



**STAFF REPORT
FOR THE PLANNING COMMISSION**

FILE NUMBER: DR-18-03/ VAR-18-01

HEARING DATE: March 6, 2019

REQUEST: A Site Design Review to construct 5-residential units in two buildings and Class I Variance to lot dimensions

APPROVAL CRITERIA: Community Development Code (CDC) Chapter 15, Chapter 55, Chapter 75 and Chapter 99

STAFF REPORT PREPARED BY: Jennifer Arnold, Associate Planner

Planning Manager's Initials JRW Development Review Engineer's Initials: EL

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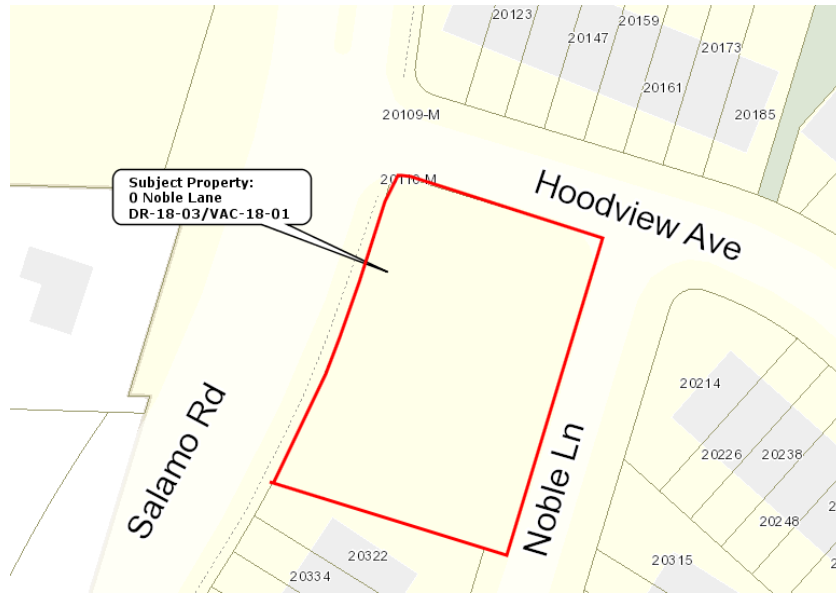
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GENERAL INFORMATION

OWNER/ APPLICANT:	Ali and Nina Ladha 9334 SE Hunters Bluff Ave. Happy Valley, OR 97086
CONSULTANT:	Robert Price 3935 NE 72 nd Ave. Portland, OR 97213
SITE LOCATION:	0 Noble Lane (Vacant property at the corner of Noble LN and Hoodview Ave.)
LEGAL DESCRIPTION:	Clackamas County Assessor's Map 2S-1E-26D, tax lot 00402
SITE SIZE:	0.4 acres (17,517 square feet)
ZONING:	Single-Family and Multiple-Family Residential Detached (R-3)
COMP PLAN DESIGNATION:	Medium High Density Residential
120-DAY PERIOD:	This application became complete on December 5, 2018. The 120-day maximum application-processing period ends on April 4, 2019, but the applicant has granted a 30 day extension to the 120 day period which expires May 4, 2019.
PUBLIC NOTICE:	Public notice was mailed to the all neighborhood associations and affected property owners on February 14, 2019. The property was posted with a notice sign on February 21, 2019. The notice was published in the West Linn Tidings on February 21, 2019. The notice requirements of CDC Chapter 99 have been met. In addition, the staff report was posted on the City's website February 22, 2019.

EXECUTIVE SUMMARY

Site Conditions: The proposed development site is currently vacant and located at the corner of Noble Lane and Hoodview Avenue. The property is 17,517 square feet but after right-of-way dedications the property will be 14,628 square feet. There are no trees on site with the exception of street trees adjacent to the right-of-way. The property slopes towards the southeastern corner of the property.



Project Description: The applicant is requesting approval to construct 5-residential units in two buildings and Class I Variance to lot dimensions. The Class I Variance is required because after land dedications for frontage improvements to the right-of-way, the subject property will be 372 square feet short of the required 15,000 square feet needed for the 5th unit.

The land use permits include:

- Class II Design Review
- Class I Variance

Surrounding Land Use and Zoning: The undeveloped site is zoned R-3. Adjacent zoning and land uses include:

Direction From Site	Zoning	Land Use
North	R-3	Single-family and Multiple-Family Residential
East	R-3	Single-family and Multiple-Family Residential
West	Clackamas County Zoning	Clackamas County Zoning, Not annexed into West Linn
South	R-3	Single-family and Multiple-Family Residential

Applicable Community Development Code Approval Criteria:

- Chapter 15, Single-family and Multiple-Family Residential - R-3;
- Chapter 55, Design Review;
- Chapter 75 Variances and Special Waivers;

Public comments:

As of the publication date of this report, staff has not received any public comments.

RECOMMENDATION

Staff recommends approval of application DR-18-03 and VAR-18-01 based on: 1) the findings submitted by the applicant, which are incorporated by this reference, 2) supplementary staff findings included in the Addendum below, and 3) the addition of conditions of approval below. With these findings, the applicable approval criteria are met. The conditions are as follows:

1. **Site Plans.** With the exception of modifications required by these conditions, the project shall substantially conform to all submitted drawings dated 7/9/18.
2. **Engineering Standards.** All public improvements and facilities associated with the approved site design, including but not limited to street improvements, driveway approaches, curb cuts, utilities, grading, onsite and offsite storm water, street lighting, easements, easement locations, and connections for future extension of utilities are subject to conformance with the City Municipal Code and Community Development Code. All improvements must be designed, constructed, and completed prior to the issuance of occupancy permits. (See Staff Findings 36, 37, 38, 41, & 44)
3. **Side Wall Transition.** The applicant must alter the nonexempt sidewalls of the proposed structures to comply with CDC 43.040.A. (See Staff Finding 5)
4. **Curb Cut Spacing.** Driveway curb cuts onto Noble Lane shall be consolidated to meet spacing requirements. (See Staff Finding 13)

ADDENDUM
PLANNING COMMISSION STAFF REPORT
March 6, 2019

**STAFF EVALUATION OF THE PROPOSAL'S COMPLIANCE
WITH APPLICABLE CODE CRITERIA**

CHAPTER 15, SINGLE-FAMILY AND MULTIPLE-FAMILY RESIDENTIAL (R-3)

15.030 PERMITTED USES

The following uses are uses permitted outright in this zoning district

- (....)
- 2. *Duplex residential unit*
- 3. *Multiple-family residential unit*
- 4. *Single-family attached residential units*
- (...)

Staff Finding 1: The property is in the R-3 zone. The applicant proposes 5 single-family residential attached housing units between two structures. One structure will have two single-family residential attached units (duplex) and one structure with three attached single-family residential units. The applicant's proposal for a multiple-family development is a permitted use in the R-3 zone. This criteria is satisfied.

15.070 DIMENSIONAL REQUIREMENTS, USES PERMITTED OUTRIGHT AND USES PERMITTED UNDER PRESCRIBED CONDITIONS

Except as may be otherwise provided by the provisions of this code, the following are requirements for uses within this zone:

- A. *The minimum lot size shall be:*
 - 1. *For a single-family detached unit, 3,000 square feet.*
 - 2. *For each attached single-family unit, 3,000 square feet.*
 - 3. *For each multiple-family dwelling unit, 3,000 square feet.*

Staff Finding 1: The applicant is required to dedicate right-of-way along Noble Lane and Hoodview Avenue. After dedication, the subject property will be 14,628 square feet. The applicant proposes five multi-family dwelling units, which requires 15,000 square feet. The applicant is requesting a Class I Variance, which allows a reduction in lot area of up to five percent. Subject to the Class I Variance approval, which reduces the lot area by 2.3 percent, the criteria are met.

B. *The minimum front lot line length or the minimum lot width at the front lot line shall be 35 feet.*

C. *Repealed*

Staff Finding 2: The subject property has a front lot line width of 135 feet. The criteria are met.

D. *The minimum yard dimensions or minimum building setback area from the lot line shall be:*

1. *For a front yard, 15 feet (20 feet for a garage); except for steeply sloped lots where the provisions of CDC 41.010 shall apply.*

2. *For an interior side yard, five feet.*

3. *For a side yard abutting a street, 15 feet.*

4. *For a rear yard, 15 feet, except that in the case of an apartment structure in this district, an additional yard area may be required between the structure in this district and any adjacent low density residential uses.*

Staff Finding 3: Staff adopts the applicant findings found on page 28 (Exhibit PC-3 applicant's submittal). The criteria are met.

E. *The maximum building height shall be 35 feet for an attached or detached single-family unit, duplex unit or multi-family unit, except for steeply sloped lots when the provisions of Chapter 41 CDC shall apply.*

F. *The maximum lot coverage shall be 50 percent.*

G. *The maximum floor area ratio for single-family homes shall be 0.45. Type I and II lands shall not be counted toward lot area when determining allowable floor area ratio, except that a minimum floor area ratio of 0.30 shall be allowed regardless of the classification of lands within the property. That 30 percent shall be based upon the entire property including Type I and II lands. Existing residences in excess of this standard may be replaced to their prior dimensions when damaged without the requirement that the homeowner obtain a non-conforming structures permit under Chapter 66 CDC.*

Staff Finding 4: The duplex and triplex are proposed to measure 35 feet in height. The lot is 14,628 square feet and lot coverage is approximately 5,000 square feet, or 34 percent. The applicant is not proposing single-family homes. The criteria are met.

H. *The sidewall provisions of Chapter 43 CDC shall apply. (Ord. 1538, 2006; Ord. 1622 § 24, 2014; Ord. 1675 § 20, 2018)*

Staff Finding 5: The submitted plans and applicant's findings (see page 94 Exhibit PC-4 applicant's resubmittal) suggest that the sidewalls do not meet the provisions of CDC Chapter 43. Per Condition of Approval 3 the applicant must alter the sidewalls to comply with the requirements of CDC 43.040.A. Subject to the Planning Commission approval, the criteria is satisfied.

15.090 OTHER APPLICABLE DEVELOPMENT STANDARDS

A. *The following standards apply to all development including permitted uses:*

1. *Chapter 34 CDC, Accessory Structures, Accessory Dwelling Units, and Accessory Uses.*
2. *Chapter 35 CDC, Temporary Structures and Uses.*

Staff Finding 6: There are no accessory or temporary structures or uses proposed in the application therefore CDC Chapters 34 and 35 do not apply.

3. *Chapter 38 CDC, Additional Yard Area Required; Exceptions to Yard Requirements; Storage in Yards; Projections into Yards.*

Staff Finding 7: The applicant is not proposing to project into any required setback area of the underlying R-3 zone. Therefore the criteria is met by the decision of the Planning Commission.

4. *Chapter 40 CDC, Building Height Limitations, Exceptions.*

Staff Finding 8: The maximum height in the R-3 zone is 35 feet. The applicant is proposing not to exceed this height restriction. Therefore, the criteria is met, subject to an affirmative decision of the Planning Commission.

5. *Chapter 41 CDC, Structures on Steep Lots, Exceptions.*

Staff Finding 9: The subject property of this application has an approximate slope of 7% which does not qualify this application for any exceptions of CDC Chapter 41. Therefore this criteria does not apply.

6. *Chapter 42 CDC, Clear Vision Areas.*

Staff Finding 10: Staff adopts the applicant's findings found on page 34 (Exhibit PC-3 applicant's submittal). The criteria is met.

7. *Chapter 44 CDC, Fences.*

Staff Finding 11: The applicant does not propose any perimeter fencing at this time. This criteria is met.

8. *Chapter 46 CDC, Off-Street Parking, Loading and Reservoir Areas.*

Staff Finding 12: The applicant proposes 2 off-street parking spaces within a garage and 1-2 additional space in the driveway for each of the 5 residential units (3-4 total parking spaces per unit). This criteria is met.

9. *Chapter 48 CDC, Access, Egress and Circulation.*

(...)

48.060 WIDTH AND LOCATION OF CURB CUTS AND ACCESS SEPARATION REQUIREMENTS.

(...)

C. No curb cuts shall be allowed any closer to an intersecting street right-of-way line than the following:

(...)

6. On a local street when intersecting any other street, 35 feet.

D. There shall be a minimum distance between any two adjacent curb cuts on the same side of a public street, except for one-way entrances and exits, as follows:

(...)

3. Between any two curb cuts on the same lot or parcel on a local street, 30 feet. (...)

Staff Finding 13: The applicant does not propose any shared driveways, but per condition of approval 4 the driveway curb cuts onto Noble Lane must be consolidated to meet spacing standards. All access is proposed via Noble Lane which is classified as a local street. Subject to approval of the proposed conditions, this criteria is met.

10. Chapter 52 CDC, Signs.

Staff Finding 14: The applicant does not propose any signs with this application. This criteria does not apply.

11. Chapter 54 CDC, Landscaping.

Staff Finding 15: Staff adopts the applicant findings found on page 35 (Exhibit PC-3 applicant submittal). The criteria is met.

CHAPTER 55, DESIGN REVIEW

55.100 APPROVAL STANDARDS – CLASS II DESIGN REVIEW

B. Relationship to the natural and physical environment.

1. The buildings and other site elements shall be designed and located so that all heritage trees, as defined in the municipal code, shall be saved. Diseased heritage trees, as determined by the City Arborist, may be removed at his/her direction.

Staff Finding 16: The subject site contains no heritage trees. This criteria does not apply.

2. All heritage trees...all trees and clusters of trees (“cluster” is defined as three or more trees with overlapping driplines; however, native oaks need not have an overlapping dripline) that are considered significant by the City Arborist...shall be protected pursuant to the criteria of subsections (B)(2)(a) through (f) of this section...

a. *Non-residential and residential projects on Type I and II lands shall protect all heritage trees and all significant trees and tree clusters by either the dedication of these areas or establishing tree conservation easements...*

Staff Finding 17: There are no heritage trees or significant trees on the site. The criteria does not apply.

b. *Non-residential and residential projects on non-Type I and II lands shall set aside up to 20 percent of the area to protect trees and tree clusters that are determined to be significant, plus any heritage trees...*

Staff Finding 18: There are no heritage trees or significant trees on the site. The criteria does not apply.

3. *The topography and natural drainage shall be preserved to the greatest degree possible.*

Staff Finding 19: Staff adopts the applicant's findings on page 98 (Exhibit PC-4 applicant's resubmittal). Staff finds that the criteria is met.

4. *The structures shall not be located in areas subject to slumping and sliding. The Comprehensive Plan Background Report's Hazard Map, or updated material as available and as deemed acceptable by the Planning Director, shall be the basis for preliminary determination.*

Staff Finding 20: Staff adopts the applicant's findings on page 98 (Exhibit PC-4 applicant's resubmittal). The criteria is met.

5. *There shall be adequate distance between on-site buildings and on-site and off-site buildings on adjoining properties to provide for adequate light and air circulation and for fire protection.*

Staff Finding 21: The property is currently vacant and the applicant is not proposing any variances to setback requirements. The submitted plans show setbacks between proposed structures and property lines meeting requirements and the applicant proposes 10 feet of separation between each of the proposed structures. The criteria is met.

