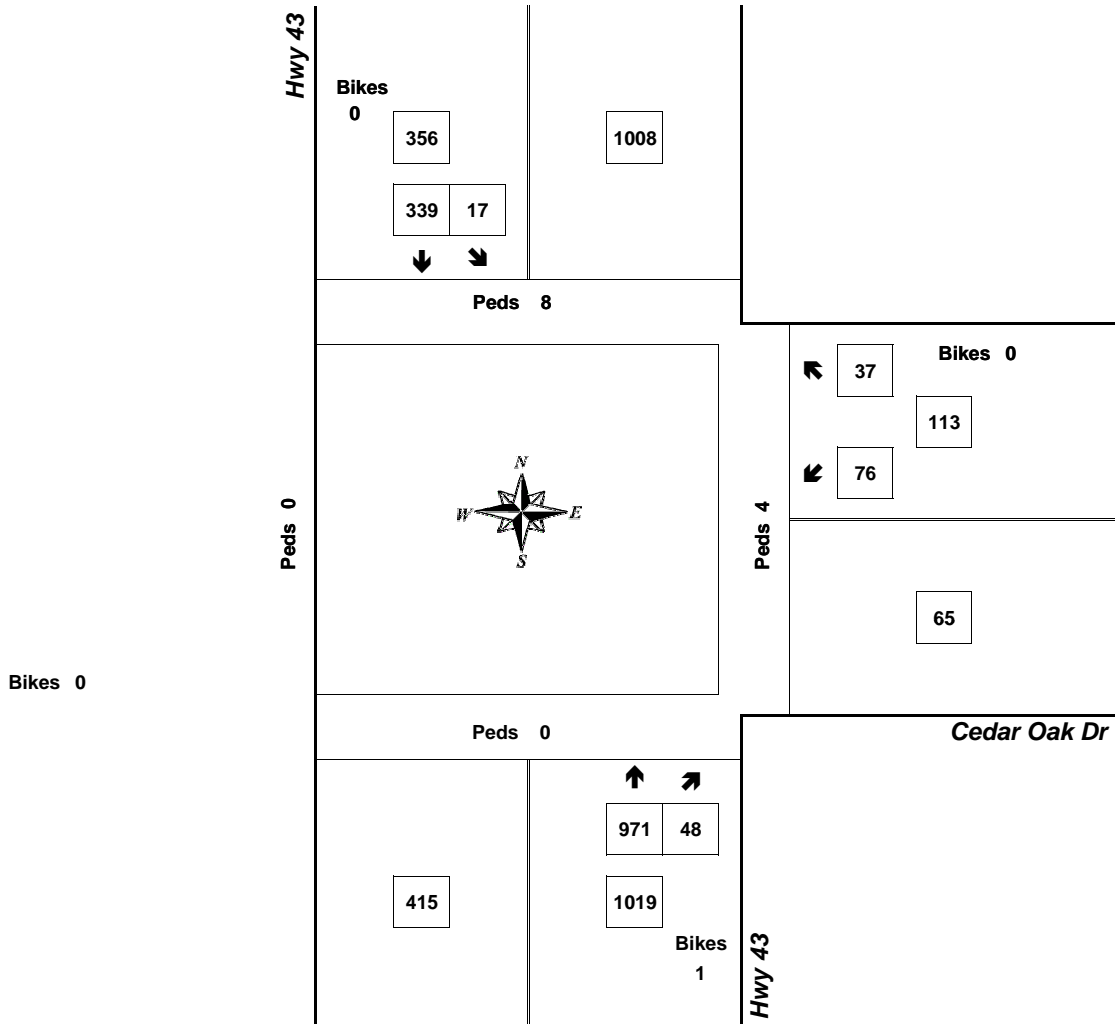
# Peak Hour Summary



Clay Carney  
(503) 833-2740

## Hwy 43 & Cedar Oak Dr

7:10 AM to 8:10 AM  
Wednesday, August 01, 2018



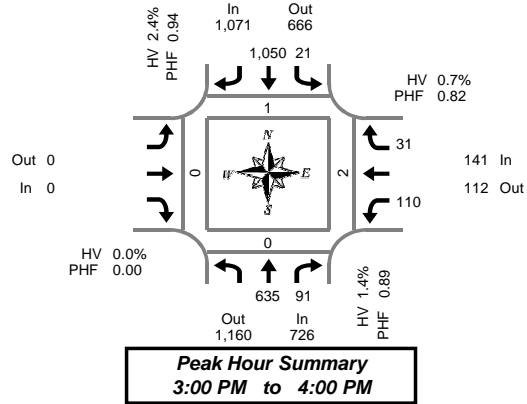
| Approach            | PHF         | HV%         | Volume       |
|---------------------|-------------|-------------|--------------|
| EB                  | 0.00        | 0.0%        | 0            |
| WB                  | 0.83        | 2.7%        | 113          |
| NB                  | 0.88        | 3.3%        | 1,019        |
| SB                  | 0.82        | 3.7%        | 356          |
| <b>Intersection</b> | <b>0.92</b> | <b>3.4%</b> | <b>1,488</b> |

Count Period: 7:00 AM to 9:00 AM

# Total Vehicle Summary



Clay Carney  
(503) 833-2740



## Hwy 43 & Cedar Oak Dr

Tuesday, July 31, 2018  
2:00 PM to 4:00 PM

### 5-Minute Interval Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Hwy 43 |     |       | Southbound Hwy 43 |       |       | Eastbound Cedar Oak Dr |       |     | Westbound Cedar Oak Dr |       |       | Interval Total | Pedestrians Crosswalk |      |      |  |
|---------------------|-------------------|-----|-------|-------------------|-------|-------|------------------------|-------|-----|------------------------|-------|-------|----------------|-----------------------|------|------|--|
|                     | T                 | R   | Bikes | L                 | T     | Bikes |                        | Bikes | L   | R                      | Bikes | North |                | South                 | East | West |  |
| 2:00 PM             | 49                | 5   | 0     | 1                 | 63    | 0     |                        | 0     | 9   | 3                      | 0     | 130   | 1              | 0                     | 0    | 0    |  |
| 2:05 PM             | 53                | 3   | 0     | 2                 | 54    | 0     |                        | 0     | 14  | 4                      | 0     | 130   | 1              | 0                     | 1    | 0    |  |
| 2:10 PM             | 40                | 4   | 0     | 2                 | 60    | 0     |                        | 0     | 12  | 1                      | 0     | 119   | 0              | 0                     | 1    | 0    |  |
| 2:15 PM             | 41                | 5   | 0     | 2                 | 56    | 0     |                        | 0     | 7   | 2                      | 0     | 113   | 0              | 0                     | 0    | 0    |  |
| 2:20 PM             | 53                | 5   | 0     | 2                 | 71    | 0     |                        | 0     | 9   | 4                      | 0     | 144   | 0              | 0                     | 0    | 0    |  |
| 2:25 PM             | 45                | 6   | 0     | 2                 | 61    | 0     |                        | 0     | 12  | 2                      | 0     | 128   | 0              | 0                     | 0    | 0    |  |
| 2:30 PM             | 50                | 10  | 0     | 3                 | 61    | 0     |                        | 0     | 6   | 1                      | 0     | 131   | 0              | 1                     | 1    | 0    |  |
| 2:35 PM             | 47                | 3   | 0     | 2                 | 67    | 0     |                        | 0     | 11  | 3                      | 0     | 133   | 1              | 0                     | 0    | 0    |  |
| 2:40 PM             | 59                | 6   | 0     | 1                 | 102   | 0     |                        | 0     | 8   | 3                      | 0     | 179   | 0              | 1                     | 1    | 0    |  |
| 2:45 PM             | 46                | 6   | 0     | 4                 | 71    | 0     |                        | 0     | 10  | 1                      | 0     | 138   | 0              | 0                     | 0    | 0    |  |
| 2:50 PM             | 53                | 8   | 0     | 1                 | 87    | 0     |                        | 0     | 12  | 2                      | 0     | 163   | 2              | 0                     | 0    | 0    |  |
| 2:55 PM             | 43                | 7   | 0     | 3                 | 73    | 0     |                        | 0     | 9   | 4                      | 0     | 139   | 0              | 0                     | 0    | 0    |  |
| 3:00 PM             | 59                | 6   | 0     | 1                 | 94    | 0     |                        | 0     | 8   | 3                      | 0     | 171   | 0              | 0                     | 0    | 0    |  |
| 3:05 PM             | 45                | 9   | 0     | 4                 | 79    | 0     |                        | 0     | 13  | 2                      | 0     | 152   | 0              | 0                     | 2    | 0    |  |
| 3:10 PM             | 58                | 4   | 0     | 1                 | 94    | 0     |                        | 0     | 12  | 4                      | 0     | 173   | 0              | 0                     | 0    | 0    |  |
| 3:15 PM             | 54                | 4   | 0     | 3                 | 92    | 0     |                        | 0     | 7   | 1                      | 0     | 161   | 0              | 0                     | 0    | 0    |  |
| 3:20 PM             | 44                | 5   | 0     | 1                 | 85    | 0     |                        | 0     | 12  | 2                      | 0     | 149   | 0              | 0                     | 0    | 0    |  |
| 3:25 PM             | 44                | 7   | 0     | 1                 | 83    | 0     |                        | 0     | 13  | 6                      | 0     | 154   | 0              | 0                     | 0    | 0    |  |
| 3:30 PM             | 52                | 12  | 0     | 2                 | 80    | 0     |                        | 0     | 9   | 1                      | 0     | 156   | 1              | 0                     | 0    | 0    |  |
| 3:35 PM             | 65                | 13  | 0     | 1                 | 89    | 0     |                        | 0     | 6   | 2                      | 0     | 176   | 0              | 0                     | 0    | 0    |  |
| 3:40 PM             | 51                | 9   | 0     | 4                 | 88    | 0     |                        | 0     | 10  | 3                      | 0     | 165   | 0              | 0                     | 0    | 0    |  |
| 3:45 PM             | 59                | 7   | 0     | 2                 | 101   | 0     |                        | 0     | 1   | 3                      | 0     | 173   | 0              | 0                     | 0    | 0    |  |
| 3:50 PM             | 45                | 4   | 0     | 0                 | 81    | 0     |                        | 0     | 11  | 2                      | 0     | 143   | 0              | 0                     | 0    | 0    |  |
| 3:55 PM             | 59                | 11  | 1     | 1                 | 84    | 0     |                        | 0     | 8   | 2                      | 0     | 165   | 0              | 0                     | 0    | 0    |  |
| Total Survey        | 1,214             | 159 | 1     | 46                | 1,876 | 0     |                        | 0     | 229 | 61                     | 0     | 3,585 | 6              | 2                     | 6    | 0    |  |

### 15-Minute Interval Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Hwy 43 |     |       | Southbound Hwy 43 |       |       | Eastbound Cedar Oak Dr |       |     | Westbound Cedar Oak Dr |       |       | Interval Total | Pedestrians Crosswalk |      |      |  |
|---------------------|-------------------|-----|-------|-------------------|-------|-------|------------------------|-------|-----|------------------------|-------|-------|----------------|-----------------------|------|------|--|
|                     | T                 | R   | Bikes | L                 | T     | Bikes |                        | Bikes | L   | R                      | Bikes | North |                | South                 | East | West |  |
| 2:00 PM             | 142               | 12  | 0     | 5                 | 177   | 0     |                        | 0     | 35  | 8                      | 0     | 379   | 2              | 0                     | 2    | 0    |  |
| 2:15 PM             | 139               | 16  | 0     | 6                 | 188   | 0     |                        | 0     | 28  | 8                      | 0     | 385   | 0              | 0                     | 0    | 0    |  |
| 2:30 PM             | 156               | 19  | 0     | 6                 | 230   | 0     |                        | 0     | 25  | 7                      | 0     | 443   | 1              | 2                     | 2    | 0    |  |
| 2:45 PM             | 142               | 21  | 0     | 8                 | 231   | 0     |                        | 0     | 31  | 7                      | 0     | 440   | 2              | 0                     | 0    | 0    |  |
| 3:00 PM             | 162               | 19  | 0     | 6                 | 267   | 0     |                        | 0     | 33  | 9                      | 0     | 496   | 0              | 0                     | 2    | 0    |  |
| 3:15 PM             | 142               | 16  | 0     | 5                 | 260   | 0     |                        | 0     | 32  | 9                      | 0     | 464   | 0              | 0                     | 0    | 0    |  |
| 3:30 PM             | 168               | 34  | 0     | 7                 | 257   | 0     |                        | 0     | 25  | 6                      | 0     | 497   | 1              | 0                     | 0    | 0    |  |
| 3:45 PM             | 163               | 22  | 1     | 3                 | 266   | 0     |                        | 0     | 20  | 7                      | 0     | 481   | 0              | 0                     | 0    | 0    |  |
| Total Survey        | 1,214             | 159 | 1     | 46                | 1,876 | 0     |                        | 0     | 229 | 61                     | 0     | 3,585 | 6              | 2                     | 6    | 0    |  |

### Peak Hour Summary 3:00 PM to 4:00 PM

| By Approach | Northbound Hwy 43 |       |       |       | Southbound Hwy 43 |     |       |       | Eastbound Cedar Oak Dr |     |       |       | Westbound Cedar Oak Dr |     |       |       | Total | Pedestrians Crosswalk |       |      |      |
|-------------|-------------------|-------|-------|-------|-------------------|-----|-------|-------|------------------------|-----|-------|-------|------------------------|-----|-------|-------|-------|-----------------------|-------|------|------|
|             | In                | Out   | Total | Bikes | In                | Out | Total | Bikes | In                     | Out | Total | Bikes | In                     | Out | Total | Bikes |       | North                 | South | East | West |
| Volume      | 726               | 1,160 | 1,886 | 1     | 1,071             | 666 | 1,737 | 0     | 0                      | 0   | 0     | 0     | 141                    | 112 | 253   | 0     | 1,938 | 1                     | 0     | 2    | 0    |
| %HV         | 1.4%              |       |       |       | 2.4%              |     |       |       | 0.0%                   |     |       |       | 0.7%                   |     |       |       | 1.9%  |                       |       |      |      |
| PHF         | 0.89              |       |       |       | 0.94              |     |       |       | 0.00                   |     |       |       | 0.82                   |     |       |       | 0.94  |                       |       |      |      |

| By Movement | Northbound Hwy 43 |      |       |       | Southbound Hwy 43 |       |       |       | Eastbound Cedar Oak Dr |       |      |      | Westbound Cedar Oak Dr |       |      |      | Total |
|-------------|-------------------|------|-------|-------|-------------------|-------|-------|-------|------------------------|-------|------|------|------------------------|-------|------|------|-------|
|             | T                 | R    | Total | Bikes | L                 | T     | Total | Bikes |                        | Total | L    | R    | Total                  | Bikes |      |      |       |
| Volume      | 635               | 91   | 726   | 1     | 21                | 1,050 | 1,071 | 0     | 0                      | 0     | 110  | 31   | 141                    | 1,938 |      |      |       |
| %HV         | NA                | 1.4% | 1.1%  | 1.4%  | 4.8%              | 2.4%  | NA    | 2.4%  | NA                     | NA    | 0.0% | 0.9% | NA                     | 0.0%  | 0.7% | 1.9% |       |
| PHF         | 0.91              | 0.67 | 0.89  | 0.66  | 0.94              | 0.94  | 0.94  | 0.00  | 0.81                   | 0.86  | 0.82 | 0.82 | 0.94                   |       |      |      |       |

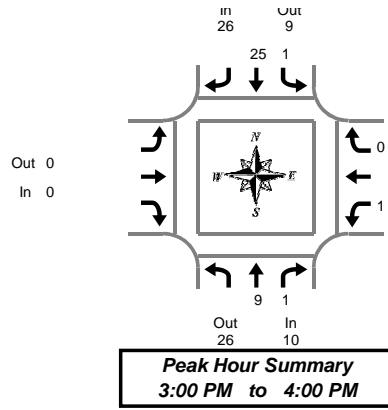
### Rolling Hour Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Hwy 43 |    |       | Southbound Hwy 43 |       |       | Eastbound Cedar Oak Dr |       |     | Westbound Cedar Oak Dr |       |       | Interval Total | Pedestrians Crosswalk |      |      |  |
|---------------------|-------------------|----|-------|-------------------|-------|-------|------------------------|-------|-----|------------------------|-------|-------|----------------|-----------------------|------|------|--|
|                     | T                 | R  | Bikes | L                 | T     | Bikes |                        | Bikes | L   | R                      | Bikes | North |                | South                 | East | West |  |
| 2:00 PM             | 579               | 68 | 0     | 25                | 826   | 0     |                        | 0     | 119 | 30                     | 0     | 1,647 | 5              | 2                     | 4    | 0    |  |
| 2:15 PM             | 599               | 75 | 0     | 26                | 916   | 0     |                        | 0     | 117 | 31                     | 0     | 1,764 | 3              | 2                     | 4    | 0    |  |
| 2:30 PM             | 602               | 75 | 0     | 25                | 988   | 0     |                        | 0     | 121 | 32                     | 0     | 1,843 | 3              | 2                     | 4    | 0    |  |
| 2:45 PM             | 614               | 90 | 0     | 26                | 1,015 | 0     |                        | 0     | 121 | 31                     | 0     | 1,897 | 3              | 0                     | 2    | 0    |  |
| 3:00 PM             | 635               | 91 | 1     | 21                | 1,050 | 0     |                        | 0     | 110 | 31                     | 0     | 1,938 | 1              | 0                     | 2    | 0    |  |

# Heavy Vehicle Summary



Clay Carney  
(503) 833-2740



## Hwy 43 & Cedar Oak Dr

Tuesday, July 31, 2018  
2:00 PM to 4:00 PM

### Heavy Vehicle 5-Minute Interval Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Hwy 43 |   |       | Southbound Hwy 43 |    |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |   |       | Interval Total |
|---------------------|-------------------|---|-------|-------------------|----|-------|------------------------|--|-------|------------------------|---|-------|----------------|
|                     | T                 | R | Total | L                 | T  | Total |                        |  | Total | L                      | R | Total |                |
| 2:00 PM             | 1                 | 1 | 2     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 2:05 PM             | 2                 | 0 | 2     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 2:10 PM             | 1                 | 0 | 1     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 2:15 PM             | 2                 | 0 | 2     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 2:20 PM             | 0                 | 0 | 0     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 1 | 1     | 3              |
| 2:25 PM             | 2                 | 0 | 2     | 0                 | 0  | 0     |                        |  | 0     | 1                      | 0 | 1     | 3              |
| 2:30 PM             | 0                 | 0 | 0     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 1              |
| 2:35 PM             | 0                 | 0 | 0     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 1              |
| 2:40 PM             | 1                 | 0 | 1     | 1                 | 1  | 2     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 2:45 PM             | 0                 | 0 | 0     | 0                 | 5  | 5     |                        |  | 0     | 1                      | 0 | 1     | 6              |
| 2:50 PM             | 2                 | 0 | 2     | 0                 | 4  | 4     |                        |  | 0     | 0                      | 0 | 0     | 6              |
| 2:55 PM             | 0                 | 0 | 0     | 0                 | 3  | 3     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 3:00 PM             | 2                 | 0 | 2     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 4              |
| 3:05 PM             | 0                 | 0 | 0     | 0                 | 4  | 4     |                        |  | 0     | 1                      | 0 | 1     | 5              |
| 3:10 PM             | 1                 | 0 | 1     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 3:15 PM             | 1                 | 0 | 1     | 1                 | 2  | 3     |                        |  | 0     | 0                      | 0 | 0     | 4              |
| 3:20 PM             | 1                 | 0 | 1     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 3:25 PM             | 0                 | 0 | 0     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 0              |
| 3:30 PM             | 1                 | 0 | 1     | 0                 | 3  | 3     |                        |  | 0     | 0                      | 0 | 0     | 4              |
| 3:35 PM             | 1                 | 0 | 1     | 0                 | 4  | 4     |                        |  | 0     | 0                      | 0 | 0     | 5              |
| 3:40 PM             | 0                 | 0 | 0     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 1              |
| 3:45 PM             | 1                 | 0 | 1     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 3:50 PM             | 0                 | 0 | 0     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 0              |
| 3:55 PM             | 1                 | 1 | 2     | 0                 | 4  | 4     |                        |  | 0     | 0                      | 0 | 0     | 6              |
| Total Survey        | 20                | 2 | 22    | 2                 | 45 | 47    |                        |  | 0     | 3                      | 1 | 4     | 73             |

### Heavy Vehicle 15-Minute Interval Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Hwy 43 |   |       | Southbound Hwy 43 |    |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |   |       | Interval Total |
|---------------------|-------------------|---|-------|-------------------|----|-------|------------------------|--|-------|------------------------|---|-------|----------------|
|                     | T                 | R | Total | L                 | T  | Total |                        |  | Total | L                      | R | Total |                |
| 2:00 PM             | 4                 | 1 | 5     | 0                 | 3  | 3     |                        |  | 0     | 0                      | 0 | 0     | 8              |
| 2:15 PM             | 4                 | 0 | 4     | 0                 | 2  | 2     |                        |  | 0     | 1                      | 1 | 2     | 8              |
| 2:30 PM             | 1                 | 0 | 1     | 1                 | 3  | 4     |                        |  | 0     | 0                      | 0 | 0     | 5              |
| 2:45 PM             | 2                 | 0 | 2     | 0                 | 12 | 12    |                        |  | 0     | 1                      | 0 | 1     | 15             |
| 3:00 PM             | 3                 | 0 | 3     | 0                 | 8  | 8     |                        |  | 0     | 1                      | 0 | 1     | 12             |
| 3:15 PM             | 2                 | 0 | 2     | 1                 | 3  | 4     |                        |  | 0     | 0                      | 0 | 0     | 6              |
| 3:30 PM             | 2                 | 0 | 2     | 0                 | 8  | 8     |                        |  | 0     | 0                      | 0 | 0     | 10             |
| 3:45 PM             | 2                 | 1 | 3     | 0                 | 6  | 6     |                        |  | 0     | 0                      | 0 | 0     | 9              |
| Total Survey        | 20                | 2 | 22    | 2                 | 45 | 47    |                        |  | 0     | 3                      | 1 | 4     | 73             |

### Heavy Vehicle Peak Hour Summary 3:00 PM to 4:00 PM

| By Approach | Northbound Hwy 43 |     |       | Southbound Hwy 43 |     |       | Eastbound Cedar Oak Dr |     |       | Westbound Cedar Oak Dr |     |       | Total |
|-------------|-------------------|-----|-------|-------------------|-----|-------|------------------------|-----|-------|------------------------|-----|-------|-------|
|             | In                | Out | Total | In                | Out | Total | In                     | Out | Total | In                     | Out | Total |       |
| Volume      | 10                | 26  | 36    | 26                | 9   | 35    | 0                      | 0   | 0     | 1                      | 2   | 3     | 37    |
| PHF         | 0.83              |     |       | 0.72              |     |       | 0.00                   |     |       | 0.25                   |     |       | 0.77  |

| By Movement | Northbound Hwy 43 |      |       | Southbound Hwy 43 |      |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |      |       | Total |
|-------------|-------------------|------|-------|-------------------|------|-------|------------------------|--|-------|------------------------|------|-------|-------|
|             | T                 | R    | Total | L                 | T    | Total |                        |  | Total | L                      | R    | Total |       |
| Volume      | 9                 | 1    | 10    | 1                 | 25   | 26    |                        |  | 0     | 1                      | 0    | 1     | 37    |
| PHF         | 0.75              | 0.25 | 0.83  | 0.25              | 0.78 | 0.72  |                        |  | 0.00  | 0.25                   | 0.00 | 0.25  | 0.77  |

### Heavy Vehicle Rolling Hour Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Hwy 43 |   |       | Southbound Hwy 43 |    |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |   |       | Interval Total |
|---------------------|-------------------|---|-------|-------------------|----|-------|------------------------|--|-------|------------------------|---|-------|----------------|
|                     | T                 | R | Total | L                 | T  | Total |                        |  | Total | L                      | R | Total |                |
| 2:00 PM             | 11                | 1 | 12    | 1                 | 20 | 21    |                        |  | 0     | 2                      | 1 | 3     | 36             |
| 2:15 PM             | 10                | 0 | 10    | 1                 | 25 | 26    |                        |  | 0     | 3                      | 1 | 4     | 40             |
| 2:30 PM             | 8                 | 0 | 8     | 2                 | 26 | 28    |                        |  | 0     | 2                      | 0 | 2     | 38             |
| 2:45 PM             | 9                 | 0 | 9     | 1                 | 31 | 32    |                        |  | 0     | 2                      | 0 | 2     | 43             |
| 3:00 PM             | 9                 | 1 | 10    | 1                 | 25 | 26    |                        |  | 0     | 1                      | 0 | 1     | 37             |

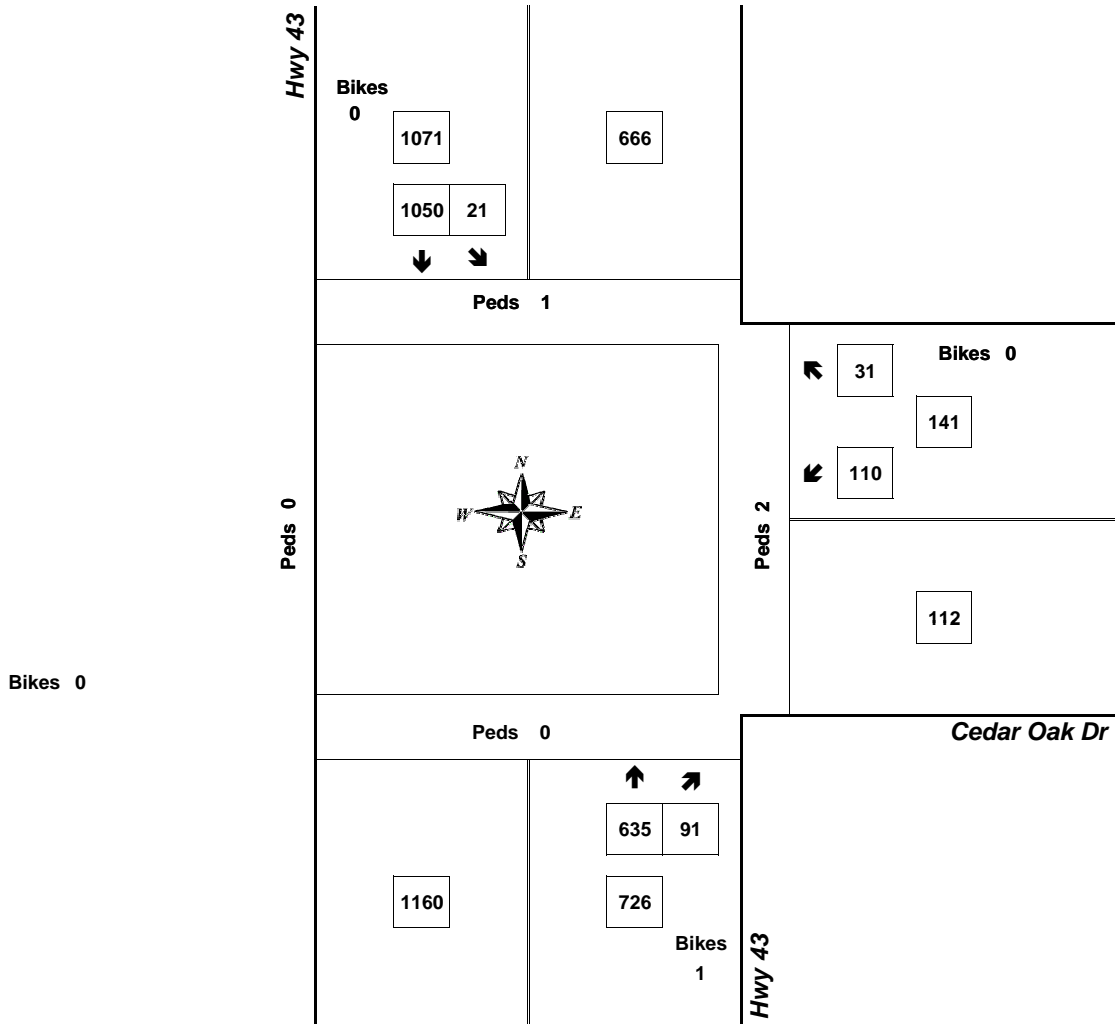
# Peak Hour Summary



Clay Carney  
(503) 833-2740

## Hwy 43 & Cedar Oak Dr

3:00 PM to 4:00 PM  
Tuesday, July 31, 2018



| Approach            | PHF         | HV%         | Volume       |
|---------------------|-------------|-------------|--------------|
| EB                  | 0.00        | 0.0%        | 0            |
| WB                  | 0.82        | 0.7%        | 141          |
| NB                  | 0.89        | 1.4%        | 726          |
| SB                  | 0.94        | 2.4%        | 1,071        |
| <b>Intersection</b> | <b>0.94</b> | <b>1.9%</b> | <b>1,938</b> |

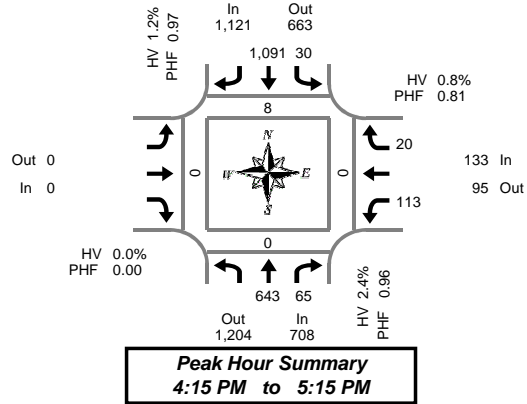
Count Period: 2:00 PM to 4:00 PM



# Total Vehicle Summary



Clay Carney  
(503) 833-2740



## Hwy 43 & Cedar Oak Dr

Tuesday, July 31, 2018  
4:00 PM to 6:00 PM

### 5-Minute Interval Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Hwy 43 |     |       | Southbound Hwy 43 |       |       | Eastbound Cedar Oak Dr |       |     | Westbound Cedar Oak Dr |       |       | Interval Total | Pedestrians Crosswalk |      |      |  |
|---------------------|-------------------|-----|-------|-------------------|-------|-------|------------------------|-------|-----|------------------------|-------|-------|----------------|-----------------------|------|------|--|
|                     | T                 | R   | Bikes | L                 | T     | Bikes |                        | Bikes | L   | R                      | Bikes | North |                | South                 | East | West |  |
| 4:00 PM             | 54                | 15  | 0     | 2                 | 77    | 1     |                        | 0     | 12  | 0                      | 0     | 160   | 2              | 0                     | 0    | 0    |  |
| 4:05 PM             | 42                | 1   | 0     | 2                 | 83    | 0     |                        | 0     | 14  | 4                      | 0     | 146   | 1              | 0                     | 0    | 0    |  |
| 4:10 PM             | 49                | 6   | 0     | 3                 | 88    | 0     |                        | 0     | 5   | 3                      | 0     | 154   | 0              | 0                     | 0    | 0    |  |
| 4:15 PM             | 60                | 2   | 0     | 4                 | 78    | 1     |                        | 0     | 17  | 3                      | 0     | 164   | 2              | 0                     | 0    | 0    |  |
| 4:20 PM             | 60                | 6   | 0     | 3                 | 92    | 1     |                        | 0     | 5   | 2                      | 0     | 168   | 1              | 0                     | 0    | 0    |  |
| 4:25 PM             | 36                | 5   | 0     | 3                 | 97    | 0     |                        | 0     | 8   | 0                      | 0     | 149   | 0              | 0                     | 0    | 0    |  |
| 4:30 PM             | 64                | 9   | 0     | 1                 | 91    | 0     |                        | 0     | 4   | 2                      | 0     | 171   | 0              | 0                     | 0    | 0    |  |
| 4:35 PM             | 52                | 5   | 0     | 3                 | 94    | 0     |                        | 0     | 13  | 2                      | 0     | 169   | 0              | 0                     | 0    | 0    |  |
| 4:40 PM             | 48                | 7   | 0     | 2                 | 89    | 0     |                        | 0     | 11  | 2                      | 0     | 159   | 0              | 0                     | 0    | 0    |  |
| 4:45 PM             | 43                | 6   | 0     | 2                 | 89    | 0     |                        | 0     | 11  | 2                      | 0     | 153   | 0              | 0                     | 0    | 0    |  |
| 4:50 PM             | 60                | 6   | 0     | 2                 | 96    | 0     |                        | 0     | 9   | 1                      | 0     | 174   | 5              | 0                     | 0    | 0    |  |
| 4:55 PM             | 62                | 7   | 0     | 1                 | 90    | 0     |                        | 0     | 9   | 1                      | 0     | 170   | 0              | 0                     | 0    | 0    |  |
| 5:00 PM             | 43                | 3   | 0     | 0                 | 93    | 0     |                        | 0     | 13  | 3                      | 0     | 155   | 0              | 0                     | 0    | 0    |  |
| 5:05 PM             | 61                | 3   | 0     | 5                 | 85    | 0     |                        | 0     | 6   | 0                      | 0     | 160   | 0              | 0                     | 0    | 0    |  |
| 5:10 PM             | 54                | 6   | 0     | 4                 | 97    | 0     |                        | 0     | 7   | 2                      | 0     | 170   | 0              | 0                     | 0    | 0    |  |
| 5:15 PM             | 48                | 2   | 0     | 2                 | 89    | 0     |                        | 0     | 16  | 0                      | 0     | 157   | 1              | 0                     | 0    | 0    |  |
| 5:20 PM             | 42                | 4   | 0     | 3                 | 100   | 0     |                        | 0     | 11  | 1                      | 0     | 161   | 0              | 0                     | 0    | 0    |  |
| 5:25 PM             | 52                | 7   | 0     | 4                 | 77    | 0     |                        | 0     | 8   | 4                      | 0     | 152   | 0              | 0                     | 0    | 0    |  |
| 5:30 PM             | 51                | 3   | 0     | 2                 | 78    | 0     |                        | 0     | 11  | 2                      | 0     | 147   | 0              | 0                     | 0    | 0    |  |
| 5:35 PM             | 55                | 7   | 0     | 2                 | 93    | 0     |                        | 0     | 14  | 4                      | 0     | 175   | 1              | 0                     | 0    | 0    |  |
| 5:40 PM             | 46                | 9   | 0     | 6                 | 83    | 0     |                        | 0     | 11  | 2                      | 0     | 157   | 0              | 0                     | 1    | 0    |  |
| 5:45 PM             | 47                | 6   | 0     | 3                 | 88    | 0     |                        | 0     | 5   | 2                      | 0     | 151   | 0              | 0                     | 0    | 0    |  |
| 5:50 PM             | 43                | 5   | 0     | 2                 | 69    | 0     |                        | 0     | 10  | 1                      | 0     | 130   | 0              | 0                     | 0    | 0    |  |
| 5:55 PM             | 34                | 6   | 0     | 1                 | 70    | 0     |                        | 0     | 12  | 2                      | 0     | 125   | 0              | 1                     | 0    | 0    |  |
| Total Survey        | 1,206             | 136 | 0     | 62                | 2,086 | 3     |                        | 0     | 242 | 45                     | 0     | 3,777 | 13             | 1                     | 1    | 0    |  |

### 15-Minute Interval Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Hwy 43 |     |       | Southbound Hwy 43 |       |       | Eastbound Cedar Oak Dr |       |     | Westbound Cedar Oak Dr |       |       | Interval Total | Pedestrians Crosswalk |      |      |  |
|---------------------|-------------------|-----|-------|-------------------|-------|-------|------------------------|-------|-----|------------------------|-------|-------|----------------|-----------------------|------|------|--|
|                     | T                 | R   | Bikes | L                 | T     | Bikes |                        | Bikes | L   | R                      | Bikes | North |                | South                 | East | West |  |
| 4:00 PM             | 145               | 22  | 0     | 7                 | 248   | 1     |                        | 0     | 31  | 7                      | 0     | 460   | 3              | 0                     | 0    | 0    |  |
| 4:15 PM             | 156               | 13  | 0     | 10                | 267   | 2     |                        | 0     | 30  | 5                      | 0     | 481   | 3              | 0                     | 0    | 0    |  |
| 4:30 PM             | 164               | 21  | 0     | 6                 | 274   | 0     |                        | 0     | 28  | 6                      | 0     | 499   | 0              | 0                     | 0    | 0    |  |
| 4:45 PM             | 165               | 19  | 0     | 5                 | 275   | 0     |                        | 0     | 29  | 4                      | 0     | 497   | 5              | 0                     | 0    | 0    |  |
| 5:00 PM             | 158               | 12  | 0     | 9                 | 275   | 0     |                        | 0     | 26  | 5                      | 0     | 485   | 0              | 0                     | 0    | 0    |  |
| 5:15 PM             | 142               | 13  | 0     | 9                 | 266   | 0     |                        | 0     | 35  | 5                      | 0     | 470   | 1              | 0                     | 0    | 0    |  |
| 5:30 PM             | 152               | 19  | 0     | 10                | 254   | 0     |                        | 0     | 36  | 8                      | 0     | 479   | 1              | 0                     | 1    | 0    |  |
| 5:45 PM             | 124               | 17  | 0     | 6                 | 227   | 0     |                        | 0     | 27  | 5                      | 0     | 406   | 0              | 1                     | 0    | 0    |  |
| Total Survey        | 1,206             | 136 | 0     | 62                | 2,086 | 3     |                        | 0     | 242 | 45                     | 0     | 3,777 | 13             | 1                     | 1    | 0    |  |

### Peak Hour Summary 4:15 PM to 5:15 PM

| By Approach | Northbound Hwy 43 |       |       |       | Southbound Hwy 43 |     |       |       | Eastbound Cedar Oak Dr |     |       |       | Westbound Cedar Oak Dr |     |       |       | Total | Pedestrians Crosswalk |       |      |      |
|-------------|-------------------|-------|-------|-------|-------------------|-----|-------|-------|------------------------|-----|-------|-------|------------------------|-----|-------|-------|-------|-----------------------|-------|------|------|
|             | In                | Out   | Total | Bikes | In                | Out | Total | Bikes | In                     | Out | Total | Bikes | In                     | Out | Total | Bikes |       | North                 | South | East | West |
| Volume      | 708               | 1,204 | 1,912 | 0     | 1,121             | 663 | 1,784 | 2     | 0                      | 0   | 0     | 0     | 133                    | 95  | 228   | 0     | 1,962 | 8                     | 0     | 0    | 0    |
| %HV         | 2.4%              |       |       |       | 1.2%              |     |       |       | 0.0%                   |     |       |       | 0.8%                   |     |       |       | 1.6%  |                       |       |      |      |
| PHF         | 0.96              |       |       |       | 0.97              |     |       |       | 0.00                   |     |       |       | 0.81                   |     |       |       | 0.98  |                       |       |      |      |

| By Movement | Northbound Hwy 43 |      |       |       | Southbound Hwy 43 |       |       |       | Eastbound Cedar Oak Dr |       |      |      | Westbound Cedar Oak Dr |       |      |      | Total |
|-------------|-------------------|------|-------|-------|-------------------|-------|-------|-------|------------------------|-------|------|------|------------------------|-------|------|------|-------|
|             | T                 | R    | Total | Bikes | L                 | T     | Total | Bikes |                        | Total | L    | R    | Total                  | Bikes |      |      |       |
| Volume      | 643               | 65   | 708   | 0     | 30                | 1,091 | 1,121 | 3     |                        | 0     | 113  | 20   | 133                    | 1,962 |      |      |       |
| %HV         | NA                | 2.5% | 1.5%  | 2.4%  | 3.3%              | 1.2%  | NA    | 1.2%  | NA                     | NA    | NA   | 0.0% | 0.9%                   | NA    | 0.0% | 0.8% | 1.6%  |
| PHF         | 0.97              | 0.77 | 0.96  | 0.75  | 0.97              | 0.97  |       |       |                        | 0.00  | 0.81 | 0.83 | 0.81                   | 0.98  |      |      |       |

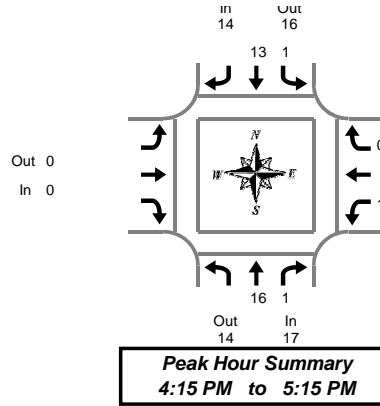
### Rolling Hour Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Hwy 43 |    |       | Southbound Hwy 43 |       |       | Eastbound Cedar Oak Dr |       |     | Westbound Cedar Oak Dr |       |       | Interval Total | Pedestrians Crosswalk |      |      |  |
|---------------------|-------------------|----|-------|-------------------|-------|-------|------------------------|-------|-----|------------------------|-------|-------|----------------|-----------------------|------|------|--|
|                     | T                 | R  | Bikes | L                 | T     | Bikes |                        | Bikes | L   | R                      | Bikes | North |                | South                 | East | West |  |
| 4:00 PM             | 630               | 75 | 0     | 28                | 1,064 | 3     |                        | 0     | 118 | 22                     | 0     | 1,937 | 11             | 0                     | 0    | 0    |  |
| 4:15 PM             | 643               | 65 | 0     | 30                | 1,091 | 2     |                        | 0     | 113 | 20                     | 0     | 1,962 | 8              | 0                     | 0    | 0    |  |
| 4:30 PM             | 629               | 65 | 0     | 29                | 1,090 | 0     |                        | 0     | 118 | 20                     | 0     | 1,951 | 6              | 0                     | 0    | 0    |  |
| 4:45 PM             | 617               | 63 | 0     | 33                | 1,070 | 0     |                        | 0     | 126 | 22                     | 0     | 1,931 | 7              | 0                     | 1    | 0    |  |
| 5:00 PM             | 576               | 61 | 0     | 34                | 1,022 | 0     |                        | 0     | 124 | 23                     | 0     | 1,840 | 2              | 1                     | 1    | 0    |  |

# Heavy Vehicle Summary



Clay Carney  
(503) 833-2740



## Hwy 43 & Cedar Oak Dr

Tuesday, July 31, 2018  
4:00 PM to 6:00 PM

### Heavy Vehicle 5-Minute Interval Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Hwy 43 |   |       | Southbound Hwy 43 |    |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |   |       | Interval Total |
|---------------------|-------------------|---|-------|-------------------|----|-------|------------------------|--|-------|------------------------|---|-------|----------------|
|                     | T                 | R | Total | L                 | T  | Total |                        |  | Total | L                      | R | Total |                |
| 4:00 PM             | 0                 | 0 | 0     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 4:05 PM             | 0                 | 0 | 0     | 0                 | 3  | 3     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 4:10 PM             | 0                 | 0 | 0     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 4:15 PM             | 2                 | 0 | 2     | 0                 | 3  | 3     |                        |  | 0     | 0                      | 0 | 0     | 5              |
| 4:20 PM             | 0                 | 1 | 1     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 1              |
| 4:25 PM             | 1                 | 0 | 1     | 0                 | 0  | 0     |                        |  | 0     | 1                      | 0 | 1     | 2              |
| 4:30 PM             | 0                 | 0 | 0     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 4:35 PM             | 3                 | 0 | 3     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 4              |
| 4:40 PM             | 1                 | 0 | 1     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 4:45 PM             | 1                 | 0 | 1     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 4:50 PM             | 1                 | 0 | 1     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 4:55 PM             | 3                 | 0 | 3     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 5:00 PM             | 0                 | 0 | 0     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 1              |
| 5:05 PM             | 3                 | 0 | 3     | 1                 | 0  | 1     |                        |  | 0     | 0                      | 0 | 0     | 4              |
| 5:10 PM             | 1                 | 0 | 1     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 5:15 PM             | 1                 | 0 | 1     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 5:20 PM             | 0                 | 0 | 0     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 0              |
| 5:25 PM             | 1                 | 0 | 1     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 1              |
| 5:30 PM             | 1                 | 0 | 1     | 0                 | 2  | 2     |                        |  | 0     | 1                      | 0 | 1     | 4              |
| 5:35 PM             | 0                 | 0 | 0     | 0                 | 3  | 3     |                        |  | 0     | 0                      | 1 | 1     | 4              |
| 5:40 PM             | 0                 | 1 | 1     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 1              |
| 5:45 PM             | 1                 | 0 | 1     | 0                 | 0  | 0     |                        |  | 0     | 0                      | 0 | 0     | 1              |
| 5:50 PM             | 0                 | 1 | 1     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 2              |
| 5:55 PM             | 2                 | 0 | 2     | 0                 | 2  | 2     |                        |  | 0     | 0                      | 0 | 0     | 4              |
| Total Survey        | 22                | 3 | 25    | 1                 | 29 | 30    |                        |  | 0     | 2                      | 1 | 3     | 58             |

### Heavy Vehicle 15-Minute Interval Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Hwy 43 |   |       | Southbound Hwy 43 |    |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |   |       | Interval Total |
|---------------------|-------------------|---|-------|-------------------|----|-------|------------------------|--|-------|------------------------|---|-------|----------------|
|                     | T                 | R | Total | L                 | T  | Total |                        |  | Total | L                      | R | Total |                |
| 4:00 PM             | 0                 | 0 | 0     | 0                 | 7  | 7     |                        |  | 0     | 0                      | 0 | 0     | 7              |
| 4:15 PM             | 3                 | 1 | 4     | 0                 | 3  | 3     |                        |  | 0     | 1                      | 0 | 1     | 8              |
| 4:30 PM             | 4                 | 0 | 4     | 0                 | 4  | 4     |                        |  | 0     | 0                      | 0 | 0     | 8              |
| 4:45 PM             | 5                 | 0 | 5     | 0                 | 4  | 4     |                        |  | 0     | 0                      | 0 | 0     | 9              |
| 5:00 PM             | 4                 | 0 | 4     | 1                 | 2  | 3     |                        |  | 0     | 0                      | 0 | 0     | 7              |
| 5:15 PM             | 2                 | 0 | 2     | 0                 | 1  | 1     |                        |  | 0     | 0                      | 0 | 0     | 3              |
| 5:30 PM             | 1                 | 1 | 2     | 0                 | 5  | 5     |                        |  | 0     | 1                      | 1 | 2     | 9              |
| 5:45 PM             | 3                 | 1 | 4     | 0                 | 3  | 3     |                        |  | 0     | 0                      | 0 | 0     | 7              |
| Total Survey        | 22                | 3 | 25    | 1                 | 29 | 30    |                        |  | 0     | 2                      | 1 | 3     | 58             |

### Heavy Vehicle Peak Hour Summary 4:15 PM to 5:15 PM

| By Approach | Northbound Hwy 43 |     |       | Southbound Hwy 43 |     |       | Eastbound Cedar Oak Dr |     |       | Westbound Cedar Oak Dr |     |       | Total |
|-------------|-------------------|-----|-------|-------------------|-----|-------|------------------------|-----|-------|------------------------|-----|-------|-------|
|             | In                | Out | Total | In                | Out | Total | In                     | Out | Total | In                     | Out | Total |       |
| Volume      | 17                | 14  | 31    | 14                | 16  | 30    | 0                      | 0   | 0     | 1                      | 2   | 3     | 32    |
| PHF         | 0.71              |     |       | 0.70              |     |       | 0.00                   |     |       | 0.25                   |     |       | 0.89  |

| By Movement | Northbound Hwy 43 |      |       | Southbound Hwy 43 |      |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |      |       | Total |
|-------------|-------------------|------|-------|-------------------|------|-------|------------------------|--|-------|------------------------|------|-------|-------|
|             | T                 | R    | Total | L                 | T    | Total |                        |  | Total | L                      | R    | Total |       |
| Volume      | 16                | 1    | 17    | 1                 | 13   | 14    |                        |  | 0     | 1                      | 0    | 1     | 32    |
| PHF         | 0.67              | 0.25 | 0.71  | 0.25              | 0.65 | 0.70  |                        |  | 0.00  | 0.25                   | 0.00 | 0.25  | 0.89  |

### Heavy Vehicle Rolling Hour Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Hwy 43 |   |       | Southbound Hwy 43 |    |       | Eastbound Cedar Oak Dr |  |       | Westbound Cedar Oak Dr |   |       | Interval Total |
|---------------------|-------------------|---|-------|-------------------|----|-------|------------------------|--|-------|------------------------|---|-------|----------------|
|                     | T                 | R | Total | L                 | T  | Total |                        |  | Total | L                      | R | Total |                |
| 4:00 PM             | 12                | 1 | 13    | 0                 | 18 | 18    |                        |  | 0     | 1                      | 0 | 1     | 32             |
| 4:15 PM             | 16                | 1 | 17    | 1                 | 13 | 14    |                        |  | 0     | 1                      | 0 | 1     | 32             |
| 4:30 PM             | 15                | 0 | 15    | 1                 | 11 | 12    |                        |  | 0     | 0                      | 0 | 0     | 27             |
| 4:45 PM             | 12                | 1 | 13    | 1                 | 12 | 13    |                        |  | 0     | 1                      | 1 | 2     | 28             |
| 5:00 PM             | 10                | 2 | 12    | 1                 | 11 | 12    |                        |  | 0     | 1                      | 1 | 2     | 26             |