6. Architecture.

a. *The proposed structure(s) scale shall be compatible with the existing structure(s) on site and on adjoining sites. Contextual design is required. Contextual design means respecting and incorporating prominent architectural styles, building lines, roof forms, rhythm of windows, building scale and massing of surrounding buildings in the proposed structure. The materials and colors shall be complementary to the surrounding buildings.*

Staff Finding 22: Staff incorporates applicant findings (page 99 (Exhibit PC-4 applicant’s resubmittal)) relating to architecture as represented in the following applicant finding:

“Based on the preliminary design prepared by Mascord, the two structures will complement the architectural style of the existing residential structures adjacent properties to the north, south, and east. To the west is Salamo Road, and Rosemont Middle School across Salamo Road. While the design of the two structures reflects today’s styles and trends, it will be both complementary and compatible with the styles that have already been established in the Parker Crest Neighborhood. The two structures reflect the ‘step down’ transition from development around it by going from larger structures to smaller ones, and finally to the Hoodview Avenue right-of-way. The extended setback on the north side of the site promotes the transition to a more open setting. Thus, the transition between structures along Noble Lane and across Hoodview Avenue is promoted by the design of the structures”

This criteria is met.

b. While there has been discussion in Chapter 24 CDC about transition, it is appropriate that new buildings should architecturally transition in terms of bulk and mass to work with, or fit, adjacent existing buildings. This transition can be accomplished by selecting designs that “step down” or “step up” from small to big structures and vice versa (see figure below). Transitions may also take the form of carrying building patterns and lines (e.g., parapets, windows, etc.) from the existing building to the new one.

c. Contrasting architecture shall only be permitted when the design is manifestly superior to adjacent architecture in terms of creativity, design, and workmanship, and/or it is adequately separated from other buildings by distance, screening, grade variations, or is part of a development site that is large enough to set its own style of architecture.

d. Human scale is a term that seeks to accommodate the users of the building and the notion that buildings should be designed around the human scale (i.e., their size and the average range of their perception). Human scale shall be accommodated in all designs by, for example, multi-light windows that are broken up into numerous panes, intimately scaled entryways, and visual breaks (exaggerated eaves, indentations, ledges, parapets, awnings, engaged columns, etc.) in the facades of buildings, both vertically and horizontally.

Staff Finding 23: Staff incorporates applicant findings on page 100 (Exhibit PC-4 applicant’s resubmittal) and refer to Staff Finding 22. These criteria are met.

e. The main front elevation of commercial and office buildings shall provide at least 60 percent windows or transparency at the pedestrian level to create more interesting streetscape and window shopping opportunities. One side elevation shall provide at least 30 percent transparency. Any additional side or rear elevation, which is visible from a collector road or greater classification, shall also have at least 30 percent transparency...

Staff Finding 24: The applicant is proposing multi-family housing units (5 units) in two buildings with no commercial or office use on the property. This criteria does not apply.

f. *Variations in depth and roof line are encouraged for all elevations.*

To vary the otherwise blank wall of most rear elevations, continuous flat elevations of over 100 feet in length should be avoided by indents or variations in the wall. The use of decorative brick, masonry, or stone insets and/or designs is encouraged. Another way to vary or soften this elevation is through terrain variations such as an undulating grass area with trees to provide vertical relief.

g. *Consideration of the micro-climate (e.g., sensitivity to wind, sun angles, shade, etc.) shall be made for building users, pedestrians, and transit users, including features like awnings.*

h. *The vision statement identified a strong commitment to developing safe and attractive pedestrian environments with broad sidewalks, canopied with trees and awnings*
(...)

Staff Finding 25: The applicant is proposing multi-family housing units (5 units) in two buildings. The applicant has proposed design elements that break up the building's elevation to avoid blank walls. There are existing sidewalks around the three sides adjacent to public right-of-way. This criterion is met.

7. *Transportation Planning Rule (TPR) compliance. The automobile shall be shifted from a dominant role, relative to other modes of transportation, by the following means:*
(...)

b. *Multi-family projects shall be required to keep the parking at the side or rear of the buildings or behind the building line of the structure as it would appear from the right-of-way inside the multi-family project. For any garage which is located behind the building line of the structure, but still facing the front of the structure, architectural features such as patios, patio walls, trellis, porch roofs, overhangs, pergolas, etc., shall be used to downplay the visual impact of the garage, and to emphasize the rest of the house and front entry.*
(...)

Staff Finding 26: Staff incorporates the applicant's findings on page 99 (Exhibit PC-4 applicant's resubmittal). The criteria has been satisfied.

d. *Accessways, parking lots, and internal driveways shall accommodate pedestrian circulation and access by specially textured, colored, or clearly defined footpaths at least six feet wide. Paths shall be eight feet wide when abutting parking areas or travel lanes. Paths shall be separated from parking or travel lanes by either landscaping, planters, curbs, bollards, or raised surfaces...*

e. *Paths shall provide direct routes that pedestrians will use between buildings, adjacent rights-of-way, and adjacent commercial developments. They shall be clearly identified. They shall be laid out to attract use and to discourage people from cutting through parking lots and impacting environmentally sensitive areas.*

(...)

Staff Finding 27: See Staff Finding 26; these criteria are met.

(...)

h. Projects shall bring at least part of the project adjacent to or near the main street right-of-way in order to enhance the height-to-width ratio along that particular street. (The “height-to-width ratio” is an architectural term that emphasizes height or vertical dimension of buildings adjacent to streets. The higher and closer the building is, and the narrower the width of the street, the more attractive and intimate the streetscape becomes.) For every one foot in street width, the adjacent building ideally should be one to two feet higher. This ratio is considered ideal in framing and defining the streetscape.

(...)

Staff Finding 28: The applicant proposes two three story buildings at a maximum height of 35 feet as allowed in the R-3 zone. The applicant proposed a front yard setback of 20 feet, which is required in the R-3 zone. The Hoodview Avenue and Noble Lane street pavement width is 28 feet. These criteria are met.

C. Compatibility between adjoining uses, buffering, and screening.

1. In addition to the compatibility requirements contained in Chapter 24 CDC, buffering shall be provided between different types of land uses; for example, buffering between single-family homes and apartment blocks. However, no buffering is required between single-family homes and duplexes or single-family attached units. The following factors shall be considered in determining the adequacy of the type and extent of the buffer:

- a. The purpose of the buffer, for example to decrease noise levels, absorb air pollution, filter dust, or to provide a visual barrier.*
- b. The size of the buffer required to achieve the purpose in terms of width and height.*
- c. The direction(s) from which buffering is needed.*
- d. The required density of the buffering.*
- e. Whether the viewer is stationary or mobile.*

Staff Finding 29: Staff incorporates applicant findings on page 100 (Exhibit PC-4 applicant’s resubmittal). These criteria are met.

2. On-site screening from view from adjoining properties of such things as service areas, storage areas, and parking lots shall be provided and the following factors will be considered in determining the adequacy of the type and extent of the screening:

- a. What needs to be screened?*
- b. The direction from which it is needed.*
- c. How dense the screen needs to be.*
- d. Whether the viewer is stationary or mobile.*
- e. Whether the screening needs to be year-round.*

Staff Finding 30: Staff incorporates applicant findings on page 100 (Exhibit PC-4 applicant's resubmittal). These criteria are met.

3. Rooftop air cooling and heating systems and other mechanical equipment shall be screened from view from adjoining properties.

Staff Finding 31: Staff incorporates applicant findings on page 100 (Exhibit PC-4 applicant's resubmittal). These criteria are met.

D. Privacy and noise.

1. Structures which include residential dwelling units shall provide private outdoor areas for each ground floor unit which is screened from view from adjoining units.

2. Residential dwelling units shall be placed on the site in areas having minimal noise exposure to the extent possible. Natural-appearing sound barriers shall be used to lessen noise impacts where noise levels exceed the noise standards contained in West Linn Municipal Code Section 5.487.

(...)

Staff Finding 32: The subject property is currently screened from the neighboring home to the south by a line of vegetation, and the proposed structure closest to the southern property line will be setback 5 feet. Staff incorporates the applicant's findings on page 101 (Exhibit PC-4 applicant's resubmittal). The criteria is met.

(...)

G. Demarcation of public, semi-public, and private spaces. The structures and site improvements shall be designed so that public areas such as streets or public gathering places, semi-public areas, and private outdoor areas are clearly defined in order to establish persons having a right to be in the space, to provide for crime prevention, and to establish maintenance responsibility. These areas may be defined by:

1. A deck, patio, fence, low wall, hedge, or draping vine;

2. A trellis or arbor;

3. A change in level;

4. A change in the texture of the path material;

5. Sign; or

6. Landscaping.

Staff Finding 33: Staff incorporates applicant findings on page 101 (Exhibit PC-4 applicant's resubmittal). These criteria are met.

H. Public transit.

1. Provisions for public transit may be required where the site abuts an existing or planned public transit route. The required facilities shall be based on the following:

a. The location of other transit facilities in the area.

(...)

Staff Finding 34: Staff incorporates applicant findings on page 101 (Exhibit PC-4 applicant's resubmittal). These criteria are met.

I. Public facilities. An application may only be approved if adequate public facilities will be available to provide service to the property prior to occupancy.

1. Streets. Sufficient right-of-way and slope easement shall be dedicated to accommodate all abutting streets to be improved to the City's Improvement Standards and Specifications. The City Engineer shall determine the appropriate level of street and traffic control improvements to be required, including any off-site street and traffic control improvements, based upon the transportation analysis submitted. The City Engineer's determination of developer obligation, the extent of road improvement and City's share, if any, of improvements and the timing of improvements shall be made based upon the City's systems development charge ordinance and capital improvement program, and the rough proportionality between the impact of the development and the street improvements...

Staff Finding 35: Noble Lane and Hoodview Avenue are classified as Local Streets in the West Linn Transportation System Plan and both will have a right-of-way (ROW) width of 52 feet after dedication of additional ROW by the applicant. The ROW is sufficient to accommodate existing street improvements with parking on one-side. Salamo Road is classified as a Minor Arterial and is built to City standards. The applicant submitted a Transportation Study and found no off-site improvements are warranted. See technical memorandum dated December 22, 2017. The criteria are met.

2. Storm detention and treatment and geologic hazards. Per the submittals required by CDC 55.130 and 92.010(E), all proposed storm detention and treatment facilities must comply with the standards for the improvement of public and private drainage systems located in the West Linn Public Works Design Standards, there will be no adverse off-site impacts caused by the development (including impacts from increased intensity of runoff downstream or constrictions causing ponding upstream), and the applicant must provide sufficient factual data to support the conclusions of the submitted plan.

Per the submittals required by CDC 55.130(E), the applicant must demonstrate that the proposed methods of rendering known or potential hazard sites safe for development, including proposed geotechnical remediation, are feasible and adequate to prevent landslides or other damage to property and safety. The review authority may impose conditions, including limits on type or intensity of land use, which it determines are necessary to mitigate known risks of landslides or property damage.

Staff Finding 36: The applicant has submitted a Preliminary Storm Drainage Report, prepared by a licensed engineer, which complies with the West Linn Public Works Design Standards, shows no adverse off-site impacts, and provides sufficient factual data to support the conclusions of the plan. The subject property does not contain any known landslide hazards. The applicant shall comply with the requirements and install improvements to meet the West

Linn Public Works Design Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.

3. Municipal water. A registered civil engineer shall prepare a plan for the provision of water which demonstrates to the City Engineer's satisfaction the availability of sufficient volume, capacity, and pressure to serve the proposed development's domestic, commercial, and industrial fire flows. All plans will then be reviewed by the City Engineer.

Staff Finding 37: Water is available in Noble Lane to serve the proposed development. A registered civil engineer prepared the water provision plan. The City's public water system has sufficient capacity and pressure in this area. The applicant shall comply with the requirements and install improvements to meet the West Linn Public Works Design Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.

4. Sanitary sewers. A registered civil engineer shall prepare a sewerage collection system plan which demonstrates sufficient on-site capacity to serve the proposed development. The City Engineer shall determine whether the existing City system has sufficient capacity to serve the development.

Staff Finding 38: The applicant has submitted a plan prepared by a registered civil engineer that will gravity flow to the existing sanitary sewer lines in either Noble Lane or Hoodview Avenue. The City's public sanitary sewer system has sufficient capacity to service the proposed use. The applicant shall comply with the requirements and install improvements to meet the West Linn Public Works Design Standards per Condition of Approval 2. Subject to the Conditions of Approval, these criteria are met.

5. Solid waste and recycling storage areas. Appropriately sized and located solid waste and recycling storage areas shall be provided. Metro standards shall be used.

Staff Finding 39: Solid waste storage will be stored in private garages for each unit, and no common storage is proposed with this application. The criteria is met.

J. Crime prevention and safety/defensible space.

- 1. Windows shall be located so that areas vulnerable to crime can be surveyed by the occupants.*
- 2. Interior laundry and service areas shall be located in a way that they can be observed by others.*
- 3. Mailboxes, recycling, and solid waste facilities shall be located in lighted areas having vehicular or pedestrian traffic.*

Staff Finding 40: Staff incorporates applicant findings on page 102 (Exhibit PC-4 applicant's resubmittal). The criteria is met.

4. *The exterior lighting levels shall be selected and the angles shall be oriented towards areas vulnerable to crime.*
 5. *Light fixtures shall be provided in areas having heavy pedestrian or vehicular traffic and in potentially dangerous areas such as parking lots, stairs, ramps, and abrupt grade changes.*
 6. *Fixtures shall be placed at a height so that light patterns overlap at a height of seven feet which is sufficient to illuminate a person. All commercial, industrial, residential, and public facility projects undergoing design review shall use low or high pressure sodium bulbs and be able to demonstrate effective shielding so that the light is directed downwards rather than omni-directional. Omni-directional lights of an ornamental nature may be used in general commercial districts only.*
- (...)

Staff Finding 41: Sheet 3 of 7 from the applicant’s resubmittal (Exhibit PC-4) indicates the existing street lighting. Any additional street lighting shall meet engineering standards per Condition of Approval 2. Subject to approval of the Conditions of Approval, these criteria are met.

K. Provisions for persons with disabilities.

1. *The needs of a person with a disability shall be provided for. Accessible routes shall be provided between all buildings and accessible site facilities. The accessible route shall be the most practical direct route between accessible building entries, accessible site facilities, and the accessible entry to the site. An accessible route shall connect to the public right-of-way and to at least one on-site or adjacent transit stop (if the area is served by transit). All facilities shall conform to, or exceed, the Americans with Disabilities Act (ADA) standards, including those included in the Uniform Building Code.*

Staff Finding 42: Staff incorporates applicant finding on page 102 (Exhibit PC-4 applicant’s resubmittal). These criteria are met.

L. Signs.

(...)

Staff Finding 43: The applicant is not proposing any signs with this application. This criteria does not apply.

M. Utilities. The developer shall make necessary arrangements with utility companies or other persons or corporations affected for the installation of underground lines and facilities. Electrical lines and other wires, including but not limited to communication, street lighting, and cable television, shall be placed underground, as practical. The design standards of Tables 1 and 2 above, and of subsection 5.487 of the West Linn Municipal Code relative to existing high ambient noise levels shall apply to this section.

Staff Finding 44: The subject property does not contain any overhead utilities. All utilities on or near the subject property are already in place and underground. Per Condition of Approval

2, any additional utility placement by the applicant must be reviewed and approved by the City Engineer. Subject to approval of the Conditions of Approval, this criteria is met.

(...)

55.125 TRANSPORTATION ANALYSIS

Certain development proposals required that a Traffic Impact Analysis (TIA) be provided which may result in modifications to the site plan or conditions of approval to address or minimize any adverse impacts created by the proposal. The purpose, applicability and standards of this analysis are found in CDC 85.170(B)(2).

Staff Finding 45: Staff incorporates applicant finding- see technical memorandum dated December 22, 2017 prepared by Lancaster Engineering in applicant’s submittal. These criteria are met.

55.170 EXCEPTIONS TO UNDERLYING ZONE, YARD, PARKING, SIGN PROVISIONS, AND LANDSCAPING PROVISIONS

A. The Planning Director may grant an exception to the dimensional building setback or yard requirements in the applicable zone based on findings that the approval will satisfy the following criteria:

- 1. A minor exception that is not greater than 20 percent of the required setback.*
- (...)

B. The Planning Director may grant an exception to the off-street parking dimensional and minimum number of space requirements in the applicable zone so long as the following criteria are met:

- 1. The minor exception is not greater than 10 percent of the required parking;*
- (...)

C. The Planning Director may grant an exception to the sign dimensional requirements in the applicable zone when the following criteria are met:

- 1. The minor exception is not greater than 10 percent of the required applicable dimensional standard for signs;*
- (...)

Staff Finding 46: The applicant is not requesting any setback, parking, sign or landscape exceptions under this section. The applicant has submitted for a Class I Variance to lot dimensions due to right-of-way dedications (See Staff Finding 47). Therefore the criteria does not apply.

CHAPTER 75, VARIANCES AND SPECIAL WAIVERS

75.020 CLASSIFICATION OF VARIANCES

A. *Class I Variance. Class I variances provide minor relief from certain code provisions where it can be demonstrated that the modification will not harm adjacent properties, and it conforms with any other code requirements. Class I variances are allowed for the following code provisions:*

1. Required Yard and Minimum Lot Dimensional Requirements. Required yards may be modified up to 20 percent, lot dimensions by up to 10 percent and lot area by up to five percent if the decision-making authority finds that the resulting approval:

- a. Provides for a more efficient use of the site;*
- b. Preserves and incorporates natural features into the overall design of the project;*
- c. Does not adversely affect adjoining properties in terms of light, air circulation, noise levels, privacy, and fire hazards; and*
- d. Provides for safe vehicular and pedestrian access to the site and safe on-site vehicular and pedestrian circulation.*

(...)

Staff Finding 47: The applicant is applying for a Class I Variance to reduce the lot area by 2.3%. All existing sidewalks along Hoodview Avenue and Noble Lane are outside of the public right-of-way. The City requires dedication of the sidewalk and planter strip area along these local streets. This dedication will take the current property size (17,517 square feet) down to 14,628 square feet. For the proposed multi-family project 15,000 square feet is required for 5 units. The applicant request for a 2.5% lot area reduction meets the “up to five percent” criteria to approve the. Staff also incorporates the applicant’s findings on pages 15-16 of the applicant’s resubmittal.

PC-1 AFFIDAVIT AND NOTICE PACKET

AFFIDAVIT OF NOTICE

We, the undersigned do hereby certify that, in the interest of the party (parties) initiating a proposed land use, the following took place on the dates indicated below:

GENERAL

File No. SR-18-03/VAR18.01 Applicant's Name Ari + Nina Ladha
Development Name _____
Scheduled Meeting/Decision Date 3-6-19

NOTICE: Notices were sent at least 20 days prior to the scheduled hearing, meeting, or decision date per Section 99.080 of the Community Development Code. (check below)

TYPE A

- A. The applicant (date) 2-14-19 (signed) S. Shroyer
- B. Affected property owners (date) 2-14-19 (signed) S. Shroyer
- C. School District/Board (date) 2-14-19 (signed) S. Shroyer
- D. Other affected gov't. agencies (date) _____ (signed) _____
- E. Affected neighborhood assns. (date) 2-14-19 (ALL) (signed) S. Shroyer
- F. All parties to an appeal or review (date) _____ (signed) _____

At least 10 days prior to the scheduled hearing or meeting, notice was published/posted:

Tidings (published date) 2-21-19 (signed) S. Shroyer
City's website (posted date) 2-14-19 (signed) S. Shroyer

SIGN

At least 10 days prior to the scheduled hearing, meeting or decision date, a sign was posted on the property per Section 99.080 of the Community Development Code.

(date) 2/21/19 (signed) Jenish Auld

NOTICE: Notices were sent at least 14 days prior to the scheduled hearing, meeting, or decision date per Section 99.080 of the Community Development Code. (check below)

TYPE B _____

- A. The applicant (date) _____ (signed) _____
- B. Affected property owners (date) _____ (signed) _____
- C. School District/Board (date) _____ (signed) _____
- D. Other affected gov't. agencies (date) _____ (signed) _____
- E. Affected neighborhood assns. (date) _____ (signed) _____

Notice was posted on the City's website at least 10 days prior to the scheduled hearing or meeting.
Date: _____ (signed) _____

STAFF REPORT mailed to applicant, City Council/Planning Commission and any other applicable parties 10 days prior to the scheduled hearing.

(date) _____ (signed) _____

FINAL DECISION notice mailed to applicant, all other parties with standing, and, if zone change, the County surveyor's office.

(date) _____ (signed) _____

**CITY OF WEST LINN PLANNING COMMISSION
PUBLIC HEARING NOTICE
FILE NO. DR-18-03/VAR-18-01**

The West Linn Planning Commission will hold a public hearing, on **Wednesday, March 6, 2019, starting at 6:30 p.m.** in the Council Chambers of City Hall, 22500 Salamo Road, West Linn, to consider a request for a Class II Design Review to construct 5-residential units in two buildings and Class I Variance to lot dimensions at 0 Noble Lane (Vacant property at the corner of Noble Ln and Hoodview Ave).

Criteria applicable to the requested Class II Design Review, and Class I Variance (variance to lot dimensions) are in Chapters 15, 55, 75, and 99 of the Community Development Code (CDC). The decision by the Planning Commission to approve or deny this request will be based upon the applicable criteria. At the hearing, it is important that comments relate specifically to the applicable criteria.

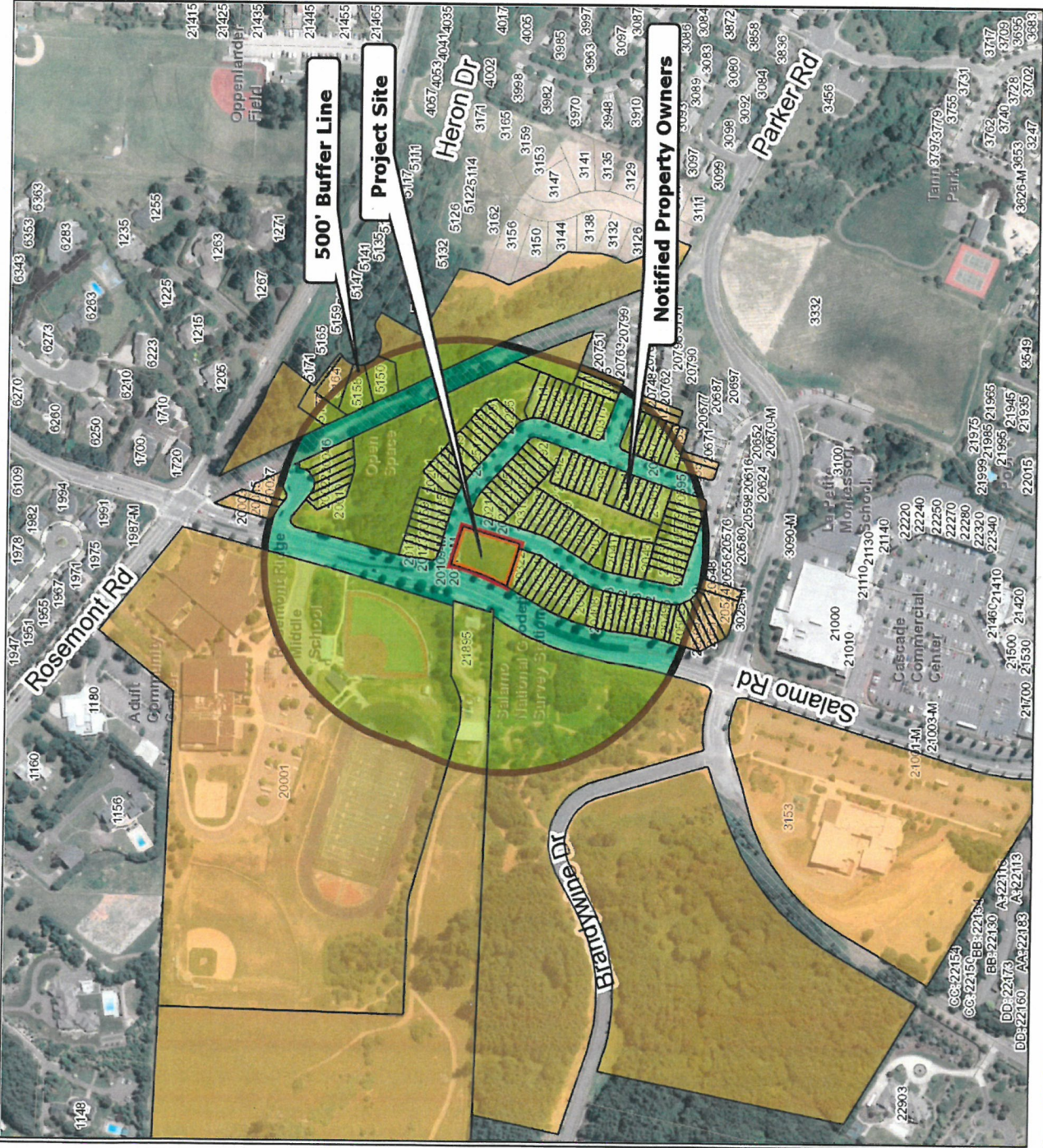
You have been notified of this proposal because County records indicate that you own property within 500 feet of the subject property (Clackamas County Assessor's Map 2S-1E-26D, tax lot 00402), or as otherwise required by Chapter 99 of the CDC.

The complete application in the above noted file is available for inspection at no cost at City Hall or via the web site at <https://westlinnoregon.gov/planning/0-noble-lane-class-ii-design-review-class-i-variance-five-attached-multi-family-homes> or copies can be obtained for a minimal charge per page. At least ten days prior to the hearing, a copy of the staff report will be available for inspection. For further information, please contact Associate Planner Jennifer Arnold at jarnold@westlinnoregon.gov or 503-742-6057. Alternately, visit City Hall, 22500 Salamo Road, West Linn, OR 97068.

The hearing will be conducted in accordance with the rules of Section 99.170 of the CDC. Anyone wishing to present written testimony on this proposed action may do so in writing prior to, or at the public hearing. Oral testimony may be presented at the public hearing. At the public hearing, the Planning Commission will receive a staff presentation, and invite both oral and written testimony. The Planning Commission may continue the public hearing to another meeting to obtain additional information, leave the record open for additional evidence, arguments, or testimony, or close the public hearing and take action on the application as provided by state law. **In the event that the Planning Commission decision is appealed, City Council review of the appeal will be de novo.** Failure to raise an issue in person or by letter at some point prior to the close of the hearing, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes an appeal to the Land Use Board of Appeals (LUBA) based on that issue.

Publish: West Linn Tidings: February 21, 2019

Notification Map



0 200
Feet

Scale 1:4,800 - 1 in = 400 ft
Scale is based on 8-1/2 x 11 paper size



Map created by: SSHROYER
Date Created: 05-Feb-19 04:46 PM

WEST LINN GIS

DISCLAIMER: This product is for informational purposes and may not have been prepared for, or be suitable for, legal, engineering, or surveying purposes. Users of this information should review or consult the primary data and information sources to ascertain the usability of the information. Map scale is approximate. Source: West Linn GIS (Geographic Information System) MapOptix.



CITY OF West Linn

CITY OF WEST LINN NOTICE OF UPCOMING PLANNING COMMISSION HEARING

**PROJECT # DR-18-03/VAR-18-01
MAIL: 2/14/19 TIDINGS: 2/21/19**

CITIZEN CONTACT INFORMATION

To lessen the bulk of agenda packets, land use application notice, and to address the worries of some City residents about testimony contact information and online application packets containing their names and addresses as a reflection of the mailing notice area, this sheet substitutes for the photocopy of the testimony forms and/or mailing labels. A copy is available upon request.

PC-2 COMPLETENESS LETTER



City of
West Linn

December 5, 2018

Ali & Nina Ladha
9334 SE Hunters Bluff Ave.
Happy Valley, OR. 97086

SUBJECT: DR-18-03 application for design review at 0 Noble Lane (Corner of Noble Lane and Hoodview Ave.)

Dear Ali & Ladha:

You submitted this application on July 31, 2018. The Planning and Engineering Departments found that this application was incomplete on August 27, 2018. All required information was subsequently provided on November 14, 2018 and the application has now been deemed **complete**. The city has 120 days to exhaust all local review; that period ends April 4, 2019.

Please be aware that a determination of a complete application does not guarantee a recommendation of approval from staff for your proposal as submitted – it signals that staff believes you have provided the necessary information for the Planning Commission to render a decision on your proposal.

A 20-day public notice will be prepared and mailed. This notice will identify the earliest potential hearing date by the Planning Commission.

Please contact me at 503-742-6057, or by email at jarnold@westlinnoregon.gov if you have any questions or comments.

Sincerely,

A handwritten signature in blue ink that reads "Jennifer Arnold". The signature is fluid and cursive, written over the printed name.

Jennifer Arnold
Associate Planner

PC-3 APPLICANT'S SUBMITTAL

DEVELOPMENT REVIEW APPLICATION

For Office Use Only		
STAFF CONTACT <i>Jennifer Arnold</i>	PROJECT NO(S). <i>DR-18-B VAR-18-01</i>	
NON-REFUNDABLE FEE(S) <i>\$300; 825</i>	REFUNDABLE DEPOSIT(S) <i>\$400</i>	TOTAL <i>5125</i>

Type of Review (Please check all that apply):

- | | | |
|--|---|--|
| <input type="checkbox"/> Annexation (ANX) | <input type="checkbox"/> Historic Review | <input type="checkbox"/> Subdivision (SUB) |
| <input type="checkbox"/> Appeal and Review (AP) * | <input type="checkbox"/> Legislative Plan or Change | <input type="checkbox"/> Temporary Uses * |
| <input type="checkbox"/> Conditional Use (CUP) | <input type="checkbox"/> Lot Line Adjustment (LLA) */** | <input type="checkbox"/> Time Extension * |
| <input checked="" type="checkbox"/> Design Review (DR) <i>CLASS II</i> | <input type="checkbox"/> Minor Partition (MIP) (Preliminary Plat or Plan) | <input checked="" type="checkbox"/> Variance (VAR) <i>CLASS I</i> |
| <input type="checkbox"/> Easement Vacation | <input type="checkbox"/> Non-Conforming Lots, Uses & Structures | <input type="checkbox"/> Water Resource Area Protection/Single Lot (WAP) |
| <input type="checkbox"/> Extraterritorial Ext. of Utilities | <input type="checkbox"/> Planned Unit Development (PUD) | <input type="checkbox"/> Water Resource Area Protection/Wetland (WAP) |
| <input type="checkbox"/> Final Plat or Plan (FP) | <input type="checkbox"/> Pre-Application Conference (PA) */** | <input type="checkbox"/> Willamette & Tualatin River Greenway (WRG) |
| <input type="checkbox"/> Flood Management Area | <input type="checkbox"/> Street Vacation | <input type="checkbox"/> Zone Change |
| <input type="checkbox"/> Hillside Protection & Erosion Control | | |

Home Occupation, Pre-Application, Sidewalk Use, Sign Review Permit, and Temporary Sign Permit applications require different or additional application forms, available on the City website or at City Hall.

Site Location/Address:

0 NOBLE LANE, SITE IS UNADDRESSED BECAUSE IT IS VACANT.

Assessor's Map No.: *T2S, R1E, SEC. 26D*

Tax Lot(s): *0402*

Total Land Area: *17,517 SF*

Brief Description of Proposal:

DEVELOPMENT OF FIVE (5) RESIDENTIAL UNITS IN TWO (2) BUILDINGS ON VACANT SITE.

Applicant Name:
(please print)

ALI & NINA LAOHA, INVESTED REALTY GROUP
Address: *9334 SE HUNTERS BLUFF AVE.*
City State Zip: *HAPPY VALLEY, OR 97086*

Phone: *503-720-0021*
Email:

Owner Name
(please print)

ALI & NINA LAOHA, INVESTED REALTY GROUP
Address: *9334 SE HUNTERS BLUFF AVE.*
City State Zip: *HAPPY VALLEY, OR 97086*

Phone: *503-720-0021*
Email:

Consultant Name:
(please print)

ROBERT PRICE
Address: *3935 NE 72ND AVE.*
City State Zip: *PORTLAND, OR 97213*

Phone: *503-807-4009*
Email: *rprice1145@gmail.com*

- All application fees are non-refundable (excluding deposit). Any overruns to deposit will result in additional billing.
- The owner/applicant or their representative should be present at all public hearings.
- A denial or approval may be reversed on appeal. No permit will be in effect until the appeal period has expired.
- Three (3) complete hard-copy sets (single sided) of application materials must be submitted with this application. One (1) complete set of digital application materials must also be submitted on CD in PDF format. If large sets of plans are required in application please submit only two sets.

* No CD required / ** Only one hard-copy set needed

The undersigned property owner(s) hereby authorizes the filing of this application, and authorizes on site review by authorized staff. I hereby agree to comply with all code requirements applicable to my application. Acceptance of this application does not infer a complete submittal. All amendments to the Community Development Code and to other regulations adopted after the application is approved shall be enforced where applicable. Approved applications and subsequent development is not vested under the provisions in place at the time of the initial application.