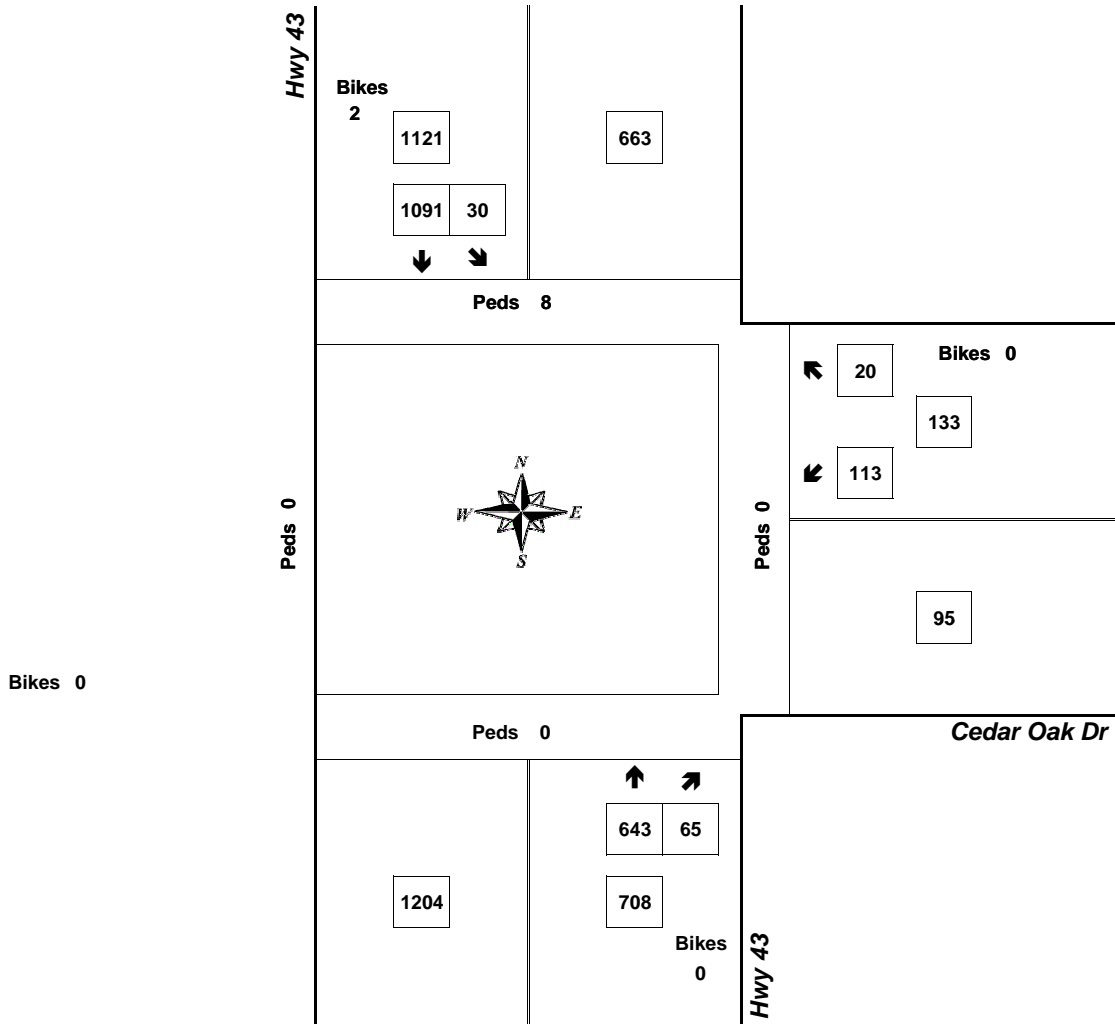
# Peak Hour Summary



Clay Carney  
(503) 833-2740

## Hwy 43 & Cedar Oak Dr

4:15 PM to 5:15 PM  
Tuesday, July 31, 2018



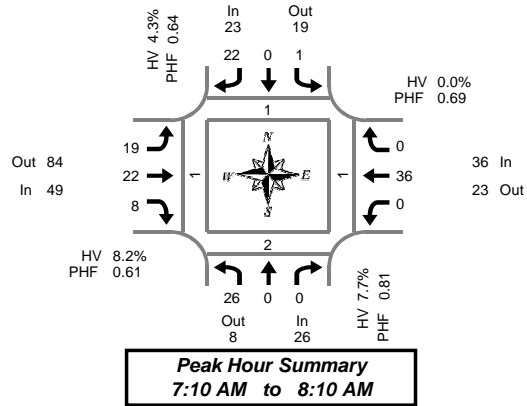
| Approach            | PHF         | HV%         | Volume       |
|---------------------|-------------|-------------|--------------|
| EB                  | 0.00        | 0.0%        | 0            |
| WB                  | 0.81        | 0.8%        | 133          |
| NB                  | 0.96        | 2.4%        | 708          |
| SB                  | 0.97        | 1.2%        | 1,121        |
| <b>Intersection</b> | <b>0.98</b> | <b>1.6%</b> | <b>1,962</b> |

Count Period: 4:00 PM to 6:00 PM

# Total Vehicle Summary



Clay Carney  
(503) 833-2740



## Old River Rd & Cedar Oak Dr

Wednesday, August 01, 2018

7:00 AM to 9:00 AM

### 5-Minute Interval Summary

7:00 AM to 9:00 AM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |    |       | Eastbound Cedar Oak Dr |    |    |       | Westbound Cedar Oak Dr |    |   |       | Interval Total | Pedestrians Crosswalk |       |      |      |
|---------------------|-------------------------|---|---|-------|-------------------------|---|----|-------|------------------------|----|----|-------|------------------------|----|---|-------|----------------|-----------------------|-------|------|------|
|                     | L                       | T | R | Bikes | L                       | T | R  | Bikes | L                      | T  | R  | Bikes | L                      | T  | R | Bikes |                | North                 | South | East | West |
| 7:00 AM             | 2                       | 0 | 0 | 0     | 1                       | 0 | 1  | 0     | 2                      | 0  | 0  | 0     | 0                      | 0  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 7:05 AM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 1                      | 0  | 1  | 0     | 0                      | 0  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 7:10 AM             | 4                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 3                      | 0  | 1  | 0     | 0                      | 2  | 0 | 0     | 1              | 0                     | 0     | 0    |      |
| 7:15 AM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 3  | 0     | 0                      | 1  | 0  | 0     | 0                      | 5  | 0 | 0     | 0              | 1                     | 1     | 0    |      |
| 7:20 AM             | 3                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 3                      | 2  | 1  | 0     | 0                      | 2  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 7:25 AM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 0  | 1     | 2                      | 0  | 1  | 0     | 0                      | 0  | 0 | 0     | 1              | 0                     | 0     | 0    |      |
| 7:30 AM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 3  | 0     | 2                      | 0  | 3  | 0     | 0                      | 3  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 7:35 AM             | 4                       | 0 | 0 | 1     | 0                       | 0 | 5  | 0     | 1                      | 0  | 0  | 0     | 0                      | 2  | 0 | 0     | 0              | 0                     | 0     | 1    |      |
| 7:40 AM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 1                      | 1  | 0  | 0     | 0                      | 3  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 7:45 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 1                      | 2  | 0  | 0     | 0                      | 5  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 7:50 AM             | 3                       | 0 | 0 | 0     | 1                       | 0 | 2  | 0     | 3                      | 1  | 0  | 0     | 0                      | 5  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 7:55 AM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 0                      | 3  | 0  | 0     | 0                      | 3  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 8:00 AM             | 3                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 3                      | 5  | 0  | 0     | 0                      | 2  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 8:05 AM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 0                      | 7  | 2  | 0     | 0                      | 4  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 8:10 AM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 2                      | 3  | 0  | 0     | 0                      | 2  | 0 | 0     | 0              | 3                     | 0     | 0    |      |
| 8:15 AM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 0  | 0     | 3                      | 1  | 0  | 0     | 0                      | 2  | 0 | 0     | 0              | 2                     | 0     | 0    |      |
| 8:20 AM             | 0                       | 0 | 0 | 1     | 0                       | 0 | 2  | 1     | 0                      | 2  | 0  | 0     | 0                      | 2  | 0 | 0     | 0              | 2                     | 1     | 2    |      |
| 8:25 AM             | 4                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 0                      | 1  | 0  | 0     | 0                      | 2  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 8:30 AM             | 3                       | 1 | 0 | 1     | 0                       | 0 | 5  | 0     | 0                      | 0  | 1  | 0     | 0                      | 3  | 0 | 0     | 0              | 0                     | 1     | 0    |      |
| 8:35 AM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 1                      | 1  | 0  | 0     | 0                      | 1  | 0 | 0     | 0              | 2                     | 0     | 0    |      |
| 8:40 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 3                      | 0  | 2  | 0     | 0                      | 6  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 8:45 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 0                      | 3  | 3  | 0     | 0                      | 3  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 8:50 AM             | 3                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 1                      | 0  | 0  | 0     | 0                      | 4  | 0 | 0     | 0              | 1                     | 0     | 0    |      |
| 8:55 AM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 0  | 0     | 1                      | 1  | 0  | 0     | 0                      | 4  | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| Total Survey        | 46                      | 1 | 0 | 3     | 2                       | 0 | 41 | 2     | 33                     | 34 | 15 | 0     | 0                      | 65 | 0 | 0     | 0              | 1                     | 10    | 5    | 3    |

### 15-Minute Interval Summary

7:00 AM to 9:00 AM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |    |       | Eastbound Cedar Oak Dr |    |    |       | Westbound Cedar Oak Dr |    |   |       | Interval Total | Pedestrians Crosswalk |       |      |      |
|---------------------|-------------------------|---|---|-------|-------------------------|---|----|-------|------------------------|----|----|-------|------------------------|----|---|-------|----------------|-----------------------|-------|------|------|
|                     | L                       | T | R | Bikes | L                       | T | R  | Bikes | L                      | T  | R  | Bikes | L                      | T  | R | Bikes |                | North                 | South | East | West |
| 7:00 AM             | 8                       | 0 | 0 | 0     | 1                       | 0 | 3  | 0     | 6                      | 0  | 2  | 0     | 0                      | 2  | 0 | 0     | 0              | 1                     | 0     | 0    |      |
| 7:15 AM             | 6                       | 0 | 0 | 0     | 0                       | 0 | 4  | 1     | 5                      | 3  | 2  | 0     | 0                      | 7  | 0 | 0     | 0              | 2                     | 1     | 0    |      |
| 7:30 AM             | 7                       | 0 | 0 | 1     | 0                       | 0 | 9  | 0     | 4                      | 1  | 3  | 0     | 0                      | 8  | 0 | 0     | 0              | 0                     | 0     | 1    |      |
| 7:45 AM             | 5                       | 0 | 0 | 0     | 1                       | 0 | 5  | 0     | 4                      | 6  | 0  | 0     | 0                      | 13 | 0 | 0     | 0              | 0                     | 0     | 0    |      |
| 8:00 AM             | 5                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 5                      | 15 | 2  | 0     | 0                      | 8  | 0 | 0     | 0              | 3                     | 0     | 0    |      |
| 8:15 AM             | 6                       | 0 | 0 | 1     | 0                       | 0 | 3  | 1     | 3                      | 4  | 0  | 0     | 0                      | 6  | 0 | 0     | 0              | 4                     | 1     | 2    |      |
| 8:30 AM             | 4                       | 1 | 0 | 1     | 0                       | 0 | 11 | 0     | 4                      | 1  | 3  | 0     | 0                      | 10 | 0 | 0     | 0              | 0                     | 3     | 0    |      |
| 8:45 AM             | 5                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 2                      | 4  | 3  | 0     | 0                      | 11 | 0 | 0     | 0              | 1                     | 0     | 0    |      |
| Total Survey        | 46                      | 1 | 0 | 3     | 2                       | 0 | 41 | 2     | 33                     | 34 | 15 | 0     | 0                      | 65 | 0 | 0     | 0              | 1                     | 10    | 5    | 3    |

### Peak Hour Summary

7:10 AM to 8:10 AM

| By Approach | Northbound Old River Rd |     |       |       | Southbound Old River Rd |     |       |       | Eastbound Cedar Oak Dr |     |       |       | Westbound Cedar Oak Dr |     |       |       | Total | Pedestrians Crosswalk |       |      |      |
|-------------|-------------------------|-----|-------|-------|-------------------------|-----|-------|-------|------------------------|-----|-------|-------|------------------------|-----|-------|-------|-------|-----------------------|-------|------|------|
|             | In                      | Out | Total | Bikes | In                      | Out | Total | Bikes | In                     | Out | Total | Bikes | In                     | Out | Total | Bikes |       | North                 | South | East | West |
| Volume      | 26                      | 8   | 34    | 1     | 23                      | 19  | 42    | 1     | 49                     | 84  | 133   | 0     | 36                     | 23  | 59    | 0     | 1     | 2                     | 1     | 1    |      |
| %HV         | 7.7%                    |     |       |       | 4.3%                    |     |       |       | 8.2%                   |     |       |       | 0.0%                   |     |       |       | 5.2%  |                       |       |      |      |
| PHF         | 0.81                    |     |       |       | 0.64                    |     |       |       | 0.61                   |     |       |       | 0.69                   |     |       |       | 0.86  |                       |       |      |      |

| By Movement | Northbound Old River Rd |      |      |       | Southbound Old River Rd |      |      |       | Eastbound Cedar Oak Dr |      |      |       | Westbound Cedar Oak Dr |      |      |       | Total |
|-------------|-------------------------|------|------|-------|-------------------------|------|------|-------|------------------------|------|------|-------|------------------------|------|------|-------|-------|
|             | L                       | T    | R    | Total | L                       | T    | R    | Total | L                      | T    | R    | Total | L                      | T    | R    | Total |       |
| Volume      | 26                      | 0    | 0    | 26    | 1                       | 0    | 22   | 23    | 19                     | 22   | 8    | 49    | 0                      | 36   | 0    | 36    | 134   |
| %HV         | 7.7%                    | 0.0% | 0.0% | 7.7%  | 0.0%                    | 0.0% | 4.5% | 4.3%  | 10.5%                  | 9.1% | 0.0% | 8.2%  | 0.0%                   | 0.0% | 0.0% | 0.0%  | 5.2%  |
| PHF         | 0.81                    | 0.00 | 0.00 | 0.81  | 0.25                    | 0.00 | 0.61 | 0.64  | 0.68                   | 0.37 | 0.40 | 0.61  | 0.00                   | 0.69 | 0.00 | 0.69  | 0.86  |

### Rolling Hour Summary

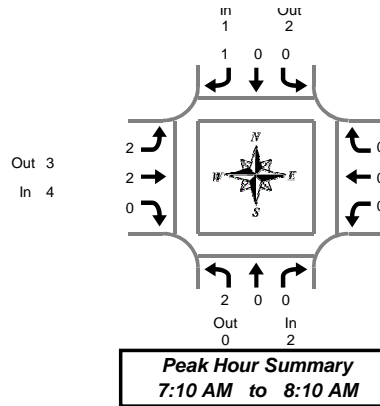
7:00 AM to 9:00 AM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |    |       | Eastbound Cedar Oak Dr |    |   |       | Westbound Cedar Oak Dr |    |   |       | Interval Total | Pedestrians Crosswalk |       |      |      |
|---------------------|-------------------------|---|---|-------|-------------------------|---|----|-------|------------------------|----|---|-------|------------------------|----|---|-------|----------------|-----------------------|-------|------|------|
|                     | L                       | T | R | Bikes | L                       | T | R  | Bikes | L                      | T  | R | Bikes | L                      | T  | R | Bikes |                | North                 | South | East | West |
| 7:00 AM             | 26                      | 0 | 0 | 1     | 2                       | 0 | 21 | 1     | 19                     | 10 | 7 | 0     | 0                      | 30 | 0 | 0     | 0              | 1                     | 2     | 1    |      |
| 7:15 AM             | 23                      | 0 | 0 | 1     | 1                       | 0 | 22 | 1     | 18                     | 25 | 7 | 0     | 0                      | 36 | 0 | 0     | 0              | 5                     | 1     | 1    |      |
| 7:30 AM             | 23                      | 0 | 0 | 2     | 1                       | 0 | 21 | 1     | 16                     | 26 | 5 | 0     | 0                      | 35 | 0 | 0     | 0              | 7                     | 1     | 3    |      |
| 7:45 AM             | 20                      | 1 | 0 | 2     | 1                       | 0 | 23 | 1     | 16                     | 26 | 5 | 0     | 0                      | 37 | 0 | 0     | 0              | 7                     | 4     | 2    |      |
| 8:00 AM             | 20                      | 1 | 0 | 2     | 0                       | 0 | 20 | 1     | 14                     | 24 | 8 | 0     | 0                      | 35 | 0 | 0     | 0              | 8                     | 4     | 2    |      |

# Heavy Vehicle Summary



Clay Carney  
(503) 833-2740



## Old River Rd & Cedar Oak Dr

Wednesday, August 01, 2018

7:00 AM to 9:00 AM

### Heavy Vehicle 5-Minute Interval Summary 7:00 AM to 9:00 AM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |   |       | Eastbound Cedar Oak Dr |   |   |       | Westbound Cedar Oak Dr |   |   |       | Interval Total |
|---------------------|-------------------------|---|---|-------|-------------------------|---|---|-------|------------------------|---|---|-------|------------------------|---|---|-------|----------------|
|                     | L                       | T | R | Total | L                       | T | R | Total | L                      | T | R | Total | L                      | T | R | Total |                |
| 7:00 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 7:05 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 1 | 0 | 0     | 1                      | 0 | 0 | 0     | 0              |
| 7:10 AM             | 1                       | 0 | 0 | 1     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 7:15 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 1     | 1                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 7:20 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 7:25 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 7:30 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 1                      | 0 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 7:35 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 7:40 AM             | 1                       | 0 | 0 | 1     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 7:45 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 7:50 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 1                      | 0 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 7:55 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 8:00 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 8:05 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 8:10 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 0                      | 1 | 0 | 1     | 2              |
| 8:15 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 8:20 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 8:25 AM             | 1                       | 0 | 0 | 1     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 8:30 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 1 | 1     | 1                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 8:35 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 8:40 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 8:45 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 8:50 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 8:55 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| Total Survey        | 3                       | 0 | 0 | 3     | 0                       | 0 | 2 | 2     | 3                      | 3 | 0 | 6     | 0                      | 1 | 0 | 1     | 12             |

### Heavy Vehicle 15-Minute Interval Summary 7:00 AM to 9:00 AM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |   |       | Eastbound Cedar Oak Dr |   |   |       | Westbound Cedar Oak Dr |   |   |       | Interval Total |
|---------------------|-------------------------|---|---|-------|-------------------------|---|---|-------|------------------------|---|---|-------|------------------------|---|---|-------|----------------|
|                     | L                       | T | R | Total | L                       | T | R | Total | L                      | T | R | Total | L                      | T | R | Total |                |
| 7:00 AM             | 1                       | 0 | 0 | 1     | 0                       | 0 | 0 | 0     | 1                      | 0 | 0 | 1     | 0                      | 0 | 0 | 0     | 2              |
| 7:15 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 7:30 AM             | 1                       | 0 | 0 | 1     | 0                       | 0 | 0 | 0     | 1                      | 0 | 0 | 1     | 0                      | 0 | 0 | 0     | 2              |
| 7:45 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 1                      | 0 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 8:00 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 3 | 0 | 3     | 0                      | 1 | 0 | 1     | 4              |
| 8:15 AM             | 1                       | 0 | 0 | 1     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 8:30 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 8:45 AM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| Total Survey        | 3                       | 0 | 0 | 3     | 0                       | 0 | 2 | 2     | 3                      | 3 | 0 | 6     | 0                      | 1 | 0 | 1     | 12             |

### Heavy Vehicle Peak Hour Summary 7:10 AM to 8:10 AM

| By Approach | Northbound Old River Rd |     |       | Southbound Old River Rd |     |       | Eastbound Cedar Oak Dr |     |       | Westbound Cedar Oak Dr |     |       | Total |
|-------------|-------------------------|-----|-------|-------------------------|-----|-------|------------------------|-----|-------|------------------------|-----|-------|-------|
|             | In                      | Out | Total | In                      | Out | Total | In                     | Out | Total | In                     | Out | Total |       |
| Volume      | 2                       | 0   | 2     | 1                       | 2   | 3     | 4                      | 3   | 7     | 0                      | 2   | 2     | 7     |
| PHF         | 0.50                    |     |       | 0.25                    |     |       | 0.50                   |     |       | 0.00                   |     |       | 0.88  |

| By Movement | Northbound Old River Rd |      |      |       | Southbound Old River Rd |      |      |       | Eastbound Cedar Oak Dr |      |      |       | Westbound Cedar Oak Dr |      |      |       | Total |
|-------------|-------------------------|------|------|-------|-------------------------|------|------|-------|------------------------|------|------|-------|------------------------|------|------|-------|-------|
|             | L                       | T    | R    | Total | L                       | T    | R    | Total | L                      | T    | R    | Total | L                      | T    | R    | Total |       |
| Volume      | 2                       | 0    | 0    | 2     | 0                       | 0    | 1    | 1     | 2                      | 2    | 0    | 4     | 0                      | 0    | 0    | 0     | 7     |
| PHF         | 0.50                    | 0.00 | 0.00 | 0.50  | 0.00                    | 0.00 | 0.25 | 0.25  | 0.50                   | 0.25 | 0.00 | 0.50  | 0.00                   | 0.00 | 0.00 | 0.00  | 0.88  |

### Heavy Vehicle Rolling Hour Summary 7:00 AM to 9:00 AM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |   |       | Eastbound Cedar Oak Dr |   |   |       | Westbound Cedar Oak Dr |   |   |       | Interval Total |
|---------------------|-------------------------|---|---|-------|-------------------------|---|---|-------|------------------------|---|---|-------|------------------------|---|---|-------|----------------|
|                     | L                       | T | R | Total | L                       | T | R | Total | L                      | T | R | Total | L                      | T | R | Total |                |
| 7:00 AM             | 2                       | 0 | 0 | 2     | 0                       | 0 | 1 | 1     | 3                      | 0 | 0 | 3     | 0                      | 0 | 0 | 0     | 6              |
| 7:15 AM             | 1                       | 0 | 0 | 1     | 0                       | 0 | 1 | 1     | 2                      | 3 | 0 | 5     | 0                      | 1 | 0 | 1     | 8              |
| 7:30 AM             | 2                       | 0 | 0 | 2     | 0                       | 0 | 0 | 0     | 2                      | 3 | 0 | 5     | 0                      | 1 | 0 | 1     | 8              |
| 7:45 AM             | 1                       | 0 | 0 | 1     | 0                       | 0 | 1 | 1     | 1                      | 3 | 0 | 4     | 0                      | 1 | 0 | 1     | 7              |
| 8:00 AM             | 1                       | 0 | 0 | 1     | 0                       | 0 | 1 | 1     | 0                      | 3 | 0 | 3     | 0                      | 1 | 0 | 1     | 6              |

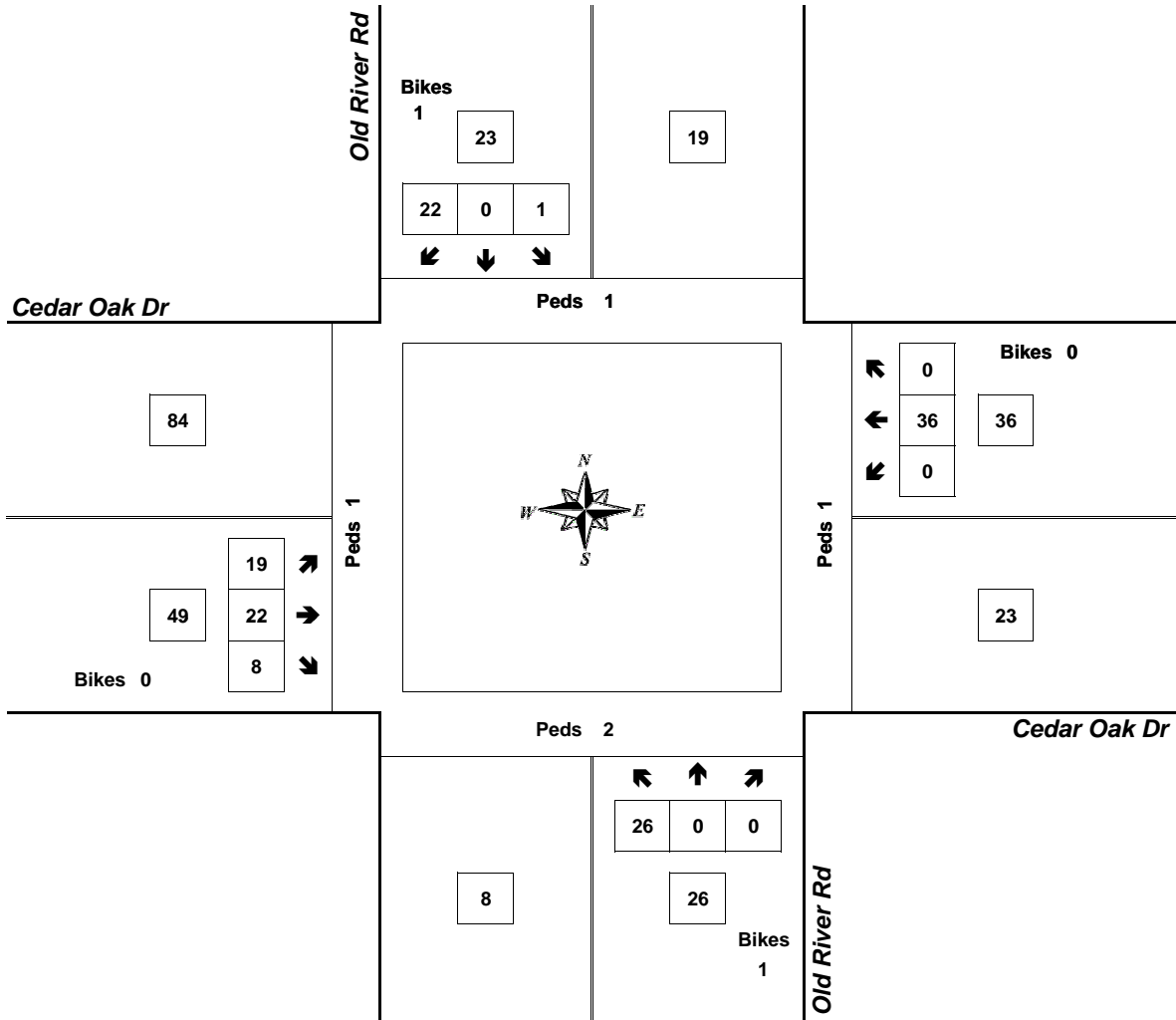
# Peak Hour Summary



Clay Carney  
(503) 833-2740

## Old River Rd & Cedar Oak Dr

7:10 AM to 8:10 AM  
Wednesday, August 01, 2018



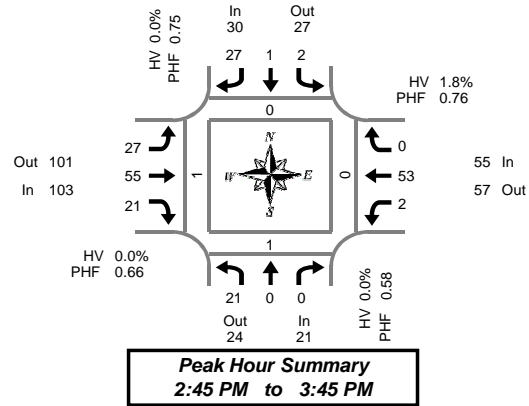
| Approach            | PHF         | HV%         | Volume     |
|---------------------|-------------|-------------|------------|
| EB                  | 0.61        | 8.2%        | 49         |
| WB                  | 0.69        | 0.0%        | 36         |
| NB                  | 0.81        | 7.7%        | 26         |
| SB                  | 0.64        | 4.3%        | 23         |
| <b>Intersection</b> | <b>0.86</b> | <b>5.2%</b> | <b>134</b> |

Count Period: 7:00 AM to 9:00 AM

# Total Vehicle Summary



Clay Carney  
(503) 833-2740



## Old River Rd & Cedar Oak Dr

Tuesday, July 31, 2018  
2:00 PM to 4:00 PM

### 5-Minute Interval Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |    |       | Eastbound Cedar Oak Dr |    |    |       | Westbound Cedar Oak Dr |    |   |       | Interval Total | Pedestrians Crosswalk |       |      |      |   |
|---------------------|-------------------------|---|---|-------|-------------------------|---|----|-------|------------------------|----|----|-------|------------------------|----|---|-------|----------------|-----------------------|-------|------|------|---|
|                     | L                       | T | R | Bikes | L                       | T | R  | Bikes | L                      | T  | R  | Bikes | L                      | T  | R | Bikes |                | North                 | South | East | West |   |
| 2:00 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 1                      | 4  | 0  | 0     | 0                      | 4  | 0 | 0     | 0              | 11                    | 0     | 2    | 0    | 0 |
| 2:05 PM             | 1                       | 0 | 0 | 0     | 0                       | 1 | 5  | 0     | 1                      | 2  | 1  | 0     | 0                      | 3  | 0 | 0     | 0              | 14                    | 0     | 1    | 0    | 0 |
| 2:10 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 1                      | 2  | 2  | 0     | 0                      | 3  | 0 | 0     | 0              | 10                    | 0     | 0    | 0    | 0 |
| 2:15 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 3  | 0     | 3                      | 3  | 0  | 0     | 0                      | 0  | 0 | 0     | 0              | 10                    | 0     | 0    | 0    | 0 |
| 2:20 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 2  | 1     | 1                      | 5  | 2  | 0     | 0                      | 6  | 1 | 0     | 0              | 18                    | 0     | 0    | 0    | 0 |
| 2:25 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 1                      | 3  | 3  | 0     | 0                      | 4  | 0 | 0     | 0              | 16                    | 0     | 0    | 0    | 0 |
| 2:30 PM             | 1                       | 0 | 1 | 0     | 0                       | 0 | 1  | 0     | 4                      | 3  | 4  | 0     | 0                      | 5  | 0 | 0     | 0              | 19                    | 0     | 0    | 0    | 0 |
| 2:35 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 1                      | 0  | 1  | 0     | 0                      | 3  | 0 | 0     | 0              | 10                    | 0     | 0    | 0    | 0 |
| 2:40 PM             | 4                       | 0 | 0 | 0     | 1                       | 0 | 4  | 0     | 1                      | 3  | 2  | 0     | 0                      | 3  | 0 | 0     | 0              | 18                    | 0     | 0    | 0    | 0 |
| 2:45 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 1                      | 5  | 1  | 0     | 0                      | 5  | 0 | 0     | 0              | 14                    | 0     | 0    | 0    | 0 |
| 2:50 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 0                      | 3  | 0  | 0     | 1                      | 4  | 0 | 0     | 0              | 11                    | 0     | 0    | 0    | 0 |
| 2:55 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 3                      | 6  | 3  | 0     | 1                      | 7  | 0 | 0     | 0              | 25                    | 0     | 1    | 0    | 0 |
| 3:00 PM             | 3                       | 0 | 0 | 0     | 0                       | 0 | 0  | 0     | 1                      | 5  | 0  | 0     | 0                      | 4  | 0 | 0     | 0              | 13                    | 0     | 0    | 0    | 0 |
| 3:05 PM             | 2                       | 0 | 0 | 0     | 0                       | 1 | 2  | 0     | 5                      | 5  | 3  | 0     | 0                      | 5  | 0 | 0     | 0              | 23                    | 0     | 0    | 0    | 1 |
| 3:10 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 3  | 0     | 0                      | 1  | 3  | 0     | 0                      | 6  | 0 | 0     | 0              | 14                    | 0     | 0    | 0    | 0 |
| 3:15 PM             | 3                       | 0 | 0 | 0     | 0                       | 0 | 3  | 0     | 1                      | 5  | 1  | 0     | 0                      | 4  | 0 | 0     | 0              | 17                    | 0     | 0    | 0    | 0 |
| 3:20 PM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 3  | 0     | 2                      | 4  | 0  | 0     | 0                      | 4  | 0 | 0     | 0              | 15                    | 0     | 0    | 0    | 0 |
| 3:25 PM             | 4                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 2                      | 3  | 1  | 0     | 0                      | 4  | 0 | 0     | 0              | 18                    | 0     | 0    | 0    | 0 |
| 3:30 PM             | 0                       | 0 | 0 | 0     | 1                       | 0 | 1  | 0     | 5                      | 7  | 5  | 0     | 0                      | 0  | 0 | 0     | 0              | 19                    | 0     | 0    | 0    | 0 |
| 3:35 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 2  | 1     | 4                      | 8  | 1  | 0     | 0                      | 4  | 0 | 0     | 0              | 20                    | 0     | 0    | 0    | 0 |
| 3:40 PM             | 2                       | 0 | 0 | 0     | 1                       | 0 | 2  | 0     | 3                      | 3  | 3  | 0     | 0                      | 6  | 0 | 0     | 0              | 20                    | 0     | 0    | 0    | 0 |
| 3:45 PM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 1                      | 0  | 1  | 0     | 0                      | 0  | 0 | 0     | 0              | 5                     | 0     | 0    | 0    | 0 |
| 3:50 PM             | 2                       | 0 | 0 | 0     | 0                       | 1 | 2  | 0     | 1                      | 4  | 0  | 0     | 0                      | 2  | 0 | 0     | 0              | 12                    | 0     | 0    | 2    | 0 |
| 3:55 PM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 1                      | 6  | 6  | 1     | 0                      | 3  | 0 | 0     | 0              | 19                    | 0     | 0    | 0    | 0 |
| Total Survey        | 38                      | 0 | 1 | 0     | 3                       | 3 | 57 | 2     | 44                     | 90 | 43 | 1     | 2                      | 89 | 1 | 0     | 0              | 371                   | 0     | 4    | 2    | 1 |

### 15-Minute Interval Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |    |       | Eastbound Cedar Oak Dr |    |    |       | Westbound Cedar Oak Dr |    |   |       | Interval Total | Pedestrians Crosswalk |       |      |      |   |
|---------------------|-------------------------|---|---|-------|-------------------------|---|----|-------|------------------------|----|----|-------|------------------------|----|---|-------|----------------|-----------------------|-------|------|------|---|
|                     | L                       | T | R | Bikes | L                       | T | R  | Bikes | L                      | T  | R  | Bikes | L                      | T  | R | Bikes |                | North                 | South | East | West |   |
| 2:00 PM             | 2                       | 0 | 0 | 0     | 0                       | 1 | 8  | 0     | 3                      | 8  | 3  | 0     | 0                      | 10 | 0 | 0     | 0              | 35                    | 0     | 3    | 0    | 0 |
| 2:15 PM             | 3                       | 0 | 0 | 0     | 0                       | 0 | 9  | 1     | 5                      | 11 | 5  | 0     | 0                      | 10 | 1 | 0     | 0              | 44                    | 0     | 0    | 0    | 0 |
| 2:30 PM             | 6                       | 0 | 1 | 0     | 1                       | 0 | 9  | 0     | 6                      | 6  | 7  | 0     | 0                      | 11 | 0 | 0     | 0              | 47                    | 0     | 0    | 0    | 0 |
| 2:45 PM             | 3                       | 0 | 0 | 0     | 0                       | 0 | 7  | 0     | 4                      | 14 | 4  | 0     | 2                      | 16 | 0 | 0     | 0              | 50                    | 0     | 1    | 0    | 0 |
| 3:00 PM             | 6                       | 0 | 0 | 0     | 0                       | 1 | 5  | 0     | 6                      | 11 | 6  | 0     | 0                      | 15 | 0 | 0     | 0              | 50                    | 0     | 0    | 0    | 1 |
| 3:15 PM             | 9                       | 0 | 0 | 0     | 0                       | 0 | 10 | 0     | 5                      | 12 | 2  | 0     | 0                      | 12 | 0 | 0     | 0              | 50                    | 0     | 0    | 0    | 0 |
| 3:30 PM             | 3                       | 0 | 0 | 0     | 2                       | 0 | 5  | 1     | 12                     | 18 | 9  | 0     | 0                      | 10 | 0 | 0     | 0              | 59                    | 0     | 0    | 0    | 0 |
| 3:45 PM             | 6                       | 0 | 0 | 0     | 0                       | 1 | 4  | 0     | 3                      | 10 | 7  | 1     | 0                      | 5  | 0 | 0     | 0              | 36                    | 0     | 0    | 2    | 0 |
| Total Survey        | 38                      | 0 | 1 | 0     | 3                       | 3 | 57 | 2     | 44                     | 90 | 43 | 1     | 2                      | 89 | 1 | 0     | 0              | 371                   | 0     | 4    | 2    | 1 |

### Peak Hour Summary 2:45 PM to 3:45 PM

| By Approach | Northbound Old River Rd |     |       |       | Southbound Old River Rd |     |       |       | Eastbound Cedar Oak Dr |     |       |       | Westbound Cedar Oak Dr |     |       |       | Total | Pedestrians Crosswalk |       |      |      |
|-------------|-------------------------|-----|-------|-------|-------------------------|-----|-------|-------|------------------------|-----|-------|-------|------------------------|-----|-------|-------|-------|-----------------------|-------|------|------|
|             | In                      | Out | Total | Bikes | In                      | Out | Total | Bikes | In                     | Out | Total | Bikes | In                     | Out | Total | Bikes |       | North                 | South | East | West |
| Volume      | 21                      | 24  | 45    | 0     | 30                      | 27  | 57    | 1     | 103                    | 101 | 204   | 0     | 55                     | 57  | 112   | 0     | 209   | 0                     | 1     | 0    | 1    |
| %HV         | 0.0%                    |     |       |       | 0.0%                    |     |       |       | 0.0%                   |     |       |       | 1.8%                   |     |       |       | 0.5%  |                       |       |      |      |
| PHF         | 0.58                    |     |       |       | 0.75                    |     |       |       | 0.66                   |     |       |       | 0.76                   |     |       |       | 0.86  |                       |       |      |      |

| By Movement | Northbound Old River Rd |      |      |       | Southbound Old River Rd |      |      |       | Eastbound Cedar Oak Dr |      |      |       | Westbound Cedar Oak Dr |      |      |       | Total |
|-------------|-------------------------|------|------|-------|-------------------------|------|------|-------|------------------------|------|------|-------|------------------------|------|------|-------|-------|
|             | L                       | T    | R    | Total | L                       | T    | R    | Total | L                      | T    | R    | Total | L                      | T    | R    | Total |       |
| Volume      | 21                      | 0    | 0    | 21    | 2                       | 1    | 27   | 30    | 27                     | 55   | 21   | 103   | 2                      | 53   | 0    | 55    | 209   |
| %HV         | 0.0%                    | 0.0% | 0.0% | 0.0%  | 0.0%                    | 0.0% | 0.0% | 0.0%  | 0.0%                   | 0.0% | 0.0% | 0.0%  | 0.0%                   | 1.9% | 0.0% | 1.8%  | 0.5%  |
| PHF         | 0.58                    | 0.00 | 0.00 | 0.58  | 0.25                    | 0.25 | 0.68 | 0.75  | 0.56                   | 0.76 | 0.58 | 0.66  | 0.25                   | 0.83 | 0.00 | 0.76  | 0.86  |

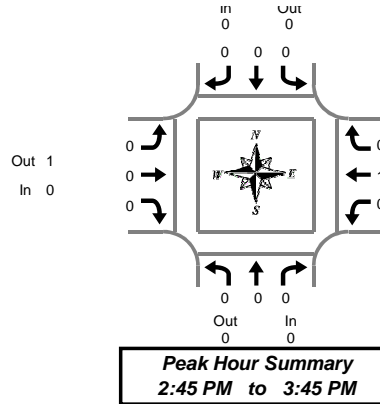
### Rolling Hour Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |    |       | Eastbound Cedar Oak Dr |    |    |       | Westbound Cedar Oak Dr |    |   |       | Interval Total | Pedestrians Crosswalk |       |      |      |   |
|---------------------|-------------------------|---|---|-------|-------------------------|---|----|-------|------------------------|----|----|-------|------------------------|----|---|-------|----------------|-----------------------|-------|------|------|---|
|                     | L                       | T | R | Bikes | L                       | T | R  | Bikes | L                      | T  | R  | Bikes | L                      | T  | R | Bikes |                | North                 | South | East | West |   |
| 2:00 PM             | 14                      | 0 | 1 | 0     | 1                       | 1 | 33 | 1     | 18                     | 39 | 19 | 0     | 2                      | 47 | 1 | 0     | 0              | 176                   | 0     | 4    | 0    | 0 |
| 2:15 PM             | 18                      | 0 | 1 | 0     | 1                       | 1 | 30 | 1     | 21                     | 42 | 22 | 0     | 2                      | 52 | 1 | 0     | 0              | 191                   | 0     | 1    | 0    | 1 |
| 2:30 PM             | 24                      | 0 | 1 | 0     | 1                       | 1 | 31 | 0     | 21                     | 43 | 19 | 0     | 2                      | 54 | 0 | 0     | 0              | 197                   | 0     | 1    | 0    | 1 |
| 2:45 PM             | 21                      | 0 | 0 | 0     | 2                       | 1 | 27 | 1     | 27                     | 55 | 21 | 0     | 2                      | 53 | 0 | 0     | 0              | 209                   | 0     | 1    | 0    | 1 |
| 3:00 PM             | 24                      | 0 | 0 | 0     | 2                       | 2 | 24 | 1     | 26                     | 51 | 24 | 1     | 0                      | 42 | 0 | 0     | 0              | 195                   | 0     | 0    | 2    | 1 |

# Heavy Vehicle Summary



Clay Carney  
(503) 833-2740



## Old River Rd & Cedar Oak Dr

Tuesday, July 31, 2018  
2:00 PM to 4:00 PM

### Heavy Vehicle 5-Minute Interval Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |   |       | Eastbound Cedar Oak Dr |   |   |       | Westbound Cedar Oak Dr |   |   |       | Interval Total |
|---------------------|-------------------------|---|---|-------|-------------------------|---|---|-------|------------------------|---|---|-------|------------------------|---|---|-------|----------------|
|                     | L                       | T | R | Total | L                       | T | R | Total | L                      | T | R | Total | L                      | T | R | Total |                |
| 2:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 2:05 PM             | 0                       | 0 | 0 | 0     | 0                       | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 2:10 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 2:15 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 2:20 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 1              |
| 2:25 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 2:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 2:35 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 2:40 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 2:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1                      | 0 | 1 | 1     | 1              |
| 2:50 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 2:55 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:05 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:10 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:15 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:20 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:25 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:35 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:40 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:50 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:55 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 1              |
| Total Survey        | 0                       | 0 | 0 | 0     | 0                       | 1 | 0 | 1     | 0                      | 1 | 1 | 2     | 0                      | 2 | 0 | 2     | 5              |

### Heavy Vehicle 15-Minute Interval Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |   |       | Eastbound Cedar Oak Dr |   |   |       | Westbound Cedar Oak Dr |   |   |       | Interval Total |
|---------------------|-------------------------|---|---|-------|-------------------------|---|---|-------|------------------------|---|---|-------|------------------------|---|---|-------|----------------|
|                     | L                       | T | R | Total | L                       | T | R | Total | L                      | T | R | Total | L                      | T | R | Total |                |
| 2:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 1 | 0 | 1     | 0                      | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 2              |
| 2:15 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 1              |
| 2:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 2:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1                      | 0 | 1 | 1     | 1              |
| 3:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:15 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 3:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 1              |
| Total Survey        | 0                       | 0 | 0 | 0     | 0                       | 1 | 0 | 1     | 0                      | 1 | 1 | 2     | 0                      | 2 | 0 | 2     | 5              |

### Heavy Vehicle Peak Hour Summary 2:45 PM to 3:45 PM

| By Approach | Northbound Old River Rd |     |       | Southbound Old River Rd |     |       | Eastbound Cedar Oak Dr |     |       | Westbound Cedar Oak Dr |     |       | Total |
|-------------|-------------------------|-----|-------|-------------------------|-----|-------|------------------------|-----|-------|------------------------|-----|-------|-------|
|             | In                      | Out | Total | In                      | Out | Total | In                     | Out | Total | In                     | Out | Total |       |
| Volume      | 0                       | 0   | 0     | 0                       | 0   | 0     | 0                      | 1   | 1     | 1                      | 0   | 1     | 1     |
| PHF         | 0.00                    |     |       | 0.00                    |     |       | 0.00                   |     |       | 0.25                   |     |       | 0.25  |

| By Movement | Northbound Old River Rd |      |      |       | Southbound Old River Rd |      |      |       | Eastbound Cedar Oak Dr |      |      |       | Westbound Cedar Oak Dr |      |      |       | Total |
|-------------|-------------------------|------|------|-------|-------------------------|------|------|-------|------------------------|------|------|-------|------------------------|------|------|-------|-------|
|             | L                       | T    | R    | Total | L                       | T    | R    | Total | L                      | T    | R    | Total | L                      | T    | R    | Total |       |
| Volume      | 0                       | 0    | 0    | 0     | 0                       | 0    | 0    | 0     | 0                      | 0    | 0    | 0     | 0                      | 1    | 0    | 1     | 1     |
| PHF         | 0.00                    | 0.00 | 0.00 | 0.00  | 0.00                    | 0.00 | 0.00 | 0.00  | 0.00                   | 0.00 | 0.00 | 0.00  | 0.00                   | 0.25 | 0.00 | 0.25  | 0.25  |

### Heavy Vehicle Rolling Hour Summary 2:00 PM to 4:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |   |       | Eastbound Cedar Oak Dr |   |   |       | Westbound Cedar Oak Dr |   |   |       | Interval Total |
|---------------------|-------------------------|---|---|-------|-------------------------|---|---|-------|------------------------|---|---|-------|------------------------|---|---|-------|----------------|
|                     | L                       | T | R | Total | L                       | T | R | Total | L                      | T | R | Total | L                      | T | R | Total |                |
| 2:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 1 | 0 | 1     | 0                      | 1 | 0 | 1     | 0                      | 2 | 0 | 2     | 4              |
| 2:15 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 2 | 0 | 2     | 2              |
| 2:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1                      | 0 | 1 | 1     | 1              |
| 2:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1                      | 0 | 1 | 1     | 1              |
| 3:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 1              |