Ali Laoha
Applicant's signature

7/30/18
Date

Ali Laoha
Owner's signature (required)

7/30/18
Date

I. Introduction

Ali and Nina Ladha, doing business as Investco Realty Group, submit the accompanying application for a Class II Design Review for development of a 5-unit attached multi-family housing project located at 0 Noble Lane, adjacent to 20322 Noble Lane, and at the intersection of Noble Lane, Hoodview Avenue, and Salamo Road. Also, the applicants are applying for a Class I Variance because the subject site will be less than 15,000 net square feet after dedications, and such Variance is required to develop five (5) units on a site of less than 15,000 square feet. The legal description of the site is T2S, R1E, Section 26D, Tax Lot 0402. The site is 17,517 square feet in total area, and is in the Parker Crest Neighborhood. Zoning is R-3 (Single Family and Multiple Family Residential/3,000 square foot minimum lot size). Preliminarily, the project carries the name "Hoodview Summit", but that could change by the time of final platting.

The site is currently vacant and appears never to have been developed for any purpose. Existing vegetation is comprised of grasses, with no trees or shrubs on the site. The site is basically flat and almost level, with some minor slope to the north and east. Gross size of the subject site is 17,517 square feet. After required dedications for additional right-of-way on both Noble Lane and Hoodview Avenue, the net site size will be 14,628 square feet. There will be no dedication on Salamo Road, as the finished street is in place, including sidewalks, and no widening of the right-of-way will be required.

The applicants propose to develop five (5) units in two structures on the site. One structure will contain three (3) units, while the second structure will contain two (2) units. All five units will be approximately the same size (approximately 2,000 square feet) and configuration, with each unit having three (3) bedrooms. Each unit will provide three (3) onsite parking spaces, two in the drive-in garage and one on the driveway. There will be no common parking area, or any area where visitors might park other than curbside parking on local streets.

The local vicinity surrounding the site is almost completely developed with similar attached housing along Hoodview Avenue and Noble Lane. According to the Parker Crest Neighborhood Association, homes in the Parker Crest neighborhood are 53% owner occupied. The applicants propose that all five units will be rental units. They will be managed by the applicants, who have their own property management organization, managing several properties in West Linn and in other urban locations.

A Pre-Application Conference Meeting was held on April 6, 2017, and the Summary Notes of that meeting are included in the application package. Based on those Summary Notes, the following sections of the West Linn Community Development Code are applicable to these two applications:

- Chapter 15: R-3
- Chapter 55: Design Review, Class II
- Chapter 75: Variances and Special Waivers – Class I Variance
- Chapter 99.060(B)

As required by the Code, a neighborhood meeting was held on June 6, 2018 at the West Linn Adult Community Center. Approximately 10 persons attended that neighborhood meeting. A complete mailing list is included with this application package, plus a sign in sheet for the meeting, written meeting notes taken by the applicants, and a recording of the meeting.

A traffic study has been completed by Lancaster Engineering, the results of which are included in their report that is included with this application package. In addition, the project engineer, Bill Kehrl, P.E. of CWK2 Consultants, has prepared a complete set of preliminary engineering drawings and calculations which are also part of the application package.

A public hearing is required for these applications before the West Linn Planning Commission. The decision of the Planning Commission is final, unless an appeal of the decision is filed in a timely manner. Any appeal goes to the West Linn City Council.

The applicants request approval of these two applications from the City of West Linn. Once approved, the applicants anticipate moving forward promptly to begin site preparation and ultimate development.

II. West Linn Community Development Code

In accordance with the items listed in the Summary Notes taken at the Pre-Application Conference Meeting held on April 6, 2017, several chapters of the West Linn Community Development Code must be addressed in the application narrative to demonstrate compliance with the various requirements, criteria, standards, and guidelines set forth in the Notes. The following is an addressing of those appropriate and applicable chapters of the Code.

- **Chapter 15 – Single-Family and Multiple-Family Residential, R-3**

15.010 PURPOSE

Comment: The intent of the R-3 zoning district is “to provide for a choice in housing types and is intended to implement the policies and locational criteria in the Comprehensive Plan for high density residential housing.” The development of the subject site will complement the housing that already exists in the immediate vicinity of the local neighborhood. The subject site is one of the few sites of this character that remains undeveloped in this area of the Parker Crest Neighborhood. The development of five (5) units on the site fits the city’s purpose to see higher density residential development in this area of the city.

15.030 PERMITTED USES

Comment: Listed among the “PERMITTED USES” is “4. Single family attached residential units”. Because the applicants propose to develop five (5) single-family attached units in two (2) structures on the subject site, the proposed land use fits within the category to allow single-family attached residential units on the subject site.

15.040 ACCESSORY USES

Comment: There will be no accessory uses built on the site. As such, this standard does not apply.

15.050 USES AND DEVELOPMENT PERMITTED UNDER PRESCRIBED CONDITIONS

Comment: The proposed use of the site is for five (5) single-family attached residential units in two buildings. This use is not one of the uses listed under this section. Therefore, this section does not apply to the current application.

15.060 CONDITIONAL USES

Comment: The proposed use of the site is for five (5) single-family attached residential units in two (2) buildings. This use is not one of the uses listed under this section. Therefore, this section does not apply to the current application.

15.070 DIMENSIONAL REQUIREMENTS, USES PERMITTED
OUTRIGHT AND USES PERMITTED UNDER PRESCRIBED
CONDITIONS

Comment: The subject site, at 17,517 square feet in gross area, and 14,628 square feet in net area, after deductions.

- In accordance with A.2, this gross lot size meets the standard for minimum lot size of 3,000 square feet for each single-family unit.
- The minimum front lot line or the minimum lot width at the front lot line shall be 35 feet, and the subject site will exceed this stated minimum (B.).
- D.1. requires the front yard be 15 feet, or 20 feet for a garage. Lot depths for this project will be in excess of 30 feet for all units.
- D.2 requires interior side yards to be five (5) feet, and the site plan illustrates that the minimum side yard setback will be 5 feet at the southerly property line. The side yard area between the two buildings will be 10 feet, and the side yard at the northerly property line, adjacent to Hoodview Avenue, will be about 15 feet to the property line and approximately 25 feet to the curbface of Hoodview Avenue. If this is considered a side yard abutting a street (because Hoodview Avenue bounds the site on the north side), required setback is 15 feet according to D.3.
- In accordance with D.4, rear yards must be 15 feet. As shown on the site plan, all rear yards will be at least 20 feet, with the rear yard for Unit #1 being the least at about 20 feet.
- In E., maximum building height is to be 35 feet for attached single family units. With each unit being three stories (including the lower garage/recreation room, maximum height will be less than 35 feet.
- F. requires that maximum lot coverage be 50 percent. With a net lot size of 14,628 square feet, allowable coverage would be 7,300+ feet. Based on the site plan, lot coverage is approximately 5,000 square feet, or 2,300 square feet less than the allowable limit.
- G. requires a floor area ration of not more than 0.45, nor less than 0.30. The site is not either Type I or Type II lands. The floor area ratio (FAR) for this site is between the minimum of 0.30 and the maximum of 0.45.
- H. requires that side walls of the structures comply with the side walk requirements set forth in this description. The basic design prepared by Mascord portrays side walls that are not exempt from this Chapter (43.040.C) because all side walls are less than 20 feet from the side lot line, the site is not steeply sloped, the basic lot (site) is more than 50 feet in width, structures are not contiguous to open space, unbuildable area, or non-residentially zoned lands. As such, the design is required to meet either A or B of 43.040. A. requires that the side elevation must be divided into "distinct planes of 700 square feet or less", while B. requires that the sidewall shall be no higher than 22 feet measured from grade. While Mascord has designed the structures with sidewalls higher than 22 feet, meaning that B. will not apply, the sidewall can be separated into areas of 700 square feet or less through the placement of windows in the sidewalls. By doing

this, the design allows for separation into smaller spaces of less than 700 square feet.

15.080 DIMENSIONAL REQUIREMENTS, CONDITIONAL USES

Comment: Because the proposed development of five (5) attached single family dwellings in two buildings on the subject site is not a conditional use, this standard does not apply to this project.

15.090 OTHER APPLICABLE DEVELOPMENT STANDARDS

Comment: After review of the 11 standards listed under this particular standard, the applicant finds that the basic design either meets each of the individual sections of the appropriate and applicable Chapters set forth in 15.090.A., as addressed later in this narrative.

In 15.090.B., the requirement is that Chapter 55, Design Review, applies "to all uses except detached single-family dwellings, residential homes and residential facilities." Because this project involves five (5) single-family attached dwellings, Chapter 55, Design Review, is applicable. The requirements, standards, and criteria of Chapter 55, Design Review, is addressed later in this application narrative under 55.100.

• **Chapter 55 – Design Review, Class II**

Comment: Based on the Summary Notes from the Pre-Application Conference Meeting held on April 6, 2017, one of the requirements for this application is a Class II Design Review. Design Review is encompassed in Chapter 55 of the West Linn Community Development Code (CDC), and the appropriate and applicable portions of that Chapter are addressed as follows:

• **55.010 Purpose and Intent – General**

Comment: As stated in the opening paragraph of this section, the purpose of Design Review is "to conserve and enhance the appearance of the City and to promote functional, safe, and innovative site development." Design Review seeks to ensure compatibility between adjoining uses, the vehicular access and circulation are safe, and areas of the City are safe, attractive and welcoming. The Design Review process also seeks to reduce reliance on autos by encouraging other modes of transportation.

The proposed five (5) unit development on the subject site seeks to achieve all of the goals and objectives of Design Review and to make the development a good addition to the City's residential framework. By blending in with the other already developed areas of the City, and especially of the Parker Crest Neighborhood, this project will achieve the purposes of the Design Review process.

• **55.020 Classes of Design Review**

Comment: As noted in the Summary Notes, this project will be subject to the Class II process of Design Review. This class is set forth in 55.020.B. Because the project is a "major" multi-family construction project, the Class II Design Review process will apply.

- **55.025 Exemptions**

Comment: Exemptions to the Design Review process are listed under A. through D. of this section. This proposed project does not fit any of the exemptions listed and, therefore, is not exempt from the Design Review process.

- **55.030 Administration and Approval Process**

Comment: Under the provisions of 55.030.A., a pre-application conference is required for a project as is proposed. A Pre-Application Conference Meeting was held on April 6, 2017. The Summary Notes for PA-17-13 are included in this application package, thus fulfilling 55.030.A. 55.030.B. requires that the record owner(s) of the property or the authorized agent. Ali and Nina Ladha (Investco Realty Group) are the record owners of the site and have signed the required application. Robert Price, Consultant, is the planning and development consultant for the property owners, and is authorized to sign for the owners, and to represent the project on behalf of the owners.

- **55.060 Staged or Phased Development**

Comment: This project is not planned to be developed in stages.

- **55.070 Submittal Requirements**

Comment: As required under A., B., and C. the proper application form, signed by the applicants is included, as are the Summary Notes from the Pre-Application Conference Meeting held on April 6, 2017, and documentation from the Parker Crest Neighborhood Association. In accordance with 55.070.D.2, the application package includes a site analysis, a site plan, a grading plan, architectural drawings of the proposed two (2) structures that will contain the five (5) units, a utility plan, and addressing of the tree issues in the Tree Technical Manual, where necessary. Certainly, all of the items set forth in the section are included, except any tree issues because there are no trees on the site at the present time. In addition, samples of all exterior building materials and colors are also included.

- **55.100 Approval Standards – Class II Design Review**

Comment: As required for this section in 55.100.A., various other Chapters of the CDC must be addressed.

1. Chapter 34 – Accessory Structures, Dwelling Units, and Uses

Comment: There are no accessory structures, units, or uses planned for this site and the five (5) attached single family residential dwellings.

2. Chapter 38 – Additional Yard Area Required, etc.

Comment: There is no structure on the site at the present time, resulting in 38.020, No Yard Required not being applicable to this site. Also, 38.030, Setback from Street Centerline Required is not applicable to this site because the three streets that bound the site (i.e., Salamo Road, Hoodview Avenue, and Noble Lane) are finished streets and do not

required widening. The fourth side of the site (south) is adjacent to established dwellings.

Section 38.040, Exceptions to Yard Requirements, does not apply because the two (2) structures that will contain five (5) dwelling units are designed specifically for the subject site and in compliance with all development standards. As a result, no Exceptions are necessary.

There will be no storage in the front yard of any of the five (5) units, in accordance with 38.050, Storage in Front Yard.

As designed by Mascord, any projections into any required yards are within the limits of allowable projections. There will be no easements to allow projections into any yards.

3. Chapter 40, Building Height Limitations

Comment: This section does not apply because it has been repealed by Ordinance 1604.

4. Chapter 42, Clear Vision Areas

Comment: Clear vision areas are required at all corners of the property that is adjacent to an intersection. As illustrated by the Site Plan, the location and placement of the buildings on the site will allow for any needed and required clear vision areas at the intersection of Hoodview Avenue and Noble Lane, or the intersection of Hoodview Avenue and Salamo Road. This site is not within the Willamette area of the City, and, as such, any allowance for exceptions does not apply.

5. Chapter 44, Fences

Comment: The applicants do not plan for any fences along extended building lines between units, or around the perimeter of the site. However, any fences constructed during or after site development will meet the requirements of this section, particularly 44.030 and 44.050.

6. Chapter 46, Off-Street Parking, Loading and Reservoir Areas.

Comment: The Site Plan illustrates that there is no "parking area" within the subject site. Instead, all parking will be on the site for all residents of the five (5) units. Each unit will have two (2) off-street parking spaces, two in the garage that is within the building, and a third space on the driveway. Because each driveway is approximately 35 feet in length (garage door to street), up to two vehicles could be parked on the driveway, making for a total of four (4) vehicles parked at each unit. However, the applicants are only planning for three vehicles for each unit. There will be no loading area(s) for dwellings on this site, there are no "reservoir" area(s) on the subject site, and there are no other uses on the site other than single family attached residential.

7. Chapter 48, Access, Egress, and Circulation

Comment: The site is bounded by three (3) public streets, Salamo Road to the west, Hoodview Avenue to the north, and Noble Lane to the east. Access will be taken only from Noble Lane. Each of the five (5)

attached dwelling units will have its own access point (driveway) with Noble Lane. This will result in Option 3 being the only access option available to the site (48.025.B.3.c). Each driveway will be separate, and there will be no shared driveways. The site has no potential access points on either Salamo Road or Hoodview Avenue. Noble Lane is the only public street that will serve as access to the subject site.

8. Chapter 52, Signs

Comment: There will be no signs on the subject site, except for address signs. Depending on the requirements of the U.S. Postal Service, a gang mailbox may be placed on Noble Lane for use by all five units.

9. Chapter 54, Landscaping

Comment: At the present time, there is no landscaping on the subject site. There are no trees on the site, and the only vegetation is grasses that grow naturally. There is a line of arborvitae along the southerly property line, but those appear to be located on the adjacent property. This line of arborvitae may be useful in screening view between Unit #5 and the existing dwellings to the south.

Each unit will be landscaped by the applicants, and some individual landscaping may be done by residents of individual units after they move in. Street trees already exist along Noble Lane. With little area on the site for extended landscaping, the applicants do not believe that a landscape plan will illustrate anything more than the final construction plans for the two structures. Any nuisance plants will be removed, and no new nuisance plants will be planted on the site. The site will be planted in grass for the yard areas. There are no parking areas as part of this project that require landscaping. Overall, approximately 25% of the site (3,657 sf of the net site area of 14,628 sf) will be landscaped in that they will have grassy lawns, gardens, and some perimeter landscaping.

• 55.100.B Relationship to the natural and physical environment

Comment: Section 55.100.B. requires that a relationship be created between the proposed structures and dwelling units and the natural and physical environment. Because the site has no trees on it at the present time, there are no heritage trees that exist. There are no tree clusters or groves. As such, items B.1. and 2.(a and b) do not apply to the site. Again, because there are no trees on the site, there is no need for determination of driplines.

As stated previously, "*The site is basically flat and almost level, with some minor slope to the north and east.*" Site grading will do little to change that topographic character. Onsite drainage, and all site-based stormwater management, has been engineered by the project engineer to reflect that site and preserve the basic character (55.100.B.3)

The subject site is not one that is subject to slumping, sliding, and hazardous movement (55.100.B.4). Therefore, no specific mitigation measures need to be taken to protect the proposed development.

Because the site is vacant, separation between buildings is not an issue. When the site is developed with two (2) structures containing a total of five (5) dwelling units, separation of buildings, both on-site and off-site, has been taken into account. There will be five (5) feet between the southerly property line and the southerly property line of the site. Combined with the setback for the existing dwelling structure to the south, there will be adequate separation between buildings. Similarly, the two (2) structures to be built on the site will have 10 feet of separation between them. With the setback of over 15 feet between the second structure (containing 2 dwelling units) and the right-of-way of Hoodview Avenue, there is no issue of separation in this instance. These planned separations will serve to provide for light, air circulation, and fire protection (55.100.B.5).

Based on the preliminary design prepared by Mascord, the two structures will complement the architectural style of the existing residential structures on adjacent properties to the north, south and east. To the north is Salamo Road, and Rosemont Middle School across Salamo Road. While the design of the two structures reflects today's styles and trends, it will be both complementary and compatible with the styles that have already been established in the Parker Crest Neighborhood. The two structures reflect the "step down" transition from development around it by going from larger structures to smaller ones, and finally to the Hoodview Avenue right-of-way. The extended setback on the north side of the site promotes the transition to a more open setting. Thus, the transition between structures along Noble Lane and across Hoodview Avenue is promoted by the design of the structures.

The design of the structures is not a contrasting style of architecture, but one that is contemporary and complementary through distance between structures, screening (on the south side along the property line), and some minor grade variation. The size of these structures creates a scale in relationship between the environment and the humans who will occupy the site. The windows and the main entries reflect this scale and the attempt to make the structures more warm and welcoming. The east facing orientation of the structures will aid in capturing solar energy to aid in heating of the units earlier in the day, and to promote use of shade later in the day. Building orientation will protect from the prevalent wind direction (from the northwest in summer and from the southwest in winter), making the development sensitive to the local climatic conditions (55.100.B.6).

While this project is a single family attached residential housing development, there are no parking lots, or auxiliary parking areas. The front façade exhibits breaks in the building line by the recessed main entries and porch roofs, the second floor projections, and the minimization of the garage entry through use of a single garage door for the two indoor parking spots, lending credence to the residential nature of the front of the buildings.

The size of this project, five (5) units contained in two (2) separate buildings, is not so large that there are, or need to be accessways, courtyard type parking areas, or internal driveway and pedestrian circulation. On a site this size, these features may not even be possible. Because the site is only 14,628 square feet, or 0.34 acre, and the density is five (5) units, this is a small project that may be considered an infill project on a site that is basically surrounded by residential development. This is a site where some neighbors questioned when or if this site would be developed.

With the site being bounded on three sides by public rights-of-way, building orientation is problematic. No vehicle access is proposed on Salamo Road, for obvious reasons, meaning that access must be on either Hoodview Avenue or Noble Lane. Because the length of frontage on Hoodview Avenue is much less than the frontage on Noble Lane, trying to front on and have access to Hoodview Avenue does not make sense and would be difficult to accomplish. As such, the primary street for this development is Noble Lane. Orienting the development inward to Noble Lane, and being consistent with the existing development, is really the only practical and suitable option. The location of the project site is not conducive to public transit. Routes 35 and 154 serve the West Linn area, but neither route is close enough to the site to make transit use a practical consideration. Therefore, the project will be primarily auto oriented, as is most of the residential development in the Parker Crest Neighborhood.

The structures will be three stories in height, including the garage/recreation area on the ground level, or less than 35 feet. With front setback of about 35 feet, the height-to-width ratio is about 1:1. The ratio of the existing residential structures appears to be greater than 1:1, with two story structures and front setbacks of less than 30 feet. However, the difference between the ratio of the existing structures and that for the proposed structures is not significant and will not appear to be inappropriate or out of place (55.100.B.7).

- 55.100.C Compatibility between adjoining uses, buffering, and screening

Comment: According to 55.100.C.1, "*no buffering is required between single family homes and duplexes or single-family attached units.*" Although no buffer is required, an existing line of arborvitae along the southerly property line will serve to buffer and screen the proposed development from that existing residential development across the southerly property line. This stand of vegetation will provide some visual and noise protection between similar uses, even with a 5-foot setback. Characteristically, residential uses are not large noise generators, so this stand of vegetation combined with appropriate setbacks, should provide some degree of separation and protection for the two residential uses on different but adjoining properties in terms of visual barriers and absorption of air pollution and dust. This row of vegetation will be a good "built-in" barrier or separator.

- 55.100.D Privacy and Noise

Comment: Each of the five units will have a patio and rear yard area for outdoor recreation (both passive and active), and internal fences to separate the individual "rear yards" may be a possibility. This project area will be separated from existing development to the south by a line of vegetation that will add to the privacy of residents on both sides of the property line. Since residential uses tend to be fairly passive in terms of noise, there should be little to no conflict between the various units of this project, and between other development areas to the north, east and south. Lighting, both indoor and outdoor, for each unit will be minimized to the greatest extent possible in order to provide privacy and protection from direct lighting and glare.

- 55.100.E Private outdoor space

Comment: Each of the five units will have their own outdoor area. These are generally considered to be "back yards" that will face Salamo Road. These will not be shared outdoor recreation areas, nor will any part be indoors. These outdoor areas will be spacious and will be in areas that are fully usable in one way or another for the resident's enjoyment.

- 55.100.G Demarcation of public, semi-public and private spaces

Comment: The overall plan for the subject site will result in a very clear demarcation between public and private spaces. Being bounded by three public streets, it is in the best interests of the applicant, and the future residents, to keep a distinct separation between public areas on those three sides and the private nature of single family dwellings. Some interior fences may be constructed to separate "back yard areas", to provide an even greater demarcation between private spaces.

- 55.100.H Public transit

Comment: The subject site is not served directly by public transit. Unfortunately, the site is not that close to public transit. Lines 35 and 154 serve the West Linn area, and neither route serves this area of West Linn. As such, public transit should not be an issue for this application.

- 55.100.I Public facilities

Comment: This area of the City, and the Parker Crest Neighborhood, is mostly built out. As a result, public facilities such as streets, water, and sanitary sewer are already in place to serve the proposed development. Some dedication for additional right-of-way along Hoodview Avenue and Noble Lane may be necessary, but only for sidewalk and planter strip purposes. The streets are already in place and are constructed curb-to-curb. Water and sanitary sewer lines are in place and are of sufficient capacity to serve the project. Connections will be made on the Noble Lane side of the site. There will be no rebuild or realignment of the local streets, based on the nature of the proposed development. Storm detention and treatment has been designed by the project engineer to current City standards in order to provide for the needs of the site based on the type of development proposed. This design will not create any

adverse off-site impacts. Solid waste and recycling will be stored in the individual garages, and there will be no "common" storage areas for solid waste and recycling.

- 55.100.J Crime prevention and safety/defensible space

Comment: Windows on all exterior sides of the structures will provide good visibility in all appropriate directions. Laundry and service areas for each unit will be interior to each unit, and there will not be common areas that could require protection for crime prevention. Depending on the wishes of the U.S. Postal Service, there may be either individual mail delivery to each unit, or a "gang" mailbox structure somewhere along the frontage of the site on Noble Lane. If there is a gang mailbox situation, it will be located so as to be in an open area that is visible from all units, and from existing units across Noble Lane. Exterior lighting will be provided for and on each unit, but lighting levels, directions, and shielding will all be taken into account when considering the location of individual exterior lighting fixtures. It is not anticipated that there will be the need for security fencing for utilities because there likely will not be any "bulk" utility facilities that would be large enough for security fencing. There will be street lighting on the streets that bound the site, to provide better safety and crime prevention. The location of the street lights are illustrated on the engineer's technical plans.

- 55.100.K Provisions for persons with disabilities

Comment: The route between the main entry and the street will be provided to make access by persons with disabilities a bit easier and more convenient. This route to each unit will likely be up the driveway from the street to the front entry stairs. This hard surfaced route will make it somewhat easier for those with disabilities to access the units.

- 55.100.L Signs

Comment: There will be no signs on the site, other than address signs. No commercial activities will take place within this project site. At most, a resident might operate a home-based business, or home occupation, but no signs will be allowed to "advertise" or "identify" the home-based business. There will be no "monument" sign to announce this project, located anywhere on the site. Because the site is small, the development will largely identify itself by virtue of its unique location on three public streets.

- 55.100.M Utilities

Comment: Existing utilities in the local neighborhood are already located and sized to serve the proposed development of five (5) residential units. All utilities, whether public or private, will be buried underground.

- 55.100.N Wireless communication facilities (WCF)

Comment: There are no existing wireless communication facilities on the subject site, and it appears unlikely that there will ever be wireless communication facilities located anywhere on the subject site.

- **55.100.O Refuse and recycling standards**

Comment: Because solid waste and recycling will be managed by collection from each individual unit, receptacles will be kept in or near each garage for ease of access. The solid waste/recycling hauler for the local area will provide garbage and recycling bins or receptacles for use by the individual resident, and it will be incumbent on the residents to work within the hauler's schedule for service to provide the receptacles for collection or emptying. Keeping the receptacles "out in the open" on the driveway or in a side yard, will be prohibited by the applicant.

- **55.110 Site Analysis**

Comment: A Site Analysis map is required for each project such as this one. A Site Analysis has been prepared by the project engineer, and has all of the required elements listed in A. and B.1 through 10 included in the mapping.

- **55.120 Site Plan**

Comment: A Site Plan map is required for this project. The Site Plan map has been prepared by the project engineer and contains all of the elements listed under this subsection, from 55.120.A through H.

- **55.125 Transportation Analysis**

Comment: Referring to 48.025 of the CDC, it appears that this project may not require a formal traffic impact study. As noted previously, this project will have only one option for access control, specifically, 48.025.B.3.c. However, a Technical Memorandum, dated December 22, 2017, prepared by Lancaster Engineering has been completed for this project. Because this is a small project, only 5 units, site generated traffic should not exceed about 5+ vehicle trips per day, based on the latest edition of the ITE Manual. This will result in about 28 total daily trips. All five units will access Noble Lane via separate driveways, and no shared driveways will exist. With the exception of the occasional delivery van or truck, all vehicles associated with the proposed development will be personal vehicles. The anticipated site generated traffic volume of about 28 vehicles per day will be far beneath the threshold of 250 trips set forth in 85.170.B.2.c.1.C.1.

- **55.130 Grading and Drainage Plans**

Comment: The project engineer has prepared grading and drainage plans as part of the technical plan set for this project. These plans meet the requirements of the City of West Linn, including a sufficient scale, location and extent of grading, and the like as listed in 55.130.A. through F.

- **55.140 Architectural Drawings**

Comment: Architectural drawings for this project have been prepared by Mascord, and are included with the application. These graphic exhibits include, but may not be limited to, a Site Plan, building elevations

and sections, and material and color boards. These architectural drawings accurately illustrates what the project will look like.

- **55.150 Landscape Plan**

Comment: No site specific landscape plan has been prepared for this application because the site is a basic development involving only five units in two buildings.

- **55.170 Exceptions to Underlying Zone, Yard, Parking, Sign Provisions, and Landscaping Provisions**

Comment: The only "exception" being requested is a Class I variance to allow for five (5) units to be built on a size that is slightly less than 15,000 square feet (14,628 net square feet, after deductions). Otherwise, no exceptions are proposed to any uses in the R-3 zone, yard dimensions, signs, or landscaping provisions. While we doubt that any exception will be identified for this project under this section, it is likely that any exception will be less than 20% of any required setback, or 10% of required parking.

- **55.180 Maintenance**

Comment: Because these will be rental units, the applicant will be responsible for maintaining the site and any features, uses, etc. that are associated with it. As the owner of other rental properties in West Linn and other parts of the metro area, the applicant is fully aware of the need for maintenance and the property owner's obligation to keep up with maintenance.

- **Shared Open Space**

Comment: There will be no shared open space within this project site.

- **55.195 Annexation and Street Lights**

Comment: The subject site is already within the City of West Linn, so no new annexation activities are required. Therefore, the applicant (developer) is not required to pay expenses for street lighting energy and maintenance. In fact, street lights already exist on Hoodview Avenue and Noble Lane. Nothing more is required of the applicant as a result of this section.

- **Chapter 75, Variances and Special Waivers**

Comment: At 3,000 square feet per unit in the R-3 zone, the overall site with five (5) units should be 15,000 square feet. Although the site is currently a gross size of 17,517, in excess of 15,000 square feet, some deductions for right-of-way along Hoodview Avenue and Noble Lane will result in a net site size of 14,628 square feet, or just less than 15,000 square feet needed to avoid a Variance. This difference is only 372 square feet, or less than 2.5% square footage of the total required. The two alternatives are: 1.) reduce the number of units to 4; or, 2.) apply for a Class I Variance. The applicant has chosen to apply for a Class I Variance for the 372 square feet that is short of the required lot area of 15,000 square feet for five (5) units. Based on 75.020.A.1., the lot area

can be modified by up to 5% if the application meets certain requirements. These requirements are addressed below:

a. *Provides for a more efficient use of the site;*

Comment: The actual difference between 4 units on 14,628 square feet and 5 units as proposed on the same lot area is one where the land is better utilized, the use of public utilities and services is more efficient and those facilities are already constructed and installed, and will not be overburdened because of 1 additional unit (75.020.A.1.a). At 4 units on this site, the density would be 3,658 square feet per unit. If five (5) units are developed, the density will be 2,926 square feet per unit. It would appear that six (6) units could be accommodated on this site, but density is limited to five (5) units.

b. *Preserves and incorporates natural features into the overall design of the project;*

Comment: The site is a "left over" site that is relatively simple and featureless. There are no trees on the site, no structures, and no distinctive vegetation. Only grasses grow on the subject site at this time. While the design reflects the relatively few attributes of the site, the lack of natural features makes the site fairly bland and easy to work with (75.020.A.1.b).

c. *Does not adversely affect adjoining properties in terms of light, air circulation, noise levels, privacy, and fire hazards;*

Comment: The site is separated from the other development areas of the City and the Parker Crest Neighborhood, and needs to be made a part of the local fabric. The addition of 5 units on the subject site will not alter the lack of impacts created on adjoining properties in terms of light, air, circulation, noise levels, privacy and fire hazards. All setbacks will be satisfied, and the existence of a line of arborvitae along the southerly property line will add greatly to the privacy of the nearest adjacent dwelling, as well as air circulation, availability of light (both direct and indirect), internal circulation, and fire hazards. As long as the plan reflects the proper measures to insure fire protection, the site will be better used for 5 units than 4 units (75.020.A.1.c).

d. *Provides for safe vehicular and pedestrian access to the site and safe on-site vehicular and pedestrian circulation.*

Comment: In terms of vehicular and pedestrian circulation both on-site and off-site, there will be no difference in these access and circulation issues for either 4 units or 5. Because there will be no difference between any impacts for 4 units or 5 units, there is no reason under this requirement that the Class I Variance cannot be acceptable and approved (75.020.A.1.d). See Technical Memorandum prepared by Lancaster Engineering.

With regard to off-street parking and number of spaces, the applicant is proposing the full number of required spaces for 5 units, without adverse impact to either the site or the surrounding neighborhood. All required parking spaces will "fit" onto the subject site, resulting in no adverse

impacts on parking for 5 units. No modification of up to 10% is being requested, as allowed by this requirement (75.020.A.2).

No dimensional sign requirements are being varied or modified as a result of fitting 5 units on the subject site, as opposed to 4 units, because there will be no signs on the site other than address signage (75.020.A.3).

No landscaping plan has been prepared for the site as a whole. Rather, landscaping will be provided for each individual unit. There is relatively little space for larger scale landscaping. This "lot-by-lot" landscaping approach will continue to be useful and efficient and will have no adverse impact on adjoining property. And with regard to incorporating natural features into the overall design of the site, there are no distinctive natural features on the site that can be identified and incorporated into the site plan (75.020.A.4).