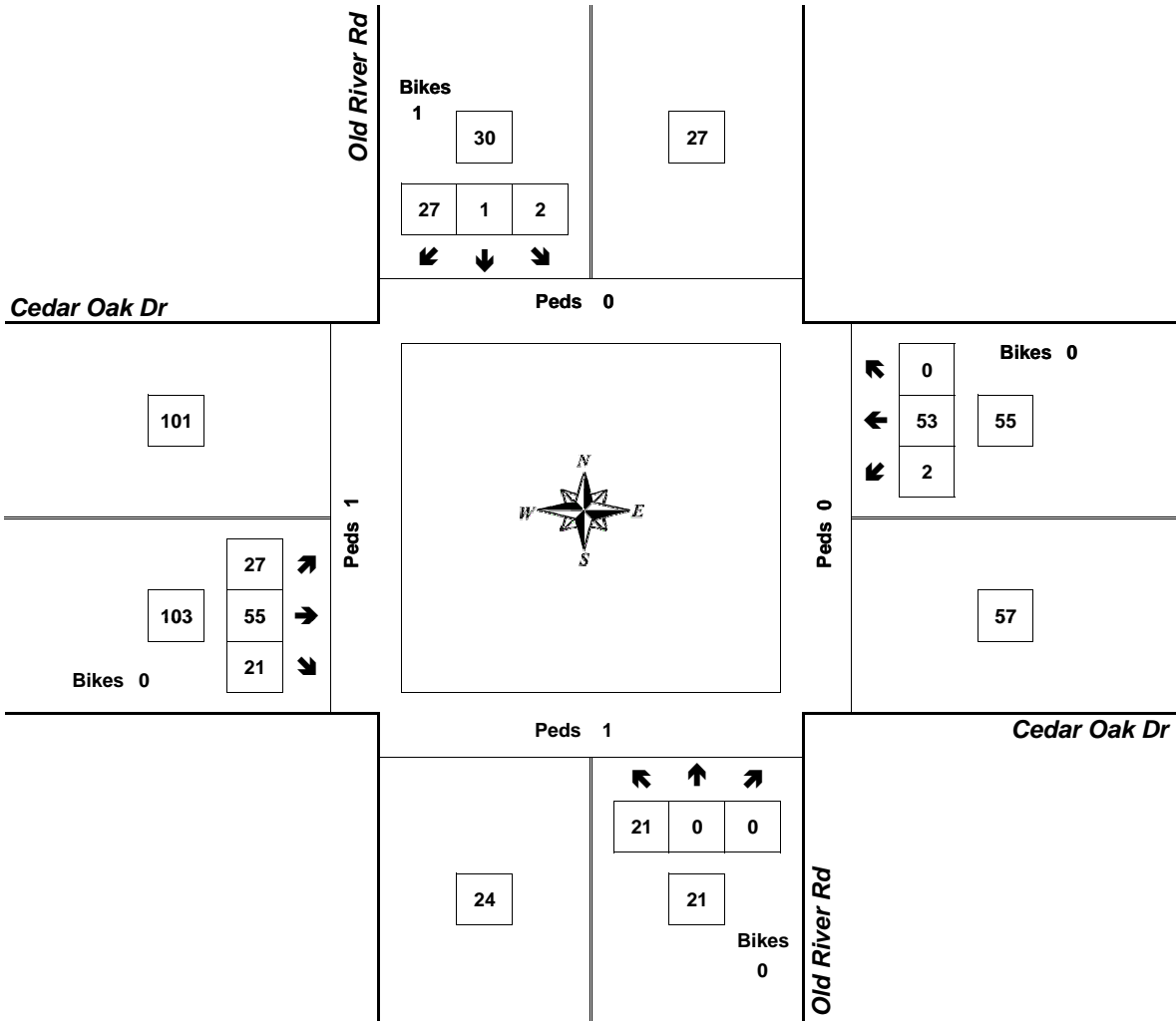
# Peak Hour Summary



Clay Carney  
(503) 833-2740

## Old River Rd & Cedar Oak Dr

2:45 PM to 3:45 PM  
Tuesday, July 31, 2018



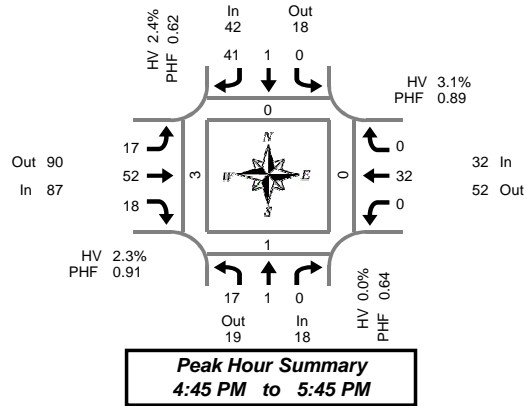
| Approach            | PHF         | HV%         | Volume     |
|---------------------|-------------|-------------|------------|
| EB                  | 0.66        | 0.0%        | 103        |
| WB                  | 0.76        | 1.8%        | 55         |
| NB                  | 0.58        | 0.0%        | 21         |
| SB                  | 0.75        | 0.0%        | 30         |
| <b>Intersection</b> | <b>0.86</b> | <b>0.5%</b> | <b>209</b> |

Count Period: 2:00 PM to 4:00 PM

# Total Vehicle Summary



Clay Carney  
(503) 833-2740



## Old River Rd & Cedar Oak Dr

Tuesday, July 31, 2018  
4:00 PM to 6:00 PM

### 5-Minute Interval Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |    |       | Eastbound Cedar Oak Dr |     |    |       | Westbound Cedar Oak Dr |   |    |       | Interval Total | Pedestrians Crosswalk |       |      |      |   |
|---------------------|-------------------------|---|---|-------|-------------------------|---|----|-------|------------------------|-----|----|-------|------------------------|---|----|-------|----------------|-----------------------|-------|------|------|---|
|                     | L                       | T | R | Bikes | L                       | T | R  | Bikes | L                      | T   | R  | Bikes | L                      | T | R  | Bikes |                | North                 | South | East | West |   |
| 4:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 1  | 5     | 0                      | 2   | 12 | 5     | 0                      | 0 | 3  | 0     | 0              | 28                    | 0     | 0    | 0    | 0 |
| 4:05 PM             | 2                       | 0 | 0 | 0     | 0                       | 1 | 1  | 0     | 0                      | 1   | 2  | 0     | 0                      | 0 | 2  | 0     | 0              | 9                     | 0     | 0    | 0    | 0 |
| 4:10 PM             | 0                       | 0 | 1 | 0     | 0                       | 0 | 1  | 0     | 0                      | 2   | 3  | 4     | 0                      | 0 | 4  | 0     | 0              | 15                    | 0     | 0    | 0    | 0 |
| 4:15 PM             | 2                       | 1 | 0 | 0     | 0                       | 1 | 0  | 4     | 0                      | 1   | 1  | 1     | 0                      | 0 | 3  | 0     | 0              | 14                    | 0     | 0    | 0    | 0 |
| 4:20 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 0                      | 1   | 3  | 3     | 0                      | 0 | 3  | 0     | 0              | 12                    | 0     | 0    | 0    | 0 |
| 4:25 PM             | 0                       | 0 | 0 | 0     | 0                       | 1 | 0  | 0     | 0                      | 0   | 5  | 2     | 0                      | 0 | 2  | 0     | 0              | 10                    | 0     | 0    | 0    | 0 |
| 4:30 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 5  | 0     | 0                      | 1   | 3  | 3     | 0                      | 0 | 3  | 0     | 0              | 16                    | 0     | 0    | 0    | 1 |
| 4:35 PM             | 0                       | 1 | 0 | 0     | 1                       | 0 | 6  | 0     | 3                      | 1   | 1  | 1     | 0                      | 0 | 2  | 0     | 0              | 15                    | 0     | 0    | 0    | 0 |
| 4:40 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 0                      | 7   | 0  | 0     | 0                      | 0 | 5  | 0     | 0              | 14                    | 0     | 0    | 0    | 0 |
| 4:45 PM             | 2                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 3                      | 3   | 2  | 0     | 0                      | 0 | 3  | 0     | 0              | 17                    | 0     | 0    | 0    | 0 |
| 4:50 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 1                      | 4   | 3  | 0     | 0                      | 0 | 3  | 0     | 0              | 13                    | 0     | 0    | 0    | 0 |
| 4:55 PM             | 2                       | 1 | 0 | 0     | 0                       | 0 | 2  | 0     | 0                      | 6   | 2  | 0     | 0                      | 0 | 1  | 0     | 0              | 14                    | 0     | 0    | 0    | 0 |
| 5:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 3                      | 1   | 0  | 0     | 0                      | 0 | 2  | 0     | 0              | 10                    | 0     | 0    | 0    | 1 |
| 5:05 PM             | 3                       | 0 | 0 | 2     | 0                       | 0 | 1  | 0     | 0                      | 4   | 1  | 0     | 0                      | 0 | 4  | 0     | 0              | 13                    | 0     | 0    | 0    | 0 |
| 5:10 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 5  | 0     | 0                      | 4   | 5  | 0     | 0                      | 0 | 3  | 0     | 0              | 18                    | 0     | 0    | 0    | 0 |
| 5:15 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 1                      | 4   | 0  | 0     | 0                      | 0 | 1  | 0     | 0              | 11                    | 0     | 0    | 0    | 0 |
| 5:20 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 1  | 0     | 1                      | 3   | 3  | 0     | 0                      | 0 | 4  | 0     | 0              | 13                    | 0     | 0    | 0    | 0 |
| 5:25 PM             | 5                       | 0 | 0 | 0     | 0                       | 0 | 4  | 0     | 3                      | 6   | 1  | 0     | 0                      | 0 | 2  | 0     | 0              | 21                    | 0     | 0    | 0    | 1 |
| 5:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 8  | 0     | 2                      | 2   | 0  | 0     | 0                      | 0 | 3  | 0     | 0              | 15                    | 0     | 0    | 0    | 1 |
| 5:35 PM             | 2                       | 0 | 0 | 0     | 0                       | 1 | 4  | 0     | 2                      | 6   | 1  | 0     | 0                      | 0 | 3  | 0     | 0              | 19                    | 0     | 0    | 0    | 0 |
| 5:40 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 1                      | 9   | 0  | 0     | 0                      | 0 | 3  | 0     | 0              | 15                    | 0     | 1    | 0    | 0 |
| 5:45 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 3  | 0     | 2                      | 2   | 1  | 0     | 0                      | 0 | 2  | 0     | 0              | 11                    | 0     | 0    | 0    | 0 |
| 5:50 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 2  | 0     | 1                      | 4   | 1  | 0     | 0                      | 0 | 3  | 0     | 0              | 12                    | 1     | 0    | 1    | 0 |
| 5:55 PM             | 1                       | 0 | 0 | 0     | 0                       | 0 | 2  | 1     | 0                      | 6   | 1  | 0     | 0                      | 0 | 7  | 0     | 0              | 17                    | 0     | 0    | 0    | 0 |
| Total Survey        | 26                      | 3 | 1 | 2     | 2                       | 4 | 73 | 1     | 31                     | 101 | 40 | 0     | 0                      | 0 | 71 | 0     | 0              | 352                   | 1     | 1    | 1    | 4 |

### 15-Minute Interval Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |    |       | Eastbound Cedar Oak Dr |     |    |       | Westbound Cedar Oak Dr |    |    |       | Interval Total | Pedestrians Crosswalk |       |      |      |   |
|---------------------|-------------------------|---|---|-------|-------------------------|---|----|-------|------------------------|-----|----|-------|------------------------|----|----|-------|----------------|-----------------------|-------|------|------|---|
|                     | L                       | T | R | Bikes | L                       | T | R  | Bikes | L                      | T   | R  | Bikes | L                      | T  | R  | Bikes |                | North                 | South | East | West |   |
| 4:00 PM             | 2                       | 0 | 1 | 0     | 0                       | 2 | 7  | 0     | 5                      | 17  | 9  | 0     | 0                      | 9  | 0  | 0     | 52             | 0                     | 0     | 0    | 0    |   |
| 4:15 PM             | 2                       | 1 | 0 | 0     | 1                       | 1 | 6  | 0     | 2                      | 9   | 6  | 0     | 0                      | 8  | 0  | 0     | 36             | 0                     | 0     | 0    | 0    |   |
| 4:30 PM             | 2                       | 1 | 0 | 0     | 1                       | 0 | 12 | 0     | 4                      | 11  | 4  | 0     | 0                      | 10 | 0  | 0     | 45             | 0                     | 0     | 0    | 1    |   |
| 4:45 PM             | 4                       | 1 | 0 | 0     | 0                       | 0 | 8  | 0     | 4                      | 13  | 7  | 0     | 0                      | 7  | 0  | 0     | 44             | 0                     | 0     | 0    | 0    |   |
| 5:00 PM             | 4                       | 0 | 0 | 2     | 0                       | 0 | 10 | 0     | 3                      | 9   | 6  | 0     | 0                      | 9  | 0  | 0     | 41             | 0                     | 0     | 0    | 1    |   |
| 5:15 PM             | 7                       | 0 | 0 | 0     | 0                       | 0 | 9  | 0     | 5                      | 13  | 4  | 0     | 0                      | 7  | 0  | 0     | 45             | 0                     | 0     | 0    | 1    |   |
| 5:30 PM             | 2                       | 0 | 0 | 0     | 0                       | 1 | 14 | 0     | 5                      | 17  | 1  | 0     | 0                      | 9  | 0  | 0     | 49             | 0                     | 1     | 0    | 1    |   |
| 5:45 PM             | 3                       | 0 | 0 | 0     | 0                       | 0 | 7  | 1     | 3                      | 12  | 3  | 0     | 0                      | 12 | 0  | 0     | 40             | 1                     | 0     | 1    | 0    |   |
| Total Survey        | 26                      | 3 | 1 | 2     | 2                       | 4 | 73 | 1     | 31                     | 101 | 40 | 0     | 0                      | 0  | 71 | 0     | 0              | 352                   | 1     | 1    | 1    | 4 |

### Peak Hour Summary 4:45 PM to 5:45 PM

| By Approach | Northbound Old River Rd |     |       |       | Southbound Old River Rd |     |       |       | Eastbound Cedar Oak Dr |     |       |       | Westbound Cedar Oak Dr |     |       |       | Total | Pedestrians Crosswalk |       |      |      |
|-------------|-------------------------|-----|-------|-------|-------------------------|-----|-------|-------|------------------------|-----|-------|-------|------------------------|-----|-------|-------|-------|-----------------------|-------|------|------|
|             | In                      | Out | Total | Bikes | In                      | Out | Total | Bikes | In                     | Out | Total | Bikes | In                     | Out | Total | Bikes |       | North                 | South | East | West |
| Volume      | 18                      | 19  | 37    | 2     | 42                      | 18  | 60    | 0     | 87                     | 90  | 177   | 0     | 32                     | 52  | 84    | 0     | 179   | 0                     | 1     | 0    | 3    |
| %HV         | 0.0%                    |     |       |       | 2.4%                    |     |       |       | 2.3%                   |     |       |       | 3.1%                   |     |       |       | 2.2%  |                       |       |      |      |
| PHF         | 0.64                    |     |       |       | 0.62                    |     |       |       | 0.91                   |     |       |       | 0.89                   |     |       |       | 0.81  |                       |       |      |      |

| By Movement | Northbound Old River Rd |      |      |       | Southbound Old River Rd |      |      |       | Eastbound Cedar Oak Dr |      |      |       | Westbound Cedar Oak Dr |      |      |       | Total |
|-------------|-------------------------|------|------|-------|-------------------------|------|------|-------|------------------------|------|------|-------|------------------------|------|------|-------|-------|
|             | L                       | T    | R    | Total | L                       | T    | R    | Total | L                      | T    | R    | Total | L                      | T    | R    | Total |       |
| Volume      | 17                      | 1    | 0    | 18    | 0                       | 1    | 41   | 42    | 17                     | 52   | 18   | 87    | 0                      | 32   | 0    | 32    | 179   |
| %HV         | 0.0%                    | 0.0% | 0.0% | 0.0%  | 0.0%                    | 0.0% | 2.4% | 2.4%  | 5.9%                   | 1.9% | 0.0% | 2.3%  | 0.0%                   | 3.1% | 0.0% | 3.1%  | 2.2%  |
| PHF         | 0.61                    | 0.25 | 0.00 | 0.64  | 0.00                    | 0.25 | 0.64 | 0.62  | 0.61                   | 0.76 | 0.56 | 0.91  | 0.00                   | 0.89 | 0.00 | 0.89  | 0.81  |

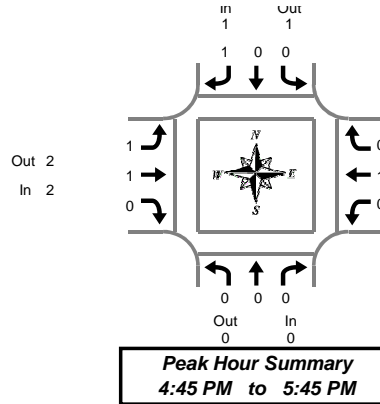
### Rolling Hour Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |    |       | Eastbound Cedar Oak Dr |    |    |       | Westbound Cedar Oak Dr |    |   |       | Interval Total | Pedestrians Crosswalk |       |      |      |
|---------------------|-------------------------|---|---|-------|-------------------------|---|----|-------|------------------------|----|----|-------|------------------------|----|---|-------|----------------|-----------------------|-------|------|------|
|                     | L                       | T | R | Bikes | L                       | T | R  | Bikes | L                      | T  | R  | Bikes | L                      | T  | R | Bikes |                | North                 | South | East | West |
| 4:00 PM             | 10                      | 3 | 1 | 0     | 2                       | 3 | 33 | 0     | 15                     | 50 | 26 | 0     | 0                      | 34 | 0 | 0     | 177            | 0                     | 0     | 0    | 1    |
| 4:15 PM             | 12                      | 3 | 0 | 2     | 2                       | 1 | 36 | 0     | 13                     | 42 | 23 | 0     | 0                      | 34 | 0 | 0     | 166            | 0                     | 0     | 0    | 2    |
| 4:30 PM             | 17                      | 2 | 0 | 2     | 1                       | 0 | 39 | 0     | 16                     | 46 | 21 | 0     | 0                      | 33 | 0 | 0     | 175            | 0                     | 0     | 0    | 3    |
| 4:45 PM             | 17                      | 1 | 0 | 2     | 0                       | 1 | 41 | 0     | 17                     | 52 | 18 | 0     | 0                      | 32 | 0 | 0     | 179            | 0                     | 1     | 0    | 3    |
| 5:00 PM             | 16                      | 0 | 0 | 2     | 0                       | 1 | 40 | 1     | 16                     | 51 | 14 | 0     | 0                      | 37 | 0 | 0     | 175            | 1                     | 1     | 1    | 3    |

# Heavy Vehicle Summary



Clay Carney  
(503) 833-2740



## Old River Rd & Cedar Oak Dr

Tuesday, July 31, 2018  
4:00 PM to 6:00 PM

### Heavy Vehicle 5-Minute Interval Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |   |       | Eastbound Cedar Oak Dr |   |   |       | Westbound Cedar Oak Dr |   |   |       | Interval Total |
|---------------------|-------------------------|---|---|-------|-------------------------|---|---|-------|------------------------|---|---|-------|------------------------|---|---|-------|----------------|
|                     | L                       | T | R | Total | L                       | T | R | Total | L                      | T | R | Total | L                      | T | R | Total |                |
| 4:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:05 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:10 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:15 PM             | 0                       | 1 | 0 | 1     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 4:20 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 4:25 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:35 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:40 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:50 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 4:55 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 5:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 5:05 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 5:10 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 5:15 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 5:20 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 5:25 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 5:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1                      | 0 | 1 | 1     | 1              |
| 5:35 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 5:40 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 1                      | 0 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 5:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 5:50 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 5:55 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| Total Survey        | 0                       | 1 | 0 | 1     | 0                       | 0 | 1 | 1     | 1                      | 2 | 1 | 4     | 0                      | 1 | 0 | 1     | 7              |

### Heavy Vehicle 15-Minute Interval Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |   |       | Eastbound Cedar Oak Dr |   |   |       | Westbound Cedar Oak Dr |   |   |       | Interval Total |
|---------------------|-------------------------|---|---|-------|-------------------------|---|---|-------|------------------------|---|---|-------|------------------------|---|---|-------|----------------|
|                     | L                       | T | R | Total | L                       | T | R | Total | L                      | T | R | Total | L                      | T | R | Total |                |
| 4:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:15 PM             | 0                       | 1 | 0 | 1     | 0                       | 0 | 0 | 0     | 0                      | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 2              |
| 4:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 4:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 1              |
| 5:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| 5:15 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0                      | 0 | 0 | 0     | 0              |
| 5:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 1                      | 0 | 0 | 1     | 0                      | 1 | 0 | 1     | 2              |
| 5:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 0                      | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 1              |
| Total Survey        | 0                       | 1 | 0 | 1     | 0                       | 0 | 1 | 1     | 1                      | 2 | 1 | 4     | 0                      | 1 | 0 | 1     | 7              |

### Heavy Vehicle Peak Hour Summary 4:45 PM to 5:45 PM

| By Approach | Northbound Old River Rd |     |       | Southbound Old River Rd |     |       | Eastbound Cedar Oak Dr |     |       | Westbound Cedar Oak Dr |     |       | Total |
|-------------|-------------------------|-----|-------|-------------------------|-----|-------|------------------------|-----|-------|------------------------|-----|-------|-------|
|             | In                      | Out | Total | In                      | Out | Total | In                     | Out | Total | In                     | Out | Total |       |
| Volume      | 0                       | 0   | 0     | 1                       | 1   | 2     | 2                      | 2   | 4     | 1                      | 1   | 2     | 4     |
| PHF         | 0.00                    |     |       | 0.25                    |     |       | 0.50                   |     |       | 0.25                   |     |       | 0.50  |

| By Movement | Northbound Old River Rd |      |      |       | Southbound Old River Rd |      |      |       | Eastbound Cedar Oak Dr |      |      |       | Westbound Cedar Oak Dr |      |      |       | Total |
|-------------|-------------------------|------|------|-------|-------------------------|------|------|-------|------------------------|------|------|-------|------------------------|------|------|-------|-------|
|             | L                       | T    | R    | Total | L                       | T    | R    | Total | L                      | T    | R    | Total | L                      | T    | R    | Total |       |
| Volume      | 0                       | 0    | 0    | 0     | 0                       | 0    | 1    | 1     | 1                      | 1    | 0    | 2     | 0                      | 1    | 0    | 1     | 4     |
| PHF         | 0.00                    | 0.00 | 0.00 | 0.00  | 0.00                    | 0.00 | 0.25 | 0.25  | 0.25                   | 0.25 | 0.00 | 0.50  | 0.00                   | 0.25 | 0.00 | 0.25  | 0.50  |

### Heavy Vehicle Rolling Hour Summary 4:00 PM to 6:00 PM

| Interval Start Time | Northbound Old River Rd |   |   |       | Southbound Old River Rd |   |   |       | Eastbound Cedar Oak Dr |   |   |       | Westbound Cedar Oak Dr |   |   |       | Interval Total |
|---------------------|-------------------------|---|---|-------|-------------------------|---|---|-------|------------------------|---|---|-------|------------------------|---|---|-------|----------------|
|                     | L                       | T | R | Total | L                       | T | R | Total | L                      | T | R | Total | L                      | T | R | Total |                |
| 4:00 PM             | 0                       | 1 | 0 | 1     | 0                       | 0 | 1 | 1     | 0                      | 0 | 1 | 1     | 0                      | 0 | 0 | 0     | 3              |
| 4:15 PM             | 0                       | 1 | 0 | 1     | 0                       | 0 | 1 | 1     | 0                      | 1 | 1 | 2     | 0                      | 0 | 0 | 0     | 4              |
| 4:30 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 1 | 1     | 0                      | 1 | 0 | 1     | 0                      | 0 | 0 | 0     | 2              |
| 4:45 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 1 | 1     | 1                      | 1 | 0 | 2     | 0                      | 1 | 0 | 1     | 4              |
| 5:00 PM             | 0                       | 0 | 0 | 0     | 0                       | 0 | 0 | 0     | 1                      | 2 | 0 | 3     | 0                      | 1 | 0 | 1     | 4              |

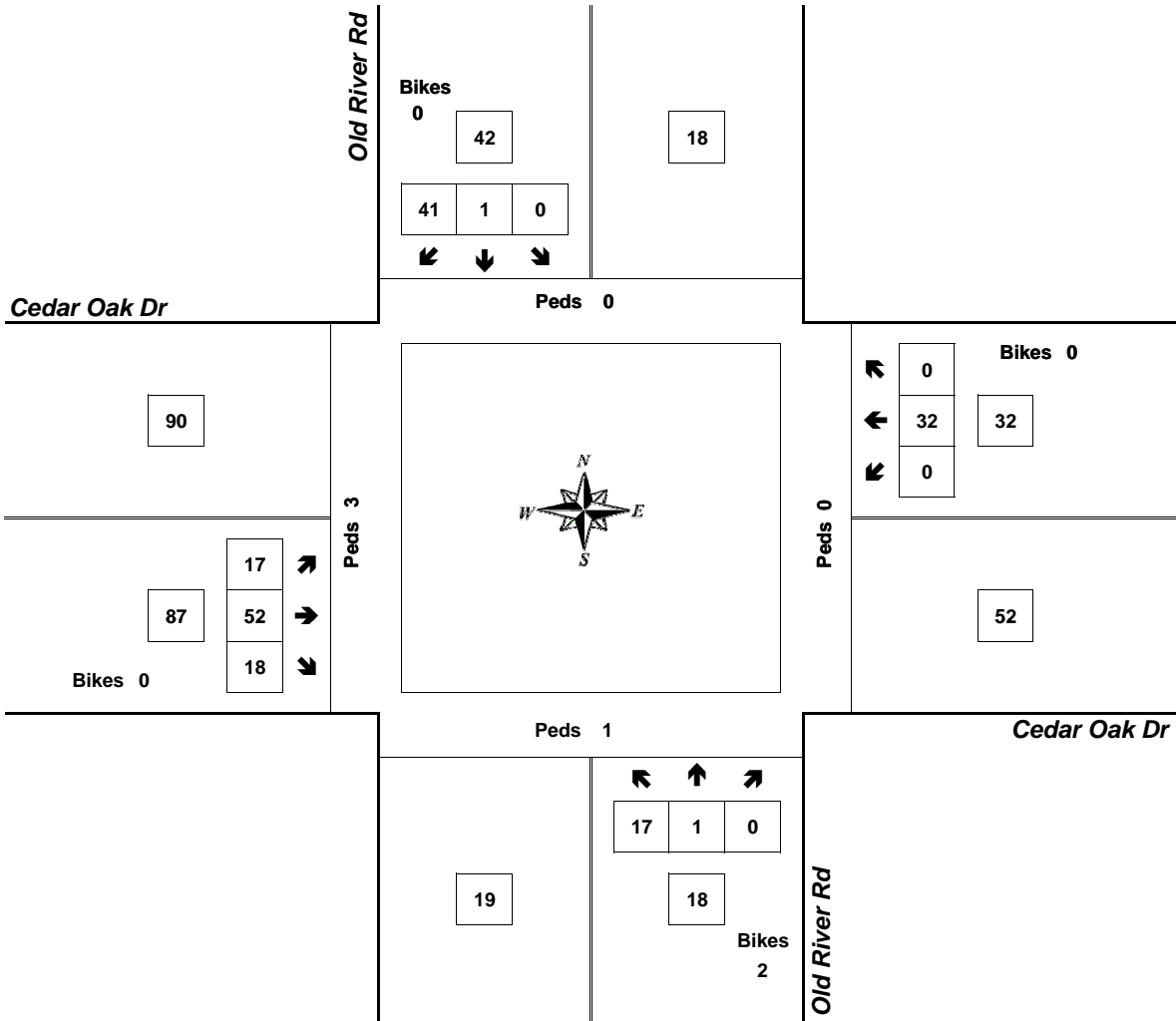
# Peak Hour Summary



Clay Carney  
(503) 833-2740

## Old River Rd & Cedar Oak Dr

4:45 PM to 5:45 PM  
Tuesday, July 31, 2018



| Approach            | PHF         | HV%         | Volume     |
|---------------------|-------------|-------------|------------|
| EB                  | 0.91        | 2.3%        | 87         |
| WB                  | 0.89        | 3.1%        | 32         |
| NB                  | 0.64        | 0.0%        | 18         |
| SB                  | 0.62        | 2.4%        | 42         |
| <b>Intersection</b> | <b>0.81</b> | <b>2.2%</b> | <b>179</b> |

Count Period: 4:00 PM to 6:00 PM

| SEASONAL TREND TABLE (Updated: 8/1/2018 ) |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        | Seasonal Trend Peak Period Factor | Seasonal Trend K30 Value |
|-------------------------------------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|-----------------------------------|--------------------------|
| TREND                                     | 1-Jan  | 15-Jan | 1-Feb  | 15-Feb | 1-Mar  | 15-Mar | 1-Apr  | 15-Apr | 1-May  | 15-May | 1-Jun  | 15-Jun | 1-Jul  | 15-Jul | 1-Aug  | 15-Aug | 1-Sep  | 15-Sep | 1-Oct  | 15-Oct | 1-Nov  | 15-Nov | 1-Dec  | 15-Dec |                                   |                          |
| INTERSTATE URBANIZED                      | 1.1818 | 1.1788 | 1.0976 | 1.0164 | 0.9998 | 0.9832 | 0.9657 | 0.9482 | 0.9460 | 0.9439 | 0.9240 | 0.9042 | 0.9115 | 0.9189 | 0.9374 | 0.9558 | 0.9558 | 0.9557 | 0.9535 | 0.9512 | 0.9625 | 0.9738 | 0.9924 | 1.0109 | 0.9042                            | 0.0817                   |
| INTERSTATE NONURBANIZED                   | 1.4606 | 1.6394 | 1.4676 | 1.2958 | 1.1933 | 1.0909 | 1.0645 | 1.0382 | 1.0025 | 0.9667 | 0.9201 | 0.8735 | 0.8557 | 0.8379 | 0.8295 | 0.8211 | 0.9545 | 1.0880 | 1.0500 | 1.0120 | 1.0458 | 1.0796 | 1.1313 | 1.1830 | 0.8211                            | 0.1213                   |
| COMMUTER                                  | 1.1573 | 1.1317 | 1.0654 | 0.9990 | 0.9841 | 0.9691 | 0.9491 | 0.9292 | 0.9207 | 0.9123 | 0.9016 | 0.8910 | 0.9014 | 0.9119 | 0.9020 | 0.8921 | 0.9074 | 0.9228 | 0.9193 | 0.9158 | 0.9372 | 0.9586 | 0.9845 | 1.0104 | 0.8910                            | 0.0974                   |
| COASTAL DESTINATION                       | 1.2740 | 1.3193 | 1.2641 | 1.2090 | 1.1609 | 1.1128 | 1.1031 | 1.0934 | 1.0569 | 1.0205 | 0.9791 | 0.9377 | 0.8842 | 0.8306 | 0.8299 | 0.8293 | 0.8775 | 0.9257 | 0.9810 | 1.0363 | 1.1041 | 1.1718 | 1.1809 | 1.1909 | 0.8293                            | 0.1192                   |
| COASTAL DESTINATION ROUTE                 | 1.5080 | 1.6791 | 1.5657 | 1.4522 | 1.3399 | 1.2876 | 1.2537 | 1.2400 | 1.1531 | 1.0662 | 1.0030 | 0.9399 | 0.8492 | 0.7584 | 0.7570 | 0.7556 | 0.8301 | 0.9045 | 1.0155 | 1.1265 | 1.2128 | 1.2962 | 1.3215 | 1.3438 | 0.7556                            | 0.1609                   |
| AGRICULTURE                               | 1.7076 | 1.8032 | 1.6535 | 1.5038 | 1.3802 | 1.2567 | 1.1986 | 1.1404 | 1.1072 | 1.0740 | 0.9827 | 0.8915 | 0.8529 | 0.8142 | 0.7179 | 0.6215 | 0.7163 | 0.8110 | 0.8614 | 0.9116 | 1.0105 | 1.1093 | 1.2415 | 1.3737 | 0.6215                            | 0.2229                   |
| RECREATIONAL SUMMER                       | 1.7585 | 2.2489 | 2.0847 | 1.9205 | 1.7358 | 1.5512 | 1.4576 | 1.3641 | 1.1766 | 0.9892 | 0.9061 | 0.8230 | 0.7650 | 0.7071 | 0.7124 | 0.7177 | 0.9130 | 1.1082 | 1.4413 | 1.7744 | 1.6928 | 1.6112 | 1.6401 | 1.6690 | 0.7071                            | 0.2037                   |
| RECREATIONAL SUMMER WINTER                | 1.2477 | 1.5073 | 1.5669 | 1.6264 | 1.6218 | 1.6172 | 1.7108 | 1.8044 | 1.5925 | 1.3807 | 1.2325 | 1.0844 | 0.9631 | 0.8419 | 0.8674 | 0.8929 | 0.9274 | 0.9619 | 1.3267 | 1.6914 | 1.9522 | 2.2130 | 1.6835 | 1.1541 | 0.8419                            | 0.2052                   |
| RECREATIONAL WINTER                       | 0.8268 | 1.0474 | 1.1721 | 1.2968 | 1.3685 | 1.4402 | 1.8693 | 2.2984 | 2.2161 | 2.1339 | 1.7818 | 1.4298 | 1.2481 | 1.0665 | 1.0903 | 1.1142 | 0.8813 | 0.6484 | 1.2488 | 1.8493 | 2.5945 | 3.3398 | 2.1613 | 0.9828 | 0.6484                            | 0.3092                   |
| SUMMER                                    | 1.3421 | 1.4546 | 1.3422 | 1.2298 | 1.1680 | 1.1061 | 1.0661 | 1.0261 | 0.9838 | 0.9415 | 0.9095 | 0.8774 | 0.8570 | 0.8366 | 0.8182 | 0.7997 | 0.8529 | 0.9060 | 0.9353 | 0.9645 | 1.0144 | 1.0643 | 1.1024 | 1.1406 | 0.7997                            | 0.1216                   |
| SUMMER < 2500                             | 1.3861 | 1.5332 | 1.4106 | 1.2881 | 1.1953 | 1.1025 | 1.0553 | 1.0080 | 0.9476 | 0.8871 | 0.8570 | 0.8268 | 0.8134 | 0.7999 | 0.7782 | 0.7565 | 0.8144 | 0.8723 | 0.8868 | 0.9013 | 0.9618 | 1.0223 | 1.0984 | 1.1745 | 0.7565                            | 0.1485                   |

\*Seasonal Trend Table factors are based on previous year ATR data. The table is updated yearly.

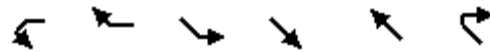
\*Grey shading indicates months where seasonal factor is greater than 30%

| HWY | MP    | DIR | HS | Location                                                                                            | 2014 | 2015  | 2016 | 2036  | RSQ   |
|-----|-------|-----|----|-----------------------------------------------------------------------------------------------------|------|-------|------|-------|-------|
| 003 | 0.02  | 1   |    | 0.02 mile south of US26                                                                             |      | 3000  |      | 4100  | MODEL |
| 003 | 0.22  | 1   |    | 0.02 mile west of S.W. Hood Avenue                                                                  |      | 4100  |      | 4800  | MODEL |
| 003 | 0.41  | 1   |    | 0.18 mile south of connection to Pacific Highway (I-5)                                              |      | 12000 |      | 15700 | MODEL |
| 003 | 0.43  | 2   | N  | 0.06 mile south of S.W. Curry Street                                                                |      | 27200 |      | 33100 | MODEL |
| 003 | 0.63  | 2   | N  | 0.01 mile south of S.W. Thomas Street                                                               |      | 14500 |      | 18600 | MODEL |
| 003 | 1.00  | 1   |    | 0.02 mile north of S.W. Julia Street                                                                |      | 22300 |      | 27300 | MODEL |
| 003 | 2.15  | 1   |    | 0.05 mile north of S.W. Taylors Ferry Road                                                          |      | 22400 |      | 25600 | MODEL |
| 003 | 2.54  | 1   |    | 0.05 mile north of Sellwood Ferry Road                                                              |      | 30000 |      | 32300 | MODEL |
| 003 | 3.64  | 1   |    | South city limits of Portland                                                                       |      | 17300 |      | 21400 | MODEL |
| 003 | 4.02  | 1   |    | 0.02 mile north of S.W. Riverwood Road                                                              |      | 17000 |      | 21000 | MODEL |
| 003 | 5.69  | 1   |    | 0.02 mile north of Terwilliger Boulevard                                                            |      | 16500 |      | 20000 | MODEL |
| 003 | 5.80  | 1   |    | 0.06 mile south of Terwilliger Boulevard                                                            |      | 21400 |      | 26000 | MODEL |
| 003 | 6.11  | 1   |    | 0.02 mile north of S. "A" Avenue                                                                    |      | 20200 |      | 24800 | MODEL |
| 003 | 6.17  | 1   |    | 0.04 mile south of S. "A" Avenue                                                                    |      | 34100 |      | 41500 | MODEL |
| 003 | 6.40  | 1   |    | 0.02 mile south of North Shore Road                                                                 |      | 28500 |      | 34700 | MODEL |
| 003 | 6.65  | 1   |    | 0.05 mile north of S. McVey Avenue                                                                  |      | 27600 |      | 33800 | MODEL |
| 003 | 6.77  | 1   |    | On Oswego Creek Bridge                                                                              |      | 17900 |      | 20500 | MODEL |
| 003 | 7.54  | 1   |    | 0.04 mile south of S. Glenmorrie Road                                                               |      | 16800 |      | 19800 | MODEL |
| 003 | 8.04  | 1   |    | South city limits of Lake Oswego, north city limits of West Linn, 0.03 mile north of S. Arbor Drive |      | 15900 |      | 18800 | MODEL |
| 003 | 9.52  | 1   |    | 0.02 mile north of Jolie Pointe Road                                                                |      | 17800 |      | 21500 | MODEL |
| 003 | 10.27 | 1   |    | 0.02 mile south of W. "A" Street                                                                    |      | 18500 |      | 22300 | MODEL |
| 003 | 11.07 | 1   |    | 0.10 mile north of East Portland Freeway (I-205)                                                    |      | 20900 |      | 25600 | MODEL |
| 003 | 11.34 | 1   |    | 0.01 mile north of S. Willamette Falls Drive                                                        |      | 13600 |      | 15600 | MODEL |
| 003 | 11.43 | 1   |    | On Willamette River Bridge, south city limits of West Linn and north city limits of Oregon City     |      | 13100 |      | 16600 | MODEL |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/07/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT   | NWR  |
|------------------------|-------|------|-------|-------|-------|------|
| Lane Configurations    |       |      |       |       |       |      |
| Traffic Volume (vph)   | 76    | 37   | 17    | 339   | 971   | 48   |
| Future Volume (vph)    | 76    | 37   | 17    | 339   | 971   | 48   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900  | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5   |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Frpb, ped/bikes        | 1.00  | 0.96 | 1.00  | 1.00  | 1.00  |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.99  |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00  |      |
| Satd. Flow (prot)      | 1752  | 1505 | 1736  | 1827  | 1830  |      |
| Flt Permitted          | 0.75  | 1.00 | 0.12  | 1.00  | 1.00  |      |
| Satd. Flow (perm)      | 1376  | 1505 | 218   | 1827  | 1830  |      |
| Peak-hour factor, PHF  | 0.92  | 0.92 | 0.92  | 0.92  | 0.92  | 0.92 |
| Adj. Flow (vph)        | 83    | 40   | 18    | 368   | 1055  | 52   |
| RTOR Reduction (vph)   | 0     | 36   | 0     | 0     | 1     | 0    |
| Lane Group Flow (vph)  | 83    | 4    | 18    | 368   | 1106  | 0    |
| Confl. Peds. (#/hr)    |       | 8    | 4     |       |       | 4    |
| Confl. Bikes (#/hr)    |       |      |       |       |       | 1    |
| Heavy Vehicles (%)     | 3%    | 3%   | 4%    | 4%    | 3%    | 3%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA    |      |
| Protected Phases       |       |      | 1     | 6     | 2     |      |
| Permitted Phases       | 8     | 8    | 6     |       |       |      |
| Actuated Green, G (s)  | 9.3   | 9.3  | 72.9  | 72.9  | 66.3  |      |
| Effective Green, g (s) | 9.3   | 9.3  | 72.9  | 72.9  | 66.3  |      |
| Actuated g/C Ratio     | 0.10  | 0.10 | 0.80  | 0.80  | 0.73  |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5   |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0   |      |
| Lane Grp Cap (vph)     | 140   | 153  | 209   | 1460  | 1330  |      |
| v/s Ratio Prot         |       |      | 0.00  | c0.20 | c0.60 |      |
| v/s Ratio Perm         | c0.06 | 0.00 | 0.07  |       |       |      |
| v/c Ratio              | 0.59  | 0.03 | 0.09  | 0.25  | 0.83  |      |
| Uniform Delay, d1      | 39.1  | 36.9 | 10.9  | 2.3   | 8.6   |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Incremental Delay, d2  | 6.6   | 0.1  | 0.2   | 0.4   | 6.2   |      |
| Delay (s)              | 45.7  | 36.9 | 11.0  | 2.7   | 14.8  |      |
| Level of Service       | D     | D    | B     | A     | B     |      |
| Approach Delay (s)     | 42.9  |      |       | 3.1   | 14.8  |      |
| Approach LOS           | D     |      |       | A     | B     |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 14.1  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.79  |                           |      |
| Actuated Cycle Length (s)         | 91.2  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 68.3% | ICU Level of Service      | C    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM 2010 TWSC  
2: Old River Road & Cedar Oak Drive

08/07/2018

| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 4.4  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 19   | 22   | 8    | 0    | 36   | 0    | 26   | 0    | 0    | 1    | 0    | 22   |
| Future Vol, veh/h        | 19   | 22   | 8    | 0    | 36   | 0    | 26   | 0    | 0    | 1    | 0    | 22   |
| Conflicting Peds, #/hr   | 1    | 0    | 2    | 2    | 0    | 1    | 1    | 0    | 1    | 1    | 0    | 1    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   |
| Heavy Vehicles, %        | 8    | 8    | 8    | 0    | 0    | 0    | 8    | 8    | 8    | 4    | 4    | 4    |
| Mvmt Flow                | 22   | 26   | 9    | 0    | 42   | 0    | 30   | 0    | 0    | 1    | 0    | 26   |

| Major/Minor          | Major1 |   |   | Major2 |   |   | Minor1 |       |       | Minor2 |       |       |
|----------------------|--------|---|---|--------|---|---|--------|-------|-------|--------|-------|-------|
| Conflicting Flow All | 43     | 0 | 0 | 37     | 0 | 0 | 132    | 119   | 33    | 118    | 124   | 44    |
| Stage 1              | -      | - | - | -      | - | - | 76     | 76    | -     | 43     | 43    | -     |
| Stage 2              | -      | - | - | -      | - | - | 56     | 43    | -     | 75     | 81    | -     |
| Critical Hdwy        | 4.18   | - | - | 4.1    | - | - | 7.18   | 6.58  | 6.28  | 7.14   | 6.54  | 6.24  |
| Critical Hdwy Stg 1  | -      | - | - | -      | - | - | 6.18   | 5.58  | -     | 6.14   | 5.54  | -     |
| Critical Hdwy Stg 2  | -      | - | - | -      | - | - | 6.18   | 5.58  | -     | 6.14   | 5.54  | -     |
| Follow-up Hdwy       | 2.272  | - | - | 2.2    | - | - | 3.572  | 4.072 | 3.372 | 3.536  | 4.036 | 3.336 |
| Pot Cap-1 Maneuver   | 1528   | - | - | 1587   | - | - | 826    | 760   | 1024  | 853    | 763   | 1020  |
| Stage 1              | -      | - | - | -      | - | - | 918    | 820   | -     | 966    | 855   | -     |
| Stage 2              | -      | - | - | -      | - | - | 941    | 847   | -     | 929    | 824   | -     |
| Platoon blocked, %   | -      | - | - | -      | - | - | -      | -     | -     | -      | -     | -     |
| Mov Cap-1 Maneuver   | 1527   | - | - | 1585   | - | - | 794    | 746   | 1021  | 842    | 749   | 1018  |
| Mov Cap-2 Maneuver   | -      | - | - | -      | - | - | 794    | 746   | -     | 842    | 749   | -     |
| Stage 1              | -      | - | - | -      | - | - | 903    | 806   | -     | 951    | 854   | -     |
| Stage 2              | -      | - | - | -      | - | - | 916    | 846   | -     | 914    | 810   | -     |

| Approach             | EB  |  |  | WB |  |  | NB  |  |  | SB  |  |  |
|----------------------|-----|--|--|----|--|--|-----|--|--|-----|--|--|
| HCM Control Delay, s | 2.9 |  |  | 0  |  |  | 9.7 |  |  | 8.7 |  |  |
| HCM LOS              |     |  |  |    |  |  | A   |  |  | A   |  |  |

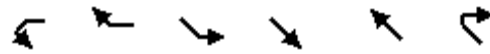
| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL  | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h)      | 794   | 1527  | -   | -   | 1585 | -   | -   | 1009  |
| HCM Lane V/C Ratio    | 0.038 | 0.014 | -   | -   | -    | -   | -   | 0.027 |
| HCM Control Delay (s) | 9.7   | 7.4   | 0   | -   | 0    | -   | -   | 8.7   |
| HCM Lane LOS          | A     | A     | A   | -   | A    | -   | -   | A     |
| HCM 95th %tile Q(veh) | 0.1   | 0     | -   | -   | 0    | -   | -   | 0.1   |



# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/07/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT  | NWR  |
|------------------------|-------|------|-------|-------|------|------|
| Lane Configurations    |       |      |       |       |      |      |
| Traffic Volume (vph)   | 110   | 31   | 21    | 1050  | 635  | 91   |
| Future Volume (vph)    | 110   | 31   | 21    | 1050  | 635  | 91   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900 | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frpb, ped/bikes        | 1.00  | 0.98 | 1.00  | 1.00  | 1.00 |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.98 |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00 |      |
| Satd. Flow (prot)      | 1752  | 1533 | 1736  | 1827  | 1808 |      |
| Flt Permitted          | 0.74  | 1.00 | 0.27  | 1.00  | 1.00 |      |
| Satd. Flow (perm)      | 1371  | 1533 | 486   | 1827  | 1808 |      |
| Peak-hour factor, PHF  | 0.94  | 0.94 | 0.94  | 0.94  | 0.94 | 0.94 |
| Adj. Flow (vph)        | 117   | 33   | 22    | 1117  | 676  | 97   |
| RTOR Reduction (vph)   | 0     | 29   | 0     | 0     | 4    | 0    |
| Lane Group Flow (vph)  | 117   | 4    | 22    | 1117  | 769  | 0    |
| Confl. Peds. (#/hr)    |       | 1    | 2     |       |      | 2    |
| Confl. Bikes (#/hr)    |       |      |       |       |      | 1    |
| Heavy Vehicles (%)     | 3%    | 3%   | 4%    | 4%    | 3%   | 3%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA   |      |
| Protected Phases       |       |      | 1     | 6     | 2    |      |
| Permitted Phases       | 8     | 8    | 6     |       |      |      |
| Actuated Green, G (s)  | 11.1  | 11.1 | 72.0  | 72.0  | 65.5 |      |
| Effective Green, g (s) | 11.1  | 11.1 | 72.0  | 72.0  | 65.5 |      |
| Actuated g/C Ratio     | 0.12  | 0.12 | 0.78  | 0.78  | 0.71 |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0  |      |
| Lane Grp Cap (vph)     | 165   | 184  | 407   | 1428  | 1285 |      |
| v/s Ratio Prot         |       |      | 0.00  | c0.61 | 0.43 |      |
| v/s Ratio Perm         | c0.09 | 0.00 | 0.04  |       |      |      |
| v/c Ratio              | 0.71  | 0.02 | 0.05  | 0.78  | 0.60 |      |
| Uniform Delay, d1      | 38.9  | 35.7 | 4.5   | 5.6   | 6.7  |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Incremental Delay, d2  | 13.1  | 0.0  | 0.1   | 4.3   | 2.1  |      |
| Delay (s)              | 52.0  | 35.8 | 4.5   | 10.0  | 8.8  |      |
| Level of Service       | D     | D    | A     | A     | A    |      |
| Approach Delay (s)     | 48.4  |      |       | 9.9   | 8.8  |      |
| Approach LOS           | D     |      |       | A     | A    |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 12.3  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.82  |                           |      |
| Actuated Cycle Length (s)         | 92.1  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 69.1% | ICU Level of Service      | C    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM 2010 TWSC  
2: Old River Road & Cedar Oak Drive

08/07/2018

**Intersection**

Int Delay, s/veh 3.3

| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 27   | 55   | 21   | 2    | 53   | 0    | 21   | 0    | 0    | 2    | 1    | 27   |
| Future Vol, veh/h        | 27   | 55   | 21   | 2    | 53   | 0    | 21   | 0    | 0    | 2    | 1    | 27   |
| Conflicting Peds, #/hr   | 0    | 0    | 1    | 1    | 0    | 0    | 1    | 0    | 0    | 0    | 0    | 1    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   |
| Heavy Vehicles, %        | 0    | 0    | 0    | 2    | 2    | 2    | 0    | 0    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 31   | 64   | 24   | 2    | 62   | 0    | 24   | 0    | 0    | 2    | 1    | 31   |

| Major/Minor          | Major1 | Major2 | Minor1 | Minor2 |
|----------------------|--------|--------|--------|--------|
| Conflicting Flow All | 62     | 0      | 0      | 89     |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |
| Critical Hdwy        | 4.1    | -      | -      | 4.12   |
| Critical Hdwy Stg 1  | -      | -      | -      | -      |
| Critical Hdwy Stg 2  | -      | -      | -      | -      |
| Follow-up Hdwy       | 2.2    | -      | -      | 2.218  |
| Pot Cap-1 Maneuver   | 1554   | -      | -      | 1506   |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |
| Platoon blocked, %   | -      | -      | -      | -      |
| Mov Cap-1 Maneuver   | 1553   | -      | -      | 1506   |
| Mov Cap-2 Maneuver   | -      | -      | -      | -      |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |

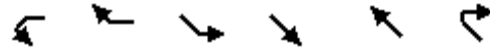
| Approach             | EB  | WB  | NB   | SB  |
|----------------------|-----|-----|------|-----|
| HCM Control Delay, s | 1.9 | 0.3 | 10.3 | 8.9 |
| HCM LOS              |     |     | B    | A   |

| Minor Lane/Major Mvmt | NBLn1 | EBL  | EBT | EBR | WBL   | WBT | WBR | SBLn1 |
|-----------------------|-------|------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h)      | 699   | 1553 | -   | -   | 1506  | -   | -   | 967   |
| HCM Lane V/C Ratio    | 0.035 | 0.02 | -   | -   | 0.002 | -   | -   | 0.036 |
| HCM Control Delay (s) | 10.3  | 7.4  | 0   | -   | 7.4   | 0   | -   | 8.9   |
| HCM Lane LOS          | B     | A    | A   | -   | A     | A   | -   | A     |
| HCM 95th %tile Q(veh) | 0.1   | 0.1  | -   | -   | 0     | -   | -   | 0.1   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/07/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT  | NWR  |
|------------------------|-------|------|-------|-------|------|------|
| Lane Configurations    |       |      |       |       |      |      |
| Traffic Volume (vph)   | 113   | 20   | 30    | 1091  | 643  | 65   |
| Future Volume (vph)    | 113   | 20   | 30    | 1091  | 643  | 65   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900 | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frpb, ped/bikes        | 1.00  | 0.96 | 1.00  | 1.00  | 1.00 |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.99 |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00 |      |
| Satd. Flow (prot)      | 1787  | 1535 | 1787  | 1881  | 1836 |      |
| Flt Permitted          | 0.74  | 1.00 | 0.29  | 1.00  | 1.00 |      |
| Satd. Flow (perm)      | 1386  | 1535 | 550   | 1881  | 1836 |      |
| Peak-hour factor, PHF  | 0.98  | 0.98 | 0.98  | 0.98  | 0.98 | 0.98 |
| Adj. Flow (vph)        | 115   | 20   | 31    | 1113  | 656  | 66   |
| RTOR Reduction (vph)   | 0     | 18   | 0     | 0     | 3    | 0    |
| Lane Group Flow (vph)  | 115   | 2    | 31    | 1113  | 719  | 0    |
| Confl. Peds. (#/hr)    |       | 8    |       |       |      |      |
| Confl. Bikes (#/hr)    |       |      |       |       |      | 2    |
| Heavy Vehicles (%)     | 1%    | 1%   | 1%    | 1%    | 2%   | 2%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA   |      |
| Protected Phases       |       |      | 1     | 6     | 2    |      |
| Permitted Phases       | 8     | 3    | 6     |       |      |      |
| Actuated Green, G (s)  | 11.0  | 11.0 | 72.5  | 72.5  | 66.0 |      |
| Effective Green, g (s) | 11.0  | 11.0 | 72.5  | 72.5  | 66.0 |      |
| Actuated g/C Ratio     | 0.12  | 0.12 | 0.78  | 0.78  | 0.71 |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0  |      |
| Lane Grp Cap (vph)     | 164   | 182  | 457   | 1474  | 1310 |      |
| v/s Ratio Prot         |       |      | 0.00  | c0.59 | 0.39 |      |
| v/s Ratio Perm         | c0.08 | 0.00 | 0.05  |       |      |      |
| v/c Ratio              | 0.70  | 0.01 | 0.07  | 0.76  | 0.55 |      |
| Uniform Delay, d1      | 39.2  | 36.0 | 4.0   | 5.3   | 6.2  |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Incremental Delay, d2  | 12.7  | 0.0  | 0.1   | 3.6   | 1.7  |      |
| Delay (s)              | 51.9  | 36.0 | 4.0   | 8.9   | 7.9  |      |
| Level of Service       | D     | D    | A     | A     | A    |      |
| Approach Delay (s)     | 49.5  |      |       | 8.8   | 7.9  |      |
| Approach LOS           | D     |      |       | A     | A    |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 11.2  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.79  |                           |      |
| Actuated Cycle Length (s)         | 92.5  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 73.2% | ICU Level of Service      | D    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM 2010 TWSC  
2: Old River Road & Cedar Oak Drive

08/07/2018

| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 3.7  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 17   | 52   | 18   | 0    | 32   | 0    | 17   | 1    | 0    | 0    | 1    | 41   |
| Future Vol, veh/h        | 17   | 52   | 18   | 0    | 32   | 0    | 17   | 1    | 0    | 0    | 1    | 41   |
| Conflicting Peds, #/hr   | 0    | 0    | 1    | 1    | 0    | 0    | 3    | 0    | 0    | 0    | 0    | 3    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 3    | 3    | 3    | 0    | 0    | 0    | 2    | 2    | 2    |
| Mvmt Flow                | 21   | 64   | 22   | 0    | 40   | 0    | 21   | 1    | 0    | 0    | 1    | 51   |

| Major/Minor          | Major1 |   |   | Major2 |   |   | Minor1 |     |     | Minor2 |       |       |
|----------------------|--------|---|---|--------|---|---|--------|-----|-----|--------|-------|-------|
| Conflicting Flow All | 40     | 0 | 0 | 87     | 0 | 0 | 186    | 158 | 76  | 158    | 169   | 43    |
| Stage 1              | -      | - | - | -      | - | - | 118    | 118 | -   | 40     | 40    | -     |
| Stage 2              | -      | - | - | -      | - | - | 68     | 40  | -   | 118    | 129   | -     |
| Critical Hdwy        | 4.12   | - | - | 4.13   | - | - | 7.1    | 6.5 | 6.2 | 7.12   | 6.52  | 6.22  |
| Critical Hdwy Stg 1  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.12   | 5.52  | -     |
| Critical Hdwy Stg 2  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.12   | 5.52  | -     |
| Follow-up Hdwy       | 2.218  | - | - | 2.227  | - | - | 3.5    | 4   | 3.3 | 3.518  | 4.018 | 3.318 |
| Pot Cap-1 Maneuver   | 1570   | - | - | 1503   | - | - | 779    | 738 | 991 | 808    | 724   | 1027  |
| Stage 1              | -      | - | - | -      | - | - | 891    | 802 | -   | 975    | 862   | -     |
| Stage 2              | -      | - | - | -      | - | - | 947    | 866 | -   | 887    | 789   | -     |
| Platoon blocked, %   | -      | - | - | -      | - | - | -      | -   | -   | -      | -     | -     |
| Mov Cap-1 Maneuver   | 1566   | - | - | 1503   | - | - | 729    | 727 | 990 | 798    | 713   | 1024  |
| Mov Cap-2 Maneuver   | -      | - | - | -      | - | - | 729    | 727 | -   | 798    | 713   | -     |
| Stage 1              | -      | - | - | -      | - | - | 878    | 790 | -   | 961    | 862   | -     |
| Stage 2              | -      | - | - | -      | - | - | 896    | 866 | -   | 873    | 777   | -     |

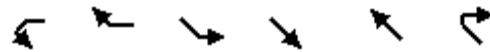
| Approach             | EB  | WB | NB   | SB  |
|----------------------|-----|----|------|-----|
| HCM Control Delay, s | 1.4 | 0  | 10.1 | 8.7 |
| HCM LOS              |     |    | B    | A   |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL  | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h)      | 729   | 1566  | -   | -   | 1503 | -   | -   | 1013  |
| HCM Lane V/C Ratio    | 0.03  | 0.013 | -   | -   | -    | -   | -   | 0.051 |
| HCM Control Delay (s) | 10.1  | 7.3   | 0   | -   | 0    | -   | -   | 8.7   |
| HCM Lane LOS          | B     | A     | A   | -   | A    | -   | -   | A     |
| HCM 95th %tile Q(veh) | 0.1   | 0     | -   | -   | 0    | -   | -   | 0.2   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/08/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT   | NWR  |
|------------------------|-------|------|-------|-------|-------|------|
| Lane Configurations    |       |      |       |       |       |      |
| Traffic Volume (vph)   | 111   | 50   | 31    | 346   | 992   | 90   |
| Future Volume (vph)    | 111   | 50   | 31    | 346   | 992   | 90   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900  | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5   |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Frpb, ped/bikes        | 1.00  | 0.96 | 1.00  | 1.00  | 1.00  |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.99  |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00  |      |
| Satd. Flow (prot)      | 1752  | 1506 | 1736  | 1827  | 1819  |      |
| Flt Permitted          | 0.73  | 1.00 | 0.06  | 1.00  | 1.00  |      |
| Satd. Flow (perm)      | 1356  | 1506 | 110   | 1827  | 1819  |      |
| Peak-hour factor, PHF  | 0.92  | 0.92 | 0.92  | 0.92  | 0.92  | 0.92 |
| Adj. Flow (vph)        | 121   | 54   | 34    | 376   | 1078  | 98   |
| RTOR Reduction (vph)   | 0     | 47   | 0     | 0     | 3     | 0    |
| Lane Group Flow (vph)  | 121   | 7    | 34    | 376   | 1173  | 0    |
| Confl. Peds. (#/hr)    |       | 8    | 4     |       |       | 4    |
| Confl. Bikes (#/hr)    |       |      |       |       |       | 1    |
| Heavy Vehicles (%)     | 3%    | 3%   | 4%    | 4%    | 3%    | 3%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA    |      |
| Protected Phases       |       |      | 1     | 6     | 2     |      |
| Permitted Phases       | 8     | 8    | 6     |       |       |      |
| Actuated Green, G (s)  | 11.3  | 11.3 | 69.5  | 69.5  | 61.9  |      |
| Effective Green, g (s) | 11.3  | 11.3 | 69.5  | 69.5  | 61.9  |      |
| Actuated g/C Ratio     | 0.13  | 0.13 | 0.77  | 0.77  | 0.69  |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5   |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0   |      |
| Lane Grp Cap (vph)     | 170   | 189  | 141   | 1413  | 1253  |      |
| v/s Ratio Prot         |       |      | 0.01  | c0.21 | c0.64 |      |
| v/s Ratio Perm         | c0.09 | 0.00 | 0.18  |       |       |      |
| v/c Ratio              | 0.71  | 0.04 | 0.24  | 0.27  | 0.94  |      |
| Uniform Delay, d1      | 37.7  | 34.5 | 18.8  | 2.9   | 12.2  |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Incremental Delay, d2  | 13.2  | 0.1  | 0.9   | 0.5   | 14.1  |      |
| Delay (s)              | 50.8  | 34.5 | 19.7  | 3.4   | 26.4  |      |
| Level of Service       | D     | C    | B     | A     | C     |      |
| Approach Delay (s)     | 45.8  |      |       | 4.7   | 26.4  |      |
| Approach LOS           | D     |      |       | A     | C     |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 23.3  | HCM 2000 Level of Service | C    |
| HCM 2000 Volume to Capacity ratio | 0.88  |                           |      |
| Actuated Cycle Length (s)         | 89.8  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 73.4% | ICU Level of Service      | D    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM 2010 TWSC  
2: Old River Road & Cedar Oak Drive

08/08/2018

**Intersection**

Int Delay, s/veh 6.2

| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 74   | 22   | 8    | 0    | 37   | 0    | 27   | 0    | 0    | 1    | 0    | 67   |
| Future Vol, veh/h        | 74   | 22   | 8    | 0    | 37   | 0    | 27   | 0    | 0    | 1    | 0    | 67   |
| Conflicting Peds, #/hr   | 1    | 0    | 2    | 2    | 0    | 1    | 1    | 0    | 1    | 1    | 0    | 1    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   |
| Heavy Vehicles, %        | 8    | 8    | 8    | 0    | 0    | 0    | 8    | 8    | 8    | 4    | 4    | 4    |
| Mvmt Flow                | 86   | 26   | 9    | 0    | 43   | 0    | 31   | 0    | 0    | 1    | 0    | 78   |

| Major/Minor          | Major1 | Major2 | Minor1 | Minor2 |
|----------------------|--------|--------|--------|--------|
| Conflicting Flow All | 44     | 0      | 0      | 37     |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |
| Critical Hdwy        | 4.18   | -      | -      | 4.1    |
| Critical Hdwy Stg 1  | -      | -      | -      | -      |
| Critical Hdwy Stg 2  | -      | -      | -      | -      |
| Follow-up Hdwy       | 2.272  | -      | -      | 2.2    |
| Pot Cap-1 Maneuver   | 1527   | -      | -      | 1587   |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |
| Platoon blocked, %   | -      | -      | -      | -      |
| Mov Cap-1 Maneuver   | 1526   | -      | -      | 1585   |
| Mov Cap-2 Maneuver   | -      | -      | -      | -      |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |

| Approach             | EB  | WB | NB   | SB  |
|----------------------|-----|----|------|-----|
| HCM Control Delay, s | 5.3 | 0  | 11.6 | 8.9 |
| HCM LOS              |     |    | B    | A   |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL  | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h)      | 575   | 1526  | -   | -   | 1585 | -   | -   | 1009  |
| HCM Lane V/C Ratio    | 0.055 | 0.056 | -   | -   | -    | -   | -   | 0.078 |
| HCM Control Delay (s) | 11.6  | 7.5   | 0   | -   | 0    | -   | -   | 8.9   |
| HCM Lane LOS          | B     | A     | A   | -   | A    | -   | -   | A     |
| HCM 95th %tile Q(veh) | 0.2   | 0.2   | -   | -   | 0    | -   | -   | 0.3   |

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 5.6  |      |      |      |      |      |
| Movement                 | EBL  | EBR  | NBL  | NBT  | SBT  | SBR  |
| Lane Configurations      | T    |      |      | T    |      | T    |
| Traffic Vol, veh/h       | 2    | 45   | 55   | 19   | 23   | 3    |
| Future Vol, veh/h        | 2    | 45   | 55   | 19   | 23   | 3    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | -    | 0    | 0    | -    |
| Grade, %                 | 0    | -    | -    | 0    | 0    | -    |
| Peak Hour Factor         | 92   | 92   | 92   | 92   | 92   | 92   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 2    | 2    | 2    |
| Mvmt Flow                | 2    | 49   | 60   | 21   | 25   | 3    |

| Major/Minor          | Minor2 | Major1 |       | Major2 |   |
|----------------------|--------|--------|-------|--------|---|
| Conflicting Flow All | 167    | 27     | 28    | 0      | 0 |
| Stage 1              | 27     | -      | -     | -      | - |
| Stage 2              | 140    | -      | -     | -      | - |
| Critical Hdwy        | 6.42   | 6.22   | 4.12  | -      | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -     | -      | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -     | -      | - |
| Follow-up Hdwy       | 3.518  | 3.318  | 2.218 | -      | - |
| Pot Cap-1 Maneuver   | 823    | 1048   | 1585  | -      | - |
| Stage 1              | 996    | -      | -     | -      | - |
| Stage 2              | 887    | -      | -     | -      | - |
| Platoon blocked, %   |        |        |       | -      | - |
| Mov Cap-1 Maneuver   | 792    | 1048   | 1585  | -      | - |
| Mov Cap-2 Maneuver   | 792    | -      | -     | -      | - |
| Stage 1              | 996    | -      | -     | -      | - |
| Stage 2              | 853    | -      | -     | -      | - |

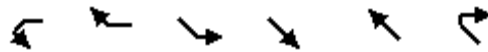
| Approach             | EB  | NB  | SB |
|----------------------|-----|-----|----|
| HCM Control Delay, s | 8.7 | 5.5 | 0  |
| HCM LOS              | A   |     |    |

| Minor Lane/Major Mvmt | NBL   | NBT | EBLn1 | SBT | SBR |
|-----------------------|-------|-----|-------|-----|-----|
| Capacity (veh/h)      | 1585  | -   | 1034  | -   | -   |
| HCM Lane V/C Ratio    | 0.038 | -   | 0.049 | -   | -   |
| HCM Control Delay (s) | 7.4   | 0   | 8.7   | -   | -   |
| HCM Lane LOS          | A     | A   | A     | -   | -   |
| HCM 95th %tile Q(veh) | 0.1   | -   | 0.2   | -   | -   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/08/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT  | NWR  |
|------------------------|-------|------|-------|-------|------|------|
| Lane Configurations    |       |      |       |       |      |      |
| Traffic Volume (vph)   | 138   | 42   | 29    | 1073  | 649  | 116  |
| Future Volume (vph)    | 138   | 42   | 29    | 1073  | 649  | 116  |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900 | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.98 |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00 |      |
| Satd. Flow (prot)      | 1752  | 1568 | 1736  | 1827  | 1800 |      |
| Flt Permitted          | 0.74  | 1.00 | 0.21  | 1.00  | 1.00 |      |
| Satd. Flow (perm)      | 1359  | 1568 | 383   | 1827  | 1800 |      |
| Peak-hour factor, PHF  | 0.94  | 0.94 | 0.94  | 0.94  | 0.94 | 0.94 |
| Adj. Flow (vph)        | 147   | 45   | 31    | 1141  | 690  | 123  |
| RTOR Reduction (vph)   | 0     | 43   | 0     | 0     | 6    | 0    |
| Lane Group Flow (vph)  | 147   | 2    | 31    | 1141  | 807  | 0    |
| Confl. Peds. (#/hr)    |       | 1    | 2     |       |      | 2    |
| Confl. Bikes (#/hr)    |       |      |       |       |      | 1    |
| Heavy Vehicles (%)     | 3%    | 3%   | 4%    | 4%    | 3%   | 3%   |
| Turn Type              | Perm  | Over | pm+pt | NA    | NA   |      |
| Protected Phases       |       | 1    | 1     | 6     | 2    |      |
| Permitted Phases       | 8     |      | 6     |       |      |      |
| Actuated Green, G (s)  | 14.1  | 4.1  | 66.8  | 66.8  | 58.2 |      |
| Effective Green, g (s) | 14.1  | 4.1  | 66.8  | 66.8  | 58.2 |      |
| Actuated g/C Ratio     | 0.16  | 0.05 | 0.74  | 0.74  | 0.65 |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0  |      |
| Lane Grp Cap (vph)     | 213   | 71   | 346   | 1357  | 1165 |      |
| v/s Ratio Prot         |       | 0.00 | 0.00  | c0.62 | 0.45 |      |
| v/s Ratio Perm         | c0.11 |      | 0.06  |       |      |      |
| v/c Ratio              | 0.69  | 0.03 | 0.09  | 0.84  | 0.69 |      |
| Uniform Delay, d1      | 35.8  | 41.0 | 7.0   | 7.9   | 10.1 |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Incremental Delay, d2  | 9.2   | 0.2  | 0.1   | 6.4   | 3.4  |      |
| Delay (s)              | 45.1  | 41.2 | 7.1   | 14.3  | 13.5 |      |
| Level of Service       | D     | D    | A     | B     | B    |      |
| Approach Delay (s)     | 44.2  |      |       | 14.1  | 13.5 |      |
| Approach LOS           | D     |      |       | B     | B    |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 16.6  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.86  |                           |      |
| Actuated Cycle Length (s)         | 89.9  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 71.9% | ICU Level of Service      | C    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |



HCM 2010 TWSC  
2: Old River Road & Cedar Oak Drive

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| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 4.6  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 59   | 56   | 21   | 2    | 54   | 0    | 21   | 0    | 0    | 2    | 1    | 64   |
| Future Vol, veh/h        | 59   | 56   | 21   | 2    | 54   | 0    | 21   | 0    | 0    | 2    | 1    | 64   |
| Conflicting Peds, #/hr   | 0    | 0    | 1    | 1    | 0    | 0    | 1    | 0    | 0    | 0    | 0    | 1    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   |
| Heavy Vehicles, %        | 0    | 0    | 0    | 2    | 2    | 2    | 0    | 0    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 69   | 65   | 24   | 2    | 63   | 0    | 24   | 0    | 0    | 2    | 1    | 74   |

| Major/Minor          | Major1 |   |   | Major2 |   |   | Minor1 |     |     | Minor2 |     |      |
|----------------------|--------|---|---|--------|---|---|--------|-----|-----|--------|-----|------|
| Conflicting Flow All | 63     | 0 | 0 | 91     | 0 | 0 | 322    | 283 | 78  | 282    | 295 | 64   |
| Stage 1              | -      | - | - | -      | - | - | 216    | 216 | -   | 67     | 67  | -    |
| Stage 2              | -      | - | - | -      | - | - | 106    | 67  | -   | 215    | 228 | -    |
| Critical Hdwy        | 4.1    | - | - | 4.12   | - | - | 7.1    | 6.5 | 6.2 | 7.1    | 6.5 | 6.2  |
| Critical Hdwy Stg 1  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.1    | 5.5 | -    |
| Critical Hdwy Stg 2  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.1    | 5.5 | -    |
| Follow-up Hdwy       | 2.2    | - | - | 2.218  | - | - | 3.5    | 4   | 3.3 | 3.5    | 4   | 3.3  |
| Pot Cap-1 Maneuver   | 1553   | - | - | 1504   | - | - | 635    | 629 | 988 | 674    | 620 | 1006 |
| Stage 1              | -      | - | - | -      | - | - | 791    | 728 | -   | 948    | 843 | -    |
| Stage 2              | -      | - | - | -      | - | - | 905    | 843 | -   | 792    | 719 | -    |
| Platoon blocked, %   | -      | - | - | -      | - | - | -      | -   | -   | -      | -   | -    |
| Mov Cap-1 Maneuver   | 1552   | - | - | 1504   | - | - | 565    | 598 | 987 | 649    | 590 | 1005 |
| Mov Cap-2 Maneuver   | -      | - | - | -      | - | - | 565    | 598 | -   | 649    | 590 | -    |
| Stage 1              | -      | - | - | -      | - | - | 753    | 693 | -   | 903    | 842 | -    |
| Stage 2              | -      | - | - | -      | - | - | 835    | 842 | -   | 755    | 685 | -    |

| Approach             | EB  |  |  | WB  |  |  | NB   |  |  | SB |  |  |
|----------------------|-----|--|--|-----|--|--|------|--|--|----|--|--|
| HCM Control Delay, s | 3.2 |  |  | 0.3 |  |  | 11.7 |  |  | 9  |  |  |
| HCM LOS              |     |  |  |     |  |  | B    |  |  | A  |  |  |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL   | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h)      | 565   | 1552  | -   | -   | 1504  | -   | -   | 979   |
| HCM Lane V/C Ratio    | 0.043 | 0.044 | -   | -   | 0.002 | -   | -   | 0.08  |
| HCM Control Delay (s) | 11.7  | 7.4   | 0   | -   | 7.4   | 0   | -   | 9     |
| HCM Lane LOS          | B     | A     | A   | -   | A     | A   | -   | A     |
| HCM 95th %tile Q(veh) | 0.1   | 0.1   | -   | -   | 0     | -   | -   | 0.3   |

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 4.3  |      |      |      |      |      |
| Movement                 | EBL  | EBR  | NBL  | NBT  | SBT  | SBR  |
| Lane Configurations      | T    |      |      | T    |      | T    |
| Traffic Vol, veh/h       | 2    | 36   | 31   | 28   | 31   | 2    |
| Future Vol, veh/h        | 2    | 36   | 31   | 28   | 31   | 2    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | -    | 0    | 0    | -    |
| Grade, %                 | 0    | -    | -    | 0    | 0    | -    |
| Peak Hour Factor         | 92   | 92   | 92   | 92   | 92   | 92   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 2    | 2    | 2    |
| Mvmt Flow                | 2    | 39   | 34   | 30   | 34   | 2    |

| Major/Minor          | Minor2 | Major1 |       | Major2 |   |
|----------------------|--------|--------|-------|--------|---|
| Conflicting Flow All | 133    | 35     | 36    | 0      | 0 |
| Stage 1              | 35     | -      | -     | -      | - |
| Stage 2              | 98     | -      | -     | -      | - |
| Critical Hdwy        | 6.42   | 6.22   | 4.12  | -      | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -     | -      | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -     | -      | - |
| Follow-up Hdwy       | 3.518  | 3.318  | 2.218 | -      | - |
| Pot Cap-1 Maneuver   | 861    | 1038   | 1575  | -      | - |
| Stage 1              | 987    | -      | -     | -      | - |
| Stage 2              | 926    | -      | -     | -      | - |
| Platoon blocked, %   |        |        |       | -      | - |
| Mov Cap-1 Maneuver   | 842    | 1038   | 1575  | -      | - |
| Mov Cap-2 Maneuver   | 842    | -      | -     | -      | - |
| Stage 1              | 987    | -      | -     | -      | - |
| Stage 2              | 906    | -      | -     | -      | - |

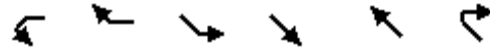
| Approach             | EB  | NB  | SB |
|----------------------|-----|-----|----|
| HCM Control Delay, s | 8.7 | 3.9 | 0  |
| HCM LOS              | A   |     |    |

| Minor Lane/Major Mvmt | NBL   | NBT | EBLn1 | SBT | SBR |
|-----------------------|-------|-----|-------|-----|-----|
| Capacity (veh/h)      | 1575  | -   | 1025  | -   | -   |
| HCM Lane V/C Ratio    | 0.021 | -   | 0.04  | -   | -   |
| HCM Control Delay (s) | 7.3   | 0   | 8.7   | -   | -   |
| HCM Lane LOS          | A     | A   | A     | -   | -   |
| HCM 95th %tile Q(veh) | 0.1   | -   | 0.1   | -   | -   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/08/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT  | NWR  |
|------------------------|-------|------|-------|-------|------|------|
| Lane Configurations    |       |      |       |       |      |      |
| Traffic Volume (vph)   | 126   | 24   | 34    | 1114  | 657  | 76   |
| Future Volume (vph)    | 126   | 24   | 34    | 1114  | 657  | 76   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900 | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frpb, ped/bikes        | 1.00  | 0.96 | 1.00  | 1.00  | 1.00 |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.99 |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00 |      |
| Satd. Flow (prot)      | 1787  | 1535 | 1787  | 1881  | 1832 |      |
| Flt Permitted          | 0.73  | 1.00 | 0.26  | 1.00  | 1.00 |      |
| Satd. Flow (perm)      | 1381  | 1535 | 489   | 1881  | 1832 |      |
| Peak-hour factor, PHF  | 0.98  | 0.98 | 0.98  | 0.98  | 0.98 | 0.98 |
| Adj. Flow (vph)        | 129   | 24   | 35    | 1137  | 670  | 78   |
| RTOR Reduction (vph)   | 0     | 21   | 0     | 0     | 4    | 0    |
| Lane Group Flow (vph)  | 129   | 3    | 35    | 1137  | 744  | 0    |
| Confl. Peds. (#/hr)    |       | 8    |       |       |      |      |
| Confl. Bikes (#/hr)    |       |      |       |       |      | 2    |
| Heavy Vehicles (%)     | 1%    | 1%   | 1%    | 1%    | 2%   | 2%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA   |      |
| Protected Phases       |       |      | 1     | 6     | 2    |      |
| Permitted Phases       | 8     | 3    | 6     |       |      |      |
| Actuated Green, G (s)  | 13.3  | 13.3 | 69.7  | 69.7  | 62.1 |      |
| Effective Green, g (s) | 13.3  | 13.3 | 69.7  | 69.7  | 62.1 |      |
| Actuated g/C Ratio     | 0.14  | 0.14 | 0.76  | 0.76  | 0.68 |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0  |      |
| Lane Grp Cap (vph)     | 199   | 221  | 414   | 1425  | 1236 |      |
| v/s Ratio Prot         |       |      | 0.00  | c0.60 | 0.41 |      |
| v/s Ratio Perm         | c0.09 | 0.00 | 0.06  |       |      |      |
| v/c Ratio              | 0.65  | 0.02 | 0.08  | 0.80  | 0.60 |      |
| Uniform Delay, d1      | 37.1  | 33.7 | 5.3   | 6.8   | 8.2  |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Incremental Delay, d2  | 7.1   | 0.0  | 0.1   | 4.7   | 2.2  |      |
| Delay (s)              | 44.2  | 33.8 | 5.4   | 11.6  | 10.4 |      |
| Level of Service       | D     | C    | A     | B     | B    |      |
| Approach Delay (s)     | 42.6  |      |       | 11.4  | 10.4 |      |
| Approach LOS           | D     |      |       | B     | B    |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 13.3  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.82  |                           |      |
| Actuated Cycle Length (s)         | 92.0  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 75.0% | ICU Level of Service      | D    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM 2010 TWSC  
 2: Old River Road & Cedar Oak Drive

08/08/2018

**Intersection**

Int Delay, s/veh 4.4

| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 30   | 53   | 18   | 0    | 33   | 0    | 17   | 1    | 0    | 0    | 1    | 57   |
| Future Vol, veh/h        | 30   | 53   | 18   | 0    | 33   | 0    | 17   | 1    | 0    | 0    | 1    | 57   |
| Conflicting Peds, #/hr   | 0    | 0    | 1    | 1    | 0    | 0    | 3    | 0    | 0    | 0    | 0    | 3    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 3    | 3    | 3    | 0    | 0    | 0    | 2    | 2    | 2    |
| Mvmt Flow                | 37   | 65   | 22   | 0    | 41   | 0    | 21   | 1    | 0    | 0    | 1    | 70   |

| Major/Minor          | Major1 | Major2 | Minor1 | Minor2 |
|----------------------|--------|--------|--------|--------|
| Conflicting Flow All | 41     | 0      | 0      | 89     |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |
| Critical Hdwy        | 4.12   | -      | -      | 4.13   |
| Critical Hdwy Stg 1  | -      | -      | -      | -      |
| Critical Hdwy Stg 2  | -      | -      | -      | -      |
| Follow-up Hdwy       | 2.218  | -      | -      | 2.227  |
| Pot Cap-1 Maneuver   | 1568   | -      | -      | 1500   |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |
| Platoon blocked, %   | -      | -      | -      | -      |
| Mov Cap-1 Maneuver   | 1564   | -      | -      | 1500   |
| Mov Cap-2 Maneuver   | -      | -      | -      | -      |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |

| Approach             | EB  | WB | NB   | SB  |
|----------------------|-----|----|------|-----|
| HCM Control Delay, s | 2.2 | 0  | 10.6 | 8.8 |
| HCM LOS              |     |    | B    | A   |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL  | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h)      | 662   | 1564  | -   | -   | 1500 | -   | -   | 1014  |
| HCM Lane V/C Ratio    | 0.034 | 0.024 | -   | -   | -    | -   | -   | 0.071 |
| HCM Control Delay (s) | 10.6  | 7.4   | 0   | -   | 0    | -   | -   | 8.8   |
| HCM Lane LOS          | B     | A     | A   | -   | A    | -   | -   | A     |
| HCM 95th %tile Q(veh) | 0.1   | 0.1   | -   | -   | 0    | -   | -   | 0.2   |

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 2.6  |      |      |      |      |      |
| Movement                 | EBL  | EBR  | NBL  | NBT  | SBT  | SBR  |
| Lane Configurations      | T    |      |      | T    |      | T    |
| Traffic Vol, veh/h       | 1    | 15   | 13   | 18   | 43   | 1    |
| Future Vol, veh/h        | 1    | 15   | 13   | 18   | 43   | 1    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | -    | 0    | 0    | -    |
| Grade, %                 | 0    | -    | -    | 0    | 0    | -    |
| Peak Hour Factor         | 92   | 92   | 92   | 92   | 92   | 92   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 2    | 2    | 2    |
| Mvmt Flow                | 1    | 16   | 14   | 20   | 47   | 1    |

| Major/Minor          | Minor2 | Major1 |       | Major2 |   |
|----------------------|--------|--------|-------|--------|---|
| Conflicting Flow All | 95     | 47     | 48    | 0      | 0 |
| Stage 1              | 47     | -      | -     | -      | - |
| Stage 2              | 48     | -      | -     | -      | - |
| Critical Hdwy        | 6.42   | 6.22   | 4.12  | -      | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -     | -      | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -     | -      | - |
| Follow-up Hdwy       | 3.518  | 3.318  | 2.218 | -      | - |
| Pot Cap-1 Maneuver   | 905    | 1022   | 1559  | -      | - |
| Stage 1              | 975    | -      | -     | -      | - |
| Stage 2              | 974    | -      | -     | -      | - |
| Platoon blocked, %   |        |        |       | -      | - |
| Mov Cap-1 Maneuver   | 897    | 1022   | 1559  | -      | - |
| Mov Cap-2 Maneuver   | 897    | -      | -     | -      | - |
| Stage 1              | 975    | -      | -     | -      | - |
| Stage 2              | 965    | -      | -     | -      | - |

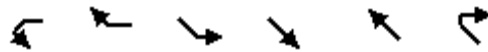
| Approach             | EB  | NB  | SB |
|----------------------|-----|-----|----|
| HCM Control Delay, s | 8.6 | 3.1 | 0  |
| HCM LOS              | A   |     |    |

| Minor Lane/Major Mvmt | NBL   | NBT | EBLn1 | SBT | SBR |
|-----------------------|-------|-----|-------|-----|-----|
| Capacity (veh/h)      | 1559  | -   | 1013  | -   | -   |
| HCM Lane V/C Ratio    | 0.009 | -   | 0.017 | -   | -   |
| HCM Control Delay (s) | 7.3   | 0   | 8.6   | -   | -   |
| HCM Lane LOS          | A     | A   | A     | -   | -   |
| HCM 95th %tile Q(veh) | 0     | -   | 0.1   | -   | -   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/13/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT   | NWR  |
|------------------------|-------|------|-------|-------|-------|------|
| Lane Configurations    |       |      |       |       |       |      |
| Traffic Volume (vph)   | 81    | 39   | 18    | 353   | 1010  | 51   |
| Future Volume (vph)    | 81    | 39   | 18    | 353   | 1010  | 51   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900  | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5   |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Frbp, ped/bikes        | 1.00  | 0.96 | 1.00  | 1.00  | 1.00  |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.99  |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00  |      |
| Satd. Flow (prot)      | 1752  | 1505 | 1736  | 1827  | 1830  |      |
| Flt Permitted          | 0.74  | 1.00 | 0.09  | 1.00  | 1.00  |      |
| Satd. Flow (perm)      | 1373  | 1505 | 173   | 1827  | 1830  |      |
| Peak-hour factor, PHF  | 0.92  | 0.92 | 0.92  | 0.92  | 0.92  | 0.92 |
| Adj. Flow (vph)        | 88    | 42   | 20    | 384   | 1098  | 55   |
| RTOR Reduction (vph)   | 0     | 38   | 0     | 0     | 1     | 0    |
| Lane Group Flow (vph)  | 88    | 4    | 20    | 384   | 1152  | 0    |
| Confl. Peds. (#/hr)    |       | 8    | 4     |       |       | 4    |
| Confl. Bikes (#/hr)    |       |      |       |       |       | 1    |
| Heavy Vehicles (%)     | 3%    | 3%   | 4%    | 4%    | 3%    | 3%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA    |      |
| Protected Phases       |       |      | 1     | 6     | 2     |      |
| Permitted Phases       | 8     | 8    | 6     |       |       |      |
| Actuated Green, G (s)  | 9.6   | 9.6  | 72.7  | 72.7  | 66.1  |      |
| Effective Green, g (s) | 9.6   | 9.6  | 72.7  | 72.7  | 66.1  |      |
| Actuated g/C Ratio     | 0.11  | 0.11 | 0.80  | 0.80  | 0.72  |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5   |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0   |      |
| Lane Grp Cap (vph)     | 144   | 158  | 173   | 1454  | 1324  |      |
| v/s Ratio Prot         |       |      | 0.00  | c0.21 | c0.63 |      |
| v/s Ratio Perm         | c0.06 | 0.00 | 0.09  |       |       |      |
| v/c Ratio              | 0.61  | 0.03 | 0.12  | 0.26  | 0.87  |      |
| Uniform Delay, d1      | 39.1  | 36.7 | 13.4  | 2.4   | 9.4   |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Incremental Delay, d2  | 7.5   | 0.1  | 0.3   | 0.4   | 8.0   |      |
| Delay (s)              | 46.5  | 36.7 | 13.7  | 2.8   | 17.4  |      |
| Level of Service       | D     | D    | B     | A     | B     |      |
| Approach Delay (s)     | 43.4  |      |       | 3.4   | 17.4  |      |
| Approach LOS           | D     |      |       | A     | B     |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 16.0  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.83  |                           |      |
| Actuated Cycle Length (s)         | 91.3  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 70.7% | ICU Level of Service      | C    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM 2010 TWSC  
2: Old River Road & Cedar Oak Drive

08/13/2018

| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 4.5  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 20   | 23   | 8    | 0    | 38   | 0    | 28   | 0    | 0    | 1    | 0    | 23   |
| Future Vol, veh/h        | 20   | 23   | 8    | 0    | 38   | 0    | 28   | 0    | 0    | 1    | 0    | 23   |
| Conflicting Peds, #/hr   | 1    | 0    | 2    | 2    | 0    | 1    | 1    | 0    | 1    | 1    | 0    | 1    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   |
| Heavy Vehicles, %        | 8    | 8    | 8    | 0    | 0    | 0    | 8    | 8    | 8    | 4    | 4    | 4    |
| Mvmt Flow                | 23   | 27   | 9    | 0    | 44   | 0    | 33   | 0    | 0    | 1    | 0    | 27   |

| Major/Minor          | Major1 |   |   | Major2 |   |   | Minor1 |       |       | Minor2 |       |       |
|----------------------|--------|---|---|--------|---|---|--------|-------|-------|--------|-------|-------|
| Conflicting Flow All | 45     | 0 | 0 | 38     | 0 | 0 | 139    | 125   | 34    | 124    | 130   | 46    |
| Stage 1              | -      | - | - | -      | - | - | 80     | 80    | -     | 45     | 45    | -     |
| Stage 2              | -      | - | - | -      | - | - | 59     | 45    | -     | 79     | 85    | -     |
| Critical Hdwy        | 4.18   | - | - | 4.1    | - | - | 7.18   | 6.58  | 6.28  | 7.14   | 6.54  | 6.24  |
| Critical Hdwy Stg 1  | -      | - | - | -      | - | - | 6.18   | 5.58  | -     | 6.14   | 5.54  | -     |
| Critical Hdwy Stg 2  | -      | - | - | -      | - | - | 6.18   | 5.58  | -     | 6.14   | 5.54  | -     |
| Follow-up Hdwy       | 2.272  | - | - | 2.2    | - | - | 3.572  | 4.072 | 3.372 | 3.536  | 4.036 | 3.336 |
| Pot Cap-1 Maneuver   | 1525   | - | - | 1585   | - | - | 818    | 754   | 1022  | 846    | 757   | 1018  |
| Stage 1              | -      | - | - | -      | - | - | 914    | 817   | -     | 964    | 853   | -     |
| Stage 2              | -      | - | - | -      | - | - | 938    | 846   | -     | 925    | 820   | -     |
| Platoon blocked, %   | -      | - | - | -      | - | - | -      | -     | -     | -      | -     | -     |
| Mov Cap-1 Maneuver   | 1524   | - | - | 1583   | - | - | 785    | 741   | 1019  | 835    | 744   | 1016  |
| Mov Cap-2 Maneuver   | -      | - | - | -      | - | - | 785    | 741   | -     | 835    | 744   | -     |
| Stage 1              | -      | - | - | -      | - | - | 899    | 803   | -     | 949    | 852   | -     |
| Stage 2              | -      | - | - | -      | - | - | 912    | 845   | -     | 910    | 806   | -     |

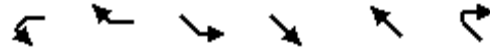
| Approach             | EB  |  |  | WB |  |  | NB  |  |  | SB  |  |  |
|----------------------|-----|--|--|----|--|--|-----|--|--|-----|--|--|
| HCM Control Delay, s | 2.9 |  |  | 0  |  |  | 9.8 |  |  | 8.7 |  |  |
| HCM LOS              |     |  |  |    |  |  | A   |  |  | A   |  |  |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL  | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h)      | 785   | 1524  | -   | -   | 1583 | -   | -   | 1007  |
| HCM Lane V/C Ratio    | 0.041 | 0.015 | -   | -   | -    | -   | -   | 0.028 |
| HCM Control Delay (s) | 9.8   | 7.4   | 0   | -   | 0    | -   | -   | 8.7   |
| HCM Lane LOS          | A     | A     | A   | -   | A    | -   | -   | A     |
| HCM 95th %tile Q(veh) | 0.1   | 0     | -   | -   | 0    | -   | -   | 0.1   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/13/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT  | NWR  |
|------------------------|-------|------|-------|-------|------|------|
| Lane Configurations    |       |      |       |       |      |      |
| Traffic Volume (vph)   | 117   | 33   | 22    | 1093  | 661  | 97   |
| Future Volume (vph)    | 117   | 33   | 22    | 1093  | 661  | 97   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900 | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frpb, ped/bikes        | 1.00  | 0.98 | 1.00  | 1.00  | 1.00 |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.98 |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00 |      |
| Satd. Flow (prot)      | 1752  | 1533 | 1736  | 1827  | 1807 |      |
| Flt Permitted          | 0.74  | 1.00 | 0.24  | 1.00  | 1.00 |      |
| Satd. Flow (perm)      | 1369  | 1533 | 439   | 1827  | 1807 |      |
| Peak-hour factor, PHF  | 0.94  | 0.94 | 0.94  | 0.94  | 0.94 | 0.94 |
| Adj. Flow (vph)        | 124   | 35   | 23    | 1163  | 703  | 103  |
| RTOR Reduction (vph)   | 0     | 30   | 0     | 0     | 4    | 0    |
| Lane Group Flow (vph)  | 124   | 5    | 23    | 1163  | 802  | 0    |
| Confl. Peds. (#/hr)    |       | 1    | 2     |       |      | 2    |
| Confl. Bikes (#/hr)    |       |      |       |       |      | 1    |
| Heavy Vehicles (%)     | 3%    | 3%   | 4%    | 4%    | 3%   | 3%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA   |      |
| Protected Phases       |       |      | 1     | 6     | 2    |      |
| Permitted Phases       | 8     | 8    | 6     |       |      |      |
| Actuated Green, G (s)  | 13.1  | 13.1 | 71.1  | 71.1  | 64.6 |      |
| Effective Green, g (s) | 13.1  | 13.1 | 71.1  | 71.1  | 64.6 |      |
| Actuated g/C Ratio     | 0.14  | 0.14 | 0.76  | 0.76  | 0.69 |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0  |      |
| Lane Grp Cap (vph)     | 192   | 215  | 362   | 1393  | 1252 |      |
| v/s Ratio Prot         |       |      | 0.00  | c0.64 | 0.44 |      |
| v/s Ratio Perm         | c0.09 | 0.00 | 0.05  |       |      |      |
| v/c Ratio              | 0.65  | 0.02 | 0.06  | 0.83  | 0.64 |      |
| Uniform Delay, d1      | 37.9  | 34.5 | 5.6   | 7.2   | 7.9  |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Incremental Delay, d2  | 7.3   | 0.0  | 0.1   | 6.0   | 2.5  |      |
| Delay (s)              | 45.1  | 34.6 | 5.7   | 13.3  | 10.4 |      |
| Level of Service       | D     | C    | A     | B     | B    |      |
| Approach Delay (s)     | 42.8  |      |       | 13.1  | 10.4 |      |
| Approach LOS           | D     |      |       | B     | B    |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 14.3  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.85  |                           |      |
| Actuated Cycle Length (s)         | 93.2  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 71.8% | ICU Level of Service      | C    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |



**Intersection**

Int Delay, s/veh 3.4

| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 29   | 58   | 22   | 2    | 56   | 0    | 22   | 0    | 0    | 2    | 1    | 29   |
| Future Vol, veh/h        | 29   | 58   | 22   | 2    | 56   | 0    | 22   | 0    | 0    | 2    | 1    | 29   |
| Conflicting Peds, #/hr   | 0    | 0    | 1    | 1    | 0    | 0    | 1    | 0    | 0    | 0    | 0    | 1    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   |
| Heavy Vehicles, %        | 0    | 0    | 0    | 2    | 2    | 2    | 0    | 0    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 34   | 67   | 26   | 2    | 65   | 0    | 26   | 0    | 0    | 2    | 1    | 34   |

| Major/Minor          | Major1 | Major2 | Minor1 | Minor2 |
|----------------------|--------|--------|--------|--------|
| Conflicting Flow All | 65     | 0      | 0      | 94     |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |
| Critical Hdwy        | 4.1    | -      | -      | 4.12   |
| Critical Hdwy Stg 1  | -      | -      | -      | -      |
| Critical Hdwy Stg 2  | -      | -      | -      | -      |
| Follow-up Hdwy       | 2.2    | -      | -      | 2.218  |
| Pot Cap-1 Maneuver   | 1550   | -      | -      | 1500   |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |
| Platoon blocked, %   | -      | -      | -      | -      |
| Mov Cap-1 Maneuver   | 1549   | -      | -      | 1500   |
| Mov Cap-2 Maneuver   | -      | -      | -      | -      |
| Stage 1              | -      | -      | -      | -      |
| Stage 2              | -      | -      | -      | -      |

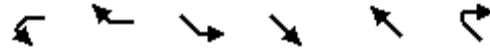
| Approach             | EB | WB  | NB   | SB  |
|----------------------|----|-----|------|-----|
| HCM Control Delay, s | 2  | 0.3 | 10.5 | 8.9 |
| HCM LOS              |    |     | B    | A   |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL   | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h)      | 683   | 1549  | -   | -   | 1500  | -   | -   | 963   |
| HCM Lane V/C Ratio    | 0.037 | 0.022 | -   | -   | 0.002 | -   | -   | 0.039 |
| HCM Control Delay (s) | 10.5  | 7.4   | 0   | -   | 7.4   | 0   | -   | 8.9   |
| HCM Lane LOS          | B     | A     | A   | -   | A     | A   | -   | A     |
| HCM 95th %tile Q(veh) | 0.1   | 0.1   | -   | -   | 0     | -   | -   | 0.1   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/13/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT  | NWR  |
|------------------------|-------|------|-------|-------|------|------|
| Lane Configurations    |       |      |       |       |      |      |
| Traffic Volume (vph)   | 120   | 21   | 32    | 1135  | 669  | 69   |
| Future Volume (vph)    | 120   | 21   | 32    | 1135  | 669  | 69   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900 | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frpb, ped/bikes        | 1.00  | 0.96 | 1.00  | 1.00  | 1.00 |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.99 |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00 |      |
| Satd. Flow (prot)      | 1787  | 1535 | 1787  | 1881  | 1836 |      |
| Flt Permitted          | 0.74  | 1.00 | 0.27  | 1.00  | 1.00 |      |
| Satd. Flow (perm)      | 1384  | 1535 | 503   | 1881  | 1836 |      |
| Peak-hour factor, PHF  | 0.98  | 0.98 | 0.98  | 0.98  | 0.98 | 0.98 |
| Adj. Flow (vph)        | 122   | 21   | 33    | 1158  | 683  | 70   |
| RTOR Reduction (vph)   | 0     | 18   | 0     | 0     | 3    | 0    |
| Lane Group Flow (vph)  | 122   | 3    | 33    | 1158  | 750  | 0    |
| Confl. Peds. (#/hr)    |       | 8    |       |       |      |      |
| Confl. Bikes (#/hr)    |       |      |       |       |      | 2    |
| Heavy Vehicles (%)     | 1%    | 1%   | 1%    | 1%    | 2%   | 2%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA   |      |
| Protected Phases       |       |      | 1     | 6     | 2    |      |
| Permitted Phases       | 8     | 3    | 6     |       |      |      |
| Actuated Green, G (s)  | 11.2  | 11.2 | 70.4  | 70.4  | 62.9 |      |
| Effective Green, g (s) | 11.2  | 11.2 | 70.4  | 70.4  | 62.9 |      |
| Actuated g/C Ratio     | 0.12  | 0.12 | 0.78  | 0.78  | 0.69 |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0  |      |
| Lane Grp Cap (vph)     | 171   | 189  | 433   | 1461  | 1274 |      |
| v/s Ratio Prot         |       |      | 0.00  | c0.62 | 0.41 |      |
| v/s Ratio Perm         | c0.09 | 0.00 | 0.06  |       |      |      |
| v/c Ratio              | 0.71  | 0.01 | 0.08  | 0.79  | 0.59 |      |
| Uniform Delay, d1      | 38.2  | 34.9 | 4.6   | 5.9   | 7.2  |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Incremental Delay, d2  | 13.2  | 0.0  | 0.1   | 4.5   | 2.0  |      |
| Delay (s)              | 51.3  | 34.9 | 4.7   | 10.4  | 9.2  |      |
| Level of Service       | D     | C    | A     | B     | A    |      |
| Approach Delay (s)     | 48.9  |      |       | 10.2  | 9.2  |      |
| Approach LOS           | D     |      |       | B     | A    |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 12.5  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.83  |                           |      |
| Actuated Cycle Length (s)         | 90.6  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 75.8% | ICU Level of Service      | D    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM 2010 TWSC  
 2: Old River Road & Cedar Oak Drive

08/13/2018

**Intersection**

Int Delay, s/veh 3.8

| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 17   | 53   | 18   | 0    | 33   | 0    | 17   | 1    | 0    | 0    | 1    | 42   |
| Future Vol, veh/h        | 17   | 53   | 18   | 0    | 33   | 0    | 17   | 1    | 0    | 0    | 1    | 42   |
| Conflicting Peds, #/hr   | 0    | 0    | 1    | 1    | 0    | 0    | 3    | 0    | 0    | 0    | 0    | 3    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 3    | 3    | 3    | 0    | 0    | 0    | 2    | 2    | 2    |
| Mvmt Flow                | 21   | 65   | 22   | 0    | 41   | 0    | 21   | 1    | 0    | 0    | 1    | 52   |

| Major/Minor          | Major1 |   |   | Major2 |   |   | Minor1 |     |     | Minor2 |       |       |
|----------------------|--------|---|---|--------|---|---|--------|-----|-----|--------|-------|-------|
| Conflicting Flow All | 41     | 0 | 0 | 89     | 0 | 0 | 190    | 161 | 78  | 160    | 172   | 44    |
| Stage 1              | -      | - | - | -      | - | - | 120    | 120 | -   | 41     | 41    | -     |
| Stage 2              | -      | - | - | -      | - | - | 70     | 41  | -   | 119    | 131   | -     |
| Critical Hdwy        | 4.12   | - | - | 4.13   | - | - | 7.1    | 6.5 | 6.2 | 7.12   | 6.52  | 6.22  |
| Critical Hdwy Stg 1  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.12   | 5.52  | -     |
| Critical Hdwy Stg 2  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.12   | 5.52  | -     |
| Follow-up Hdwy       | 2.218  | - | - | 2.227  | - | - | 3.5    | 4   | 3.3 | 3.518  | 4.018 | 3.318 |
| Pot Cap-1 Maneuver   | 1568   | - | - | 1500   | - | - | 774    | 735 | 988 | 806    | 721   | 1026  |
| Stage 1              | -      | - | - | -      | - | - | 889    | 800 | -   | 974    | 861   | -     |
| Stage 2              | -      | - | - | -      | - | - | 945    | 865 | -   | 885    | 788   | -     |
| Platoon blocked, %   | -      | - | - | -      | - | - | -      | -   | -   | -      | -     | -     |
| Mov Cap-1 Maneuver   | 1564   | - | - | 1500   | - | - | 723    | 724 | 987 | 796    | 710   | 1023  |
| Mov Cap-2 Maneuver   | -      | - | - | -      | - | - | 723    | 724 | -   | 796    | 710   | -     |
| Stage 1              | -      | - | - | -      | - | - | 876    | 788 | -   | 960    | 861   | -     |
| Stage 2              | -      | - | - | -      | - | - | 893    | 865 | -   | 871    | 776   | -     |

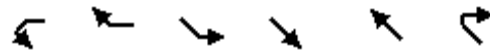
| Approach             | EB  | WB | NB   | SB  |
|----------------------|-----|----|------|-----|
| HCM Control Delay, s | 1.4 | 0  | 10.1 | 8.8 |
| HCM LOS              |     |    | B    | A   |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL  | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h)      | 723   | 1564  | -   | -   | 1500 | -   | -   | 1013  |
| HCM Lane V/C Ratio    | 0.031 | 0.013 | -   | -   | -    | -   | -   | 0.052 |
| HCM Control Delay (s) | 10.1  | 7.3   | 0   | -   | 0    | -   | -   | 8.8   |
| HCM Lane LOS          | B     | A     | A   | -   | A    | -   | -   | A     |
| HCM 95th %tile Q(veh) | 0.1   | 0     | -   | -   | 0    | -   | -   | 0.2   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

01/18/2019



| Movement               | WBL   | WBR  | SEL   | SET   | NWT   | NWR  |
|------------------------|-------|------|-------|-------|-------|------|
| Lane Configurations    |       |      |       |       |       |      |
| Traffic Volume (vph)   | 137   | 59   | 42    | 353   | 1010  | 119  |
| Future Volume (vph)    | 137   | 59   | 42    | 353   | 1010  | 119  |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900  | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5   |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Frpb, ped/bikes        | 1.00  | 0.96 | 1.00  | 1.00  | 1.00  |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.99  |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00  |      |
| Satd. Flow (prot)      | 1752  | 1505 | 1736  | 1827  | 1813  |      |
| Flt Permitted          | 0.73  | 1.00 | 0.06  | 1.00  | 1.00  |      |
| Satd. Flow (perm)      | 1341  | 1505 | 112   | 1827  | 1813  |      |
| Peak-hour factor, PHF  | 0.92  | 0.92 | 0.92  | 0.92  | 0.92  | 0.92 |
| Adj. Flow (vph)        | 149   | 64   | 46    | 384   | 1098  | 129  |
| RTOR Reduction (vph)   | 0     | 54   | 0     | 0     | 4     | 0    |
| Lane Group Flow (vph)  | 149   | 10   | 46    | 384   | 1223  | 0    |
| Confl. Peds. (#/hr)    |       | 8    | 4     |       |       | 4    |
| Confl. Bikes (#/hr)    |       |      |       |       |       | 1    |
| Heavy Vehicles (%)     | 3%    | 3%   | 4%    | 4%    | 3%    | 3%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA    |      |
| Protected Phases       |       |      | 1     | 6     | 2     |      |
| Permitted Phases       | 8     | 8    | 6     |       |       |      |
| Actuated Green, G (s)  | 14.4  | 14.4 | 68.3  | 68.3  | 60.7  |      |
| Effective Green, g (s) | 14.4  | 14.4 | 68.3  | 68.3  | 60.7  |      |
| Actuated g/C Ratio     | 0.16  | 0.16 | 0.74  | 0.74  | 0.66  |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5   |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0   |      |
| Lane Grp Cap (vph)     | 210   | 236  | 138   | 1360  | 1200  |      |
| v/s Ratio Prot         |       |      | 0.01  | c0.21 | c0.67 |      |
| v/s Ratio Perm         | c0.11 | 0.01 | 0.24  |       |       |      |
| v/c Ratio              | 0.71  | 0.04 | 0.33  | 0.28  | 1.02  |      |
| Uniform Delay, d1      | 36.7  | 32.8 | 24.0  | 3.8   | 15.5  |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00  |      |
| Incremental Delay, d2  | 10.5  | 0.1  | 1.4   | 0.5   | 30.9  |      |
| Delay (s)              | 47.1  | 32.9 | 25.4  | 4.3   | 46.4  |      |
| Level of Service       | D     | C    | C     | A     | D     |      |
| Approach Delay (s)     | 42.8  |      |       | 6.6   | 46.4  |      |
| Approach LOS           | D     |      |       | A     | D     |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 36.8  | HCM 2000 Level of Service | D    |
| HCM 2000 Volume to Capacity ratio | 0.94  |                           |      |
| Actuated Cycle Length (s)         | 91.7  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 77.2% | ICU Level of Service      | D    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM 2010 TWSC  
2: Old River Road & Cedar Oak Drive

01/18/2019

| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 6.9  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 112  | 23   | 8    | 0    | 38   | 0    | 28   | 0    | 0    | 1    | 0    | 99   |
| Future Vol, veh/h        | 112  | 23   | 8    | 0    | 38   | 0    | 28   | 0    | 0    | 1    | 0    | 99   |
| Conflicting Peds, #/hr   | 1    | 0    | 2    | 2    | 0    | 1    | 1    | 0    | 1    | 1    | 0    | 1    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   |
| Heavy Vehicles, %        | 8    | 8    | 8    | 0    | 0    | 0    | 8    | 8    | 8    | 4    | 4    | 4    |
| Mvmt Flow                | 130  | 27   | 9    | 0    | 44   | 0    | 33   | 0    | 0    | 1    | 0    | 115  |

| Major/Minor          | Major1 |   |   | Major2 |   |   | Minor1 |       |       | Minor2 |       |       |
|----------------------|--------|---|---|--------|---|---|--------|-------|-------|--------|-------|-------|
| Conflicting Flow All | 45     | 0 | 0 | 38     | 0 | 0 | 397    | 339   | 34    | 338    | 344   | 46    |
| Stage 1              | -      | - | - | -      | - | - | 294    | 294   | -     | 45     | 45    | -     |
| Stage 2              | -      | - | - | -      | - | - | 103    | 45    | -     | 293    | 299   | -     |
| Critical Hdwy        | 4.18   | - | - | 4.1    | - | - | 7.18   | 6.58  | 6.28  | 7.14   | 6.54  | 6.24  |
| Critical Hdwy Stg 1  | -      | - | - | -      | - | - | 6.18   | 5.58  | -     | 6.14   | 5.54  | -     |
| Critical Hdwy Stg 2  | -      | - | - | -      | - | - | 6.18   | 5.58  | -     | 6.14   | 5.54  | -     |
| Follow-up Hdwy       | 2.272  | - | - | 2.2    | - | - | 3.572  | 4.072 | 3.372 | 3.536  | 4.036 | 3.336 |
| Pot Cap-1 Maneuver   | 1525   | - | - | 1585   | - | - | 552    | 573   | 1022  | 612    | 575   | 1018  |
| Stage 1              | -      | - | - | -      | - | - | 701    | 659   | -     | 964    | 853   | -     |
| Stage 2              | -      | - | - | -      | - | - | 888    | 846   | -     | 711    | 663   | -     |
| Platoon blocked, %   | -      | - | - | -      | - | - | -      | -     | -     | -      | -     | -     |
| Mov Cap-1 Maneuver   | 1524   | - | - | 1583   | - | - | 456    | 522   | 1019  | 570    | 523   | 1016  |
| Mov Cap-2 Maneuver   | -      | - | - | -      | - | - | 456    | 522   | -     | 570    | 523   | -     |
| Stage 1              | -      | - | - | -      | - | - | 639    | 601   | -     | 879    | 852   | -     |
| Stage 2              | -      | - | - | -      | - | - | 787    | 845   | -     | 649    | 604   | -     |

| Approach             | EB  |  |  | WB |  |  | NB   |  |  | SB |  |  |
|----------------------|-----|--|--|----|--|--|------|--|--|----|--|--|
| HCM Control Delay, s | 5.9 |  |  | 0  |  |  | 13.5 |  |  | 9  |  |  |
| HCM LOS              |     |  |  |    |  |  | B    |  |  | A  |  |  |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL  | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h)      | 456   | 1524  | -   | -   | 1583 | -   | -   | 1008  |
| HCM Lane V/C Ratio    | 0.071 | 0.085 | -   | -   | -    | -   | -   | 0.115 |
| HCM Control Delay (s) | 13.5  | 7.6   | 0   | -   | 0    | -   | -   | 9     |
| HCM Lane LOS          | B     | A     | A   | -   | A    | -   | -   | A     |
| HCM 95th %tile Q(veh) | 0.2   | 0.3   | -   | -   | 0    | -   | -   | 0.4   |

HCM 2010 TWSC  
3: Old River Road & Southern Site Access

01/18/2019

**Intersection**

Int Delay, s/veh 3.4

**Movement** EBL EBR NBL NBT SBT SBR

|                          |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Lane Configurations      |      |      |      |      |      |      |
| Traffic Vol, veh/h       | 4    | 76   | 5    | 107  | 24   | 0    |
| Future Vol, veh/h        | 4    | 76   | 5    | 107  | 24   | 0    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | -    | 0    | 0    | -    |
| Grade, %                 | 0    | -    | -    | 0    | 0    | -    |
| Peak Hour Factor         | 92   | 92   | 92   | 92   | 92   | 92   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 2    | 2    | 2    |
| Mvmt Flow                | 4    | 83   | 5    | 116  | 26   | 0    |

**Major/Minor** Minor2 Major1 Major2

|                      |       |       |       |   |   |   |
|----------------------|-------|-------|-------|---|---|---|
| Conflicting Flow All | 153   | 26    | 26    | 0 | - | 0 |
| Stage 1              | 26    | -     | -     | - | - | - |
| Stage 2              | 127   | -     | -     | - | - | - |
| Critical Hdwy        | 6.42  | 6.22  | 4.12  | - | - | - |
| Critical Hdwy Stg 1  | 5.42  | -     | -     | - | - | - |
| Critical Hdwy Stg 2  | 5.42  | -     | -     | - | - | - |
| Follow-up Hdwy       | 3.518 | 3.318 | 2.218 | - | - | - |
| Pot Cap-1 Maneuver   | 839   | 1050  | 1588  | - | - | - |
| Stage 1              | 997   | -     | -     | - | - | - |
| Stage 2              | 899   | -     | -     | - | - | - |
| Platoon blocked, %   |       |       |       | - | - | - |
| Mov Cap-1 Maneuver   | 836   | 1050  | 1588  | - | - | - |
| Mov Cap-2 Maneuver   | 836   | -     | -     | - | - | - |
| Stage 1              | 997   | -     | -     | - | - | - |
| Stage 2              | 896   | -     | -     | - | - | - |

**Approach** EB NB SB

|                      |     |     |   |
|----------------------|-----|-----|---|
| HCM Control Delay, s | 8.8 | 0.3 | 0 |
| HCM LOS              | A   |     |   |

**Minor Lane/Major Mvmt** NBL NBT EBLn1 SBT SBR

|                       |       |   |       |   |   |
|-----------------------|-------|---|-------|---|---|
| Capacity (veh/h)      | 1588  | - | 1037  | - | - |
| HCM Lane V/C Ratio    | 0.003 | - | 0.084 | - | - |
| HCM Control Delay (s) | 7.3   | 0 | 8.8   | - | - |
| HCM Lane LOS          | A     | A | A     | - | - |
| HCM 95th %tile Q(veh) | 0     | - | 0.3   | - | - |

HCM 2010 TWSC  
 4: Old River Road & Northern Site Access

01/18/2019

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 4.4  |      |      |      |      |      |
| Movement                 | EBL  | EBR  | NBL  | NBT  | SBT  | SBR  |
| Lane Configurations      | T    |      |      | T    |      | T    |
| Traffic Vol, veh/h       | 0    | 0    | 77   | 24   | 24   | 5    |
| Future Vol, veh/h        | 0    | 0    | 77   | 24   | 24   | 5    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | -    | 0    | 0    | -    |
| Grade, %                 | 0    | -    | -    | 0    | 0    | -    |
| Peak Hour Factor         | 92   | 92   | 92   | 92   | 92   | 92   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 2    | 2    | 2    |
| Mvmt Flow                | 0    | 0    | 84   | 26   | 26   | 5    |

| Major/Minor          | Minor2 | Major1 |       | Major2 |   |
|----------------------|--------|--------|-------|--------|---|
| Conflicting Flow All | 222    | 29     | 32    | 0      | 0 |
| Stage 1              | 29     | -      | -     | -      | - |
| Stage 2              | 193    | -      | -     | -      | - |
| Critical Hdwy        | 6.42   | 6.22   | 4.12  | -      | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -     | -      | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -     | -      | - |
| Follow-up Hdwy       | 3.518  | 3.318  | 2.218 | -      | - |
| Pot Cap-1 Maneuver   | 766    | 1046   | 1580  | -      | - |
| Stage 1              | 994    | -      | -     | -      | - |
| Stage 2              | 840    | -      | -     | -      | - |
| Platoon blocked, %   |        |        |       | -      | - |
| Mov Cap-1 Maneuver   | 725    | 1046   | 1580  | -      | - |
| Mov Cap-2 Maneuver   | 725    | -      | -     | -      | - |
| Stage 1              | 994    | -      | -     | -      | - |
| Stage 2              | 795    | -      | -     | -      | - |

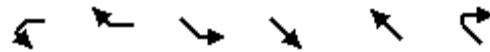
| Approach             | EB | NB  | SB |
|----------------------|----|-----|----|
| HCM Control Delay, s | 0  | 5.6 | 0  |
| HCM LOS              | A  |     |    |

| Minor Lane/Major Mvmt | NBL   | NBT | EBLn1 | SBT | SBR |
|-----------------------|-------|-----|-------|-----|-----|
| Capacity (veh/h)      | 1580  | -   | -     | -   | -   |
| HCM Lane V/C Ratio    | 0.053 | -   | -     | -   | -   |
| HCM Control Delay (s) | 7.4   | 0   | 0     | -   | -   |
| HCM Lane LOS          | A     | A   | A     | -   | -   |
| HCM 95th %tile Q(veh) | 0.2   | -   | -     | -   | -   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/13/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT  | NWR  |
|------------------------|-------|------|-------|-------|------|------|
| Lane Configurations    |       |      |       |       |      |      |
| Traffic Volume (vph)   | 162   | 49   | 36    | 1093  | 661  | 136  |
| Future Volume (vph)    | 162   | 49   | 36    | 1093  | 661  | 136  |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900 | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.98 |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00 |      |
| Satd. Flow (prot)      | 1752  | 1568 | 1736  | 1827  | 1794 |      |
| Flt Permitted          | 0.73  | 1.00 | 0.18  | 1.00  | 1.00 |      |
| Satd. Flow (perm)      | 1351  | 1568 | 338   | 1827  | 1794 |      |
| Peak-hour factor, PHF  | 0.94  | 0.94 | 0.94  | 0.94  | 0.94 | 0.94 |
| Adj. Flow (vph)        | 172   | 52   | 38    | 1163  | 703  | 145  |
| RTOR Reduction (vph)   | 0     | 50   | 0     | 0     | 7    | 0    |
| Lane Group Flow (vph)  | 172   | 2    | 38    | 1163  | 841  | 0    |
| Confl. Peds. (#/hr)    |       | 1    | 2     |       |      | 2    |
| Confl. Bikes (#/hr)    |       |      |       |       |      | 1    |
| Heavy Vehicles (%)     | 3%    | 3%   | 4%    | 4%    | 3%   | 3%   |
| Turn Type              | Perm  | Over | pm+pt | NA    | NA   |      |
| Protected Phases       |       | 1    | 1     | 6     | 2    |      |
| Permitted Phases       | 8     |      | 6     |       |      |      |
| Actuated Green, G (s)  | 15.3  | 4.1  | 66.7  | 66.7  | 58.1 |      |
| Effective Green, g (s) | 15.3  | 4.1  | 66.7  | 66.7  | 58.1 |      |
| Actuated g/C Ratio     | 0.17  | 0.05 | 0.73  | 0.73  | 0.64 |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0  |      |
| Lane Grp Cap (vph)     | 227   | 70   | 310   | 1339  | 1145 |      |
| v/s Ratio Prot         |       | 0.00 | 0.01  | c0.64 | 0.47 |      |
| v/s Ratio Perm         | c0.13 |      | 0.08  |       |      |      |
| v/c Ratio              | 0.76  | 0.03 | 0.12  | 0.87  | 0.73 |      |
| Uniform Delay, d1      | 36.1  | 41.6 | 8.3   | 8.9   | 11.2 |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Incremental Delay, d2  | 13.5  | 0.2  | 0.2   | 7.8   | 4.2  |      |
| Delay (s)              | 49.5  | 41.8 | 8.5   | 16.8  | 15.4 |      |
| Level of Service       | D     | D    | A     | B     | B    |      |
| Approach Delay (s)     | 47.7  |      |       | 16.5  | 15.4 |      |
| Approach LOS           | D     |      |       | B     | B    |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 19.2  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.90  |                           |      |
| Actuated Cycle Length (s)         | 91.0  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 74.2% | ICU Level of Service      | D    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |



HCM 2010 TWSC  
 2: Old River Road & Cedar Oak Drive

08/13/2018

| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 5.3  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 82   | 58   | 22   | 2    | 56   | 0    | 22   | 0    | 0    | 2    | 1    | 90   |
| Future Vol, veh/h        | 82   | 58   | 22   | 2    | 56   | 0    | 22   | 0    | 0    | 2    | 1    | 90   |
| Conflicting Peds, #/hr   | 0    | 0    | 1    | 1    | 0    | 0    | 1    | 0    | 0    | 0    | 0    | 1    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   | 86   |
| Heavy Vehicles, %        | 0    | 0    | 0    | 2    | 2    | 2    | 0    | 0    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 95   | 67   | 26   | 2    | 65   | 0    | 26   | 0    | 0    | 2    | 1    | 105  |

| Major/Minor          | Major1 |   |   | Major2 |   |   | Minor1 |     |     | Minor2 |     |      |
|----------------------|--------|---|---|--------|---|---|--------|-----|-----|--------|-----|------|
| Conflicting Flow All | 65     | 0 | 0 | 94     | 0 | 0 | 396    | 342 | 81  | 341    | 355 | 66   |
| Stage 1              | -      | - | - | -      | - | - | 272    | 272 | -   | 70     | 70  | -    |
| Stage 2              | -      | - | - | -      | - | - | 124    | 70  | -   | 271    | 285 | -    |
| Critical Hdwy        | 4.1    | - | - | 4.12   | - | - | 7.1    | 6.5 | 6.2 | 7.1    | 6.5 | 6.2  |
| Critical Hdwy Stg 1  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.1    | 5.5 | -    |
| Critical Hdwy Stg 2  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.1    | 5.5 | -    |
| Follow-up Hdwy       | 2.2    | - | - | 2.218  | - | - | 3.5    | 4   | 3.3 | 3.5    | 4   | 3.3  |
| Pot Cap-1 Maneuver   | 1550   | - | - | 1500   | - | - | 568    | 583 | 985 | 617    | 574 | 1003 |
| Stage 1              | -      | - | - | -      | - | - | 738    | 688 | -   | 945    | 841 | -    |
| Stage 2              | -      | - | - | -      | - | - | 885    | 841 | -   | 739    | 679 | -    |
| Platoon blocked, %   | -      | - | - | -      | - | - | -      | -   | -   | -      | -   | -    |
| Mov Cap-1 Maneuver   | 1549   | - | - | 1500   | - | - | 481    | 544 | 984 | 586    | 536 | 1002 |
| Mov Cap-2 Maneuver   | -      | - | - | -      | - | - | 481    | 544 | -   | 586    | 536 | -    |
| Stage 1              | -      | - | - | -      | - | - | 689    | 643 | -   | 884    | 840 | -    |
| Stage 2              | -      | - | - | -      | - | - | 790    | 840 | -   | 691    | 634 | -    |

| Approach             | EB  |  |  | WB  |  |  | NB   |  |  | SB  |  |  |
|----------------------|-----|--|--|-----|--|--|------|--|--|-----|--|--|
| HCM Control Delay, s | 3.8 |  |  | 0.3 |  |  | 12.9 |  |  | 9.1 |  |  |
| HCM LOS              |     |  |  |     |  |  | B    |  |  | A   |  |  |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL   | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h)      | 481   | 1549  | -   | -   | 1500  | -   | -   | 978   |
| HCM Lane V/C Ratio    | 0.053 | 0.062 | -   | -   | 0.002 | -   | -   | 0.111 |
| HCM Control Delay (s) | 12.9  | 7.5   | 0   | -   | 7.4   | 0   | -   | 9.1   |
| HCM Lane LOS          | B     | A     | A   | -   | A     | A   | -   | A     |
| HCM 95th %tile Q(veh) | 0.2   | 0.2   | -   | -   | 0     | -   | -   | 0.4   |

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| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 3.3  |      |      |      |      |      |
| Movement                 | EBL  | EBR  | NBL  | NBT  | SBT  | SBR  |
| Lane Configurations      | W    |      |      | W    | W    |      |
| Traffic Vol, veh/h       | 3    | 61   | 3    | 79   | 32   | 0    |
| Future Vol, veh/h        | 3    | 61   | 3    | 79   | 32   | 0    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | -    | 0    | 0    | -    |
| Grade, %                 | 0    | -    | -    | 0    | 0    | -    |
| Peak Hour Factor         | 92   | 92   | 92   | 92   | 92   | 92   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 2    | 2    | 2    |
| Mvmt Flow                | 3    | 66   | 3    | 86   | 35   | 0    |

| Major/Minor          | Minor2 | Major1 | Major2 |   |   |   |
|----------------------|--------|--------|--------|---|---|---|
| Conflicting Flow All | 127    | 35     | 35     | 0 | - | 0 |
| Stage 1              | 35     | -      | -      | - | - | - |
| Stage 2              | 92     | -      | -      | - | - | - |
| Critical Hdwy        | 6.42   | 6.22   | 4.12   | - | - | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -      | - | - | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -      | - | - | - |
| Follow-up Hdwy       | 3.518  | 3.318  | 2.218  | - | - | - |
| Pot Cap-1 Maneuver   | 868    | 1038   | 1576   | - | - | - |
| Stage 1              | 987    | -      | -      | - | - | - |
| Stage 2              | 932    | -      | -      | - | - | - |
| Platoon blocked, %   |        |        |        | - | - | - |
| Mov Cap-1 Maneuver   | 866    | 1038   | 1576   | - | - | - |
| Mov Cap-2 Maneuver   | 866    | -      | -      | - | - | - |
| Stage 1              | 987    | -      | -      | - | - | - |
| Stage 2              | 930    | -      | -      | - | - | - |

| Approach             | EB  | NB  | SB |
|----------------------|-----|-----|----|
| HCM Control Delay, s | 8.8 | 0.3 | 0  |
| HCM LOS              | A   |     |    |

| Minor Lane/Major Mvmt | NBL   | NBT | EBLn1 | SBT | SBR |
|-----------------------|-------|-----|-------|-----|-----|
| Capacity (veh/h)      | 1576  | -   | 1028  | -   | -   |
| HCM Lane V/C Ratio    | 0.002 | -   | 0.068 | -   | -   |
| HCM Control Delay (s) | 7.3   | 0   | 8.8   | -   | -   |
| HCM Lane LOS          | A     | A   | A     | -   | -   |
| HCM 95th %tile Q(veh) | 0     | -   | 0.2   | -   | -   |

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 3.2  |      |      |      |      |      |
| Movement                 | EBL  | EBR  | NBL  | NBT  | SBT  | SBR  |
| Lane Configurations      | T    |      |      | T    |      | T    |
| Traffic Vol, veh/h       | 0    | 0    | 50   | 32   | 32   | 3    |
| Future Vol, veh/h        | 0    | 0    | 50   | 32   | 32   | 3    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | -    | 0    | 0    | -    |
| Grade, %                 | 0    | -    | -    | 0    | 0    | -    |
| Peak Hour Factor         | 92   | 92   | 92   | 92   | 92   | 92   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 2    | 2    | 2    |
| Mvmt Flow                | 0    | 0    | 54   | 35   | 35   | 3    |

| Major/Minor          | Minor2 | Major1 |       | Major2 |   |
|----------------------|--------|--------|-------|--------|---|
| Conflicting Flow All | 179    | 36     | 38    | 0      | 0 |
| Stage 1              | 36     | -      | -     | -      | - |
| Stage 2              | 143    | -      | -     | -      | - |
| Critical Hdwy        | 6.42   | 6.22   | 4.12  | -      | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -     | -      | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -     | -      | - |
| Follow-up Hdwy       | 3.518  | 3.318  | 2.218 | -      | - |
| Pot Cap-1 Maneuver   | 811    | 1037   | 1572  | -      | - |
| Stage 1              | 986    | -      | -     | -      | - |
| Stage 2              | 884    | -      | -     | -      | - |
| Platoon blocked, %   |        |        |       | -      | - |
| Mov Cap-1 Maneuver   | 783    | 1037   | 1572  | -      | - |
| Mov Cap-2 Maneuver   | 783    | -      | -     | -      | - |
| Stage 1              | 986    | -      | -     | -      | - |
| Stage 2              | 853    | -      | -     | -      | - |

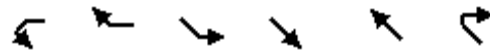
| Approach             | EB | NB  | SB |
|----------------------|----|-----|----|
| HCM Control Delay, s | 0  | 4.5 | 0  |
| HCM LOS              | A  |     |    |

| Minor Lane/Major Mvmt | NBL   | NBT | EBLn1 | SBT | SBR |
|-----------------------|-------|-----|-------|-----|-----|
| Capacity (veh/h)      | 1572  | -   | -     | -   | -   |
| HCM Lane V/C Ratio    | 0.035 | -   | -     | -   | -   |
| HCM Control Delay (s) | 7.4   | 0   | 0     | -   | -   |
| HCM Lane LOS          | A     | A   | A     | -   | -   |
| HCM 95th %tile Q(veh) | 0.1   | -   | -     | -   | -   |

# HCM Signalized Intersection Capacity Analysis

## 1: Willamette Drive & Cedar Oak Drive

08/13/2018



| Movement               | WBL   | WBR  | SEL   | SET   | NWT  | NWR  |
|------------------------|-------|------|-------|-------|------|------|
| Lane Configurations    |       |      |       |       |      |      |
| Traffic Volume (vph)   | 139   | 28   | 38    | 1135  | 669  | 85   |
| Future Volume (vph)    | 139   | 28   | 38    | 1135  | 669  | 85   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900  | 1900 | 1900 |
| Total Lost time (s)    | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Lane Util. Factor      | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frpb, ped/bikes        | 1.00  | 0.96 | 1.00  | 1.00  | 1.00 |      |
| Flpb, ped/bikes        | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Frt                    | 1.00  | 0.85 | 1.00  | 1.00  | 0.98 |      |
| Flt Protected          | 0.95  | 1.00 | 0.95  | 1.00  | 1.00 |      |
| Satd. Flow (prot)      | 1787  | 1535 | 1787  | 1881  | 1830 |      |
| Flt Permitted          | 0.73  | 1.00 | 0.24  | 1.00  | 1.00 |      |
| Satd. Flow (perm)      | 1376  | 1535 | 460   | 1881  | 1830 |      |
| Peak-hour factor, PHF  | 0.98  | 0.98 | 0.98  | 0.98  | 0.98 | 0.98 |
| Adj. Flow (vph)        | 142   | 29   | 39    | 1158  | 683  | 87   |
| RTOR Reduction (vph)   | 0     | 25   | 0     | 0     | 4    | 0    |
| Lane Group Flow (vph)  | 142   | 4    | 39    | 1158  | 766  | 0    |
| Confl. Peds. (#/hr)    |       | 8    |       |       |      |      |
| Confl. Bikes (#/hr)    |       |      |       |       |      | 2    |
| Heavy Vehicles (%)     | 1%    | 1%   | 1%    | 1%    | 2%   | 2%   |
| Turn Type              | Perm  | Perm | pm+pt | NA    | NA   |      |
| Protected Phases       |       |      | 1     | 6     | 2    |      |
| Permitted Phases       | 8     | 3    | 6     |       |      |      |
| Actuated Green, G (s)  | 14.0  | 14.0 | 69.2  | 69.2  | 61.6 |      |
| Effective Green, g (s) | 14.0  | 14.0 | 69.2  | 69.2  | 61.6 |      |
| Actuated g/C Ratio     | 0.15  | 0.15 | 0.75  | 0.75  | 0.67 |      |
| Clearance Time (s)     | 4.5   | 4.5  | 4.5   | 4.5   | 4.5  |      |
| Vehicle Extension (s)  | 3.0   | 3.0  | 3.0   | 3.0   | 3.0  |      |
| Lane Grp Cap (vph)     | 208   | 233  | 389   | 1411  | 1222 |      |
| v/s Ratio Prot         |       |      | 0.00  | c0.62 | 0.42 |      |
| v/s Ratio Perm         | c0.10 | 0.00 | 0.07  |       |      |      |
| v/c Ratio              | 0.68  | 0.02 | 0.10  | 0.82  | 0.63 |      |
| Uniform Delay, d1      | 37.0  | 33.3 | 5.9   | 7.5   | 8.7  |      |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00  | 1.00 |      |
| Incremental Delay, d2  | 8.9   | 0.0  | 0.1   | 5.5   | 2.4  |      |
| Delay (s)              | 45.9  | 33.3 | 6.1   | 12.9  | 11.2 |      |
| Level of Service       | D     | C    | A     | B     | B    |      |
| Approach Delay (s)     | 43.8  |      |       | 12.7  | 11.2 |      |
| Approach LOS           | D     |      |       | B     | B    |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 14.6  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.84  |                           |      |
| Actuated Cycle Length (s)         | 92.2  | Sum of lost time (s)      | 13.5 |
| Intersection Capacity Utilization | 76.6% | ICU Level of Service      | D    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM 2010 TWSC  
2: Old River Road & Cedar Oak Drive

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| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 4.8  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 40   | 55   | 19   | 0    | 34   | 0    | 18   | 1    | 0    | 0    | 1    | 70   |
| Future Vol, veh/h        | 40   | 55   | 19   | 0    | 34   | 0    | 18   | 1    | 0    | 0    | 1    | 70   |
| Conflicting Peds, #/hr   | 0    | 0    | 1    | 1    | 0    | 0    | 3    | 0    | 0    | 0    | 0    | 3    |
| Sign Control             | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   | 81   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 3    | 3    | 3    | 0    | 0    | 0    | 2    | 2    | 2    |
| Mvmt Flow                | 49   | 68   | 23   | 0    | 42   | 0    | 22   | 1    | 0    | 0    | 1    | 86   |

| Major/Minor          | Major1 |   |   | Major2 |   |   | Minor1 |     |     | Minor2 |       |       |
|----------------------|--------|---|---|--------|---|---|--------|-----|-----|--------|-------|-------|
| Conflicting Flow All | 42     | 0 | 0 | 92     | 0 | 0 | 268    | 221 | 81  | 221    | 233   | 45    |
| Stage 1              | -      | - | - | -      | - | - | 179    | 179 | -   | 42     | 42    | -     |
| Stage 2              | -      | - | - | -      | - | - | 89     | 42  | -   | 179    | 191   | -     |
| Critical Hdwy        | 4.12   | - | - | 4.13   | - | - | 7.1    | 6.5 | 6.2 | 7.12   | 6.52  | 6.22  |
| Critical Hdwy Stg 1  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.12   | 5.52  | -     |
| Critical Hdwy Stg 2  | -      | - | - | -      | - | - | 6.1    | 5.5 | -   | 6.12   | 5.52  | -     |
| Follow-up Hdwy       | 2.218  | - | - | 2.227  | - | - | 3.5    | 4   | 3.3 | 3.518  | 4.018 | 3.318 |
| Pot Cap-1 Maneuver   | 1567   | - | - | 1496   | - | - | 689    | 681 | 985 | 735    | 667   | 1025  |
| Stage 1              | -      | - | - | -      | - | - | 827    | 755 | -   | 972    | 860   | -     |
| Stage 2              | -      | - | - | -      | - | - | 923    | 864 | -   | 823    | 742   | -     |
| Platoon blocked, %   |        | - | - |        | - | - |        |     |     |        |       |       |
| Mov Cap-1 Maneuver   | 1563   | - | - | 1496   | - | - | 612    | 658 | 984 | 715    | 644   | 1022  |
| Mov Cap-2 Maneuver   | -      | - | - | -      | - | - | 612    | 658 | -   | 715    | 644   | -     |
| Stage 1              | -      | - | - | -      | - | - | 799    | 729 | -   | 940    | 860   | -     |
| Stage 2              | -      | - | - | -      | - | - | 841    | 864 | -   | 794    | 717   | -     |

| Approach             | EB  |  |  | WB |  |  | NB   |  |  | SB  |  |  |
|----------------------|-----|--|--|----|--|--|------|--|--|-----|--|--|
| HCM Control Delay, s | 2.6 |  |  | 0  |  |  | 11.1 |  |  | 8.9 |  |  |
| HCM LOS              |     |  |  |    |  |  | B    |  |  | A   |  |  |

| Minor Lane/Major Mvmt | NBLn1 | EBL   | EBT | EBR | WBL  | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h)      | 614   | 1563  | -   | -   | 1496 | -   | -   | 1014  |
| HCM Lane V/C Ratio    | 0.038 | 0.032 | -   | -   | -    | -   | -   | 0.086 |
| HCM Control Delay (s) | 11.1  | 7.4   | 0   | -   | 0    | -   | -   | 8.9   |
| HCM Lane LOS          | B     | A     | A   | -   | A    | -   | -   | A     |
| HCM 95th %tile Q(veh) | 0.1   | 0.1   | -   | -   | 0    | -   | -   | 0.3   |

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| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 2.1  |      |      |      |      |      |
| Movement                 | EBL  | EBR  | NBL  | NBT  | SBT  | SBR  |
| Lane Configurations      |      |      |      |      |      |      |
| Traffic Vol, veh/h       | 1    | 26   | 1    | 40   | 44   | 0    |
| Future Vol, veh/h        | 1    | 26   | 1    | 40   | 44   | 0    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | -    | 0    | 0    | -    |
| Grade, %                 | 0    | -    | -    | 0    | 0    | -    |
| Peak Hour Factor         | 92   | 92   | 92   | 92   | 92   | 92   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 2    | 2    | 2    |
| Mvmt Flow                | 1    | 28   | 1    | 43   | 48   | 0    |

| Major/Minor          | Minor2 | Major1 | Major2 |   |   |   |
|----------------------|--------|--------|--------|---|---|---|
| Conflicting Flow All | 94     | 48     | 48     | 0 | - | 0 |
| Stage 1              | 48     | -      | -      | - | - | - |
| Stage 2              | 46     | -      | -      | - | - | - |
| Critical Hdwy        | 6.42   | 6.22   | 4.12   | - | - | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -      | - | - | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -      | - | - | - |
| Follow-up Hdwy       | 3.518  | 3.318  | 2.218  | - | - | - |
| Pot Cap-1 Maneuver   | 906    | 1021   | 1559   | - | - | - |
| Stage 1              | 974    | -      | -      | - | - | - |
| Stage 2              | 976    | -      | -      | - | - | - |
| Platoon blocked, %   |        |        |        | - | - | - |
| Mov Cap-1 Maneuver   | 905    | 1021   | 1559   | - | - | - |
| Mov Cap-2 Maneuver   | 905    | -      | -      | - | - | - |
| Stage 1              | 974    | -      | -      | - | - | - |
| Stage 2              | 975    | -      | -      | - | - | - |

| Approach             | EB  | NB  | SB |
|----------------------|-----|-----|----|
| HCM Control Delay, s | 8.6 | 0.2 | 0  |
| HCM LOS              | A   |     |    |

| Minor Lane/Major Mvmt | NBL   | NBT | EBLn1 | SBT | SBR |
|-----------------------|-------|-----|-------|-----|-----|
| Capacity (veh/h)      | 1559  | -   | 1016  | -   | -   |
| HCM Lane V/C Ratio    | 0.001 | -   | 0.029 | -   | -   |
| HCM Control Delay (s) | 7.3   | 0   | 8.6   | -   | -   |
| HCM Lane LOS          | A     | A   | A     | -   | -   |
| HCM 95th %tile Q(veh) | 0     | -   | 0.1   | -   | -   |

HCM 2010 TWSC  
 4: Old River Road & Nothern Site Access

08/13/2018

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 1.8  |      |      |      |      |      |
| Movement                 | EBL  | EBR  | NBL  | NBT  | SBT  | SBR  |
| Lane Configurations      | T    |      |      | T    |      | T    |
| Traffic Vol, veh/h       | 0    | 0    | 21   | 20   | 44   | 1    |
| Future Vol, veh/h        | 0    | 0    | 21   | 20   | 44   | 1    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | -    | 0    | 0    | -    |
| Grade, %                 | 0    | -    | -    | 0    | 0    | -    |
| Peak Hour Factor         | 92   | 92   | 92   | 92   | 92   | 92   |
| Heavy Vehicles, %        | 2    | 2    | 2    | 2    | 2    | 2    |
| Mvmt Flow                | 0    | 0    | 23   | 22   | 48   | 1    |

| Major/Minor          | Minor2 | Major1 |       | Major2 |   |
|----------------------|--------|--------|-------|--------|---|
| Conflicting Flow All | 115    | 48     | 49    | 0      | 0 |
| Stage 1              | 48     | -      | -     | -      | - |
| Stage 2              | 67     | -      | -     | -      | - |
| Critical Hdwy        | 6.42   | 6.22   | 4.12  | -      | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -     | -      | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -     | -      | - |
| Follow-up Hdwy       | 3.518  | 3.318  | 2.218 | -      | - |
| Pot Cap-1 Maneuver   | 881    | 1021   | 1558  | -      | - |
| Stage 1              | 974    | -      | -     | -      | - |
| Stage 2              | 956    | -      | -     | -      | - |
| Platoon blocked, %   |        |        |       | -      | - |
| Mov Cap-1 Maneuver   | 868    | 1021   | 1558  | -      | - |
| Mov Cap-2 Maneuver   | 868    | -      | -     | -      | - |
| Stage 1              | 974    | -      | -     | -      | - |
| Stage 2              | 942    | -      | -     | -      | - |

| Approach             | EB | NB  | SB |
|----------------------|----|-----|----|
| HCM Control Delay, s | 0  | 3.8 | 0  |
| HCM LOS              | A  |     |    |

| Minor Lane/Major Mvmt | NBL   | NBT | EBLn1 | SBT | SBR |
|-----------------------|-------|-----|-------|-----|-----|
| Capacity (veh/h)      | 1558  | -   | -     | -   | -   |
| HCM Lane V/C Ratio    | 0.015 | -   | -     | -   | -   |
| HCM Control Delay (s) | 7.3   | 0   | 0     | -   | -   |
| HCM Lane LOS          | A     | A   | A     | -   | -   |
| HCM 95th %tile Q(veh) | 0     | -   | -     | -   | -   |

Queuing and Blocking Report  
Background 2021 + Phase 2 AM Peak Hour

08/13/2018

Intersection: 1: Willamette Drive & Cedar Oak Drive

| Movement              | WB  | WB  | SE  | SE  | NW  |
|-----------------------|-----|-----|-----|-----|-----|
| Directions Served     | L   | R   | L   | T   | TR  |
| Maximum Queue (ft)    | 153 | 74  | 92  | 118 | 720 |
| Average Queue (ft)    | 70  | 17  | 32  | 44  | 612 |
| 95th Queue (ft)       | 131 | 50  | 73  | 95  | 870 |
| Link Distance (ft)    |     | 390 |     | 541 | 666 |
| Upstream Blk Time (%) |     |     |     |     | 32  |
| Queuing Penalty (veh) |     |     |     |     | 0   |
| Storage Bay Dist (ft) | 170 |     | 110 |     |     |
| Storage Blk Time (%)  | 0   |     | 0   | 0   |     |
| Queuing Penalty (veh) | 0   |     | 1   | 0   |     |

Intersection: 2: Old River Road & Cedar Oak Drive

| Movement              | EB  | WB  | NB  | SB  |
|-----------------------|-----|-----|-----|-----|
| Directions Served     | LTR | LTR | LTR | LTR |
| Maximum Queue (ft)    | 37  | 4   | 60  | 70  |
| Average Queue (ft)    | 3   | 0   | 21  | 34  |
| 95th Queue (ft)       | 19  | 3   | 50  | 55  |
| Link Distance (ft)    | 390 | 573 | 293 | 213 |
| Upstream Blk Time (%) |     |     |     |     |
| Queuing Penalty (veh) |     |     |     |     |
| Storage Bay Dist (ft) |     |     |     |     |
| Storage Blk Time (%)  |     |     |     |     |
| Queuing Penalty (veh) |     |     |     |     |

Intersection: 3: Old River Road & Southern Site Access

| Movement              | EB  | NB  |
|-----------------------|-----|-----|
| Directions Served     | LR  | LT  |
| Maximum Queue (ft)    | 61  | 5   |
| Average Queue (ft)    | 33  | 0   |
| 95th Queue (ft)       | 53  | 4   |
| Link Distance (ft)    | 161 | 213 |
| Upstream Blk Time (%) |     |     |
| Queuing Penalty (veh) |     |     |
| Storage Bay Dist (ft) |     |     |
| Storage Blk Time (%)  |     |     |
| Queuing Penalty (veh) |     |     |



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Intersection: 4: Old River Road & Northern Site Access

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|                       |     |
|-----------------------|-----|
| Movement              | NB  |
| Directions Served     | LT  |
| Maximum Queue (ft)    | 24  |
| Average Queue (ft)    | 2   |
| 95th Queue (ft)       | 15  |
| Link Distance (ft)    | 185 |
| Upstream Blk Time (%) |     |
| Queuing Penalty (veh) |     |
| Storage Bay Dist (ft) |     |
| Storage Blk Time (%)  |     |
| Queuing Penalty (veh) |     |

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Network Summary

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Network wide Queuing Penalty: 1

CITY OF WEST LINN, CLACKAMAS COUNTY

**CEDAROAK DR at OLD RIVER RD, City of West Linn, Clackamas County, 01/01/2012 to 12/31/2016**

| SER# | P | R | S | W | DATE | CLASS | CITY STREET | INT-TYPE | SPCL USE | MOVE | A | S | RD DPT | E | L | G | H | R | TIME | FROM | SECOND STREET | DIRECT | LEGS | TRAF- | RNDBT | SURF | COLL | OWNER | FROM | PRTC | INJ | G | E | LICNS | PED | UNLOC? | D | C | S | L | K | LAT | LONG | LRS | LOCTN | (#LANES) | CONTL | DRVWY | LIGHT | SVRTY | V# | TYPE | TO | P# | TYPE | SVRTY | E | X | RES | LOC | ERROR | ACT | EVENT | CAUSE |
|------|---|---|---|---|------|-------|-------------|----------|----------|------|---|---|--------|---|---|---|---|---|------|------|---------------|--------|------|-------|-------|------|------|-------|------|------|-----|---|---|-------|-----|--------|---|---|---|---|---|-----|------|-----|-------|----------|-------|-------|-------|-------|----|------|----|----|------|-------|---|---|-----|-----|-------|-----|-------|-------|
|------|---|---|---|---|------|-------|-------------|----------|----------|------|---|---|--------|---|---|---|---|---|------|------|---------------|--------|------|-------|-------|------|------|-------|------|------|-----|---|---|-------|-----|--------|---|---|---|---|---|-----|------|-----|-------|----------|-------|-------|-------|-------|----|------|----|----|------|-------|---|---|-----|-----|-------|-----|-------|-------|

*Disclaimer: The information contained in this report is compiled from individual driver and police crash reports submitted to the Oregon Department of Transportation as required in ORS 811.720. The Crash Analysis and Reporting Unit is committed to providing the highest quality crash data to customers. However, because submittal of crash report forms is the responsibility of the individual driver, the Crash Analysis and Reporting Unit can not guarantee that all qualifying crashes are represented nor can assurances be made that all details pertaining to a single crash are accurate. Note: Legislative changes to DMV's vehicle crash reporting requirement, effective 01/01/2004, may result in fewer property damage only crashes being eligible for inclusion in the Statewide Crash Data File.*







# The Marylhurst School

|               |               |               | Monday   |        | Tuesday  |        | Wednesday |        | Thursday |        | Friday   |        |
|---------------|---------------|---------------|----------|--------|----------|--------|-----------|--------|----------|--------|----------|--------|
| # of Students | Grades        |               | Drop Off | Pickup | Drop Off | Pickup | Drop Off  | Pickup | Drop Off | Pickup | Drop Off | Pickup |
| 15            | Middle School | Drive Through | 8:30     | 3:15   | 8:30     | 3:15   | 8:30      | 3:15   | 8:30     | 3:15   | 8:30     | 3:15   |
| 72            | Primary       | Drive Through | 8:30     | 2:30   | 8:30     | 2:30   | 8:30      | 2:30   | 8:30     | 2:30   | 8:30     | 2:30   |
| 18            | Threes/Fours  | Park & Drop   | 9:00     | 1:00   | 9:00     | 1:00   | 9:00      | 1:00   | 9:00     | 1:00   | No Class |        |
| Full-Time     | Faculty       | 10            |          |        |          |        |           |        |          |        |          |        |

\*Incentives for teachers who take public transportation

\*\*Some families carpool

\*\*\*On average 20 Extended care children stay later; get picked up between 2:30 - 5:00

\*\*\*On average 10 Extended care children arrive early; get dropped off between 7:30 - 8:00

Variables:

- Our teachers currently park in a park and ride across the street from our current location. There is a park and ride on Cedar Oak where teachers could potentially park.
- 36 of our families have multiple children that go to this school so they would arrive in the same car.
- There are 11 students who have parents who teach at our school. They would be included in the faculty parking rather than parent drop off.

|           | Morning:  | 8:00-8:30 | 8:30-9:00 | 9:00-9:30 | 9:30-10:00 | 10:00-10:30 | 10:30-11:00 | 11:00-11:30 | 11:30-12:00 |
|-----------|-----------|-----------|-----------|-----------|------------|-------------|-------------|-------------|-------------|
| Staff: 14 | Monday    |           |           |           |            |             |             |             |             |
| Staff: 14 | Tuesday   |           |           |           |            |             |             |             |             |
| Staff: 14 | Wednesday |           |           |           |            |             |             |             |             |
| Staff: 14 | Thursday  |           |           |           |            |             |             |             |             |
| Staff: 14 | Friday    |           |           |           |            |             |             |             |             |

|           | Afternoon: | 12:00-12:30 | 12:30-1:00 | 1:00-1:30 | 1:30-2:00 | 2:00-2:30 | 2:30-3:00 | 3:00-3:30 | 3:30-4:00 |
|-----------|------------|-------------|------------|-----------|-----------|-----------|-----------|-----------|-----------|
| Staff: 14 | Monday     |             |            |           |           |           |           |           |           |
| Staff: 14 | Tuesday    |             |            |           |           |           |           |           |           |
| Staff: 14 | Wednesday  |             |            |           |           |           |           |           |           |
| Staff: 14 | Thursday   |             |            |           |           |           |           |           |           |
| Staff: 14 | Friday     |             |            |           |           |           |           |           |           |

|                     |
|---------------------|
| Staff Only          |
| 10 - 15 cars parked |
| 15 - 20 Cars parked |
|                     |
|                     |

**Special School Events:**

- Parent Association meetings; once a month alternating am meeting and pm meeting
- Back to School Night; one for Primary & one for Preschool both in September
- Work Party; twice a year on a Saturday one October & one April
- Info night; once a year November
- Winter Performance; once a year December
- Preschool Celebration; once a year December
- Open House; once a year January
- Preschool Picnic; once a year June
- Parent Education Night; possibly twice a year month varies



# Portland Tree Consultancy

Lou Phemister  
ASCA Registered Consulting Arborist #590  
(573) 999-3886 / loupphemister@outlook.com

## ARBORIST REPORT and TREE PROTECTION PLAN

Tree Survey and Tree Protection Plan conforming to the West Linn, Oregon Community Tree Ordinance  
(Chapter 8.500) and West Linn Tree Technical Manual

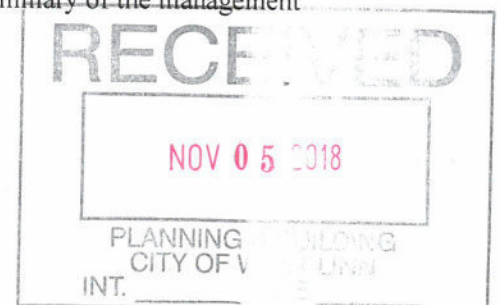
DATE: 9.4.18  
PROPERTY ADDRESS: 19915 Old Lower River Road, West Linn, Oregon  
CITY REFERENCE:  
PROJECT DESCRIPTION: Marylhurst School  
Construction of temporary and new building and infrastructure improvements

### Introduction

This project involves the placement of a temporary building at the southwest corner of the site and construction of a new classroom building at the northwest corner of the property and associated infrastructure. Phase 1 of the project will involve placement of a temporary manufactured building, repaving and re-design of the parking areas, and the construction of a stormwater facility along the east property line. Phase 2 will involve the construction of a two story permanent building on the northwest corner of the property and stormwater facilities along the west property line.