- 75.020.B Class II Variance Approval Criteria

Comment: None of this portion of the Chapter is applicable because the Variance needed is a Class I Variance, not a Class II Variance.

- 75.020.C Special Waivers

Comment: Because there are no "special waivers" required for this application, this section of the Chapter does not apply.

- 75.050 Application

Comment: In accordance with the requirements of this section as listed in A. through F., all needs have been satisfied. This variance request is filed by the applicant who is also the property owner. It should be noted that items B., D., and E. refer specifically to a Class II variance. Because this application is for a Class I Variance, these items do not apply. As required by C., a completed application form, signed by the applicant, has been prepared and includes a narrative addressing the approval criteria set forth in 75.020. The required site plan is also part of the application for the Class I Variance. This site plan is the same site plan as prepared for the Design Review application. Finally, as required by F., the applicant submits the required fee with this application.

- 75.060 Site Plans and Map

Comment: As required under this section, the site plan prepared for this project is submitted with the application. This site plan contains the information required by 75.060.B.1 and 2. 75.060.B.3 does not apply because there is no request for a variance to building height provisions.

- **Chapter 99 Procedures for Decision Making: Quasi Judicial**

Comment: Section 99.038, *Neighborhood Contact Required for Some Applications*, sets forth the applications that require a neighborhood contact. As noted in the Summary Notes of the April 6, 2017 Pre-Application Conference Meeting, "The application requires a neighborhood meeting per 99.038". A neighborhood meeting with the

Parker Crest Neighborhood Association was held on June 6, 2018. All of the requirements for this meeting (i.e., mailed notifications, sign-in sheet, notes from that meeting, etc. are included with this application.

Within this Chapter, 99.060.B specifies that the Planning Commission shall have authority "*to approve, deny or approve with conditions*" certain types of applications, including Design Review, Class II (99.060.B.2.h). The applicant is aware of this requirement, and agrees with the determination that this application must be reviewed in an advertise, noticed public hearing.

Section 99.070.A specifies that certain applications can be consolidated for review by the Planning Commission. This means that the two applications submitted by the applicant, Class II Design Review and Class I Variance, will be consolidated and heard jointly by the Planning Commission. The applicant understands this requirement, and accepts this process.

City of West Linn
PRE-APPLICATION CONFERENCE MEETING
SUMMARY NOTES
April 6, 2017

SUBJECT: Application for a 5 unit attached multi-family housing development and Class I variance for lot size at 0 Noble Lane (adjacent to 20322 Noble Lane).

FILE: PA-17-13

ATTENDEES: Applicants: Investco Realty Group: Ali & Nina Ladha
Staff: Jennifer Arnold (Planning), Amy Pepper (Engineering)

The following is a summary of the meeting discussion provided to you from staff meeting notes. Additional information may be provided to address any "follow-up" items identified during the meeting. These comments are PRELIMINARY in nature. Please contact the Planning Department with any questions regarding approval criteria, submittal requirements, or any other planning-related items. Please note disclaimer statement below.

SITE INFORMATION:

Site Address: 0 Noble Lane (adjacent to 20322 Noble Lane) (21E26D tax lot 0402)
Site Area: 17,517 square feet
Neighborhood: Parker Crest NA
Comp. Plan: High density residential
Zoning: R-3 (Single Family and Multiple-Family Residential / 3,000 square foot minimum lot size)

Applicable code: CDC Chapter 15: R-3
CDC Chapter 55: Design Review- Class II
CDC Chapter 75: Variances and Special Waivers-Class I variance
CDC Chapter 99.060(B)

PROJECT DETAILS:

The proposal is to develop multi-family housing similar to the surrounding development of Hoodview Estates. Development on this lot would require dedications along Hoodview Avenue and Noble Lane. This dedication would adjust the front and side property lines behind the existing sidewalks. The applicant proposes two options for this development. Option 1: develop one structure with 5 housing units on the one unit of land. This would require a Class I variance to allow for 5 units on less than 15,000 sq. ft. of land. Option 2: would be to construct one multi-family structure with 4 units to avoid the Class I variance because the site would be over 12,000 sq. ft. This development would require extension of the existing sewer line on Noble Lane to the proposed site.

Engineering/TVFR Comments: Contact Amy Pepper at apepper@westlinnoregon.gov for Engineering comments and Ty Darby at tdarby@tvfr.com for TVFR comments.

PROCESS:

The application requires a neighborhood meeting per CDC 99.038. Please follow those requirements very carefully. Contact the Parker Crest Neighborhood Association, at ParkerCrestNA@westlinnoregon.gov

Land use applications include a Class II Design Review (Chapter 55) and a Class I Variance (Chapter 75). The CDC is online at <http://westlinnoregon.gov/cdc>.

A traffic study may be required. The applicant should verify CDC Chapter 48.025 for requirements.

N/A is not an acceptable response to the approval criteria. The submittal requirements may be waived, but the applicant must first identify the specific submittal requirement and request, in letter form, that it be waived by the Planning Manager and must identify the specific grounds for that waiver. The waiver may or may not be granted by the Planning Director. Waivers may also be subsequently overruled by the decision making body.

Submit the application to the Planning Department with an application form signed by the property owner. The Class II Design Review permit application deposit/fee is as follows: Based on construction value (CV), less than \$100,000 of CV the deposit is 4% of CV (\$2,000 minimum deposit); CV of \$100,000<\$500,000 the deposit is 4% of CV (\$8,000 Maximum deposit); CV of \$500,000+ the deposit is \$4,000 plus 4% of CV (\$20,000 maximum deposit); an inspection fee is assessed for all projects with a fee of \$300. An application fee for the Class I variance is \$825.

Once the application and deposit/fee are submitted, the City has 30 days to determine if the application is complete or not. If the application is not complete, the applicant has 180 days to make it complete or provide written notice to staff that no other information will be provided. Once the submittal is deemed complete, staff will provide notice per CDC Chapter 99. For a standard design review application, staff will schedule a public hearing with the Planning Commission. Appeals of the Planning Commission's decision on a standard subdivision application are heard by City Council.

Typical land use applications can take 6-10 months from beginning to end.

DISCLAIMER: This summary discussion covers issues identified to date. It does not imply that these are the only issues. The burden of proof is on the applicant to demonstrate that all approval criteria have been met. These notes do not constitute an endorsement of the proposed application **or provide any assurance of potential outcomes.** Staff responses are based on limited material presented at this pre-application meeting. New issues, requirements, etc. could emerge as the application is developed. **A new pre-application conference would have to be scheduled one that period lapses and these notes would no longer be valid. Any changes to the CDC standards may require a different design or submittal.**

APPLICANT NOTE:

Upon completion, submit this form with your Current Planning development application Attachment E

NEIGHBORHOOD MEETING AFFIDAVIT OF MAILING

STATE OF OREGON)
COUNTY OF CLATSOP) ss
County of CLATSOP RP)
CITY OF WEST LINN)

I, ROBERT PRICE, being duly sworn, depose and say that on the 16th day of MAY, 2018 I caused to have mailed to each of the persons on the attached list a notice of a meeting to discuss a proposed development at NOBLE LAKE HOODVIEW SUMMIT, a copy of which notice so mailed is attached hereto and made a part hereof.

I further state that said notices were enclosed in envelopes plainly addressed to said persons and were deposited on the date indicated above in the United States Post Office with postage prepaid thereon.

Signature: [Handwritten Signature]

Subscribed and sworn to, or affirmed, before me this 30th day of July, 2018.



[Handwritten Signature]
Notary Public for the State of OREGON
County of MULTNOMAH

My Commission expires: 10/28/2019

APPLICANT NOTE:

Upon completion, submit this form with your Current Planning development application

NEIGHBORHOOD MEETING AFFIDAVIT OF POSTING NOTICE

Name of Applicant ALI & NINA LADHA, dba INVESTED REALTY GROUP
Subject Property: Tax Lot(s) 0402 Tax Map(s) T2S, R15, SECTION 26D
Address or General Location: 0 NOBLE LANE

I, ALI LADHA, do swear or affirm that I am (~~represent~~) the party
initiating interest in a proposed FIVE (5) UNIT DEVELOPMENT
affecting the land located at 0 NOBLE LANE
and that pursuant to CDC 99.03B did on the 17th day of MAY, 2018
personally post the notice indicating that the site may be proposed for a CLASS II DESIGN REVIEW
AND A CLASS I VARIANCE application.

The sign was posted at ON WEST SIDE FACING SALAMO ROAD, ON NORTH SIDE FACING HODDUM, AND ON THE EAST SIDE FACING NOBLE LANE

 Map attached showing approximate sign notice location. (check if attached)

This July day of 30th, 2018

Ali Ladha

Signature

Subscribed and sworn to, or affirmed, before me this 30th day of July, 2018

Nancy Lea Gaylord

Notary Public for the State of OR

County of Clackamas

My Commission expires: 02, 01, 2021



Public Notice

This site will be the subject of as development proposal for a five unit attached housing project. Project sponsors are Ali and Nina Ladha of Investco Realty Group. An informational meeting will be held to review the site plan and to discuss this project before an application is submitted to the City of West Linn

Date: Wednesday, June 6, 2018 at 6:00 PM

**Place: West Linn Adult Community Center
1180 Rosemont in West Linn**

For additional information please call Ali Nadha at 503-720-0021 or Robert Price at 503-807-4009

Minutes of the Meeting - Parker Crest Neighborhood

June 6, 2018 at the West Linn Adult Community Center
1180 SW Rosemont Road, West Linn

Attendance: See attached "Sign Up Sheet" – 12 people including
Ali and Nina Ladha, and Robert Price, Consultant

The meeting started at 6:00 PM on Wednesday, June 6th at the West Linn Adult Community Center. A sign-up sheet was provided and attendees were asked to sign in. The applicants, Ali and Nina Ladha, provided a 24" x 36" site plan, copies of the site survey, and renderings of the proposed design of the new residential units for review by all in attendance.

Ali Ladha explained the goal of the project, to provide 5 quality residential units in 2 structures on the site. Each unit will be approximately 2,000 square feet in floor area. Ali explained the layout of the units and the siting of the structures on the 17,500 sf (approx.) site. He also explained some of the history of the site, in which Ali and Nina are not the first to propose development of the site. For some reasons, none of the previous development proposals moved forward.

Robert Price, Consultant to Ali and Nina Ladha, explained the process in the City of West Linn for obtaining approval for the project. A public hearing will be held before the West Linn Planning Commission on this application when it is prepared, submitted, deemed complete and a hearing date is set by the city.

One of the first subjects to arise during the open discussion of the project was whether the units would be owner occupied or renter occupied. Lynne Saphir who, with her husband Bill, own and live in the property directly adjacent to the east, was concerned about renters occupying the units because she felt that renters might not be able to take care of the properties and may drive down home values. A discussion ensued about owner-occupied versus renter-occupied units. Ali pointed out that the goal is not to affect the existing community and they always look for up and coming professionals families who are looking for safe places with great schools to raise their families.

Lynne Saphir stated that she felt the site itself has not been "kept up" and she wondered if this is indicative of how the developed site would be managed. Nina Ladha provided a response, stating that they own several properties, not only in West Linn but in other locations, and she personally makes sure those rental properties are well maintained and occupied by qualified and responsible tenants.

On street parking was also an issue. Folks in general are worried about how the city dictates, manages and controls on street (curbside) parking. While the Ladha's are not able to control the on street parking, they recognize it as an issue

in the neighborhood. The current plan is for two full sized garages for each unit, plus a driveway of sufficient length to allow for parking of a third vehicle on the premises, off the street. Lynne Saphir is also worried about short term parking by contractors, workers, material suppliers, etc. and how parking would be managed during the construction period.

Angela Sullivan expressed a concern that the design of the new units, while current and contemporary in its design, may not fit with the rest of the neighborhood design. Gregg Creighton, a local resident and an architect in his own right, stated that the design reflected the current trends and styles.

Robert Price provided a brief discussion of public facilities and services to the site, and that all existing facilities and services are of adequate size, capacity, and general service levels to provide for the proposed 5 new residential units.

There were no other concerns for views expressed by others.

Nina Ladha wrapped up the meeting by stating that she would lead a short tour of local properties they owned, in order to demonstrate that the Ladha's are good, conscientious property owners and landlords. Lynne and Bill Saphir agreed that they would like to see Ali and Nina's properties.

The meeting concluded at approximately 7:10 PM.

Sign Up Sheet

Investco Realty Group

Meeting with Parker Crest Neighborhood Association

Date: Wednesday, June 3, 2018, 6:00PM -7:00PM

Where: West Linn Adult Community Center

	NAME	Address	PHONE	EMAIL
1	KEVIN SEGURA	20367 NOBLE LN, WEST LINN	503 347-0766	kevunmedia@mail.com
2	Rita Segura	" " "	503-867-2994	rita.segura@comcast.net
3	Gregg Creighton	20413 S. Noble Ln.	503-635-0777	greggcreighton@comcast.net
4	LYNNE SAPHIR	20322 Noble Ln, WL	(503) 449-2944	Lpsaphir@aol.com
5	BILL SAPHIR	" "	" "	" "
6	Angela Sullivan	20213 Hoodview	503-358-8182	angela.sullivan.sip@gmail.com
7	Lois Wagner	20371 Noble Ln	503-390-7160	Loiswagner@sprintmail.com
8	Cardace Toyama	20315 Noble Ln	971-266-9172	569Kawai@gmail.com
9	Kimo Toyama	" "	" "	" "
10				
11				
12				
13				
14				
15				

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INVESTCO

to me

Greetings,

I was at City Hall today to speak with the planners about the parcel of land on Hoodview Ave, that we have been discussing.

Based on our previous phone call, it's my understanding that this is the same parcel of land that the Parker Crest NA discussed different applicant. It is my understanding that if the application is substantially the same as the previous application and the 18 association meeting is sufficient and a new meeting is not needed.

The planner that I spoke with is Peter Spir.

Please let me know your thoughts on the matter.

Best regards, Bill R.

From: INVESTCO REALTY GROUP [mailto:investcorealtygroup@gmail.com]
Sent: Tuesday, October 17, 2017 11:38 AM
To: wrlyea <wrlyea@comcast.net>
Subject: Re: Parker Crest Neighborhood Association meeting



ALI LADHA <investcorealtygroup@gmail.com>

to William

Bill

Thanks for the info. I did confirm but they need documentation or written record of the previous meeting. Can you email me some

Thanks
Ali, Nina Ladha

No recent chats
Start a new one



William <william@comcast.net>

City Of West Linn
22500 Salamo Rd #600
West Linn, OR 97068

Roic Cascade Summit LLC
8905 Towne Centre Dr Ste 108
San Diego, CA 92122

Thomas Finn
9010 Terrace Falls Dr
Soddy Daisy, TN 37379

Willamette Christian Ch Of West Linn
3153 S Brandywine Dr
West Linn, OR 97068

Aaron Davison
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Tualatin, OR 97062

Dnj Properties LLC
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Jochen Moskopp
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Clyde Fladwood
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Dinesh Jain
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West Linn, OR 97068

Meredith Smith
20437 Hoodview Ave
West Linn, OR 97068

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20445 Hoodview Ave
West Linn, OR 97068

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West Linn, OR 97068

Valerie MacDonald
20683 Noble Ln
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Daniel Killam
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James Caliva
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West Linn, OR 97068

Mollie Giles
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Scott Brandt
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Robert Wagner
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Patricia Kool
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Susan Molnar
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Kleth Miller
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West Linn, OR 97068

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West Linn, OR 97068

Timothy Ferguson
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West Linn, OR 97068

Tath Rautio
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Cunha Marlene L Trustee
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West Linn, OR 97068

Matthew Eriksen
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West Linn, OR 97068

Salamo Terrace Homeowners Assn

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3563 Vista Ridge
West Linn, OR 97068

Hoodview Estates LLC
3563 Vista Ridge
West Linn, OR 97068

Hoodview Estates LLC
3563 Vista Ridge
West Linn, OR 97068

Hoodview Estates LLC
3563 Vista Ridge
West Linn, OR 97068

Salamo Terrace Homeowners Assn

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San Diego, CA 92122

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Heather Bengtson
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Uzuegbunam Uju Miriam Trustee
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Oregon City, OR 97045

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Ralph Prelligera
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Jeffrey Maiden
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Jordan Marona
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West Linn, OR 97068

Cameron McMinn
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Suzanne Anderson
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West Linn, OR 97068

Hoodview Townhomes Owners Assn
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Lake Oswego, OR 97034

Koss Brod Goodrich & Assoc Inc
22400 Salamo Rd #204
West Linn, OR 97068

May 16, 2018

From: Ali and Nina Ladha
Investco Realty Group

Subject: Neighborhood Review Meeting for Proposed Development at 20590 Noble Lane in West Linn (T2S, R1E, Section 26D, Tax Lot 0402)

Dear Neighbor:

Ali and Nina Ladha of Investco Realty Group, owners of the property located at 20590 Noble Lane, in the R-3 District (Single Family Residential and Multiple Family Residential / 3,000 square foot min. lot size), are proposing to develop the property with a five (5) unit attached residential development. The existing 17,517 square foot (0.40 acre) property is currently undeveloped. The property is of a size to allow development of five (5) units in a row house configuration. This development proposal is similar to one that was previously considered by the Parker Crest Neighborhood a year or so ago. So, prior to submitting a current application to the City of West Linn, we wish to give you the opportunity to discuss this project with us.