### Tree Survey

A survey of all trees on or over 6-inches DBH on or adjacent to the property was carried out on August 31<sup>st</sup>, 2018. The condition of the trees on the property was examined to ISA Level 2 standards using the standard arboricultural techniques of Visual Tree Assessment. The offsite trees adjacent to the property line were examined as closely as possible, and their size and condition estimated (these estimates are noted by an asterisk in the survey). The survey has produced management recommendations for the trees based both on the expected disturbance and construction impacts at the site, and on the future of the site as a place where children will be moving around under the trees. A detailed summary of the management recommendations is given in the survey table at the end of the report.



### Summary of Actions on Surveyed Trees

|                                                                                               |    |
|-----------------------------------------------------------------------------------------------|----|
| Trees removed due to construction impacts (includes boundary trees along north property line) | 17 |
| Trees removed due to existing condition                                                       | 4  |
| Trees to be protected on the property                                                         | 5  |
| Trees to be protected off site                                                                | 12 |

### Significant Trees

The City of West Linn has designated four of the trees impacted by the development as 'significant' trees. The trees are numbered #7, #22, #23, #29 (see Landscape Plan sheet and Tree Inventory table at the end of this report). Tree #23 will be removed for the construction of a stormwater basin leaving 75% of the significant trees to be preserved and protected by tree protection fencing and by additional measures noted in the Tree Protection Plan.

### Tree Protection Discussion

#### *Phase 1*

Phase 1 of the project will involve the construction of a stormwater basin along the east property line, the repaving, regrading, and re-alignment of the parking area at the southeast section of the site, and the placement of a temporary classroom on paved surface at the southwest section.

The repaving of the parking area will result in the removal and replacement of curbing that is between 5 to 10 feet from seven fence line trees along the south boundary of the property; this work will be just within the required tree protection distance for some of the trees. However, the finished grade of the adjacent parking area will be slightly higher than the existing grade, so any root zone disturbance will be minimal. Extraction of the old concrete curbing should be completed carefully, and if done so, will not result in significant root severance (see items 8 and 9 of the Tree Protection Plan). Tree Protection Fencing will be placed as near to the curb-line as practicable.

Tree Protection Fencing will also be placed at the southwest corner to protect the high value offsite trees there. Fencing will also be placed as close as possible to the north edge of the site proposed for the stormwater basin along the east property line. This will give adequate protection to the mature ash and oak trees at the north end of the property.

#### *Phase 2*

Phase 2 of the project will involve the construction of a large building at the northwest corner of the site. A stormwater basin will be constructed along the west property line and a large trench will be dug along the north property line to accommodate stormwater lines.

Excavation for the trench to contain large diameter pipes will be the biggest tree issue Phase 2. The excavated area should be kept as close to the north property line as possible and the



protection fencing as tight to the line of excavation as possible. Fortunately, Tree 29 is in good condition, but it is likely that it will lose at least 20% of its roots during this excavation (note items 8 thru 11 of the Tree Protection Plan). There is also a 4-inch sanitary sewer proposed to pass within around 5-ft of Tree 29. If this alignment cannot be altered the line should either be directionally drilled, or trenched using hand tools, within 15-ft of the base of the tree.

The southwest root zone area of Tree 22 will be affected by a change in grade resulting from the construction of a childrens playground. This may negatively affect around 10-15% of the tree's root zone, which is significant considering the other construction impacts and the maturity of this tree. Any proposed reduction of construction impacts will be significant in reducing stress.

It is important that the root zone of the grouping of oaks and ash trees (Trees 22,27,28,29) is protected from compaction as much as possible. In addition to the area required to be protected under code, the Tree Protection Fencing should be placed to protect the entire lawn area that is not necessary for access or construction.

Access to the site for construction of the building at the northwest section of the property should protect the surface grade. Although the access alignment is over the area to be excavated for stormwater lines the general area should still be protected from compaction by using steel plates supported by 18-inches to 24-inches depth of wood chip underlain by high strength geotextile fabric.

### **Tree Protection Plan**

The following conditions will be part of the Tree Protection Plan and displayed on the construction plan sheets. *See also the Protection Notes for individual trees in the Tree Inventory table below.*

#### *Site Management Standards*

1. The Project Arborist will be an ISA Certified Arborist with proven experience of managing trees on construction sites. All pre-construction tree work (including tree removals) will be undertaken under the direct supervision of an ISA Certified Arborist.
2. The Project Arborist will be on site at all times when any of the following occur: actions that result in disturbance of the existing grade level; the placement of the materials for the Phase 2 access point to the construction area; to approve the initial placement of Tree Protection Fencing; before any movement or modification of a Tree Protection Fence location occurs during the project.
3. A site meeting must take place between a site supervisor representing the contractor and the Project Arborist to discuss the tree protection requirements. This must occur before any movement of materials or equipment onto the site and will occur before both Phase 1 and Phase 2 of the project.

4. All tree work detailed in this report will be completed before any construction work is initiated at the site.
5. The Project Arborist will provide a letter of verification to the City of West Linn showing that the pre-construction standards have been met for the site.
6. The Project Arborist will visit the site on a regular basis and will provide a monthly report and a project completion report to the City of West Linn providing a condition assessment of the protected trees and detailing any breaches of the Tree Protection Plan.
7. The project arborist may recommend and require tree care measures designed to ameliorate the health of protected trees due to unforeseen weather events or in the event that construction impacts are having a detrimental effect on the protected trees.

#### *Ground Disturbance Standards*

8. Any excavation or disturbance of the existing grade will be undertaken with the Project Arborist present at the site. The Project Arborist may require hand held equipment to be used when excavating adjacent to the Tree Protection Fencing.
9. Any existing paving or curbing to be removed shall be broken and lifted carefully out of the root protection zone area with the Project Arborist present to approve the machinery used and to examine any exposed roots.
10. Any roots 2-inches or more in diameter uncovered during construction and required to be removed must be first approved for removal by the Project Arborist.
11. Any severed or badly damaged roots of any size must be cut cleanly using hand-held tools (e.g. hand saw, reciprocating saw, circular saw or angle grinder)

#### *Tree Protection Fencing Standards*

12. The placement of all Tree Protection Fencing as shown on the plans must occur before any grading, construction, excavation or storage of materials or equipment takes place at the site.
13. Tree Protection Fencing will conform to the standard city requirements as detailed in the Tree Technical Manual See applicable detail for location and type on the plans. Fence poles will be secured firmly into the ground unless otherwise approved by the Project Arborist.
14. No ground disturbance, including vehicle access, or any storage of spoil or equipment will occur within the tree protection fence. The fence location will not be altered or breached without approval of the Project Arborist.
15. Erosion control devices will be placed at the Tree Protection Fencing if the base of the tree is at, or below, the new grade elevation. Any erosion control device installed must be able to prevent the ingress of any materials or fluids beyond the fence line.
16. Any damage to the Tree Protection Fencing must be reported immediately to the Project Arborist and remedied within 24 hrs.

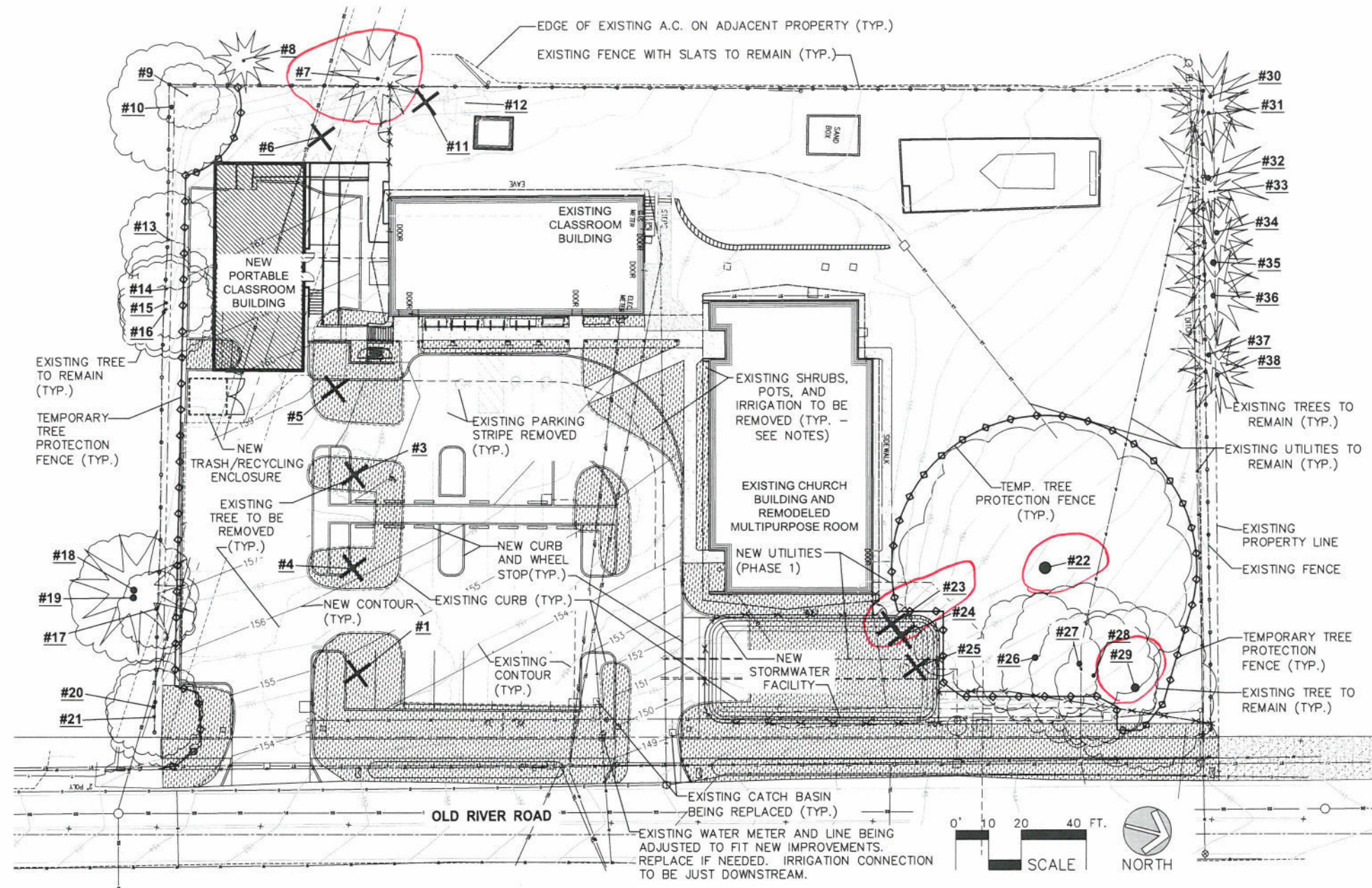
| ID | Tree Species                                         | DBH  | Condition       | Tree work & Tree Notes                                                    | Site redesign/ Protection Notes                                                                  | Action             |
|----|------------------------------------------------------|------|-----------------|---------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------|--------------------|
| 1  | Oregon White oak<br><i>Quercus garryana</i>          | 25   | Fair/Poor       | Significant basal defects & likely decay resulting from loss of twin stem | Existing planter area will be removed                                                            | Remove             |
| 2  | Japanese flowering cherry<br><i>Prunus serrulata</i> | 11   | Fair            | Heavily crown reduced                                                     | Proposed paved walkway within 1-ft of tree. Significantly modified planter area                  | Remove             |
| 3  | Japanese flowering cherry<br><i>Prunus serrulata</i> | 9    | Fair            | Heavily crown reduced                                                     | Existing planter area will be paved over                                                         | Remove             |
| 4  | Japanese flowering cherry<br><i>Prunus serrulata</i> | 8    | Fair            | Heavily crown reduced                                                     | Existing planter area will be paved over                                                         | Remove             |
| 5  | Japanese flowering cherry<br><i>Prunus serrulata</i> | 8    | Fair/Poor       | Heavily crown reduced                                                     | Existing planter area will be paved over                                                         | Remove             |
| 6  | Austrian Pine<br><i>Pinus nigra</i>                  | 11.5 | Poor            | Stem lean 20-deg. Poor crown form. Soil cracking around root plate        |                                                                                                  | Remove (Condition) |
| 7  | Douglas Fir<br><i>Pseudotsuga menziesii</i>          | 36   | Fair/Good*      |                                                                           | OFFSITE TREE. Install tree protection fencing meeting or exceeding minimum distance requirements | Protect            |
| 8  | Norway spruce<br><i>Picea abies</i>                  | 9*   | Fair/Poor*      |                                                                           | OFFSITE TREE. Tree will not be influenced by site work                                           | Protect            |
| 9  | Blue elderberry<br><i>Sambucus nigra</i>             | 6    | Dying           | Heavy dieback on multi-stem tree.                                         | No protection required                                                                           | Remove (Condition) |
| 10 | Big leaf maple<br><i>Acer macrophyllum</i>           | 14   | Fair            | Fence line volunteer                                                      | OFFSITE TREE Install tree protection fencing meeting or exceeding minimum distance requirements  | Protect            |
| 11 | Red alder<br><i>Alnus rubra</i>                      | 15   | Dying/Dangerous | Heavy dieback in crown. Large branches will fail                          |                                                                                                  | Remove (Condition) |
| 12 | Red alder<br><i>Alnus rubra</i>                      | 19   | Dying           | Partial failure. Loss of upper crown. Heavy decay in bole                 |                                                                                                  | Remove (Condition) |
| 13 | Big leaf maple<br><i>Acer macrophyllum</i>           | 10   | Fair/Good       | Fence line volunteer. Crown raise over parking area                       | OFFSITE TREE Install tree protection fencing within 1-ft of existing park place curbing          | Protect            |

| ID | Tree Species                                | DBH  | Condition | Tree work & Tree Notes                                                                                                          | Site redesign/ Protection Notes                                                                                            | Action  |
|----|---------------------------------------------|------|-----------|---------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------|---------|
| 14 | Big leaf maple<br><i>Acer macrophyllum</i>  | 14   | Fair      | Fence line volunteer. Twin stems. Crown raise over parking area                                                                 | OFFSITE TREE Install tree protection fencing within 1-ft of existing park place curbing                                    | Protect |
| 15 | Big leaf maple<br><i>Acer macrophyllum</i>  | 9    | Fair      | Fence line volunteer. Crown raise over parking area                                                                             | OFFSITE TREE Install tree protection fencing within 1-ft of existing park place curbing                                    | Protect |
| 16 | Big leaf maple<br><i>Acer macrophyllum</i>  | 14   | Fair      | Fence line volunteer. Crown raise over parking area                                                                             | OFFSITE TREE Install tree protection fencing within 1-ft of existing park place curbing                                    | Protect |
| 17 | Big leaf maple<br><i>Acer macrophyllum</i>  | 12   | Fair      | Fence line volunteer. Crown raise over parking area                                                                             | OFFSITE TREE Install tree protection fencing within 1-ft of existing park place curbing                                    | Protect |
| 18 | Big leaf maple<br><i>Acer macrophyllum</i>  | 22   | Fair      | Three fused stems                                                                                                               | OFFSITE TREE. Tree will not be influenced by site work                                                                     | Protect |
| 19 | Douglas fir<br><i>Pseudotsuga menziesii</i> | 20   | Fair      |                                                                                                                                 | OFFSITE TREE. Tree will not be influenced by site work                                                                     | Protect |
| 20 | Big leaf maple<br><i>Acer macrophyllum</i>  | 13   | Fair      |                                                                                                                                 | OFFSITE TREE. Build out adjacent will be expanded slightly Install tree protection fencing adjacent to park place curbing  | Protect |
| 21 | Big leaf maple<br><i>Acer macrophyllum</i>  | 12   | Fair/Poor | Stem defect                                                                                                                     | OFFSITE TREE. Build out adjacent will be expanded slightly Install tree protection fencing within adjacent to park curbing | Protect |
| 22 | Oregon White oak<br><i>Quercus garryana</i> | 42   | Fair      | Crown raise to 8-ft from grade; Remove deadwood 2" diam.or greater; 20% crown reduction on S side of tree to reduce stress load | Install tree protection fencing meeting minimum distance standards and exceed wherever possible                            | Protect |
| 23 | Oregon White oak<br><i>Quercus garryana</i> | 24   | Fair      | Codominant crown. Ash anthracnose                                                                                               | Site of Phase 1 stormwater basin                                                                                           | Remove  |
| 24 | Oregon ash<br><i>Fraxinus latifolia</i>     | 18   | Fair      | Ash anthracnose                                                                                                                 | Site of Phase 1 stormwater basin                                                                                           | Remove  |
| 25 | Oregon ash<br><i>Fraxinus latifolia</i>     | 18   | Fair      | Codominant crown. Ash anthracnose                                                                                               | Site of Phase 1 stormwater basin                                                                                           | Remove  |
| 26 | Oregon ash<br><i>Fraxinus latifolia</i>     | 20.5 | Fair/Poor | 4-ftx5" lower stem wound with internal decay. Remove if exposed by loss of trees 22 or 27                                       | Install tree protection fencing meeting minimum distance standards and exceeding wherever possible                         | Protect |

| ID | Tree Species                                | DBH | Condition  | Tree work & Tree Notes                                     | Site redesign/ Protection Notes                                                                                                                            | Action                           |
|----|---------------------------------------------|-----|------------|------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------|
| 27 | Oregon ash<br><i>Fraxinus latifolia</i>     | 20  | Fair       | Codominant crown. Ash anthracnose                          | Install tree protection fencing meeting minimum distance standards and exceeding wherever possible                                                         | Protect                          |
| 28 | Oregon ash<br><i>Fraxinus latifolia</i>     | 16  | Fair       | Partially suppressed. Crown raise to 10-ft over work areas | Install tree protection fencing meeting minimum distance standards and exceeding wherever possible                                                         | Protect                          |
| 29 | Oregon White oak<br><i>Quercus garryana</i> | 32  | Good/Fair  | Crown raise to 10-ft over work areas                       | Install tree protection fencing meeting minimum distance standards and exceeding wherever possible. Excavate by hand within 10-ft of the base of this tree | Protect                          |
| 30 | Douglas fir<br><i>Pseudotsuga menziesii</i> | 15* | Fair/Good* |                                                            | Boundary tree. Phase 2 building foundations within 10-ft. Utility trenching within 5-ft.                                                                   | Remove with both owners approval |
| 31 | Douglas fir<br><i>Pseudotsuga menziesii</i> | 13* | Fair*      |                                                            | Boundary tree. Phase 2 building foundations within 10-ft. Utility trenching within 5-ft.                                                                   | Remove with both owners approval |
| 32 | Douglas fir<br><i>Pseudotsuga menziesii</i> | 20* | Fair/Good* |                                                            | Boundary tree. Phase 2 building foundations within 10-ft. Utility trenching within 5-ft.                                                                   | Remove with both owners approval |
| 33 | Douglas fir<br><i>Pseudotsuga menziesii</i> | 7*  | Fair*      |                                                            | Boundary tree. Phase 2 building foundations within 10-ft. Utility trenching within 5-ft.                                                                   | Remove with both owners approval |
| 34 | Douglas fir<br><i>Pseudotsuga menziesii</i> | 18* | Fair/Poor* |                                                            | OFFSITE TREE. Boundary tree. Phase 2 building foundations within 10-ft. Utility trenching within 5-ft.                                                     | Remove with both owners approval |
| 35 | Douglas fir<br><i>Pseudotsuga menziesii</i> | 22* | Fair/Good* |                                                            | Boundary tree, Phase 2 building foundations within 10-ft. Utility trenching within 5-ft.                                                                   | Remove with both owners approval |
| 36 | Douglas fir<br><i>Pseudotsuga menziesii</i> | 20* | Fair/Good* |                                                            | Boundary tree. Phase 2 building foundations within 10-ft. Utility trenching within 5-ft.                                                                   | Remove with both owners approval |
| 37 | Douglas fir<br><i>Pseudotsuga menziesii</i> | 18* | Fair*      |                                                            | Boundary tree. Phase 2 building foundations within 10-ft. Utility trenching within 5-ft.                                                                   | Remove with both owners approval |
| 38 | Western red cedar<br><i>Thuja plicata</i>   | 18  | Fair/Good* |                                                            | OFFSITE TREE Phase 2 building foundations within 15-ft. Utility trenching within 10-ft.                                                                    | Remove with both owners approval |



PERCIVAL  
LANDSCAPE  
ARCHITECTURE  
P.O. BOX 14676  
PORTLAND,  
OREGON  
97293  
perlandscape@gmail.com  
503-939-3547



- NOTES:
1. SEE ARBORIST REPORT FOR NAME, SIZE, AND COMMENTS REGARDING EXISTING TREES.
  2. CONFIRM TREATMENT OF EACH TREE ON SITE.
  3. ALL WORK TO CONFORM WITH CITY OF WEST LINN "TREE TECHNICAL MANUAL".
  4. AREA BETWEEN THE EXISTING TREES AND THE TREE PROTECTION FENCE SHALL BE KEPT FREE OF ALL EROSION MATERIALS, TRAFFIC, CONSTRUCTION, CONSTRUCTION TRAFFIC, MATERIALS, AND EXCAVATION. THIS AREA IS TO BE CONSIDERED THE "TREE PROTECTION AREA".
  5. TREE PROTECTION FENCE SHALL REMAIN IN PLACE, IN GOOD CONDITION, AND PLUMB UNTIL APPROVED SUBSTANTIAL COMPLETION. AT THAT TIME IT SHALL BE REMOVED AND TAKEN OFF SITE.
  6. ANY MOVEMENT OF THE TREE PROTECTION FENCE AND ANY WORK WITHIN THE "TREE PROTECTION AREA" SHALL BE AS APPROVED BY THE CITY'S AND OWNER'S ARBORIST (CERTIFIED). ANY SUCH WORK SHALL BE DONE IN ACCORDANCE WITH THESE ARBORISTS' APPROVAL..
  7. THERE SHALL BE NO WORK, STORAGE, TRANSIT, AND DEMOLITION DONE IN THE AREAS BEHIND THE EXISTING BUILDINGS.
  8. 'TREE PROTECTION FENCE' SHALL BE A 6' TALL CHAIN LINK FENCE, WITH METAL POLES SET INTO GROUND AT 10 FEET ON CENTER (MAX.) AND AT CHANGES IN DIRECTION, AND AT CORNERS. IT SHALL BE KEPT IN GOOD CONDITION, PLUMB, AND IN-PLACE.
  9. INSTALL UTILITIES AND CONDUCT ALL WORK TO MINIMIZE TREE IMPACTS.

**THE MARYLHURST SCHOOL**  
NEW LIFE CHURCH SITE  
19915 Old Lower River Road  
West Linn, Oregon, 97068

LAND USE APPLICATION

SIGNIFICANT TREES CIRCLED IN RED 11-1-18

| Issue                | Revision | Date       |
|----------------------|----------|------------|
| LAND USE APPLICATION |          | 09/06/2018 |

**EXISTING TREE AND SHRUBS - PHASE 1**

Scale AS INDICATED  
Date 09/06/2018

Sheet No.  
**L0.01**

June 12, 2018

**Robinwood Neighborhood Association**

James T. O'Toole, President  
Kazi Ahmed, Vice President  
Christine Steel, Treasurer  
Jenne Henderson, Secretary  
Char Marier, at Large

Robinwood Station Community Center  
3706 Cedaroak Drive  
West Linn, Oregon 97068

**Robinwood Neighborhood Association Neighbors within 500 feet of proposed project site**

**Subject:** The Marylhurst School

**Proposed Site:** 19915 Old River Drive  
19803 Old River Drive  
West Linn, Oregon 97068

Dear James, board members of the Robinwood Neighborhood Association, and neighbors,

The purpose of this letter is to share with you our proposal to convert the existing New Life Church property at 19915 and 19803 Old River Drive into the new home for the Marylhurst School, a Pre-K through 8<sup>th</sup> grade nonprofit private school.

The Marylhurst School is an independent, progressive education community currently located in historic Oregon City. Founded in 1972 at the Marylhurst University Campus in West Linn, we embrace the creativity, questions and risk taking of childhood in a supportive, hands-on environment.

The Marylhurst School proposes to purchase the existing church property and combine the two lots to make a single property lot. The proposal may include street improvements, parking lot improvements, new stormwater treatment facilities, renovations of the existing church buildings, and a new classroom building. The proposed development would require a Conditional Use Permit (CUP) and Class II Design Review by the City of West Linn.

We would like to invite the Robinwood Neighborhood Association and any interested persons to a meeting to discuss the proposal in more detail. We formally request that within 60 days, a date and location to have your required neighborhood meeting is scheduled, with a return receipt. The 60 days shall be calculated from the date this letter is mailed. If your neighborhood association does not want to meet within the 60-day timeframe, the applicant may hold a public meeting during the evening after 6:00 p.m., or on the weekend no less than 20 days from the date of mailing of the notice.

All meetings shall be held at a location open to the public within the boundaries of the association or at a public facility within the City of West Linn. If the meeting is held at a business, it shall be posted at

the time of the meeting as the meeting place and shall note that the meeting is open to the public and all interested persons may attend. If the meeting is scheduled as part of your neighborhood association's regular monthly meeting, the proposal may not be the only topic of discussion on the meeting agenda.

Concerned citizens should contact your neighborhood association, or their association designee, with any questions that they may want to relay to the applicant.

Please let us know if you have questions or need any additional information.

Sincerely,

Sheila Walker, Head of the Marylhurst School  
1232 Linn Ave  
Oregon City, Oregon 97045  
(503) 650-0978  
sheilaw@themarylhurstschool.org

New Life Church





21E24BB01900  
William Allen  
3870 Ridgewood Way  
West Linn, OR 97068

21E24BB02700  
Lorene Bay  
2045 Ostman Rd  
West Linn, OR 97068

21E24BB02000  
William Befden  
PO Box 388  
West Linn, OR 97068

21E23AA01200  
Durward Bennett  
3320 Walling Way  
West Linn, OR 97068

21E23AA00101  
David Brandon  
19635 Old River Dr  
West Linn, OR 97068

21E24BB02900  
Carol Bryck  
PO Box 603  
West Linn, OR 97068

21E24BB01800  
Patricia Buffington  
3820 Ridgewood Way  
West Linn, OR 97068

21E24BB02100  
Brent Carlson  
19930 Old River Dr  
West Linn, OR 97068

21E23AA00703 & 00705  
Oak Cedar  
PO Box 1919  
Wichita Falls, TX 76307

21E13CC06100  
Tiffany Chong  
18649 Trillium Dr  
West Linn, OR 97068

21E24BB01700  
Dale Cook  
19844 Old River Dr  
West Linn, OR 97068

21E13CC07800  
Ramona Delano  
3737 Ridgewood Way  
West Linn, OR 97068

21E24BB01600  
David Ehlinger  
19790 Old River Dr  
West Linn, OR 97068

21E24BB02300  
Benjamin Foster  
3707 Cedaroak Dr  
West Linn, OR 97068

21E23AA01100  
Marilyn Frankel  
3354 Walling Way  
West Linn, OR 97068

21E23AA00300  
Michael Hayes  
19775 Old River Dr  
West Linn, OR 97068

21E13CC07900  
Michael Higgins  
3753 Ridgewood Way  
West Linn, OR 97068

21E24BB01400  
Ava Johnson  
3716 Ridgewood Way  
West Linn, OR 97068

21E13CC05500  
Raymond Kestek  
3536 Walling Way  
West Linn, OR 97068

21E13CC05700  
Roxanna Khosravi  
19625 Old River Dr  
West Linn, OR 97068

21E24BB00600  
Karen Kiefert  
3751 Cedaroak Dr  
West Linn, OR 97068

21E23AA00100  
Paul Knudsen  
19679 Old River Dr  
West Linn, OR 97068

21E23AA03000  
Theodore Lachman  
35301 SW Geer Rd  
Newberg, OR 97132

21E24BB00700  
Eleanora Larson  
3969 Ridgewood Way  
West Linn, OR 97068

21E13CC05900  
Judy McCartney  
3691 Ridgewood Way  
West Linn, OR 97068

21E23AA00200  
Penelope McCaslin  
18915 Beaver Ln NE  
Aurora, OR 97002

21E24BB02500  
John Micetic  
20024 Old River Dr  
West Linn, OR 97068

21E24BB01000  
John Miller  
3825 Ridgewood Way  
West Linn, OR 97068

21E24BB01500  
Edward Montpart  
19728 Old River Dr  
West Linn, OR 97068

21E13CC06000  
Scott Morris  
3711 Ridgewood Way  
West Linn, OR 97068





**21E24BB02400**  
Eric Nepom  
19970 Old River Dr  
West Linn, OR 97068

**21E24BB01401**  
Tina Olsen  
3740 Ridgewood Way  
West Linn, OR 97068

**21E24BB00800**  
Michelle Patterson  
3927 Ridgewood Way  
West Linn, OR 97068

**21E24BB01200**  
Wyljada Price  
3787 Ridgewood Way  
West Linn, OR 97068

**21E13CC05600**  
Nancy Rowinski  
3424 Walling Way  
West Linn, OR 97068

**21E23AA01300**  
William Schroeter  
PO Box 256  
Marylhurst, OR 97036

**21E23AA00700 & 00702**  
Douglas E Seely  
1780 SW Advance  
West Linn, OR 97068

**21E23AA01700**  
Douglas E Seely  
1780 SW Advance  
West Linn, OR 97068

**21E24BB02200**  
Kathleen Smith  
3950 Ridgewood Way  
West Linn, OR 97068

**21E13CC05800**  
William Swartz  
3611 Ridgewood Way  
West Linn, OR 97068

**21E24BB00900**  
James Wright  
3875 Ridgewood Way  
West Linn, OR 97068

**21E23AA00400**  
New Life Church Robinwood  
PO Box 5  
West Linn, OR 97068

**21E23AA00600**  
Tribbett Trust  
1942 Westlake Loop  
Newberg, OR 97132

**21E23AA00601**  
Cedar Linn LLC  
7831 SE Lake Rd Ste 200  
Milwaukie, OR 97267

**21E23AA00704**  
West Linn Properties  
10250 SW North Dakota St  
Tigard, OR 97223

**21E23AA01400**  
West Linn Investors LLC  
6830 SW Windemere Loop  
Portland, OR 97225

**21E23AA03100**  
City Of West Linn  
22500 Salamo Rd #600  
West Linn, OR 97068

**21E23AD06101**  
Roic Robinwood LLC  
8905 Towne Centre Dr Ste 108  
San Diego, CA 92122

**21E24BB02600**  
Hamersly Family LLC  
2695 Surrey Ln  
West Linn, OR 97068

**21E24BB02800**  
City Of West Linn  
22500 Salamo Rd #600  
West Linn, OR 97068

**21E24BB04800**  
Presbytery Of The Cascades  
19200 Willamette Dr  
West Linn, OR 97068

**21E24BB04900**  
Erfan Inc  
PO Box 2072  
Portland, OR 97208

**21E23AA00602**  
Tribbett Family Ltd Prtnshp  
1942 Westlake Loop  
Newberg, OR 97132





The Marylhurst School  
honoring the journey of each child

June 20<sup>th</sup>, 2018

This is the Marylhurst School's affidavit of mailing. On June 19<sup>th</sup>, a certified letter was received by James O'Toole, the president of the Robinwood Neighborhood Association. Please see attached page for receipt and delivery confirmation.

Sincerely,

Sheila Walker; Head of School

7014 2120 0000 7900 4069

| U.S. Postal Service™<br>CERTIFIED MAIL® RECEIPT<br>Domestic Mail Only                            |                              |
|--------------------------------------------------------------------------------------------------|------------------------------|
| For delivery information, visit our website at <a href="http://www.usps.com">www.usps.com</a> ®. |                              |
| WEST LINN, IOR 97068                                                                             |                              |
| OFFICIAL USE                                                                                     |                              |
| Postage                                                                                          | \$0.00                       |
| Certified Fee                                                                                    | \$0.00                       |
| Return Receipt Fee<br>(Endorsement Required)                                                     | \$0.00                       |
| Restricted Delivery Fee<br>(Endorsement Required)                                                | \$11.00                      |
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|                                                                                                  | Postmark<br>Here             |
|                                                                                                  | 06/18/2018                   |
| Sent To                                                                                          | James O'TOOLE                |
| Street & Apt. No.,<br>or PO Box No.                                                              |                              |
| City, State, ZIP+4                                                                               |                              |
| PS Form 3800, July 2014                                                                          | See Reverse for Instructions |

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Add a tracking number

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70142120#00079004069

**Delivered:**  
WEST LINN, OR 97068 on  
June 19, 2018 at 12:04 pm

**Expected Delivery on:**  
Tuesday, June 19, 2018 by  
8:00pm

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UPDATED 6/25/2018 10:17:11 AM



## The Marylhurst School

19915 Old River Drive  
19803 Old River Drive  
West Linn, Oregon 97068

This notice is to share with you that this site may be subject to a proposed development to convert the existing New Life Church property at 19915 and 19803 Old River Drive into the new home for the Marylhurst School, a Pre-K through 8<sup>th</sup> grade nonprofit private school.

The Marylhurst School is an independent, progressive education community currently located in historic Oregon City. Founded in 1972 at the Marylhurst University Campus in West Linn, we embrace the creativity, questions and risk taking of childhood in a supportive, hands-on environment.

The Marylhurst School proposes to purchase the existing church property and combine the two lots to make a single property lot. The proposal may include street improvements, parking lot improvements, new stormwater treatment facilities, renovations of the existing church buildings, and a new classroom building. The proposed development would require a Conditional Use Permit (CUP) and Class II Design Review by the City of West Linn.

Please contact Sheila Walker, Head of the Marylhurst School, for additional information.

Sheila Walker, Head of the Marylhurst School  
1232 Linn Ave  
Oregon City, Oregon 97045  
(503) 650-0978  
sheilaw@themaryhurstschool.org

**New Life Church**



## Robinwood Neighborhood Association

### Minutes July 10th, 2018

#### **Call to Order, Agenda Review, and Introductions:**

- Meeting of the Robinwood Neighborhood Association (RNA) called to order at 7: 08pm.
- New members to introduce: none

#### **Guests of the Association:**

- Ms. Sheila Walker, Head, The Marylhurst School, and others. The school will move to Old River Road. The proposed addition is two story, built onto the field. Parking lot changes to include a drop off loop to avoid back up onto the street. Questions were asked about traffic use, locals continuing to use the playground, a walking path, half street improvements, speed bump improvements, etc. Applications may be submitted in August, possibly appearing before the Planning Commission in September or August. The Oregon City lease is up in June 2019 and they plan to move to West Linn with Phase I, using the existing building and as funds become available the master plan will be implemented.
- 
- The Honorable Russell Axelrod, Mayor, West Linn. Changes and updates to the Code are being made. The Council has talked about looking at the Transportation System Plan *and* Neighborhood Plan and whether sidewalks make sense in different areas. Last night, the City Council took a position to evaluate new projects and if half street improvements would be required. Neighborhoods were encouraged to review and update their Neighborhood Plan.

**Treasurer Report:** (Christine Steel): The new sound system cost \$1398.84, leaving a balance of \$982.84.

**Review and Approval of Minutes:** (Jenne Henderson) May and June minutes approved as amended.

#### **Announcements, City and Community Events:**

- None

#### **Committee and Community Reports:**

- Robinwood Station: (Randall Fastabend): RNA Picnic 8/25.
- Parks and Rec: (Jim for Don Kingsborough): Summer movies, music and street dance are scheduled. Tomorrow 7/11 there is a tribute to Neal Hennelley, 2:00pm at the Adult Community Center.
- Community Garden: (Randall for Lisa Clifton): Deferred.
- Public Works: (Tony Bracco): Deferred.
- Preparedness/MYN: (Jim for Judy Wiechmann): Deferred.

#### **Old Business:**

- None

**New Business:**

- Neighborhood Picnic, Saturday August 25, 2018. Randall and Kevin are on the picnic committee.

**Adjourned** at 8:31pm.

**Attendees:** 26

Respectfully submitted:

/s/ Jenne Henderson, Secretary

Robinwood Neighborhood Association Board Members

Jim O'Toole, President      Kazi Ahmed, Vice-president  
Jenne Henderson, Secretary      Christine Steel, Treasurer  
Sharon Pullmann, Ambassador



## ROBINWOOD NEIGHBORHOOD ASSOCIATION

The Honorable Russ Axelrod  
Mayor  
West Linn, Oregon  
22500 Salamo Road  
West Linn, Oregon 97068

June 19, 2018

Dear Mr. Mayor,

During this month's meeting of the Robinwood Neighborhood Association the topic of improvements to "sidewalks and streets" was discussed at length. The Association voted, unanimously, to seek your assistance in order to finally resolve this dilemma.

Over the years the sections of Chapter 96 of the Community Development Code have been inconsistently applied to remodel and building projects within the boundaries of the Robinwood Neighborhood. Specifically, those provisions and conditions affecting the installation of concrete curbs and sidewalks, along with street improvements, have been most controversial and problematic.

As you are aware, the majority of residents within the developed sections of the Robinwood Neighborhood have long had a strong preference to maintain the current rural fabric. They enjoy the narrow shared roadways which maximizes the natural setting and minimizes hardscape.

The current thinking of the City planners regarding the piece-meal installation of curbs and sidewalks, seems to be that some day, in the far off future, all of the individual curbs and sidewalks will miraculously all "meet-up" in our life time, is illusory. In the interim, the rationale for the installation of a series of "sidewalks to no-where" contributes little to the current aesthetics and/or individual property values within the neighborhood. Given the time it will take to complete this fancy, no account is given for the value of "undeveloped" lands. Nor, does the program take into account that residents will defer upgrades and improvements to their individual properties in light of the significant added expenses which would be incurred. As a result, the neighborhood will not benefit from the improvements, including remodeled and/or new housing normally witnessed within the growth of a typical neighborhood.

There are provisions within the code which allow for a determination to be made which allows the resident to pay for the curbs, sidewalks and street improvements elsewhere in the City; in lieu of placing same at their residence. These provisions are just onerous. The concept, usually reserved for construction of commercial properties is, in most cases, considered the cost of doing business and limited to improvements within and around the exterior of the property to be developed, not properties outside the control of the developer at other locations. Other property owners in neighborhoods within the city, who make improvements to their property, are not subject to the same financial burdens, which are not inexpensive by the way, as neighbors who do not already enjoy the amenities your planning division now find necessary. In short, the application of this development criteria and extra costs to residential units seems by many, to be ambiguous, discriminatory, overreaching, and a punitive tax on certain citizens.

The “in-lieu” program was last presented to the Neighborhood Association meeting by the former City Manager, It was not received well then, and such sentiments have produced little improvement to the muddle since.

The Neighbors are very aware there are others within the neighborhood who would find the addition of sidewalks, and improved streets desirable, particularly those residents in the newer additions, along the periphery of the existing Neighborhood Boundaries; and, there are some who would advocate viable alternatives such as an asphalt pathway along one side of some streets within the older sections of the Robinwood Neighborhood itself.

The purported thinking surrounding safety and remedies can be found on both sides of the street as equal good and bad conclusions have been advanced by both advocates of developed and undeveloped camps. In addition, we have been advised the Tualatin Valley Fire and Rescue officials may have expressed some interest in this program. Certainly their views need to be considered in any new development and accommodated to the extent practical in existing developments.

We would ask you to consider addressing these issues through the City Council and/or Planning Commission, and initiate a comprehensive and narrowly focused study; produce a neighborhood plan based upon a wide-array of Robinwood resident participation, and consider subsequent code revisions which might arise from the foregoing to address future developments and improvements to the neighborhood infrastructure (i.e. curb, sidewalks and street improvements).

Most importantly, we request you and the City Council cause a moratorium be placed into effect on those policies and procedures as described in Chapter 96 of the Community Development Code concerning the consideration of curbs, sidewalks and street improvements in the application for permits from the City, for remodeling and/or the construction of residences, within the Robinwood Neighborhood until the study, plan and subsequent code revisions are finalized.

In the past, for one reason or another, residents of Robinwood have considered these issues resolved, only to find the controversy rise once again. We believe it is time to put this dispute to a final resolution.

The active members of the Robinwood Association stand ready to assist you in this endeavor in any way that will prove helpful.

In this regard, we cordially invite you and/or your designee to address these issues at our next Robinwood Meeting on Tuesday, July 10, 2018 at 7pm. Given the importance of the points of contention we will defer all other neighborhood business to allow an appropriate amount of time for discussion and questions on these issues.

Page3 of 3  
The Honorable Russ Axelrod  
June 19, 2018

We look forward to your kind response and thank you for your continued support for our neighbors.

Sincerely yours,

/s/ Jim

James T. O'Toole  
President

Cc: The Honorable Brenda Perry  
Council President, West Linn City Council

The Honorable Robert Martin  
Member of the West Linn City Council

The Honorable Teri Cummings  
Member of the West Linn City Council

The Honorable Richard Sakelik  
Member of the West Linn City Council

Bcc: Robinwood Neighborhood Association Members

**Robinwood Neighborhood Association Board Members**

Jim O'Toole, President      Kazi Ahmed, Vice-president  
Jenne Henderson, Secretary      Christine Steel, Treasurer  
Sharon Pullmann, Ambassador

# ROBINWOOD NEIGHBORHOOD ASSOCIATION

August 14, 2018

Mr. John Williams  
Community Development Director  
City of West Linn  
22500 Salamo Road  
West Linn, Oregon 97068

RE: Marylhurst School Project on Old River Drive

Dear Mr. Williams,

On July 10, 2018 the preliminary plans for the Marylhurst School Project were presented to the members attending the monthly meeting of the Robinwood Neighborhood Association.

The Association generally supports the project and believes the transformation of the NewLife Church into The Marylhurst School, on the properties on Old River Drive, will benefit our neighborhood and the larger West Linn Community.

During the meeting, the neighbors expressed two concerns for you, and the West Linn Planning Commission, to deliberate and consider:

1) The owner of the property immediately adjacent to the northern portion of the project will be impacted by the proposed new structures, from a visual perspective, as the height and close proximity will greatly intrude and inhibit the owners current environment, including diminished solar access to their gardens and grounds.

The Association is very aware and supports the concept “your personal view ends at the property line”. However; in this specific case, we would suggest relocating the proposed new structure to the opposite end of the property. The three story building would be more aesthetically in line with the existing apartment complex, situated adjacent to the southern portion of the property, rather than the single residential units to the north.

The addition of plantings would seem an appropriate buffer along the northern and western reaches of the property line.

2) As you know the issue concerning the installation of curbs and sidewalks, or the “in-lieu program”, is a contentious matter with many in the Neighborhood. (Please see the attached letter to Mayor Axelrod, dated June 19, 2018).

Page 2 of 2  
Letter to Mr. John Williams  
August 14, 2018

The Neighborhood Association is about to embark on an initiative, in conjunction with the City, to revise those portions of the Robinwood Neighborhood Plan dealing with the installation of curbs and sidewalks. While this process will take some time, it is our hope to maintain the current rural fabric enjoyed by the neighborhood.

In the interim, in this case, we would ask that consideration be given to alternatives such as a permeable, more natural pathway if it is determined such an installation is required for the safety of the school children

The members of the Robinwood Neighborhood appreciate the opportunity to review the proposal by The Marylhurst School representatives, and we wish them success in their endeavor.

Thank you for your consideration of our comments.

Best regards,

/s/ Jim

James T. O'Toole  
President

Attachment: RNA Letter to Mayor R. Axelrod  
June 19, 2018

**Robinwood Neighborhood Association Board Members**

Jim O'Toole, President      Kazi Ahmed, Vice-president  
Jenne Henderson, Secretary      Christine Steel, Treasurer  
Sharon Pullmann, Ambassador

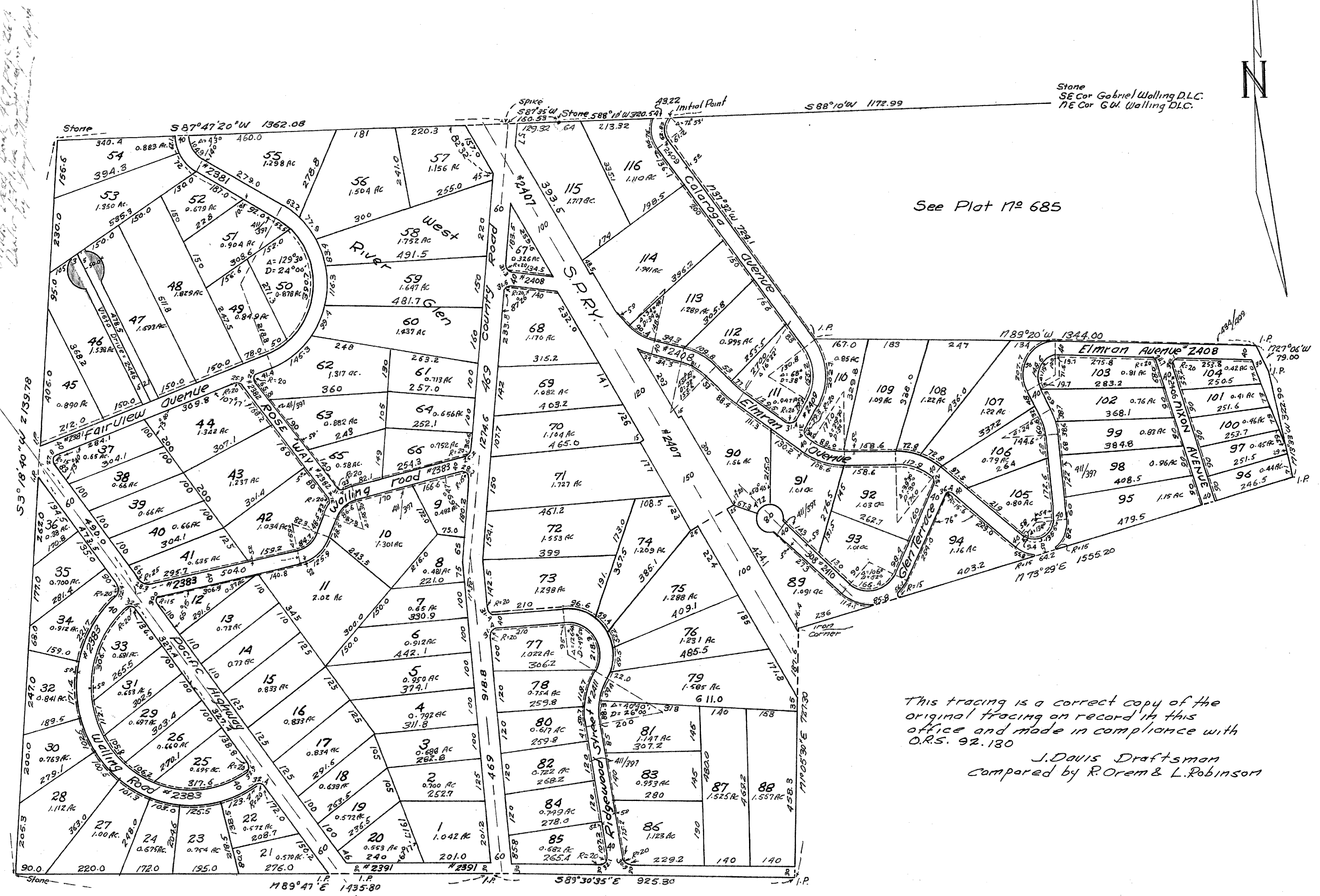
# MAP OF CEDAROK PARK

Situated in G.W. WALLING D.L.C.  
T2S. R1E. W1M.