The purpose of this meeting is to provide a forum for the applicant and surrounding property owners and local residents to review the proposal and to identify any issues that may need to be considered before the formal application is submitted to the City of West Linn. This meeting gives you the opportunity to share any special information you know about the property involved, or ask questions about the development review process. We will attempt to answer any questions which may be relevant to meeting the development standards consistent with West Linn's Community Development Code (CDC). A Variance may be necessary, depending on the final configuration and design of the proposed residential units.

Pursuant to Chapter 99.038 of the West Linn Community Development Code, you are invited to a meeting:

Date and Time: Wednesday, June 6, 2018, from 6:00 to 7:00 PM
Location: West Linn Adult Community Center
Address: 1180 Rosemont Road West Linn, OR 97068

Please note that this meeting will be an informational meeting on the preliminary development plan. This plan may ultimately be revised prior to submittal to the City. Depending on the type of land use action required for this particular application (a Type III process), you may receive official notice from the City of West Linn for you to participate with written or verbal comments provided to city staff, or a public hearing before the West Linn Planning Commission.

We look forward to more specifically discussing our proposal with you. Please feel free to call or e-mail us with any questions or comments at [503-720-0021](tel:503-720-0021), or our Planning Consultant Robert Price at [503-807-4009](tel:503-807-4009).

Sincerely,

Ali and Nina Ladha

=====
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 OR
 97238-9099
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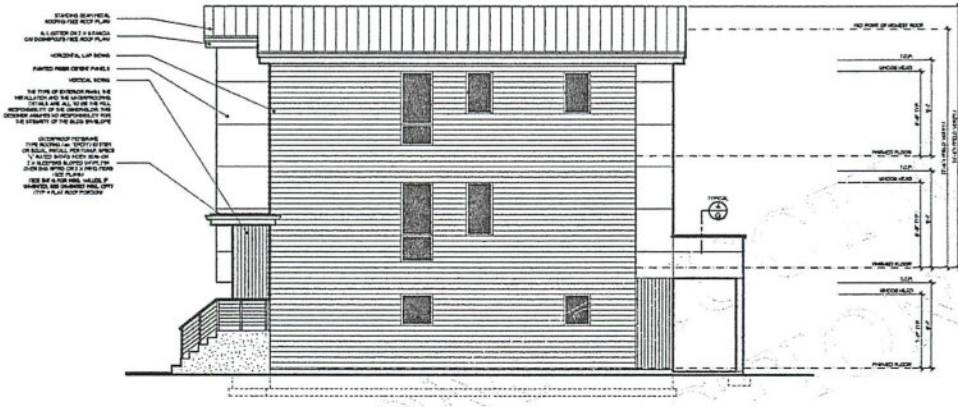
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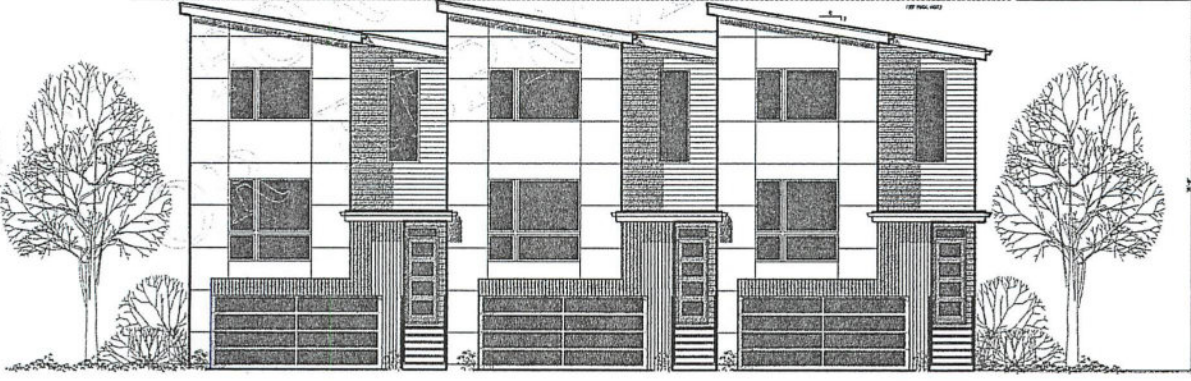
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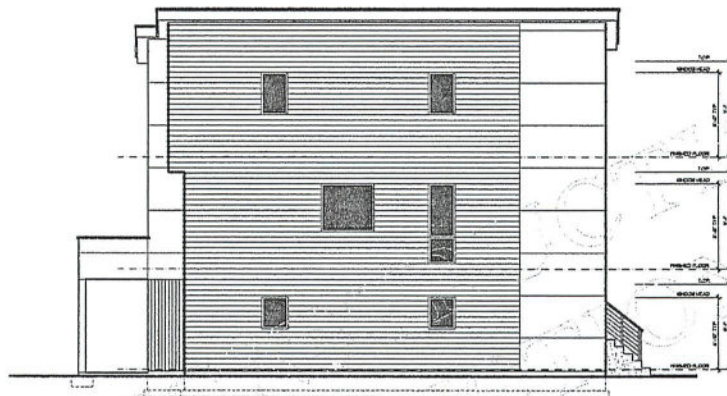
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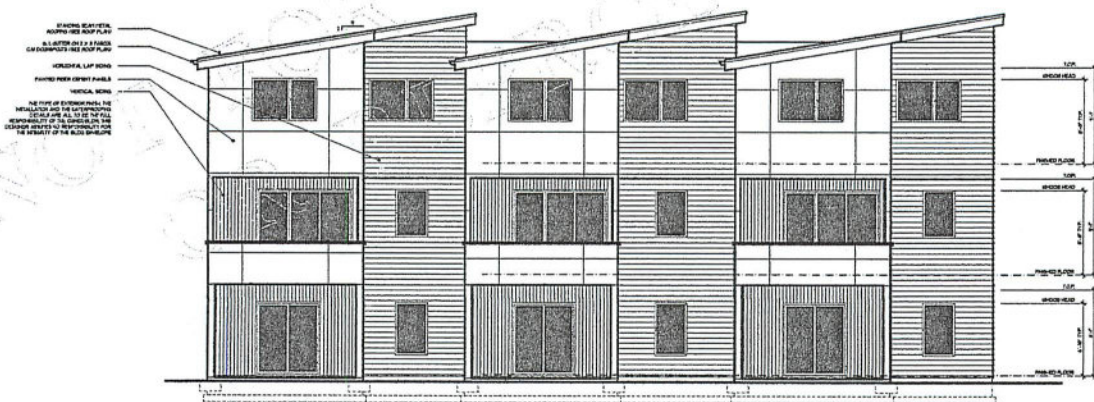
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1



LEFT ELEVATION
SCALE: 1/4" = 1'-0"



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- BRICKS CLAY/CEM, ACROSS THE ROOF PLANE
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DENVER, CO 80202

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2	ROOF	1000	50	50
3	WALL	1000	50	50
4	CEILING	1000	50	50
5	MECHANICAL	1000	50	50
6	STAIRS	1000	50	50
7	ELEVATOR	1000	50	50
8	MECHANICAL	1000	50	50
9	MECHANICAL	1000	50	50
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50	MECHANICAL	1000	50	50



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Technical Memorandum

To: Ali Ladha
Investco Realty Group LLC

From: Daniel Stumpf, EI
William Farley, PE

Date: December 22, 2017

Subject: Hoodview Avenue Townhouses – Transportation Study



Introduction

This memorandum reports and evaluates the transportation impacts related to the proposed Hoodview Avenue Townhouses, located at 20110 Hoodview Avenue in West Linn, Oregon. The proposed development will include the partitioning and development of a single property into five townhomes.

The purpose of this memorandum is to examine the projected trip generation of the proposed development and assess the safety and operation of nearby intersections and vicinity roadways.

Location Description

Project Site Description

The project site is located south of Hoodview Avenue, east of Salamo Road, and west of Noble Lane in West Linn, Oregon. The site includes a single tax lot, lot 402, which encompasses an approximate total of 0.40 acres. The site is currently undeveloped; however, frontage improvements along adjacent roadway segments are in place. Access between the site and the greater transportation system will be provided via driveways along Noble Lane.

Vicinity Roadways

The proposed development is expected to predominantly impact the following four nearby vicinity roadways: Salamo Road, Parker Road, Hoodview Avenue, and Noble Lane. Table 1 provides a description of each of the vicinity roadways.

26

Table 1 – Vicinity Roadway Descriptions

Roadway	Jurisdiction	Functional Classification	Cross-Section	Speed	On-street Parking	Bicycle Lanes	Curbs	Sidewalks
Salamo Road	West Linn	Minor Arterial	3 Lanes	35 mph Posted	Not Permitted	Both Sides	Both Sides	Partial Both Sides
Parker Road	West Linn	Minor Arterial	2 to 3 Lanes	35 mph Posted	Not Permitted	Partial Both Sides	Partial Both Sides	Partial Both Sides
Hoodview Avenue	West Linn	Local Street	2 Lanes	25 mph Posted	Partially Permitted	None	Both Sides	Both Sides
Noble Lane	West Linn	Local Street	2 Lanes	25 mph Posted	Partially Permitted	None	Both Sides	Both Sides

Vicinity Intersections

The intersection of Salamo Road at Hoodview Avenue is a three-legged intersection that is stop-controlled for the westbound approach of Hoodview Avenue. The northbound approach has one shared lane for all turning movements and a bicycle lane to the right of the standard travel lane. The southbound approach has one left-turn lane, one through lane, and a bicycle lane to the right of the outermost standard travel lane. The westbound approach has one shared lane for all turning movements. Crosswalks are unmarked across all three intersection legs.

The intersection of Parker Road at Noble Lane is a four-legged intersection that is stop-controlled for the northbound and southbound approaches. The northbound and southbound approaches each have one shared lane for all turning movements. The eastbound and westbound approaches of Parker Road have one left-turn lane, one shared through/right-turn lane, and a bicycle lane to the right of the outermost standard travel lane. Crosswalks are unmarked across all four intersection legs.

Figure 1 presents an aerial image of the nearby vicinity with the project site outlined in yellow.

1e



Figure 1: Aerial Photo of Site Vicinity (Image from Google Earth)

Trip Generation

The proposed Hoodview Avenue Townhouses includes the construction of five townhouses, each of which will have three floors. To estimate the number of trips that will be generated by the proposed development, trip rates from the *TRIP GENERATION MANUAL*¹ were used. Data from land-use code 221, *Multifamily Housing (Mid-Rise)*, was used to estimate the proposed development's trip generation of the site based on the number of dwelling units.

The trip generation calculations show that the proposed development is projected to generate 2 morning peak hour, 2 evening peak hour, and 28 average weekday site trips. The trip generation estimates are summarized in Table 2. Detailed trip generation calculations are included as an attachment to this study.

¹ Institute of Transportation Engineers (ITE), *TRIP GENERATION MANUAL*, 10th Edition, 2017.

1e

Table 2 – Trip Generation Summary

	ITE Code	Size	Morning Peak Hour			Evening Peak Hour			Weekday
			Enter	Exit	Total	Enter	Exit	Total	Total
Proposed Development	221	5 units	1	1	2	1	1	2	28

Safety Analysis

Using data obtained from the Oregon Department of Transportation's (ODOT) Crash Analysis and Reporting Unit, a review was performed for the most recent five years of available crash data (January 2011 through December 2015) at the nearby intersections of Salamo Road at Hoodview Avenue and Parker Road at Noble Lane, and along the nearby roadways of Hoodview Avenue and Noble Lane. The crash data was evaluated based on the number of crashes, the type of collisions, and the severity of the collisions at the nearby transportation facilities.

The intersection of Salamo Road at Hoodview Avenue had one reported crash during the analysis period. The crash was a rear-end collision involving two northbound passenger cars. The crash was classified as "Property Damage Only" (PDO).

No other crashes were reported within the immediate site vicinity. Due to the low number of crashes and the low severity of collisions, there do not appear to be any significant safety hazards at the nearby transportation facilities. Furthermore, based on site observations, no design deficiencies which would impact the safety of the area roadways and intersections were noted. Accordingly, no safety mitigation is necessary or recommended.

Detailed information about crashes and crash reports for the nearby roadways and intersection are included as an attachment to this memorandum.

Operational Analysis

Intersection Capacity

To determine if the nearby transportation facilities can adequately accommodate future trips to and from the proposed development in addition to the existing uses within the site vicinity, peak hour observations of traffic conditions were conducted. Traffic observations were conducted at the intersections of Salamo Road

at Hoodview Avenue and at Parker Road at Noble Lane during the morning and evening peak periods on Thursday, December 7th, 2017, from 8:15 AM to 8:40 AM and from 4:30 PM to 4:55 PM, respectively.

The level of service (LOS) of an intersection can range from LOS A, which indicates little or no delay experienced by vehicles, to LOS F, which indicates a high degree of congestion and delay. The City of West Linn requires unsignalized intersections operate at LOS D or better along non-arterial facilities. Detailed LOS descriptions are included as an attachment to this memorandum.

The intersection of Salamo Road at Hoodview Avenue was observed having less than 5 vehicles enter the intersection from the minor-street approach of Hoodview Avenue over the course of approximately 15 minutes during the morning peak period and 10 minutes during the evening peak period. Observed queuing on the minor-street approach never exceeded more than 1 vehicle during either peak period. Traffic volumes along Salamo Road were observed to be at light to moderate levels and in a state of free-flow during both peak periods. Gaps to enter the major-street traffic stream from the minor-street approach were available within an average control delay of 10 to 15 seconds during either peak period. Accordingly, the intersection was estimated to currently operate at LOS C or better during both peak periods.

The intersection of Parker Road at Noble Lane was observed to have 5 or less vehicles enter the intersection from the minor-street approach of Noble Lane over the course of approximately 10 minutes during the morning and evening peak periods. It should be noted that the northbound driveway approach opposite of Noble Lane was observed to serve twice as many entering vehicles during the morning peak period and the same number of entering vehicles during the evening peak period as Noble Lane. Observed queuing on the minor-street approach of Noble Lane never exceeded more than 1 vehicle during either peak period while the northbound driveway approach was observed to have queues of up to 2 vehicles during the morning peak period and queues of 1 vehicle during the evening peak period. Traffic volumes along Parker Road were observed to be at light levels and in a state of free-flow during both peak periods. Gaps to enter the major-street traffic stream from the highest delay minor-street approach were available within an average control delay of 5 to 10 seconds during the morning peak period and 10 to 15 seconds during the evening peak period. Accordingly, the intersection was estimated to currently operate at LOS A during the morning peak period and at LOS C or better during the evening peak period.