CLACKAMAS COUNTY, OREGON

Surveyed September, 1922. U. Ernest Nelson, Engr.  
Scale: 1 inch = 200ft.

Note:  
See Corrected plot (Henderson's)  
Cedarok Park  
Deed Records Book 40A-Page 230



See Plot 172 685

This tracing is a correct copy of the original tracing on record in this office and made in compliance with O.R.S. 92.130

J. Davis Draftsman  
compared by R. Orem & L. Robinson

Note: Although this tracing is certified as traced from the original tracing, it is apparent the original tracing had been modified (about 1957) from data of original plat. See Co. Clerk plat book 16, page 8 for original detail. Changes, in general, are supplementary information i.e.; Altered road widths shown by dashed lines, County Road numbers, deed book and page references, the location of Vista Drive within tracts 46 and 47, the title "West River Glen" in tracts 58, 59 and 60, and the "Corrected plat" note at upper left.

*Handwritten notes:*  
Cable was inserted  
see Co. Clerk File # 2408  
on incorrect plat into  
Cedarok Park Plat Book  
Center for original tracing  
8/14/75  
Dexter E. Miller C.S.

468

(Deed) 468

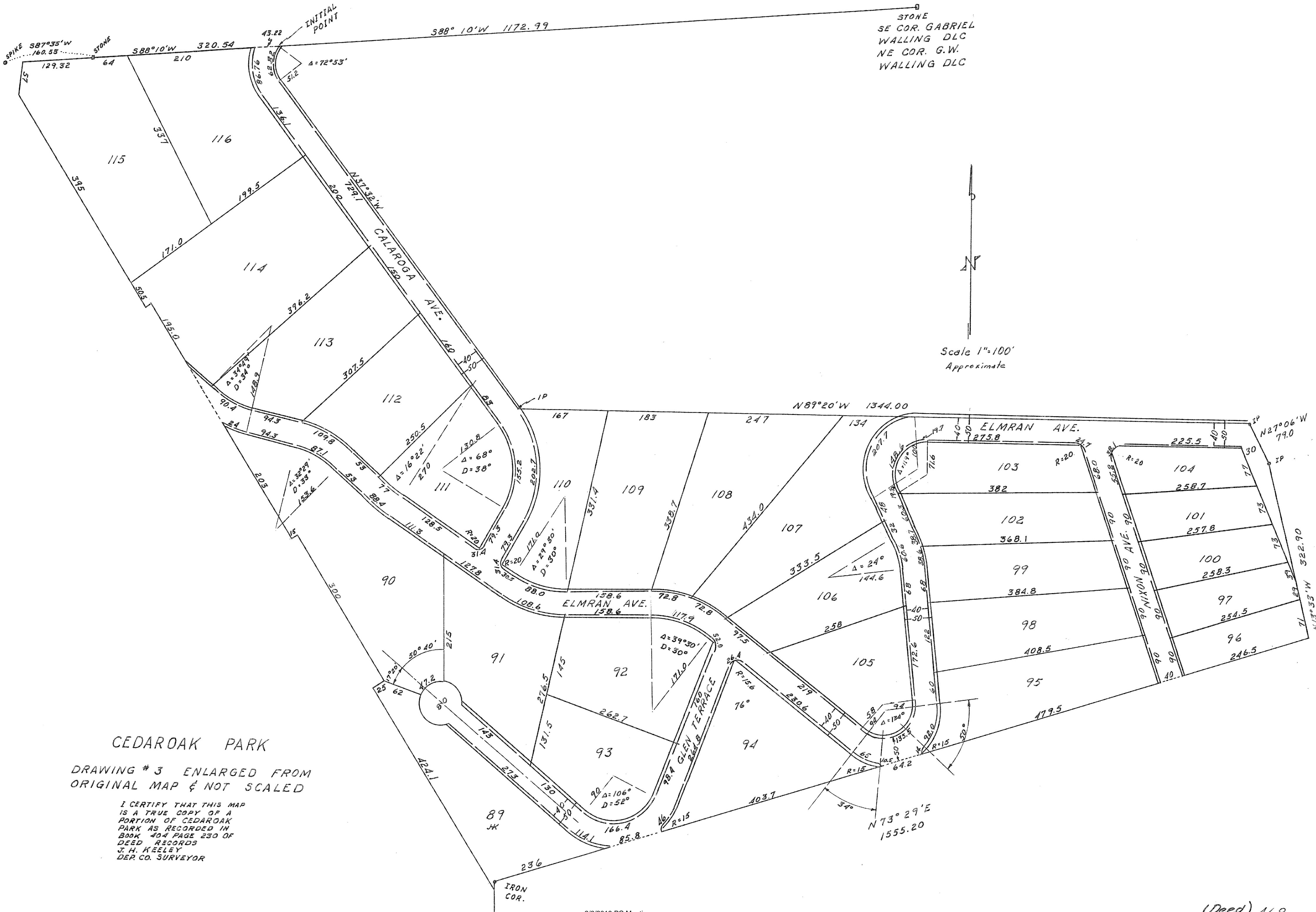


**CEDAR OAK PARK**  
 DRAWING #1 ENLARGED  
 FROM ORIGINAL MAP AND  
 NOT SCALED.

I certify that this map is a  
 true copy of a portion of Cedar-  
 oak Park as recorded in Book 404  
 Page 230 of Deed records.  
 Traced by J.H. Keeley Dep. Co. Surveyor  
 Joe Davis

1 1/2" x 4" IP reset by  
 intersection  
 8 Jan. 1957  
 Present  
 J.H. Keeley  
 L.R. Robinson  
 L. Henderson  
 PK nail top 15" stump

(Deed) 468



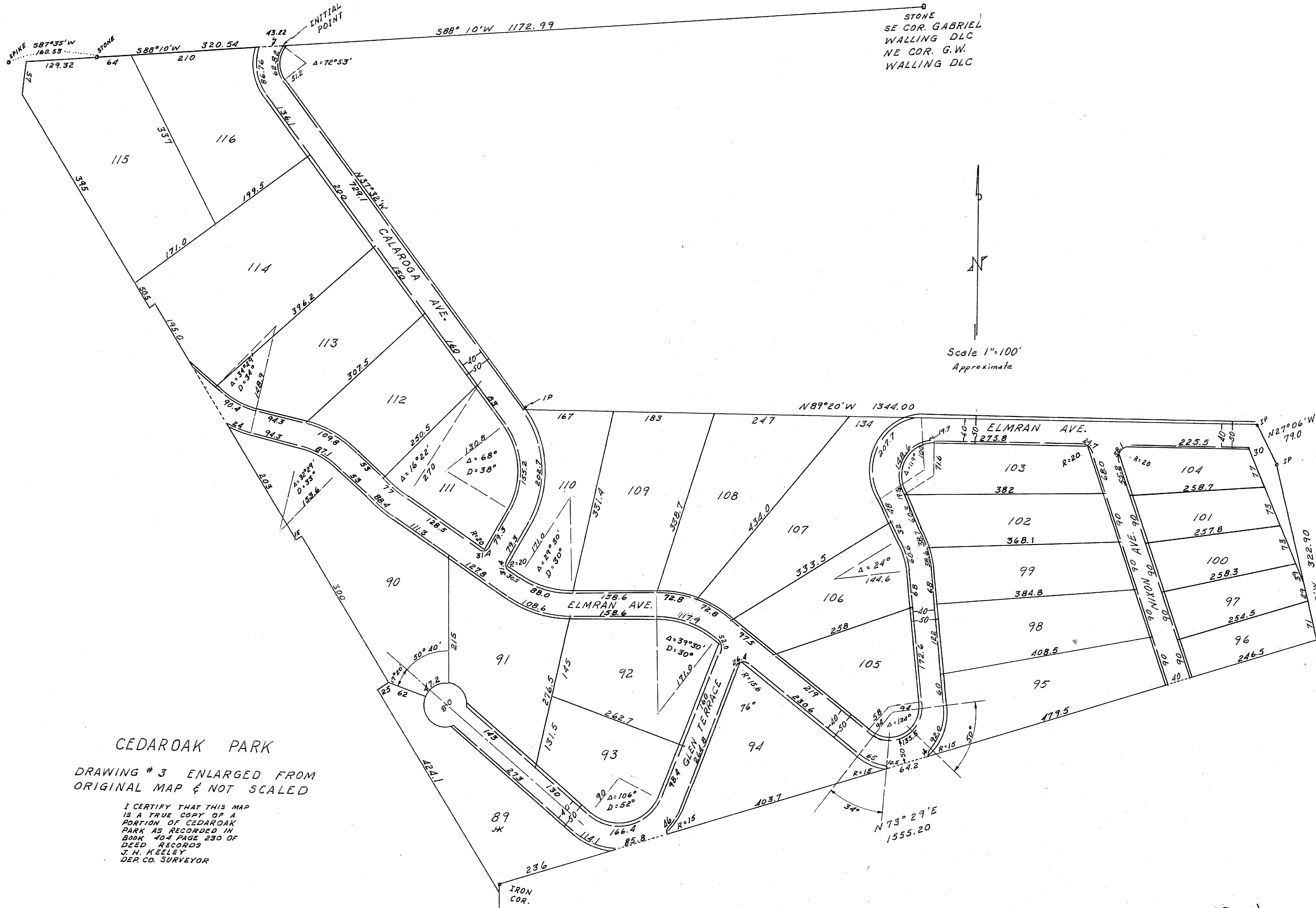
CEDAROAK PARK

DRAWING # 3 ENLARGED FROM ORIGINAL MAP & NOT SCALED

I CERTIFY THAT THIS MAP IS A TRUE COPY OF A PORTION OF CEDAROAK PARK AS RECORDED IN BOOK 404 PAGE 230 OF DEED RECORDS J. H. KEELEY DEP. CO. SURVEYOR



(Deed) 468



**CEDAR OAK PARK**  
 DRAWING # 3 ENLARGED FROM  
 ORIGINAL MAP & NOT SCALED

I CERTIFY THAT THIS MAP  
 IS A TRUE COPY OF A  
 PORTION OF CEDAR OAK  
 PARK AS RECORDED IN  
 BOOK 404 PAGE 230 OF  
 DEED RECORDS  
 J. H. KEELEY  
 DEP. CO. SURVEYOR

**CEDAR OAK  
PARK**

498

KNOW ALL MEN BY THESE PRESENTS that Province Of The Holy Name, a corporation organized and existing under the laws of the state of California and authorized to transact business in the state of Oregon does hereby declare the annexed map to be a true and correct map of the land owned and laid out by them as Said land being more particularly described in the engineer's certificate hereunto annexed and the said Province Of The Holy Name does hereby dedicate to the use of the public forever all streets and highways shown on said map.

IN WITNESS WHEREOF the Province Of The Holy Name, by the authority of its board of directors, has caused these presents to be executed by the provincial and secretary of the said corporation and has caused the corporate seal to be hereunto affixed.

Province of The Holy Name,  
By Arthur L. McMahon Provincial.  
By W.T. Lewis Secretary.

Witness: J. D. Bruen  
V.H. Palmer



STATE OF CALIFORNIA }  
COUNTY OF SAN FRANCISCO } ss.

This certifies that on this 14<sup>th</sup> day of September, 1922, before me a notary public in and for said county and state, personally appeared Arthur L. McMahon and W.T. Lewis to me personally known. Who first being duly sworn did say, that he the said Arthur L. McMahon is the provincial, and that he the said W.T. Lewis is the secretary, of the Province of the Holy Name, the corporation above named and, that the seal affixed to the above instrument is the corporate seal of the said corporation, and that the said instrument was signed and sealed in behalf of said corporation by authority of its board of directors, and they acknowledge said instrument to be the act and deed of said corporation.

Witness my hand and official seal.

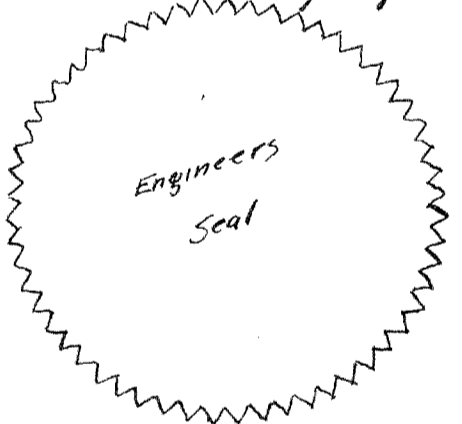
Chester Wilkinson  
Notary public for California  
My commission expires December 21, 1925.



I, U. Ernest Nelson, being first duly sworn on oath depose and say that I am a civil engineer by profession, that I have correctly surveyed the lands represented on the annexed map, that the initial point of said survey is a galvanized iron pipe 2 inches in diameter and 36 inches long driven 6 inches below the surface of the ground which point is 988°10'W 1172.99 feet from the N.E. corner of the G.W. Walling D.L.C. in Twp. 2 S., R. 1 E. W.M. thence from above described point 588°10'W 320.54 feet to a stone, thence S. 87°33'W. 160.53 feet to an iron spike in the E of the county road, thence 987°47'20" W. 1362.08 feet to a stone at the northerly N.W. corner of the G.W. Walling D.L.C., thence S. 3°18'40" W. 2139.78 feet to a stone, thence N 89°47'E. 1435.8 feet to an I.P., thence S 89°30'35"E 929.1 feet to an I.P., thence N1°05'30"E 727.30 feet to an iron bar, thence N. 73°29'E 1555.2 feet to an I.P., thence N13°33'W 322.9 feet to an I.P. thence N 27°06'W 79.0 feet to an I.P., thence N 89°20'W 1344.0 feet to an I.P., thence N 37°32' W 729.1 feet to an I.P., thence following curve to right with 72°53' angle and 51.2 feet radius, 62.82 feet to place of beginning.

Subscribed and sworn to before me this 22<sup>nd</sup> day of Sept. 1922

U. Ernest Nelson  
B. Lee Paget  
Notary Public for Oregon  
My commission expires October 3<sup>rd</sup> 1924.



Approved Oct 3<sup>rd</sup> 1922  
W. C. Criss County Judge  
W. W. Lewis  
County Commissioners.

All taxes inclusive are paid from 1911 to First 1/2 1921  
W. J. Wilson Sheriff.  
J. D. Taylor Deputy.

Attest Fred J. Miller County Clerk.  
By A. H. Pace Deputy.

Approved Oct 3 1922.  
W. B. Cook Assessor.

Approved Oct 3<sup>rd</sup> 1922  
City Planning Commission of Portland, Oregon.  
By A. C. Lewis President.

Approved Oct 3<sup>rd</sup> 1922  
D. H. Multrum County Surveyor.

468

468



150 Beaver Creek Rd  
Oregon City, OR 97045  
503-655-8671

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## Property Account Summary

1/9/2018

|                |          |                  |                                          |
|----------------|----------|------------------|------------------------------------------|
| Account Number | 00360601 | Property Address | 19915 OLD RIVER DR , WEST LINN, OR 97068 |
|----------------|----------|------------------|------------------------------------------|

### General Information

|                         |                                                        |
|-------------------------|--------------------------------------------------------|
| Alternate Property #    | 21E23AA00500                                           |
| Property Description    | 468 CEDAROAK PK PT LT 2&3 SEE RELATED PROPERTY 00500E1 |
| Last Sale Price         | \$0.00                                                 |
| Last Sale Date          | 01/10/2006                                             |
| Last Sale Excise Number | 133843                                                 |
| Property Category       | Land &/or Buildings                                    |
| Status                  | Active, Locally Assessed                               |
| Tax Code Area           | 003-002                                                |
| Remarks                 |                                                        |

### Property Characteristics

|                       |                                     |
|-----------------------|-------------------------------------|
| Neighborhood          | 15841: Calaroga/Cedar Oaks 100, 101 |
| Land Class Category   | 101: Residential land improved      |
| Acreage               | 0.00                                |
| Change property ratio | 9XX                                 |
| Not in CPR Calc       | Multiple Chg's                      |

### Property Details

| Living Area Sq Ft | Manf Struct Size | Year Built | Improvement Grade | Stories | Bedrooms | Full Baths | Half Baths |
|-------------------|------------------|------------|-------------------|---------|----------|------------|------------|
|                   |                  |            |                   |         |          |            |            |

### Parties

| Role     | Percent | Name                         | Address                          |
|----------|---------|------------------------------|----------------------------------|
| Taxpayer | 100     | NEW LIFE CHURCH<br>ROBINWOOD | PO BOX 5, WEST LINN,<br>OR 97068 |
| Owner    | 100     | NEW LIFE CHURCH<br>ROBINWOOD | PO BOX 5, WEST LINN,<br>OR 97068 |

2/6/2019 PC Meeting

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**Property Values**

| Value Type             | Tax Year<br>2017 | Tax Year<br>2016 | Tax Year<br>2015 | Tax Year<br>2014 | Tax Year<br>2013 |
|------------------------|------------------|------------------|------------------|------------------|------------------|
| AVR Total              | \$524,669        | \$421,620        | \$409,340        | \$397,417        | \$385,842        |
| Exempt                 | \$524,669        | \$421,620        | \$409,340        | \$397,417        | \$385,842        |
| TVR Total              |                  |                  |                  |                  |                  |
| Real Mkt Land          | \$312,041        | \$204,055        | \$160,998        | \$147,893        | \$136,660        |
| Real Mkt Bldg          | \$422,790        | \$419,340        | \$396,550        | \$364,640        | \$319,060        |
| Real Mkt Total         | \$734,831        | \$623,395        | \$557,548        | \$512,533        | \$455,720        |
| M5 Mkt Land            | \$312,041        | \$204,055        | \$160,998        | \$147,893        | \$136,660        |
| M5 Mkt Bldg            | \$422,790        | \$419,340        | \$396,550        | \$364,640        | \$319,060        |
| M5 SAV                 |                  |                  |                  |                  |                  |
| SAVL (MAV Use Portion) |                  |                  |                  |                  |                  |
| MAV (Market Portion)   | \$524,669        | \$421,620        | \$409,340        | \$397,417        | \$385,842        |
| Mkt Exception          |                  |                  |                  |                  |                  |
| AV Exception           |                  |                  |                  |                  |                  |

**Tax Rate**

| Description | Rate |
|-------------|------|
| Total Rate  |      |

**Tax Balance**

No Available Tax Charges Information for this Property at the Moment.

[Installments Payable/Paid for Tax Year\(Enter 4-digit Year, then Click-Here\):](#)

**Parents**

| Parcel No.       | Seg/Merge No. | Status | From Date | To Date | Continued | Document Number |
|------------------|---------------|--------|-----------|---------|-----------|-----------------|
| No Parents Found |               |        |           |         |           |                 |

**Children**

| Parcel No. | Seg/Merge No. | Status    | From Date  | To Date | Document Number |
|------------|---------------|-----------|------------|---------|-----------------|
| 05031867   | SM170494      | Completed | 01/02/2016 | Active  | 21E23AA00500    |

**Related Properties**

No Related Properties Found

**Active Exemptions**

Religious

**Events**

| Effective Date | Entry Date-Time        | Type                | Remarks                                                                                         |
|----------------|------------------------|---------------------|-------------------------------------------------------------------------------------------------|
| 07/19/2017     | 07/19/2017<br>08:23:00 | Seg/Merge Completed | Parent in Seg/Merge SM170494, Effective: 01/02/2016 by DROME                                    |
| 07/19/2017     | 07/19/2017<br>08:21:00 | Seg/Merge Initiated | SEG/MERGE BEGUN ON SM170494 EXC TL 00500E1 FROM TL 00500 BY LTR 7-11-2017, EFF 2017-18 BY DROME |

|            |                        |                                   |                                                                                                        |
|------------|------------------------|-----------------------------------|--------------------------------------------------------------------------------------------------------|
| 01/11/2006 | 01/11/2006<br>10:59:00 | Tax Bill Recalculation            | Error or Omission for 2005 performed by JUDYHAM TRC 2005-0653                                          |
| 12/15/2005 | 01/10/2006<br>14:47:00 | Recording Processed               | Property Transfer Filing No.: 133843, Letter 12/15/2005 by LAURIEB                                     |
| 12/15/2005 | 01/10/2006<br>14:47:00 | Taxpayer Changed                  | Property Transfer Filing No.: 133843 12/15/2005 by LAURIEB                                             |
| 08/20/2004 | 08/24/2004<br>14:38:00 | Taxpayer Changed                  | Property Transfer Filing No.: 103035 08/20/2004 by LINDADUN                                            |
| 08/20/2004 | 08/24/2004<br>14:38:00 | Recording Processed               | Property Transfer Filing No.: 103035, Warranty Deed, Recording No.: 2004-077253 08/20/2004 by LINDADUN |
| 04/05/2004 | 04/05/2004<br>10:04:00 | Annexation Completed For Property | Annex to TVFR, Ord 03-13 for 2004-Revise TCA Membership by JENMAYO                                     |
| 12/12/2002 | 12/12/2002<br>12:19:00 | The situs address has changed     | by LINDAPET                                                                                            |
| 07/01/1999 | 07/01/1999<br>12:00:00 | Ownership at Conversion           | Warranty Deed: 94-24436, 3/1/94, \$ 440000                                                             |

### Receipts

| Date              | Receipt No. | Amount Applied | Amount Due | Tendered | Change |
|-------------------|-------------|----------------|------------|----------|--------|
| No Receipts Found |             |                |            |          |        |

### Sales History

| Transfer Date | Receipt Date | Recording Number | Sale Amount  | Excise Number | Deed Type | Transfer Type | Grantor (Seller)               | Grantee (Buyer)           | Other Parcels |
|---------------|--------------|------------------|--------------|---------------|-----------|---------------|--------------------------------|---------------------------|---------------|
| 12/15/2005    | 01/10/2006   |                  | \$0.00       | 133843        |           | X             | WEST LINN BAPTIST CHURCH       | NEW LIFE CHURCH ROBINWOOD | No            |
| 08/19/2004    | 08/24/2004   | 2004-077253      | \$525,000.00 | 103035        |           | M             | WEST LINN CHRISTIAN FELLOWSHIP | WEST LINN BAPTIST CHURCH  | No            |
| 03/01/1994    |              | 1994-024436      | \$440,000.00 | 94-24436      |           |               |                                |                           | No            |
| 06/01/1989    |              | 1989-030157      | \$24,548.00  | 89-30157      |           |               |                                |                           | No            |

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## Property Account Summary

1/9/2018

|                |          |                  |                                          |
|----------------|----------|------------------|------------------------------------------|
| Account Number | 05031867 | Property Address | 19915 OLD RIVER DR , WEST LINN, OR 97068 |
|----------------|----------|------------------|------------------------------------------|

### General Information

|                         |                                                         |
|-------------------------|---------------------------------------------------------|
| Alternate Property #    | 21E23AA00500E1                                          |
| Property Description    | 468 CEDAROAK PARK PT LTS 2&3 SEE RELATED PROPERTY 00500 |
| Last Sale Price         |                                                         |
| Last Sale Date          |                                                         |
| Last Sale Excise Number |                                                         |
| Property Category       | Land &/or Buildings                                     |
| Status                  | Active, Locally Assessed                                |
| Tax Code Area           | 003-002                                                 |
| Remarks                 |                                                         |

### Property Characteristics

|                       |                                     |
|-----------------------|-------------------------------------|
| Neighborhood          | 15841: Calaroga/Cedar Oaks 100, 101 |
| Land Class Category   | 101: Residential land improved      |
| Change property ratio | 9XX                                 |
| Not in CPR Calc       | Multiple Chg's                      |

### Property Details

| Living Area Sq Ft | Manf Struct Size | Year Built | Improvement Grade | Stories | Bedrooms | Full Baths | Half Baths |
|-------------------|------------------|------------|-------------------|---------|----------|------------|------------|
|                   |                  |            |                   |         |          |            |            |

### Parties

| Role          | Percent | Name                         | Address                       |
|---------------|---------|------------------------------|-------------------------------|
| Taxpayer      | 100     | NEW LIFE CHURCH<br>ROBINWOOD | PO BOX 5, WEST LINN, OR 97068 |
| Owner         | 100     | NEW LIFE CHURCH<br>ROBINWOOD | PO BOX 5, WEST LINN, OR 97068 |
| Exempt Tenant |         | 2/6/2018 PC Meeting          |                               |

|                                  |                                    |
|----------------------------------|------------------------------------|
| WEST LINN COMMUNITY<br>PRESCHOOL | PO BOX 213, WEST<br>LINN, OR 97068 |
|----------------------------------|------------------------------------|

**Property Values**

| Value Type             | Tax Year<br>2017 | Tax Year<br>2016 | Tax Year<br>2015 | Tax Year<br>2014 | Tax Year<br>2013 |
|------------------------|------------------|------------------|------------------|------------------|------------------|
| AVR Total              | \$46,765         |                  |                  |                  |                  |
| Exempt                 | \$46,765         |                  |                  |                  |                  |
| TVR Total              |                  |                  |                  |                  |                  |
| Real Mkt Land          | \$27,907         |                  |                  |                  |                  |
| Real Mkt Bldg          | \$37,590         |                  |                  |                  |                  |
| Real Mkt Total         | \$65,497         |                  |                  |                  |                  |
| M5 Mkt Land            | \$27,907         |                  |                  |                  |                  |
| M5 Mkt Bldg            | \$37,590         |                  |                  |                  |                  |
| M5 SAV                 |                  |                  |                  |                  |                  |
| SAVL (MAV Use Portion) |                  |                  |                  |                  |                  |
| MAV (Market Portion)   | \$46,765         |                  |                  |                  |                  |
| Mkt Exception          |                  |                  |                  |                  |                  |
| AV Exception           |                  |                  |                  |                  |                  |

**Tax Rate**

| Description | Rate |
|-------------|------|
| Total Rate  |      |

**Tax Balance**

No Available Tax Charges Information for this Property at the Moment.

[Installments Payable/Paid for Tax Year\(Enter 4-digit Year, then Click-Here\):](#)

**Parents**

| Parcel No. | Seg/Merge No. | Status    | From Date  | To Date | Continued | Document Number |
|------------|---------------|-----------|------------|---------|-----------|-----------------|
| 00360601   | SM170494      | Completed | 01/01/1980 | Active  | Y         | 21E23AA00500    |

**Children**

| Parcel No.        | Seg/Merge No. | Status | From Date | To Date | Document Number |
|-------------------|---------------|--------|-----------|---------|-----------------|
| No Children Found |               |        |           |         |                 |

**Related Properties**

No Related Properties Found

**Active Exemptions**

Day Care

**Events**

| Effective Date | Entry Date-Time        | Type                    | Remarks                                                                    |
|----------------|------------------------|-------------------------|----------------------------------------------------------------------------|
| 07/19/2017     | 07/19/2017<br>08:23:00 | Created by<br>Seg/Merge | Created by Seg/Merge SM170494, Effective: 01/02/2016<br>2/6/2019 by DEONIE |

**Receipts**

| <b>Date</b> | <b>Receipt No.</b> | <b>Amount Applied</b> | <b>Amount Due</b> | <b>Tendered</b> | <b>Change</b> |
|-------------|--------------------|-----------------------|-------------------|-----------------|---------------|
|-------------|--------------------|-----------------------|-------------------|-----------------|---------------|

No Receipts Found

**Sales History**

| <b>Transfer Date</b> | <b>Receipt Date</b> | <b>Recording Number</b> | <b>Sale Amount</b> | <b>Excise Number</b> | <b>Deed Type</b> | <b>Transfer Type</b> | <b>Grantor (Seller)</b> | <b>Grantee (Buyer)</b> | <b>Other Parcels</b> |
|----------------------|---------------------|-------------------------|--------------------|----------------------|------------------|----------------------|-------------------------|------------------------|----------------------|
|----------------------|---------------------|-------------------------|--------------------|----------------------|------------------|----------------------|-------------------------|------------------------|----------------------|

No Sales History Found

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## Property Account Summary

1/9/2018

|                |          |                  |                                          |
|----------------|----------|------------------|------------------------------------------|
| Account Number | 00360594 | Property Address | 19803 OLD RIVER DR , WEST LINN, OR 97068 |
|----------------|----------|------------------|------------------------------------------|

### General Information

|                      |                          |
|----------------------|--------------------------|
| Alternate Property # | 21E23AA00400             |
| Property Description | 468 CEDAROAK PK PT LT 4  |
| Property Category    | Land &/or Buildings      |
| Status               | Active, Locally Assessed |
| Tax Code Area        | 003-002                  |
| Remarks              |                          |

### Property Characteristics

|                       |                                     |
|-----------------------|-------------------------------------|
| Neighborhood          | 15841: Calaroga/Cedar Oaks 100, 101 |
| Land Class Category   | 100: Residential land, vacant       |
| Change property ratio | 9XX                                 |

### Property Details

| Living Area Sq Ft | Manf Struct Size | Year Built | Improvement Grade | Stories | Bedrooms | Full Baths | Half Baths |
|-------------------|------------------|------------|-------------------|---------|----------|------------|------------|
|                   |                  |            |                   |         |          |            |            |

### Property Values

| Value Type     | Tax Year 2017 | Tax Year 2016 | Tax Year 2015 | Tax Year 2014 | Tax Year 2013 |
|----------------|---------------|---------------|---------------|---------------|---------------|
| AVR Total      | \$120,105     | \$116,607     | \$113,211     | \$109,914     | \$106,713     |
| Exempt         | \$120,105     | \$116,607     | \$113,211     | \$109,914     | \$106,713     |
| TVR Total      |               |               |               |               |               |
| Real Mkt Land  | \$227,371     | \$206,529     | \$162,949     | \$149,686     | \$138,317     |
| Real Mkt Bldg  |               |               |               |               |               |
| Real Mkt Total | \$227,371     | \$206,529     | \$162,949     | \$149,686     | \$138,317     |
| M5 Mkt Land    | \$227,371     | \$206,529     | \$162,949     | \$149,686     | \$138,317     |
| M5 Mkt Bldg    |               |               |               |               |               |

2/6/2019 PC Meeting  
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|                        |           |           |           |           |           |
|------------------------|-----------|-----------|-----------|-----------|-----------|
| M5 SAV                 |           |           |           |           |           |
| SAVL (MAV Use Portion) |           |           |           |           |           |
| MAV (Market Portion)   | \$120,105 | \$116,607 | \$113,211 | \$109,914 | \$106,713 |
| Mkt Exception          |           |           |           |           |           |
| AV Exception           |           |           |           |           |           |

### Tax Rate

| Description | Rate |
|-------------|------|
| Total Rate  |      |

### Tax Balance

No Available Tax Charges Information for this Property at the Moment.

[Installments Payable/Paid for Tax Year\(Enter 4-digit Year, then Click-Here\):](#)

### Related Properties

No Related Properties Found

### Active Exemptions

Religious

### Events

| Effective Date | Entry Date-Time     | Type                              | Remarks                                                                                                |
|----------------|---------------------|-----------------------------------|--------------------------------------------------------------------------------------------------------|
| 12/24/2014     | 12/24/2014 15:13:00 | The situs address has changed     | by ALEESHAJOE                                                                                          |
| 01/11/2006     | 01/11/2006 10:56:00 | Tax Bill Recalculation            | Error or Omission for 2005 performed by JUDYHAM TRC 2005-0652                                          |
| 12/15/2005     | 01/10/2006 14:47:00 | Taxpayer Changed                  | Property Transfer Filing No.: 133843 12/15/2005 by LAURIEB                                             |
| 12/15/2005     | 01/10/2006 14:47:00 | Recording Processed               | Property Transfer Filing No.: 133843, Letter 12/15/2005 by LAURIEB                                     |
| 08/20/2004     | 08/24/2004 14:38:00 | Recording Processed               | Property Transfer Filing No.: 103035, Warranty Deed, Recording No.: 2004-077253 08/20/2004 by LINDADUN |
| 08/20/2004     | 08/24/2004 14:38:00 | Taxpayer Changed                  | Property Transfer Filing No.: 103035 08/20/2004 by LINDADUN                                            |
| 04/05/2004     | 04/05/2004 10:04:00 | Annexation Completed For Property | Annex to TVFR, Ord 03-13 for 2004-Revise TCA Membership by JENMAYO                                     |
| 07/01/1999     | 07/01/1999 12:00:00 | Ownership at Conversion           | Warranty Deed: 94-24436, 3/1/94, \$ 440000                                                             |

### Receipts

| Date              | Receipt No. | Amount Applied | Amount Due | Tendered | Change |
|-------------------|-------------|----------------|------------|----------|--------|
| No Receipts Found |             |                |            |          |        |

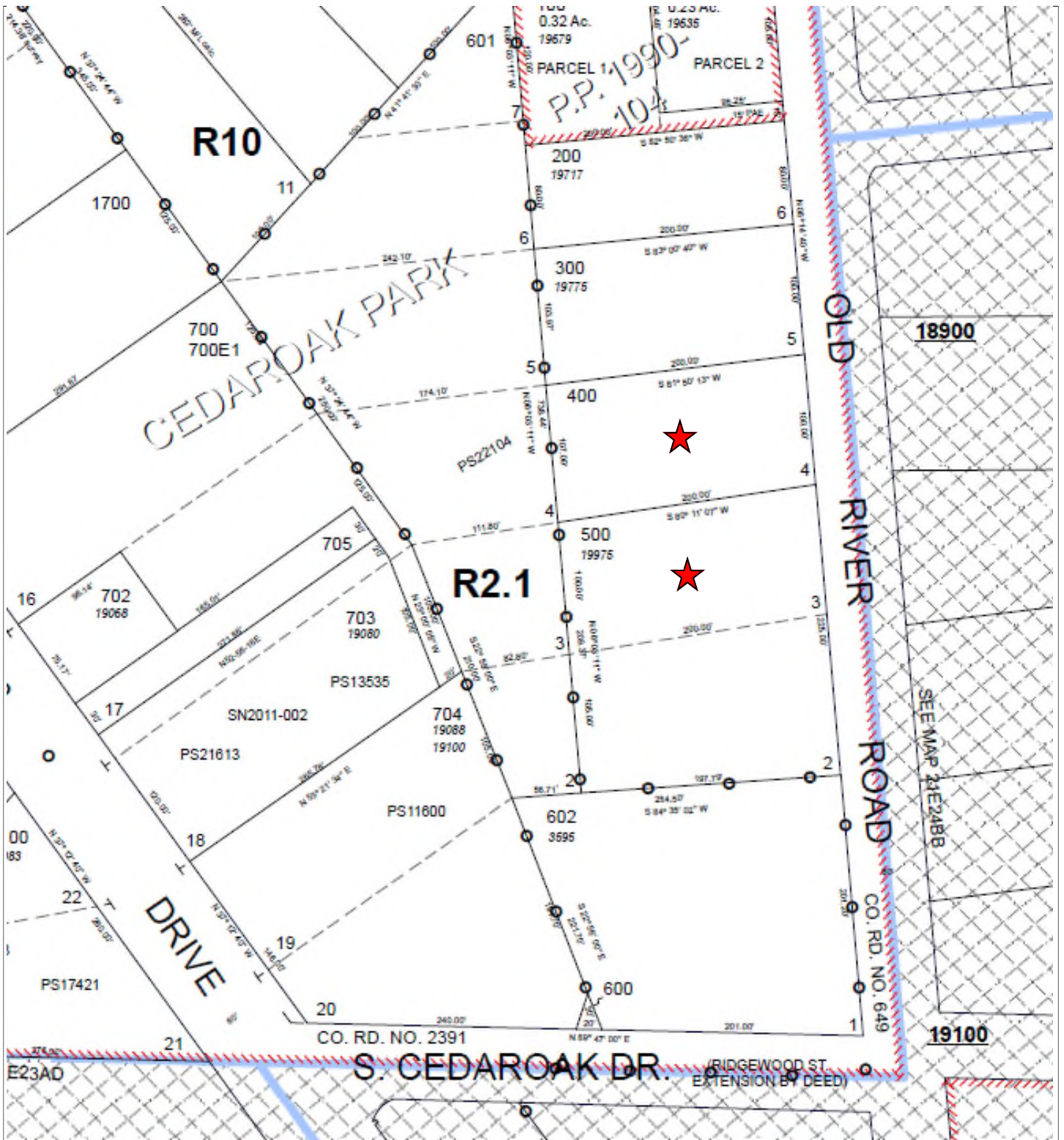
### Sales History

| Transfer Date | Receipt Date | Recording Number | Sale Amount         | Excise Number | Deed Type | Transfer Type | Other Parcels |
|---------------|--------------|------------------|---------------------|---------------|-----------|---------------|---------------|
|               |              |                  | 2/6/2019 PC Meeting |               |           |               |               |
|               |              |                  | Page 358            |               |           |               |               |

|            |            |             |              |          |  |   |    |
|------------|------------|-------------|--------------|----------|--|---|----|
| 12/15/2005 | 01/10/2006 |             | \$0.00       | 133843   |  | M | No |
| 08/19/2004 | 08/24/2004 | 2004-077253 | \$525,000.00 | 103035   |  | X | No |
| 03/01/1994 |            | 1994-024436 | \$440,000.00 | 94-24436 |  |   | No |
| 06/01/1989 |            | 1989-030157 | \$24,548.00  | 89-30157 |  |   | No |

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Order Number: 5512003441



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RESERVATIONS AND RESTRICTIONS IN CEDAROAK PARK  
and CEDAROAK PARK Plat 2.

KNOW ALL MEN BY THESE PRESENTS, That L.A. Henderson and Edna C. Henderson, his wife; and Willard G. Deardorff and Betty Jane Deardorff, his wife, do hereby certify and declare that the following reservations, conditions, covenants and agreements shall become and hereby are made a part of all conveyances of property owned by the parties herein, within the plats of CEDAROAK PARK and CEDAROAK PARK Plat 2, as the same appear in Plats recorded in book 16 at page 8, and book 25 at page 1, record of Town Plats of Clackamas County, Oregon, of which conveyances the following reservations, conditions, covenants and agreements shall become a part by reference hereto and to which they shall thereupon apply as fully and with the same effect as if set forth at large therein, during the period of twenty-five years from date thereof.

These covenants are to run with the land and shall be binding on all parties and all persons claiming under them until the end of said term, at which time said covenants shall be automatically extended for successive periods of ten years unless by vote of a majority of the then owners of the lots it is agreed to change said covenants in whole or in part.

1. All parcels of land therein shall be used exclusively for residential purposes, except those parcels fronting on the Pacific Highway; and on the West Side of the Oswego County road from the south line of the plat to Walling Road, which said parcels fronting the Pacific Highway, and said and that part of the County road aforesaid may be used for residential purposes.

2. No residential buildings including dwellings, houses and apartment houses shall exceed two and one-half stories in height; they may have family garages attached or detached.

3. Minimum set back lines as follows:

Fronting Pacific Highway 60 feet from center line, all other roads 25 feet from center lines; 20 feet from side lines of the old Southern Pacific Right of way; and 10 feet from side lot lines.

4. All out buildings shall be in the rear of the main buildings and no detached garage shall be in front of any building. No out buildings or other structures shall be obnoxious or offensive in character and exterior thereof shall be so constructed and decorated to conform with the general plan of the other buildings, except that said out buildings shall not be of concrete or masonry construction. Play houses or family green houses shall be permitted along the same general plan in the rear of the main buildings.

5. No obnoxious or offensive trade or pursuit shall be carried on upon any tract therein, nor shall anything be done thereon which may be an annoyance or a nuisance to the neighborhood.

6. No trailer, basement, tent, back, garage, barn or other out buildings shall be at any time used for residential purposes, either temporarily or otherwise.

7. Business structures shall not be of wood walls or foundations; shall be of concrete, masonry, or other fire proof material, only as regards walls and foundations.

8. No buildings of any kind shall be placed upon an area of less than 75 feet front by 100 feet in depth, as the same applies to dwelling and business structures.

9. No dwellings costing less than \$7500.00 shall be erected on any part of the land west of the West side of the Oswego County Road. And no dwellings costing less than \$10,000.00 shall be erected on any part of the land east of the East line of the Oswego County Road. (The said Oswego County Road being designated as that certain 60 foot County Road running North and South through the center part of said plat); and no business structure shall be erected at a cost of less than \$5000.00

10. No fence or wall shall be erected to a greater height than four feet, except that suitable fences may be erected on the rear portion of Tracts for confining pets or poultry. All hedges shall be kept pruned back to reasonable heights not exceeding four feet.

11. No persons of other than the Caucasian race shall use or occupy any buildings therein, except that persons of other races may be employed by the owner or tenant as domestic servants.

12. No cows, horses, goats, pigs, rabbits or any other animals except household pets shall be kept on any parcel hereof, except that poultry may be kept in the back of each of said parcels in reasonable numbers for family use. And except that not to exceed three riding horses per family may be kept for family use in suitable quarters on any tract therein lying East of said Oswego County Road.

13. Until such time as a sanitary sewer system has been installed, all sewage disposal shall be by means of septic tanks of a type and in structure, constructions and outlets in accordance with recommendations of the Oregon State Board of Health; and if and when a sanitary sewer has been installed, that means of sewage disposal shall be used exclusively. In no event shall any overflow or drainage from such be permitted to appear above ground or drain onto any street or road or any adjoining property.

14. Any restrictions covering that part of Cedar oak Park lying west of the said Oswego County Road, may be changed or modified by the signed petition or agreement of 75% of the owners therein, and any restrictions covering that part of Cedar oak Park lying east of said Oswego County Road may be changed or modified by the signed petition or agreement of 75% of the owners therein, duly placed on record in the deed records of Clackamas County, Oregon.

15. Invalidity of any one of these covenants by judgment or court order shall in no wise affect any or the other provision which shall remain in full force and effect.

16. Any breach of any covenant herein shall not work a forfeiture of the land conveyed in fee simple, but such breach shall give the grantor any owner of land in said plat the right to compel performance of these covenants, and to abate or remove any structure erected in violation thereof, or any other violation through any court having jurisdiction thereof.

WITNESSED at Oregon City, Oregon, this 20th day of February, 1943

WITNESSES our hands and seals the date above mentioned.

*Walter D. ...*  
*Betty ...*

*Edna E. ...*

State of Oregon )  
County of Clatsop ) ss

BE IT REMEMBERED, that on this 27th day of February, 1928, before me the undersigned, a Notary Public in and for said County and State, personally appeared the within named L.A. Henderson and Edna M. Henderson, his wife; and Willard G. Beardorff and Betty Jane Beardorff, his wife, and are known to me to be the identical individuals described in and who executed the within instrument, and acknowledged to me that they executed the same freely and voluntarily.

*Kenneth R. Johnson*

Notary Public in and for Oregon  
By Commission Expires 1/1/31

WEDNESDAY FEBRUARY 27 1928

Recording Clerk

*Willard G. Beardorff*  
Deputy

Notary

Notary

1928 SEP 20 AM 10 35

DEED

STATE OF OREGON  
County of Clatsop  
I, Ray B. Price, County Clerk, do hereby certify that the within instrument of February 27 1928 has been duly recorded in the office of the County Clerk of Clatsop County, Oregon, and is a true and correct copy of the original as the same is on file in the office of the County Clerk of Clatsop County, Oregon.

AMENDED RESERVATIONS AND RESTRICTIONS  
Cedar Oak Park

WHEREAS, certain Reservations and Restrictions affecting Cedar Oak Park were recorded in Book 411 of Deeds at pg. 392 and in said instrument the cost of dwelling houses lying west of the Oswego county road known as Old River Road, was restricted to not less than \$7500.00 and

WHEREAS, it is the desire of the owners of that part of Cedar Oak Park lying west of said Old River Road to amend said Reservations and Restrictions insofar as it affects Tracts numbered 1, 2, 3, 4, 5, 14, 15, 16, 17, 18, 19 and 20 of Cedar Oak Park, according to the fully recorded plat thereof.

NOW THEREFORE it is agreed by the owners of said tracts, immediately enumerated above, that said Reservations and Restrictions be amended to read as follows: That a dwelling house shall be erected on each of said tracts, to wit, 1, 2, 3, 4, 5, 14, 15, 16, 17, 18, 19 and 20, with the first floor area of not less than five hundred square feet.

AND that said Reservations and Restrictions as in said Book 411 of Deeds at pg. 392, shall remain in full force and effect except as to that part hereof amended as above.

Dated Oregon Day, the 24th day of July, 1957.

*L. A. Henderson*  
L. A. Henderson  
Her attorney in fact

*Willard G. Deardorff*  
Willard G. Deardorff  
By *Betty Jane Deardorff*  
Her attorney in fact

State of Oregon )  
County of Clackamas ) ss.

On this 24th day of July, 1957, before me, the undersigned, a Notary Public in and for said county and state personally appeared the within named L. A. Henderson, personally and as attorney in fact for his wife, Edna C. Henderson; Willard G. Deardorff, personally and as attorney in fact for his wife, Betty Jane Deardorff, who acknowledged to me that they executed the within instrument for the purposes therein mentioned.



*Stances Wagner*  
Notary Public for Oregon

Residing at My Commission Expires Sept. 10, 1962  
Commission expires:

SEAL DOCUMENT 11359 RECORDED JUL 31 1957 8:56 A.M. GUY H. PAGE COUNTY CLERK



KNOW ALL MEN BY THESE PRESENTS, That B. F. Graybill and Gayle E. Graybill, husband and wife and Charles Boelers and Leona M. Boelers, husband and wife, grantor, in consideration of Ten and No/100 Dollars,

to them paid by The Calvary Baptist Church of Cedar Oak Park, an Oregon corporation

do hereby grant, bargain, sell and convey unto the said grantee, its heirs and assigns, all the following real property, with the tenements, hereditaments and appurtenances, situated in the County of Clackamas and State of Oregon, bounded and described as follows, to-wit:

726

Part of Lots 2, 3 and 4, CEDAR OAK PARK, in the County of Clackamas and State of Oregon, described as follows: Beginning at the northeast corner of said Lot 4, thence westerly on the northerly line of said Lot 4, a distance of 200 feet; thence southerly parallel with the easterly boundary of said Lots 2, 3 and 4 to a point on the southerly boundary of said Lot 2; thence easterly along the southerly boundary of said Lot 2 to the southeast corner thereof; thence northerly along the easterly boundary of Lots 2, 3 and 4, a distance of 325 feet to the point of beginning, reserving however, an easement for sewer line over the north 5 (five) feet of said Lot 4 together with the right of access to construct and maintain the same.

To Have and to Hold the above described and granted premises unto the said grantee, its heirs and assigns forever.

And the grantor do covenant that they are lawfully seized in fee simple of the above granted premises free from all encumbrances, conditions and restrictions contained in Declaration and amendments.

and that they will and their heirs, executors and administrators, shall warrant and forever defend the above granted premises, and every part and parcel thereof, against the lawful claims and demands of all persons whomsoever.

Witness their hands and seal this 15 day of July, 1960.



STATE OF OREGON,

County of Clackamas, ss. On this 15 day of July, 1960,

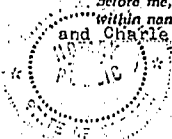
before me, the undersigned, a Notary Public in and for said County and State, personally appeared the within named B. F. Graybill and Gayle E. Graybill, husband and wife and Charles Boelers and Leona M. Boelers, husband and wife who are

known to me to be the identical individuals described in and who executed the within instrument, and acknowledged to me that they executed the same freely and voluntarily.

IN TESTIMONY WHEREOF, I have hereunto set my hand and affixed my official seal the day and year last above written.

Notary Public for Oregon.

My commission expires Oct. 20, 1960.



WARRANTY DEED

STATE OF OREGON,

TO

AFTER RECORDING RETURN TO

Robert Schumacher  
Book 574 Page 328

STATE OF OREGON, ss. County of Clackamas, I, Robert Schumacher, County Clerk, Ex-Officio Recorder of Conveyances and Ex-Officio Clerk of the Court of the State of Oregon, do hereby certify that the within instrument of writing was received for record and recorded in the records of

DEED of said County at

1960 JUL 21 PM 2 36

In Book FILE 574 PAGE 328

Witness my hand and seal of said Court: ROBERT SCHUMACHER, County Clerk.

Deputy: Recording Certificate

12990  
DOCKET NO.  
583

6

10

EASEMENT

The undersigned, CALVARY BAPTIST CHURCH

Grantors, for the consideration of One and no/100 Dollars to Grantors paid, the receipt of which is hereby acknowledged, do hereby grant unto the CITY OF WEST LINN, a municipal corporation, its successors and assigns, referred to herein as CITY, a permanent right-of-way and easement to construct, reconstruct, operate and maintain utilities and all necessary related facilities under and along the following described premises:

(See Reverse for Description)

The permanent right-of-way or easement shall include the right, privilege and authority of CITY to excavate for, and to construct, install, lay, operate, maintain and remove underground pipelines and/or cables with all appurtenances incident thereto or necessary thereafter, for the purpose of supplying public utility service under and across the said premises, together with the right of CITY to place, install, maintain, inspect, add to the number of and relocate pipelines and/or cables and necessary appurtenances and make excavations therefor from time to time, in, under and through the above described premises within said right-of-way, and to cut and remove from said right-of-way any trees and other obstructions which may endanger the safety or interfere with the use of said pipelines and/or cables or appurtenances attached to or connected therewith; and the right of ingress and egress to and over said above described premises at any and all times for the purpose of patrolling the pipelines and/or cables, or repairing, renewing or adding to the number of pipelines and/or cables and appurtenances and for doing anything necessary, useful or convenient for the enjoyment of the easement hereby granted.

CITY, upon the initial installation and upon each and every occasion that the same be repaired or removed shall restore the premises of the Grantor by removing all debris and leaving the ground surface in a neat and presentable condition; buildings and improvements to be restored as near as possible to as good a condition as the same were prior to any such installation. The only other persons, firms, or corporations known by Grantors to have any interest in the granted property are:

Dated this 27th day of October, 1977.



CALVARY BAPTIST CHURCH

BY: Les Olmsted  
Les Olmsted, Chairman  
Board of Directors

STATE OF OREGON )  
County of Clackamas ) ss

October 27, 1977.

Personally appeared the above named Les Olmsted,  
Chairman, Board of Directors, Calvary Baptist Church  
and acknowledged the foregoing instrument to be his voluntary  
act and deed.

Before me:

[Signature]  
Notary Public for Oregon  
My Commission Expires: April 23, 1980

27 45-180

A fifteen (15)-foot strip of land for sanitary sewer purposes.

A tract of land located in the Northeast one-quarter of the Northeast one-quarter of Section 23, Township 2 South, Range 1 East, Willamette Meridian, City of West Linn, Clackamas County, Oregon, more particularly described as follows:

All of that portion of fifteen (15)-foot strip of land lying within that tract conveyed to the Calvary Baptist Church recorded in Book 574, Page 328, Clackamas County Deed Records. The centerline of said fifteen (15)-foot strip of land begins at a point located on the easterly line of Lot 1, CEDAR OAK PARK and six (6) feet southerly of the northeast corner of said Lot 1, said easterly line is also the westerly line of Old River Road, sixty (60) feet wide; thence northwesterly along said centerline to a point on the westerly line of Lot 2, CEDAR OAK PARK, said point is located forty (40) feet southerly, measured along the lot line, of the northwesterly corner of said Lot 2.

Calvary Baptist Church also grants a construction easement of ten (10) feet wide on each side of the above described fifteen (15)-foot strip of land, said construction easement shall expire six (6) months after acceptance of the sanitary sewer by the City of West Linn.

After recording, please return  
to  
CITY OF WEST LINN  
CITY HALL  
WEST LINN, OREGON 97068

STATE OF OREGON )  
County of Clackamas ) ss.

I, George D. Poppen, County Clerk, Ex-Officio  
Recorder of Conveyances and Ex-Officio Clerk  
of the Circuit Court of the State of Oregon, for  
the County of Clackamas, do hereby certify that  
the within instrument of writing was received for  
and recorded in the records of said county at

77 NOV 4 P 3: 19



Witness my hand and seal of office  
GEORGE D. POPPEN  
County Clerk

Recording Certificate  
CCP-R4 77 45480

2

15  
10  
38



After recording return to:  
West Linn Baptist Church  
PO Box 5  
West Linn, OR 97068


Until a change is requested all tax statements shall be sent to the following address:  
West Linn Baptist Church  
PO Box 5  
West Linn, OR 97068

File No.: 7071-421147 (se)  
Date: August 19, 2004

THIS SPACE RESERVED FOR RECORDER'S USE

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**Clackamas County Official Records** **2004-077253**  
**Sherry Hall, County Clerk**


**\$36.00**

00724477200400772530030032 08/20/2004 03:29:43 PM

D-D Cnt=1 Str=4 MELISSA  
 \$15.00 \$11.00 \$10.00

FATCO. NO. 421147 DC

### STATUTORY WARRANTY DEED

**West Linn Christian Fellowship, an Oregon Non-Profit Corporation**, Grantor, conveys and warrants to **West Linn Baptist Church, an Oregon Non-Profit Corporation**, Grantee, the following described real property free of liens and encumbrances, except as specifically set forth herein:

See Legal Description attached hereto as Exhibit A and by this reference incorporated herein.

**This property is free from liens and encumbrances, EXCEPT:**

1. The **2004-2005** Taxes, a lien not yet payable.
2. Covenants, conditions, restrictions and/or easements, if any, affecting title, which may appear in the public record, including those shown on any recorded plat or survey.

THIS INSTRUMENT WILL NOT ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY APPROVED USES AND TO DETERMINE ANY LIMITS ON LAWSUITS AGAINST FARMING OR FOREST PRACTICES AS DEFINED IN ORS 30.930.

The true consideration for this conveyance is **\$525,000.00**. (Here comply with requirements of ORS 93.030)

Dated this 19<sup>th</sup> day of August, 2004.



APN: 00360601

Statutory Warranty Deed  
- continued

File No.: 7071-421147 (se)  
Date: 08/19/2004

**EXHIBIT A**

**LEGAL DESCRIPTION:**

**Parcel I:**

Part of Lots 2, 3 and 4, CEDAR OAK PARK, in the City of West Linn, County of Clackamas and State of Oregon, described as follows:

Beginning at the Northeast corner of said Lot 4; thence Westerly on the Northerly line of said lot 4, a distance of 200 feet; thence Southerly parallel with the Easterly boundary of said Lots 2, 3 and 4, to a point on the Southerly boundary of said Lot 2; thence Easterly along the Southerly boundary of said Lot 2 to the Southeast corner thereof; thence Northerly along the Easterly boundary of Lots 2, 3 and 4, a distance of 325 feet to the point of beginning.

3



# Oregon

Kate Brown, Governor

## Department of Transportation

Region 1 Headquarters  
123 NW Flanders Street  
Portland, Oregon 97209  
(503) 731.8200  
FAX (503) 731.8259

January 18, 2018

ODOT #8147

## ODOT Pre-Application Response

|                                                                    |                                         |
|--------------------------------------------------------------------|-----------------------------------------|
| <b>Project Name:</b> The Marylhurst School - 19915 Old River Drive | <b>Applicant:</b> The Marylhurst School |
| <b>Jurisdiction:</b> City of West Linn                             | <b>Jurisdiction Case #:</b> PA-18-04    |
| <b>Site Address:</b> 19915 Old River Drive, West Linn, OR          | <b>Legal Description:</b>               |
| <b>State Highway:</b> OR 43                                        | <b>Tax Lot(s):</b>                      |
|                                                                    | <b>Mileposts:</b> 8.8                   |

The site of this proposed land use action is (in the vicinity of Willamette Drive (OR 43). ODOT has permitting authority for this facility and an interest in ensuring that this proposed land use is compatible with its safe and efficient operation.

### COMMENTS/FINDINGS


ODOT recommends that the applicant submit a traffic impact analysis to assess the impacts of the proposed use on the State highway system. The analysis must be conducted by a Professional Engineer registered in Oregon. **Contact the ODOT Traffic representative identified below and the local jurisdiction to scope the study.**

**Please send a copy of the Land Use Notice to:**

ODOT Region 1 Planning  
Development Review  
123 NW Flanders St  
Portland, OR 97209

[Region1\\_DEVREV\\_Applications@odot.state.or.us](mailto:Region1_DEVREV_Applications@odot.state.or.us)

|                                             |                                                     |
|---------------------------------------------|-----------------------------------------------------|
| Development Review Planner: Joshua Brooking | 503.731.3049,<br>joshua.c.brooking@odot.state.or.us |
| Traffic Contact: Katherine 'Katie' Bell     | 503.731.8435                                        |

|                                                                                   |                                                                                                 |                                                                           |
|-----------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------|
|  | <b>Pre-app Comments</b>                                                                         | <b>Engineering Contact:</b>                                               |
|                                                                                   | <b>Project Number: PA-18-04</b><br><b>The Marylhurst School</b><br><b>19915 Old River Drive</b> | Amy Pepper, PE<br>apepper@westlinnoregon.gov<br>Telephone: (503) 722-5517 |

**Project Description:** Conversion of church into pre-k through 8 education facility. Applicant to provide additional information about number of staff and students proposed at facility. Proposed lot consolidation.

**Pre-application meeting date:** January 18, 2018

The comments provided below are based upon material provided as part of the pre-application packet and are intended to identify potential design challenges associated with the development. Comments are not intended to be exhaustive and do not preclude the Engineering Department from making additional comments as part of the formal land use application process.

**TRANSPORTATION**

**Minimum Required Improvement:**

- Old River Drive street improvement:
  - Existing right-of-way is unimproved and approximately 60 feet wide.
  - Old River Drive is designated in the Transportation System Plan as a Neighborhood Route. The TSP identifies 2 medium priority capital improvement projects along Old River Drive, a sidewalk extension and bike facility.
  - The existing pavement width is approximately 23 feet. Half-street improvements, including street widening, sidewalk and drainage facilities will be required.
- Street trees: coordinate with the Park Department to install appropriate number and type of tree, as applicable:
  - Parks Contact: Mike Perkins  
[mperkins@westlinnoregon.gov](mailto:mperkins@westlinnoregon.gov)  
503-723-2554
- Driveway approach and spacing: driveway approaches shall be 36' wide max including wing. Driveway spacing shall meet the Community Development Code.
- New franchise utilities: All new distribution and communication franchise utilities and their services must be placed underground.
- Street lighting: Coordinate with PGE to install appropriate number and type of street lights.
- Development shall pay all applicable Transportation System Development Charges (SDC) fees (Street and Bike/Ped).
- A Traffic Impact Analysis (TIA) is likely required. Review CDC Chapter 85 and Section 5 of the *City of West Linn Public Works Standards*. Applicant should set up a meeting with West Linn Engineering and ODOT to determine the required elements of the TIA and the level of analyses expected.





CITY OF  
**West  
Linn**

## Pre-app Comments

**Project Number: PA-18-04**  
**The Marylhurst School**  
**19915 Old River Drive**

### Engineering Contact:

Amy Pepper, PE  
apepper@westlinnoregon.gov  
Telephone: (503) 722-5517

### SANITARY SEWER

#### Minimum Required Improvement:

- There is an 8" sanitary sewer main in Old River Drive.
- The new structure shall connect to the sewer main via a separate, private lateral.
- Development shall pay all applicable Sanitary Sewer SDC fees.

### DOMESTIC WATER

#### Minimum Required Improvement:

- There is an existing 8" AC water main in Old River Drive that has adequate capacity to serve the proposed development.
- The new water service shall supply water to the new structure.
- There is an existing 6" water line on the north side of 19803 Old River Drive. Verify that this line is located within an easement. If not provide easement for continued use of this line.
- Fire flow test requests can be made through the Engineering Department. More information and a request form can be found at: <http://westlinnoregon.gov/publicworks/fire-flow-test-request> .
- Development shall pay all applicable Water SDC fees.

### SURFACE WATER (STORM SEWER)

#### Minimum Required Improvement:

- Onsite run-off generated from new impervious areas of greater than 5,000 square feet must be captured, treated, detained and conveyed to the nearest public stormwater system in accordance with the *Portland Stormwater Management Manual*, the Uniform Plumbing Code, and *City of West Linn Public Works Standards*.
- Stormwater facilities installed to capture, treat, detain and convey stormwater from the private improvements shall be privately owned and maintained.
- Development shall pay all applicable Surface Water SDC fees.

### OTHER

- If the proposed development will disturb less than 1 acre, a West Linn Erosion Control Permit Application, as outlined in Section 2.0065 of the *City of West Linn Public Works Standards*, will be required prior to the commencement of construction.
- For partitions and subdivisions, all existing overhead utilities shall be buried underground if the developments frontage is greater than 200 feet and the site is greater than 1 acre.

**From:** [Pepper, Amy](#)  
**To:** [Jessica Hjar](#)  
**Cc:** [Wyss, Darren](#); [gaa@dksassociates.com](mailto:gaa@dksassociates.com); [Debbie Pearson](#); [Beth Cantrell](#); [Mark B. Wharry](#); [Kevin Brady](#)  
**Subject:** RE: Updated report  
**Date:** Friday, January 25, 2019 3:21:19 PM

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Jessica ~

I consulted with DKS on the revisions made. The revisions do not adequately address quantitatively how much stacking is anticipated in the right-of-way. The driveway aisles must be measured from the start of the loading zone, not across the entire site. Field review of the existing facility drop off would likely provide additional information about the queuing needs of the school.

The Traffic Impact Analysis will need to be revised to further evaluate the stacking in the right-of-way and any mitigation necessary could impact right-of-way improvement design. If this Traffic Impact Analysis can be updated and entered into the record for review at the Public Hearing, the Hearing will be recommended to be held open to a date certain until this issue can be resolved. Alternatively, the applicant can request that the City delay the public hearing until this issue is resolved.

Please let me know if you need further questions about this.

Amy

**From:** Jessica Hjar [mailto:[jessica@lancasterengineering.com](mailto:jessica@lancasterengineering.com)]  
**Sent:** Friday, January 25, 2019 1:01 PM  
**To:** Pepper, Amy <[APepper@westlinnoregon.gov](mailto:APepper@westlinnoregon.gov)>  
**Cc:** Wyss, Darren <[dwyss@westlinnoregon.gov](mailto:dwyss@westlinnoregon.gov)>; [gaa@dksassociates.com](mailto:gaa@dksassociates.com); Debbie Pearson <[Debbie.Pearson@otak.com](mailto:Debbie.Pearson@otak.com)>; Beth Cantrell <[Cantrell@deca-inc.com](mailto:Cantrell@deca-inc.com)>; Mark B. Wharry <[mark.wharry@kpff.com](mailto:mark.wharry@kpff.com)>; Kevin Brady <[kevin.brady@cardno.com](mailto:kevin.brady@cardno.com)>  
**Subject:** Re: Updated report

Maybe it would help to attach report! Here it is!

On Fri, Jan 25, 2019 at 1:00 PM Jessica Hjar <[jessica@lancasterengineering.com](mailto:jessica@lancasterengineering.com)> wrote:

Amy and Darren-

I have attached the updated report as discussed with Garth. I added some explanation of on-site queuing based on the pick-up drop-off information in the appendix (near the end of the report). Based on conversation with Garth, I'm not sure what more we can add. It is more about being prepared to answer questions and explain to planning commission. Let me know if you need anything else.

Thanks,

Jessica

--



720 SW Washington St.  
Suite 500  
Portland, OR 97205  
503.243.3500  
www.dksassociates.com

## MEMORANDUM

DATE: January 10, 2019  
TO: Amy Pepper, City of West Linn  
FROM: Garth Appanaitis, PE  
SUBJECT: Marylhurst School TIS Review  
West Linn On Call - Task 12

P16043-012

Per your request, we have reviewed the traffic impact study (TIS)<sup>1</sup> provided for the proposed Marylhurst School at 19915 Old River Drive. This review focused on the technical components of the analysis, which are summarized in the following sections. Based on our review of submitted materials, additional analysis components and clarification should be provided to confirm the safe operation of the proposed site.

## TECHNICAL REVIEW SUMMARY

This section provides a summary of our technical review, which is organized into significant items and additional review notes for consideration that could be pursued at the City's discretion.

### Significant Items

The following items have significant potential to alter the finding of transportation impacts and related recommendations:

- Pages 2-3 -The proposed site circulation describes two driveways, as shown in Figure 2. The report describes the northern driveway used for entry to serve student loading and the southern driveway to be used for parking entry and vehicle exit. The proposed site plan indicates parking stalls located centrally in the parking area that would not be accessible from the southern site driveway with the indicated flow.
  - *Recommendation: Provide additional clarification for the intended vehicle circulation, including access to the central parking stalls. If drive aisles are intended to function with one-way flow, describe design treatments that will enforce such circulation, which may include drive aisle width and/or stall orientation.*
  - *Recommendation: Provide analysis related to vehicle loading that describes the number of loading vehicles that can be accommodated with the site plan, an estimate of the number of loading vehicles that need to be accommodated to serve the site demand (both for arrival and pick-up), identify on-site and off-site impacts related to vehicle loading and potential mitigation, and if off-site impacts are identified (e.g., vehicles would queue onto the public street) identify potential circulation alternatives that would improve vehicle loading and reduce potential for impacts to the public street system.*
- Page 6 - Trip generation is listed in Table 2 for both proposed phases of the project. The proposed land use (ITE 534 Private School K-8) has a higher trip rate than public school trip rates. The narrative does not describe general loading patterns and non-passenger vehicle modes at the school.

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<sup>1</sup> *The Marylhurst School Traffic Impact Study*, prepared by Lancaster Engineering, August 14, 2018.



- *Recommendation: Confirm that no bus or shuttle service is anticipated at the school. Describe the assumed arrival modes for students, including the potential for walking and biking.*
- Page 8 - Figure 5 shows only 87 vehicle trips entering at the site driveways and does not match the 97 trips indicated in the trip generation for the Phase 2 AM trip generation.
  - *Recommendation: Clarify the trip generation, distribution, and assignment, and update traffic analysis if needed.*
- Pedestrian and Bicycle Access - The report does not describe accessibility to the site from the surrounding system for pedestrians and bicyclists.
  - *Recommendation - Describe site accessibility for pedestrians and bicyclists, including connections to the surrounding transportation network. Describe any improvements needed to provide safe access for students and other site users.*
- The narrative does not describe the access spacing requirements or safe function of the site driveways.
  - *Recommendation: Clarify the spacing of site driveways and indicate if access management requirements are met. Site distance should be measured for existing site driveways (and estimated for any new proposed driveways) and indicate if sight distance requirements are met.*

## Other Review Notes

The following items were noted during the technical review and are not likely to significantly affect the analysis findings.

- Page 5 - Figure 3 indicates that the intersection of Cedar Oak rive/Willamette Drive has two through approach lanes (which matches existing facility conditions). The intersection was analyzed with a single southbound through lane as a conservative analysis. While not indicated in the text, this assumption removes the requirement to indicate lane utilization for the two lanes. No further modification to analysis required.
- Page 9 - Future traffic volumes were projected using ODOT Future Volume Tables. While this approach is not ideal for an urban area that has a TSP and travel demand model, it is not likely that a refined growth assumption would significantly change the findings.
- Page 13 - The intersection operations analysis included the full build of Phase 2 but did not include the interim Phase 1. The analysis indicates that all study intersections would meet operational standards under the Phase 2 conditions. Additional analysis for Phase 1 is not needed to determine potential interim impacts.

If you have any questions, please call.

**EXHIBIT PC-3: AFFIDAVIT AND NOTICE PACKET**

**AFFIDAVIT OF NOTICE**

We, the undersigned do hereby certify that, in the interest of the party (parties) initiating a proposed land use, the following took place on the dates indicated below:

**GENERAL**

File No. 0VP-18-01 Applicant's Name Sheila Walker  
Development Name \_\_\_\_\_  
Scheduled Meeting/Decision Date 2-6-19

**NOTICE:** Notices were sent at least 20 days prior to the scheduled hearing, meeting, or decision date per Section 99.080 of the Community Development Code. (check below)

**TYPE A**

- A. The applicant (date) 1-17-19 (signed) S. Shroyer
- B. Affected property owners (date) 1-17-19 (signed) S. Shroyer
- C. School District/Board (date) 1-17-19 (signed) S. Shroyer
- D. Other affected gov't. agencies (date) 1-17-19 (signed) S. Shroyer
- E. Affected neighborhood assns. (date) 1-17-19 (signed) S. Shroyer
- F. All parties to an appeal or review (date) \_\_\_\_\_ (signed) \_\_\_\_\_

At least 10 days prior to the scheduled hearing or meeting, notice was published/posted:

Tidings (published date) 1-24-19 (signed) S. Shroyer  
City's website (posted date) 1-17-19 (signed) S. Shroyer

**SIGN**

At least 10 days prior to the scheduled hearing, meeting or decision date, a sign was posted on the property per Section 99.080 of the Community Development Code.

(date) 1-25-2019 (signed) Don S Wp

**NOTICE:** Notices were sent at least 14 days prior to the scheduled hearing, meeting, or decision date per Section 99.080 of the Community Development Code. (check below)

**TYPE B** \_\_\_\_\_

- A. The applicant (date) \_\_\_\_\_ (signed) \_\_\_\_\_
- B. Affected property owners (date) \_\_\_\_\_ (signed) \_\_\_\_\_
- C. School District/Board (date) \_\_\_\_\_ (signed) \_\_\_\_\_
- D. Other affected gov't. agencies (date) \_\_\_\_\_ (signed) \_\_\_\_\_
- E. Affected neighborhood assns. (date) \_\_\_\_\_ (signed) \_\_\_\_\_

Notice was posted on the City's website at least 10 days prior to the scheduled hearing or meeting.  
Date: \_\_\_\_\_ (signed) \_\_\_\_\_

**STAFF REPORT** mailed to applicant, City Council/Planning Commission and any other applicable parties 10 days prior to the scheduled hearing.

(date) 1-25-2019 (signed) Don S Wp

**FINAL DECISION** notice mailed to applicant, all other parties with standing, and, if zone change, the County surveyor's office.

(date) \_\_\_\_\_ (signed) \_\_\_\_\_

**CITY OF WEST LINN PLANNING COMMISSION  
PUBLIC HEARING NOTICE  
FILE NO. CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03**

The West Linn Planning Commission will hold a public hearing, on **Wednesday, February 6, 2019, starting at 6:30 p.m.** in the Council Chambers of City Hall, 22500 Salamo Road, West Linn, to consider a request for a Conditional Use Permit, Class II Design Review, and two Class II Variances for the redevelopment of an existing church into a private school serving pre-school through 8<sup>th</sup> Grade at 19915 Old River Drive.

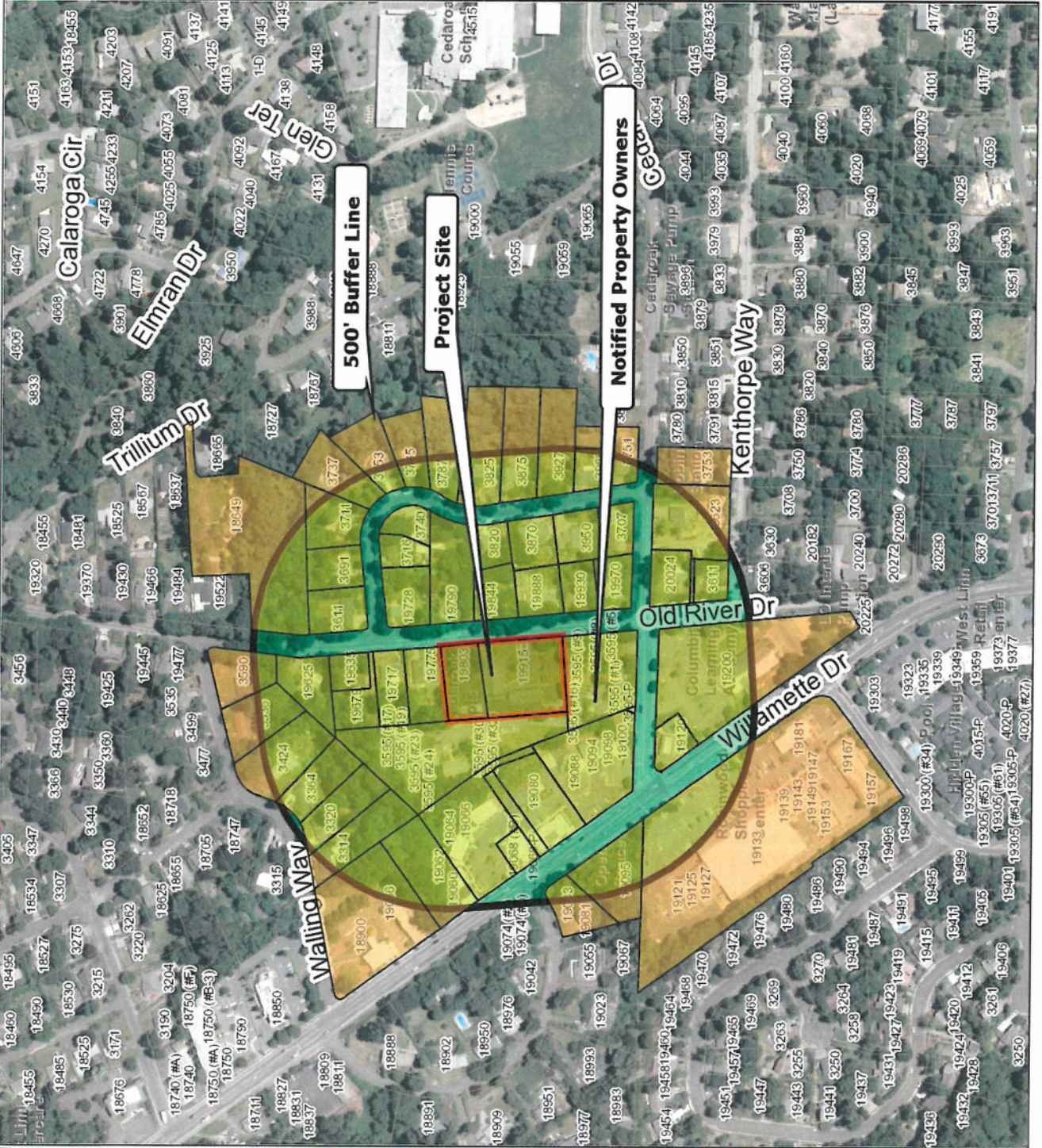
Criteria applicable to the requested Conditional Use Permit, Class II Design Review, and Class II Variances (required parking and required loading area ) are in Chapters 11, 41, 42, 44, 46, 48, 54, 55, 60, 75, 92, 96, and 99 of the Community Development Code (CDC). The decision by the Planning Commission to approve or deny this request will be based upon the applicable criteria. At the hearing, it is important that comments relate specifically to the applicable criteria.

You have been notified of this proposal because County records indicate that you own property within 500 feet of the subject property (Clackamas County Assessor's Map 2S-1E-23 AA, Tax Lots 400 and 500), or as otherwise required by Chapter 99 of the CDC.

The complete application in the above noted file is available for inspection at no cost at City Hall or via the web site at <https://westlinnoregon.gov/planning/19915-19803-old-river-drive-conditional-use-permit-change-church-use-school-use> or copies can be obtained for a minimal charge per page. At least ten days prior to the hearing, a copy of the staff report will be available for inspection. For further information, please contact Associate Planner Darren Wyss at [dwyss@westlinnoregon.gov](mailto:dwyss@westlinnoregon.gov) or 503-742-6064. Alternately, visit City Hall, 22500 Salamo Road, West Linn, OR 97068.

The hearing will be conducted in accordance with the rules of Section 99.170 of the CDC. Anyone wishing to present written testimony on this proposed action may do so in writing prior to, or at the public hearing. Oral testimony may be presented at the public hearing. At the public hearing, the Planning Commission will receive a staff presentation, and invite both oral and written testimony. The Planning Commission may continue the public hearing to another meeting to obtain additional information, leave the record open for additional evidence, arguments, or testimony, or close the public hearing and take action on the application as provided by state law. **In the event that the Planning Commission decision is appealed, City Council review of the appeal will be de novo.** Failure to raise an issue in person or by letter at some point prior to the close of the hearing, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes an appeal to the Land Use Board of Appeals (LUBA) based on that issue.

# 19915 Old River Drive Notification Map



Feet  
0 200  
Scale 1:4,800 - 1 in = 400 ft  
Scale is based on 8-1/2 x 11 paper size



Map created by: SSHROYER  
Date Created: 17-Dec-18 11:24 AM

**WEST LINN GIS**

DISCLAIMER: This product is for informational purposes and may not have been prepared for, or be suitable for legal, engineering, or surveying purposes. Users of this information should review or consult the primary data and information sources to ascertain the usability of the information. Map scale is approximate. Source: West Linn GIS (Geographic Information System) MapOpix.





# CITY OF West Linn

## **CITY OF WEST LINN NOTICE OF UPCOMING PLANNING COMMISSION HEARING**

**PROJECT # CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03  
MAIL: 1/17/19 TIDINGS: 1/24/19**

### **CITIZEN CONTACT INFORMATION**

To lessen the bulk of agenda packets, land use application notice, and to address the worries of some City residents about testimony contact information and online application packets containing their names and addresses as a reflection of the mailing notice area, this sheet substitutes for the photocopy of the testimony forms and/or mailing labels. A copy is available upon request.

**EXHIBIT PC-4: COMPLETENESS LETTER**



## CITY OF West Linn

December 18, 2018

Sheila Walker  
The Marylhurst School  
1232 Linn Avenue  
Oregon City, OR 97045

SUBJECT: CUP-18-01/DR-18-04/VAR-18-02/VAR-18-03 application for Conditional Use Permit, Class II Design Review, and two Class II Variances for phased redevelopment and operation of a school at 19915 Old River Drive

Dear Sheila:

You submitted this application on September 11, 2018. The Planning and Engineering Departments found that this application was incomplete on October 9, 2018. All required information was subsequently provided on November 5, 2018 and December 5, 2018. The application has now been deemed **complete**. The city has 120 days to exhaust all local review; that period ends April 4, 2019.

Please be aware that determination of a complete application does not guarantee a recommendation of approval from staff for your proposal as submitted – it signals that staff believes you have provided the necessary information for the Planning Commission to render a decision on your proposal.

A 20-day public notice will be prepared and mailed. This notice will identify the Planning Commission hearing date.

Please contact me at 503-742-6064, or by email at [dwyss@westlinnoregon.gov](mailto:dwyss@westlinnoregon.gov) if you have any questions or comments.

Sincerely,

A handwritten signature in blue ink, appearing to read "Darren Wyss". The signature is fluid and cursive, written over a light blue horizontal line.

Darren Wyss  
Associate Planner

## Memorandum

Date: February 6, 2019

To: West Linn Planning Commission

From: Darren Wyss, Associate Planner

Subject: CUP-18-01 Supplemental Findings – Potential Queuing in Right-of-Way

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The Planning Commission will hold a public hearing on February 6, 2019 on a proposal for a Conditional Use Permit, Class II Design Review, and two Class II Variances for the purpose of locating a private pre-school to middle school facility at 19915/19803 Old River Drive. The purpose of this memo is to provide amended findings related to staff's request for additional information from the applicant regarding potential queuing of cars in the right-of-way during drop-off/pick-up times.

The applicant provided the requested information on February 1, 2019 (see Attachment 1), after the publishing of the staff report and Planning Commission packet. City staff and the City's traffic consultant reviewed the information and determined there is sufficient on-site queuing storage and it would not extend into the public right-of-way (see Attachment 2).

Based on this updated information, the following seven findings are amended with the **new language underlined**.

### *48.025 ACCESS CONTROL*

#### *B. Access Control Standards*

*1. Traffic impact analysis requirements. The City or other agency with access jurisdiction may require a traffic study prepared by a qualified professional to determine access, circulation and other transportation requirements. (See also CDC [55.125](#), Traffic Impact Analysis.)*

**Staff Finding 35: The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.**

**The applicant provided a supplemental memo (Marylhurst School – On Site Queuing dated February 1, 2019) in response to Staff's request for more information to address the potential for stacking into the Old River Drive right-of-way caused by student drop-off/pick-up. The**

**applicant found the 120 foot drop-off lane will provide sufficient on-site drop-off/pick-up storage to prevent stacking into the right-of-way. The drop-off lane will accommodate six vehicles and the anticipated maximum que is five vehicles at the maximum enrollment of 194 after completion of Phase Two. The City’s transportation consultant found this analysis provided an adequate amount of information to support the finding (DKS Memorandum: Marylhurst School TIS Review dated February 5, 2019). The applicant will install half-street improvements, but no need for additional right-of-way improvements are warranted.**

55.100

*I. Public facilities. An application may only be approved if adequate public facilities will be available to provide service to the property prior to occupancy.*

*1. Streets. Sufficient right-of-way and slope easement shall be dedicated to accommodate all abutting streets to be improved to the City’s Improvement Standards and Specifications. The City Engineer shall determine the appropriate level of street and traffic control improvements to be required, including any off-site street and traffic control improvements, based upon the transportation analysis submitted. The City Engineer’s determination of developer obligation, the extent of road improvement and City’s share, if any, of improvements and the timing of improvements shall be made based upon the City’s systems development charge ordinance and capital improvement program, and the rough proportionality between the impact of the development and the street improvements...*

**Staff Finding 82: Old River Drive is classified as a Neighborhood Route in the West Linn Transportation System Plan and has an existing right-of-way (ROW) width of 60 feet. There are no existing pedestrian/bicycle facilities in the ROW. The ROW is sufficient to accommodate proposed street improvements for a Neighborhood Route without Parking. The applicant has submitted a Traffic Impact Study and designed the infrastructure to accommodate anticipated traffic load and pedestrian facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.**

**The applicant provided a supplemental memo (Marylhurst School – On Site Queuing dated February 1, 2019) in response to Staff’s request for more information to address the potential for stacking into the Old River Drive right-of-way caused by student drop-off/pick-up. The applicant found the 120 foot drop-off lane will provide sufficient on-site drop-off/pick-up storage to prevent stacking into the right-of-way. The drop-off lane will accommodate six vehicles and the anticipated maximum que is five vehicles at the maximum enrollment of 194 after completion of Phase Two. The City’s transportation consultant found this analysis provided an adequate amount of information to support the finding (DKS Memorandum: Marylhurst School TIS Review dated February 5, 2019). The applicant will install half-street improvements, but no need for additional right-of-way improvements are warranted.**

55.125 TRANSPORTATION ANALYSIS

Certain development proposals required that a Traffic Impact Analysis (TIA) be provided which may result in modifications to the site plan or conditions of approval to address or minimize any adverse impacts created by the proposal. The purpose, applicability and standards of this analysis are found in CDC [85.170\(B\)\(2\)](#).

**Staff Finding 98:** The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.

**The applicant provided a supplemental memo (Marylhurst School – On Site Queuing dated February 1, 2019) in response to Staff’s request for more information to address the potential for stacking into the Old River Drive right-of-way caused by student drop-off/pick-up. The applicant found the 120 foot drop-off lane will provide sufficient on-site drop-off/pick-up storage to prevent stacking into the right-of-way. The drop-off lane will accommodate six vehicles and the anticipated maximum que is five vehicles at the maximum enrollment of 194 after completion of Phase Two. The City’s transportation consultant found this analysis provided an adequate amount of information to support the finding (DKS Memorandum: Marylhurst School TIS Review dated February 5, 2019). The applicant will install half-street improvements, but no need for additional right-of-way improvements are warranted.**

60.070.A

4. Adequate public facilities will be available to provide service to the property at the time of occupancy.
5. The applicable requirements of the zone are met, except as modified by this chapter.
6. The supplementary requirements set forth in Chapters 52 to 55 CDC, if applicable, are met.
7. The use will comply with the applicable policies of the Comprehensive Plan.

**Staff Finding 103:** Staff incorporates applicant findings (pages 115 to 119 of packet). In addition, the applicant will install street improvements per Condition of Approval 2 to ensure adequate transportation facilities are available. The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.

**The applicant provided a supplemental memo (Marylhurst School – On Site Queuing dated February 1, 2019) in response to Staff’s request for more information to address the potential for stacking into the Old River Drive right-of-way caused by student drop-off/pick-up. The applicant found the 120 foot drop-off lane will provide sufficient on-site drop-off/pick-up**

**storage to prevent stacking into the right-of-way. The drop-off lane will accommodate six vehicles and the anticipated maximum que is five vehicles at the maximum enrollment of 194 after completion of Phase Two. The City’s transportation consultant found this analysis provided an adequate amount of information to support the finding (DKS Memorandum: Marylhurst School TIS Review dated February 5, 2019). The applicant will install half-street improvements, but no need for additional right-of-way improvements are warranted.**

7. *Requiring participation in making the intersection improvement or improvements identified in the Transportation System Plan when a traffic analysis (compiled as an element of a conditional use application for the property) indicates the application should contribute toward.*

**Staff Finding 111:** The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2.

**The applicant provided a supplemental memo (Marylhurst School – On Site Queuing dated February 1, 2019) in response to Staff’s request for more information to address the potential for stacking into the Old River Drive right-of-way caused by student drop-off/pick-up. The applicant found the 120 foot drop-off lane will provide sufficient on-site drop-off/pick-up storage to prevent stacking into the right-of-way. The drop-off lane will accommodate six vehicles and the anticipated maximum que is five vehicles at the maximum enrollment of 194 after completion of Phase Two. The City’s transportation consultant found this analysis provided an adequate amount of information to support the finding (DKS Memorandum: Marylhurst School TIS Review dated February 5, 2019). The applicant will install half-street improvements, but no need for additional right-of-way improvements are warranted.**

*XII. CHAPTER 96, STREET IMPROVEMENT CONSTRUCTION*

*96.010 CONSTRUCTION REQUIRED*

A. *New construction.*

*(...)*

B. *Remodeling of an existing building.*

*(...)*

**Staff Finding 133:** The applicant proposal includes construction of half-street improvements along the portion of Old River Drive that is adjacent to the subject property. The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2. Installation of all street improvements will meet the West Linn Public Works Design Standards per Condition of Approval 2.

**The applicant provided a supplemental memo (Marylhurst School – On Site Queuing dated February 1, 2019) in response to Staff’s request for more information to address the potential for stacking into the Old River Drive right-of-way caused by student drop-off/pick-up. The applicant found the 120 foot drop-off lane will provide sufficient on-site drop-off/pick-up storage to prevent stacking into the right-of-way. The drop-off lane will accommodate six vehicles and the anticipated maximum que is five vehicles at the maximum enrollment of 194 after completion of Phase Two. The City’s transportation consultant found this analysis provided an adequate amount of information to support the finding (DKS Memorandum: Marylhurst School TIS Review dated February 5, 2019). The applicant will install half-street improvements, but no need for additional right-of-way improvements are warranted.**

*96.020 STANDARDS*

*Street improvements shall be installed according to the City standards and shall be completed prior to the issuance of any occupancy permit for the new or remodeled structure or building. In unimproved areas of the City, the City Engineer may grant a time extension of the provisions of this section; provided, that the applicant provides sufficient security in amount and quantity satisfactory to the City Attorney to assure payment of such improvement costs.*

**Staff Finding 134:** The applicant proposal includes construction of half-street improvements along the portion of Old River Drive that is adjacent to the subject property. The applicant has provided a Traffic Impact Study (pages 239 to 322 of packet). The analysis found no impact to off-site transportation facilities. Staff found the analysis did not provide adequate information to determine the potential for drop-off/pick-up stacking into the Old River Drive right-of-way. Stacking into the right-of-way may warrant the need for additional right-of-way improvements for mitigation. Staff has requested additional information per the email found in Exhibit PC-2. Installation of street improvements will be complete before occupancy and meet the West Linn Public Works Design Standards per Condition of Approval 2.

**The applicant provided a supplemental memo (Marylhurst School – On Site Queuing dated February 1, 2019) in response to Staff’s request for more information to address the potential for stacking into the Old River Drive right-of-way caused by student drop-off/pick-up. The applicant found the 120 foot drop-off lane will provide sufficient on-site drop-off/pick-up storage to prevent stacking into the right-of-way. The drop-off lane will accommodate six vehicles and the anticipated maximum que is five vehicles at the maximum enrollment of 194 after completion of Phase Two. The City’s transportation consultant found this analysis provided an adequate amount of information to support the finding (DKS Memorandum: Marylhurst School TIS Review dated February 5, 2019). The applicant will install half-street improvements, but no need for additional right-of-way improvements are warranted.**



## Technical Memorandum

**To:** Amy Pepper, City of West Linn  
**Copy:** Garth Appanaitis, DKS Associates  
**From:** Jessica Hjar  
 Todd Mobley, PE  
**Date:** February 1, 2019  
**Subject:** Marylhurst School – On Site Queuing



321 SW 4th Ave., Suite 400  
 Portland, OR 97204  
 phone: 503.248.0313  
 fax: 503.248.9251  
 lancasterengineering.com

This memorandum is written to supplement the traffic impact study for the proposed Marylhurst School prepared by Lancaster Engineering in January 2019.

### ***Existing Site Drop-Off Observations***

The Marylhurst School is currently located at 1232 Linn Avenue in Oregon City. The current site has approximately eight standard marked parking spaces and two handicap spaces. In addition to these parking spaces, there is room for about 12 parallel parking spaces that are not attributed to the designated drop-off area. Therefore, there is a parking capacity of 22 vehicles for the existing site, in addition to the curbside pick-up and drop-off area, which can accommodate approximately five vehicles.

The school shares a driveway and turnaround with the City of Oregon City community planning offices. The school has prohibited parents from using the spaces designated for use by the planning office, and those spaces are not included in the count above.

Morning drop-off observations were made on January 31<sup>st</sup>, 2019 between 8:05 AM and 8:35 AM at the existing school location. Both the primary and middle school students begin their school days at 8:30 AM.

The school administrator was outside monitoring school drop-off the duration of the observation period. She directed vehicles to pull forward in the drop-off area and opened doors for children to exit vehicles. The longest queue length observed was four vehicles, which occurred at 8:33 AM. Many vehicles in the drop-off area were observed to have more than one child. About half of the incoming vehicles were observed to park and escort their children inside the school. Although the parking area for the planning office was empty, no vehicles for school drop-off were observed to park in the office spaces.

### ***Proposed Site Circulation***

The proposed school location at 19915 Old River Road will have approximately 37 on-site parking spaces. The curb frontage of the pick-up and drop-off area measures approximately 120 feet, which can



accommodate six vehicles, as shown in Figure 1 below. The drop-off area can accommodate two lanes of vehicles, one lane along the curb frontage for loading and one serves as a bypass lane.

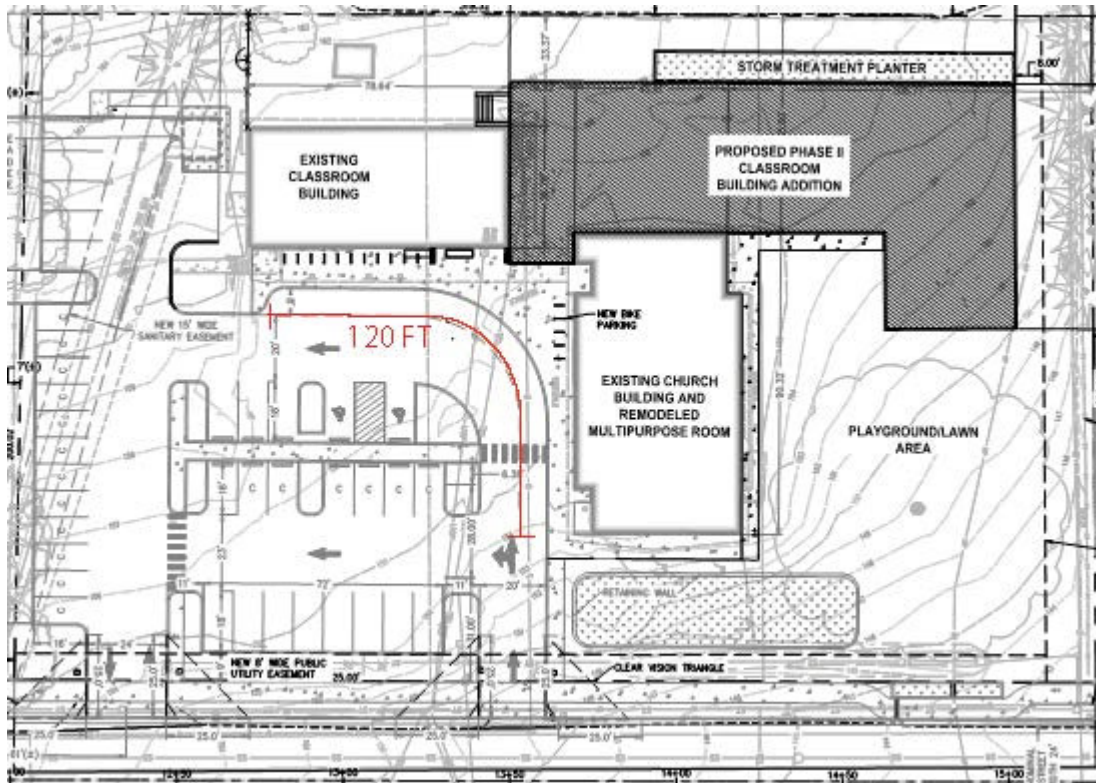


Figure 1 - Curbside Queuing Capacity (feet)

As shown in the table below, the new site in West Linn will have more capacity for parking as well as on-site queuing than the school's current location. Based on the existing site observations, the proposed site has a projected queue length of five vehicles for the phase two occupancy of 194 students. Therefore, all queuing can be accommodated on site and is not anticipated to impact public roadway within the site vicinity.

Table 1 - Site Comparison Summary

|                                   | Existing Site | Proposed Site |
|-----------------------------------|---------------|---------------|
| On-Site Parking Capacity          | 22 spaces     | 37 spaces     |
| On-Site Pick-up/Drop-off Capacity | 5 vehicles    | 6 vehicles    |
| Number of Students                | 165 students  | 194 students  |
| Maximum Queue                     | 4 vehicles    | 5 vehicles    |



720 SW Washington St.  
Suite 500  
Portland, OR 97205  
503.243.3500  
www.dksassociates.com

## MEMORANDUM

DATE: February 5, 2019  
TO: Amy Pepper, City of West Linn  
FROM: Garth Appanaitis, PE  
SUBJECT: Marylhurst School TIS Review  
West Linn On Call - Task 12

P16043-012

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Per your request, we have reviewed the revised traffic impact study (TIS)<sup>1</sup> and supplemental materials<sup>2</sup> provided for the proposed Marylhurst School at 19915 Old River Drive. The revised TIS and additional materials were prepared to address our prior technical review comments<sup>3</sup> regarding site transportation needs and traffic impacts.

The updated materials include a summary of circulation and loading patterns on site, including observations performed at the existing school site. The TIS notes that pick up times are divided into three periods (middle school, primary, and preschool), which are “staggered at least 45 minutes apart to disperse impacts and traffic congestion on site.”

In addition, on-site queuing observations were performed at the existing school site in Oregon City (1232 Linn Avenue). These observations note that the maximum observed vehicle queue was four vehicles during the peak morning period. The current site includes 165 students, which would scale to a projected five-vehicle maximum queue for the full site occupancy of 194 students.

The analysis notes that the proposed site would include 120 feet of available curb storage along the loading area, which would provide space for approximately 6 vehicles. While actual vehicle spacing may exceed 20 feet per vehicle, Figure 1 indicates that six vehicles could be accommodated on site. Therefore, for the proposed site with an enrollment of 194 students, the typical maximum vehicle queue of five vehicles could be accommodated on site and would not extend into the public street system.

If you have any questions, please call.

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<sup>1</sup> *The Marylhurst School Traffic Impact Study*, prepared by Lancaster Engineering, January 22, 2019.

<sup>2</sup> *Technical Memorandum: Marylhurst School - On Site Queuing*, prepared by Lancaster Engineering, February 1, 2019.

<sup>3</sup> *Memorandum: Marylhurst School TIS Review*, prepared by DKS Associates, January 10, 2019.