The results of the capacity observations indicate that the intersections of Salamo Road at Hoodview Avenue and Parker Road at Noble Lane operate acceptably during the weekday morning and evening peak periods under existing conditions. Based on the low volumes of additional weekday morning and evening peak hour trips expected to be added to the transportation system, the intersection is projected to continue operating acceptably by the build-out year of the site. No mitigation is necessary or recommended with regard to intersection capacity or operation as part of the proposed development.

Roadway Capacity

To ensure the vicinity roadways of Hoodview Avenue and Noble Lane will operate acceptably upon build-out of the proposed development, an assessment of current and future roadway volumes was conducted. Typically, local residential streets are designed and expected to serve traffic volumes that are less than 1,500 vehicles per day. Volumes above this threshold can affect the safety and livability of the street and surrounding neighborhood.

Noble Lane and Hoodview Avenue currently serve approximately 81 two-story townhouses and 81 three-story townhouses. According to the *TRIP GENERATION MANUAL*, land-use code 220, *Multifamily Housing (Low-Rise)*, is described as "...apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have one or two levels (floors)" whereas land-use code 221 is described as "...apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have between three and 10 levels (floors)." Based on these land-use descriptions as well as the trip generation methodologies described in the *Trip Generation* section, the 162 existing townhouses are estimated to currently generate 1,032 weekday trips. Upon build-out of the proposed development, an additional 28 weekday trips will utilize these roadways, an increase of approximately 2.7 percent, for a total of 1060 trips. Since the total daily trips generated by the existing and proposed uses are below the desirable local street carrying capacity of approximately 1,500 vehicles per day for a single roadway, it is expected that the both roadways will operate safely, and the proposed development will not have a significant impact on the livability of the surrounding neighborhood.

Requirement of a Full Traffic Impact Analysis

Per the City of West Linn Community Code Section 85.170.B.2 – *Traffic Impact Analysis* (TIA), a TIA is required when the following conditions apply:

- 1) *The development application involves one or more of the following actions:*
 - (A) *A change in zoning or a plan amendment designation; or*
 - (B) *Any proposed development or land use action that ODOT states may have operational or safety concerns along a State highway; and*
 - (C) *The development shall cause one or more of the following effects, which can be determined by field counts, site observations, traffic impact analysis or study, field measurements, crash history, Institute of Transportation Engineers Trip Generation manual; and information and studies provided by the local reviewing jurisdiction and/or ODOT:*
 - (1) *An increase in site traffic volume generation by 250 average daily trips (ADT) or more (or as required by the City Engineer); or*
 - (2) *An increase in use of adjacent streets by vehicles exceeding the 20,000-pound gross vehicle weights by 10 vehicles or more per day; or*
 - (3) *The location of the access driveway does not meet minimum intersection sight distance requirements, or is located where vehicles entering or leaving the property are restricted, or such vehicles queue or hesitate on the State highway, creating a safety hazard; or*
 - (4) *The location of the access driveway does not meet the access spacing standard of the roadway on which the driveway is located; or*
 - (5) *A change in internal traffic patterns that may cause safety problems, such as backup onto the highway or traffic crashes in the approach area.*

Criterion A

The proposed development application does not change the zoning of the subject property or a plan amendment designation. Accordingly, Criterion A is not met.

Criterion B

The proposed development is projected to generate a low number of additional site trips and is located over a mile driving distance away from the nearest ODOT facility. Therefore, the proposed development is expected to have minimal impacts to ODOT facilities whereby Criterion B is not met.

Criterion C

Regarding Criterion C, the proposed development (1) generates less than 250 additional average daily trips, (2) is not expected to generate heavy vehicle traffic, (3) provides adequate sight distance at individual lot driveways and existing nearby public intersections, (4) meets adequate access spacing standards, (5) and will not change internal traffic patterns or the residential nature of the surrounding area. Accordingly, Criterion C is not meet.

Based on an evaluation of the City of West Linn's Community Code, a full TIA is not required for approval of the proposed development application.

Conclusions

Due to the low number of crashes and the low severity of collisions, there do not appear to be any significant safety hazards at the nearby transportation facilities. Furthermore, based on site observations, no design deficiencies which would impact the safety of the area roadways and intersections were noted. Accordingly, no safety mitigation is necessary or recommended.

The intersections of Salamo Road at Hoodview Avenue and Parker Road at Noble Lane currently operate acceptably during the weekday morning and evening peak periods. Based on the low volumes of additional weekday morning and evening peak hour trips expected to be added to the transportation system, the intersection is projected to continue operating acceptably by the build-out year of the site. No mitigation is necessary or recommended with regard to intersection capacity or operation as part of the proposed development.

The total daily trips generated by the existing and proposed uses are projected to be below the desirable local street carrying capacity of 1,500 for a single roadway. Accordingly, upon build-out of the proposed development it is expected that the two roadways of Hoodview Avenue and Noble Lane will operate safely and will not have a significant impact on the livability of the surrounding neighborhood.

Based on an evaluation of the City of West Linn's Community Code, a full Transportation Impact Analysis is not required for approval of the proposed development application.

If you have any questions regarding this technical memorandum, please don't hesitate to contact us.

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TRIP GENERATION CALCULATIONS
Proposed Development

Land Use: Multifamily Housing (Mid-Rise)
Land Use Code: 221
Setting/Location: General Urban/Suburban
Variable: Dwelling Units
Variable Value: 5

AM PEAK HOUR

Trip Rate: 0.36

	Enter	Exit	Total
Directional Distribution	26%	74%	
Trip Ends	1	1	2

PM PEAK HOUR

Trip Rate: 0.44

	Enter	Exit	Total
Directional Distribution	61%	39%	
Trip Ends	1	1	2

WEEKDAY

Trip Rate: 5.44

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	14	14	28

SATURDAY

Trip Rate: 4.91

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	12	12	24

Source: TRIP GENERATION, Tenth Edition

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TRIP GENERATION CALCULATIONS
Existing Residential Uses

Land Use: Multifamily Housing (Low-Rise)
Land Use Code: 220
Setting/Location: General Urban/Suburban
Variable: Dwelling Units
Variable Value: 81

AM PEAK HOUR

Trip Rate: 0.46

	Enter	Exit	Total
Directional Distribution	23%	77%	
Trip Ends	9	28	37

PM PEAK HOUR

Trip Rate: 0.56

	Enter	Exit	Total
Directional Distribution	63%	37%	
Trip Ends	28	17	45

WEEKDAY

Trip Rate: 7.32

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	296	296	592

SATURDAY

Trip Rate: 8.14

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	330	330	660

Source: TRIP GENERATION, Tenth Edition

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TRIP GENERATION CALCULATIONS
Existing Residential Uses

Land Use: Multifamily Housing (Mid-Rise)
Land Use Code: 221
Setting/Location: General Urban/Suburban
Variable: Dwelling Units
Variable Value: 81

AM PEAK HOUR

Trip Rate: 0.36

	Enter	Exit	Total
Directional Distribution	26%	74%	
Trip Ends	8	21	29

PM PEAK HOUR

Trip Rate: 0.44

	Enter	Exit	Total
Directional Distribution	61%	39%	
Trip Ends	22	14	36

WEEKDAY

Trip Rate: 5.44

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	220	220	440

SATURDAY

Trip Rate: 4.91

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	199	199	398

Source: TRIP GENERATION, Tenth Edition

OREGON DEPARTMENT OF TRANSPORTATION - TRANSPORTATION DEVELOPMENT DIVISION
TRANSPORTATION DATA SECTION - CRASH ANALYSIS AND REPORTING UNIT
CRASH SUMMARIES BY YEAR BY COLLISION TYPE

HOODVIEW AVE at SALAMO RD, City of West Linn, Clackamas County, 01/01/2011 to 12/31/2015

COLLISION TYPE	FATAL CRASHES	NON- FATAL CRASHES	PROPERTY DAMAGE ONLY	TOTAL CRASHES	PEOPLE KILLED	PEOPLE INJURED	TRUCKS	DRY SURF	WET SURF	DAY	DARK	INTER- SECTION	INTER- SECTION RELATED	OFF- ROAD
YEAR: 2011														
REAR-END	0	0	1	1	0	0	0	1	0	1	0	1	0	0
YEAR 2011 TOTAL	0	0	1	1	0	0	0	1	0	1	0	1	0	0
FINAL TOTAL	0	0	1	1	0	0	0	1	0	1	0	1	0	0

Disclaimer: The information contained in this report is compiled from individual driver and police crash reports submitted to the Oregon Department of Transportation as required in ORS 811.720. The Crash Analysis and Reporting Unit is committed to providing the highest quality crash data to customers. However, because submittal of crash report forms is the responsibility of the individual driver, the Crash Analysis and Reporting Unit can not guarantee that all qualifying crashes are represented nor can assurances be made that all details pertaining to a single crash are accurate. Note: Legislative changes to DMV's vehicle crash reporting requirements, effective 01/01/2004, may result in fewer property damage only crashes being eligible for inclusion in the Statewide Crash Data File.

OREGON . DEPARTMENT OF TRANSPORTATION - TRANSPORTATION DEVELOPMENT DIVISION
TRANSPORTATION DATA SECTION - CRASH ANALYSIS AND REPORTING UNIT
URBAN NON-SYSTEM CRASH LISTING

CITY OF WEST LINN, CLACKAMAS COUNTY

HOODVIEW AVE at SALAMO RD, City of West Linn, Clackamas County, 01/01/2011 to 12/31/2015

1 - 1 of 1 Crash records shown.

SER#	P	R	S	W	DATE	CLASS	CITY STREET	INT-TYPE	RD CHAR	INT-REL	OFFRD	WTHR	CRASH	SFCL USE	MOVE	TRLR QTY	A	S	INJ	G	E	LICNS	PED	ACT	EVENT	CAUSE		
INVEST	E	A	U	C	O	DAY	DIST	FIRST STREET	DIRECT	LEGS	TRAF-	RNDBT	SURF	COLL	OWNER	FROM	FRTC	INJ	G	E	LICNS	PED	ACT	EVENT	CAUSE			
RD DPT	E	L	G	H	R	TIME	FROM	SECOND STREET	DIRECT	LEGS	TRAF-	RNDBT	SURF	COLL	OWNER	FROM	FRTC	INJ	G	E	LICNS	PED	ACT	EVENT	CAUSE			
UNLOC?	D	C	S	L	K	LAT	LONG	LRN	LOCTN	(#LANES)	CONTL	DRVWY	LIGHT	SVRTY	V#	TYPE	TO	PH	TYPE	SVRTY	E	X	RES	LOC	ERROR	ACT	EVENT	CAUSE
02025	N	N	N		06/08/2011	17	HOODVIEW AVE	INTER	1-LEG	N	N	CLR	S-1STOP	01	NONE	0									004	07		
NONE					WE	0	SALAMO RD	CN		UNKNOWN	N	DRY	REAR	PRVTE	S	-N									000	00		
N					9A			02	0		N	DAY	PDO	PSNGR	CAR			01	DRVR	NONE	36	F	OR-Y		026	000	07	
N					45 22	-122 38																						
					2.5727668	50.1226463																						
														01	NONE	0	STRGHT											
														PRVTE	S	-N										000	00	
														PSNGR	CAR			02	PSNG	NO<5	03	F			000	000	00	
														02	NONE	0	STOP											
														PRVTE	S	-N										011 004	00	
														PSNGR	CAR			01	DRVR	NONE	62	F	OR-Y		000	000	00	

Disclaimer: The information contained in this report is compiled from individual driver and police crash reports submitted to the Oregon Department of Transportation as required in ORS 811.720. The Crash Analysis and Reporting Unit is committed to providing the highest quality crash data to customers. However, because submittal of crash report forms is the responsibility of the individual driver, the Crash Analysis and Reporting Unit can not guarantee that all qualifying crashes are represented nor can assurances be made that all details pertaining to a single crash are accurate. Note: Legislative changes to DMV's vehicle crash reporting requirement, effective 01/01/2004, may result in fewer property damage only crashes being eligible for inclusion in the Statewide Crash Data File.

CDS380
12/20/2017

OREGON.. DEPARTMENT OF TRANSPORTATION - TRANSPORTATION DEVELOPMENT DIVISION
TRANSPORTATION DATA SECTION - CRASH ANALYSIS AND REPORTING UNIT
URBAN NON-SYSTEM CRASH LISTING

Page: 1

CITY OF WEST LINN, CLACKAMAS COUNTY

NOBLE LN and Intersectional Crashes at NOBLE LN, City of West Linn, Clackamas County, 01/01/2011 to 12/31/2015

SER#	P	R	S	W	DATE	CLASS	CITY STREET	RD CHAR	INT-TYPE	INT-REL	OFFRD	WTHR	CRASH	SPCL USE	MOVE	A	S	RD DFT	E	L	G	H	R	TIME	FROM	SECOND STREET	DIRECT	LEGS	TRAF-	RNDBT	SURF	COLL	OWNER	FROM	FRTC	INJ	G	B	LICNS	FED	UNLOC?	D	C	S	L	K	LAT	LONG	LRS	LOCTN	(#LANES)	CONTL	DRVWY	LIGHT	SVRTY	V#	TYPE	TO	F#	TYPE	SVRTY	E	X	RES	LOC	ERROR	ACT	EVENT	CAUSE
------	---	---	---	---	------	-------	-------------	---------	----------	---------	-------	------	-------	----------	------	---	---	--------	---	---	---	---	---	------	------	---------------	--------	------	-------	-------	------	------	-------	------	------	-----	---	---	-------	-----	--------	---	---	---	---	---	-----	------	-----	-------	----------	-------	-------	-------	-------	----	------	----	----	------	-------	---	---	-----	-----	-------	-----	-------	-------

Disclaimer: The information contained in this report is compiled from individual driver and police crash reports submitted to the Oregon Department of Transportation as required in CRS 811.720. The Crash Analysis and Reporting Unit is committed to providing the highest quality crash data to customers. However, because submittal of crash report forms is the responsibility of the individual driver, the Crash Analysis and Reporting Unit can not guarantee that all qualifying crashes are represented nor can assurances be made that all details pertaining to a single crash are accurate. Note: Legislative changes to DMV's vehicle crash reporting requirement, effective 01/01/2004, may result in fewer property damage only crashes being eligible for inclusion in the Statewide Crash Data File.

Preliminary Storm Drainage Report

Hoodview Summit
July 17th, 2018

Prepared For: Ali Ladha
9334 SE Hunters Bluff Ave.
Happy Valley, Oregon 97086

Prepared By: CWK2 Land Development Consultants
10700 SW Beaverton Hillsdale Hwy., Suite 533
Beaverton, Oregon 97005

Submitted To: City of West Linn



EXPIRATION DATE: 6/30/05

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Existing Conditions.....3

Proposed Conditions.....3

Conclusions.....4

PRELIMINARY STORM DRAINAGE REPORT

HOODVIEW SUMMIT

Purpose

The purpose of this analysis is to:

- Describe existing and proposed site conditions.

Introduction/Project Overview

This report addresses the storm drainage impacts associated with the Hoodview Summit development. This property occupies approximately 0.34 acres (after proposed right-of-way dedications) and resides in City of West Linn, Oregon. Private improvements are anticipated to include five attached single family residences, driveways, wet and dry utilities, and lot grading for the proposed residences.

Existing Conditions

The site is currently vacant and is covered by low quality landscaped grasses and low-lying shrubs. Well established street trees exist along Salamo Rd., and smaller street trees are present at the north and east end of the site along Hoodview Ave., and Noble Ln. The site is bordered to the north, south and east by existing single family attached residences. The site is further bordered to the west, north, and east by Salamo Rd, Hoodview Ave., and Noble Ln. which is fully built-out with sidewalks. The site currently slopes from the south to the north with an average slope of 5 percent. (See appendix A: Existing Conditions Plan). All existing storm water runoff appears to either infiltrate into the soil or runoff to Hoodview Ave. or Noble Ln.

Proposed Conditions

The proposed Hoodview Summit development consists of five new single new residences. No street frontage improvements are anticipated to be required since Salamo Rd., Hoodview Ave., and Noble Ln is fully built-out with sidewalks. Stormwater runoff associated with the new residences will be directed towards the public storm system in Noble Lane. (See appendix A: Storm Sewer Plan). Water quality will be managed via the existing storm water facility which was constructed for this neighborhood at the south end of Viewpoint Drive.

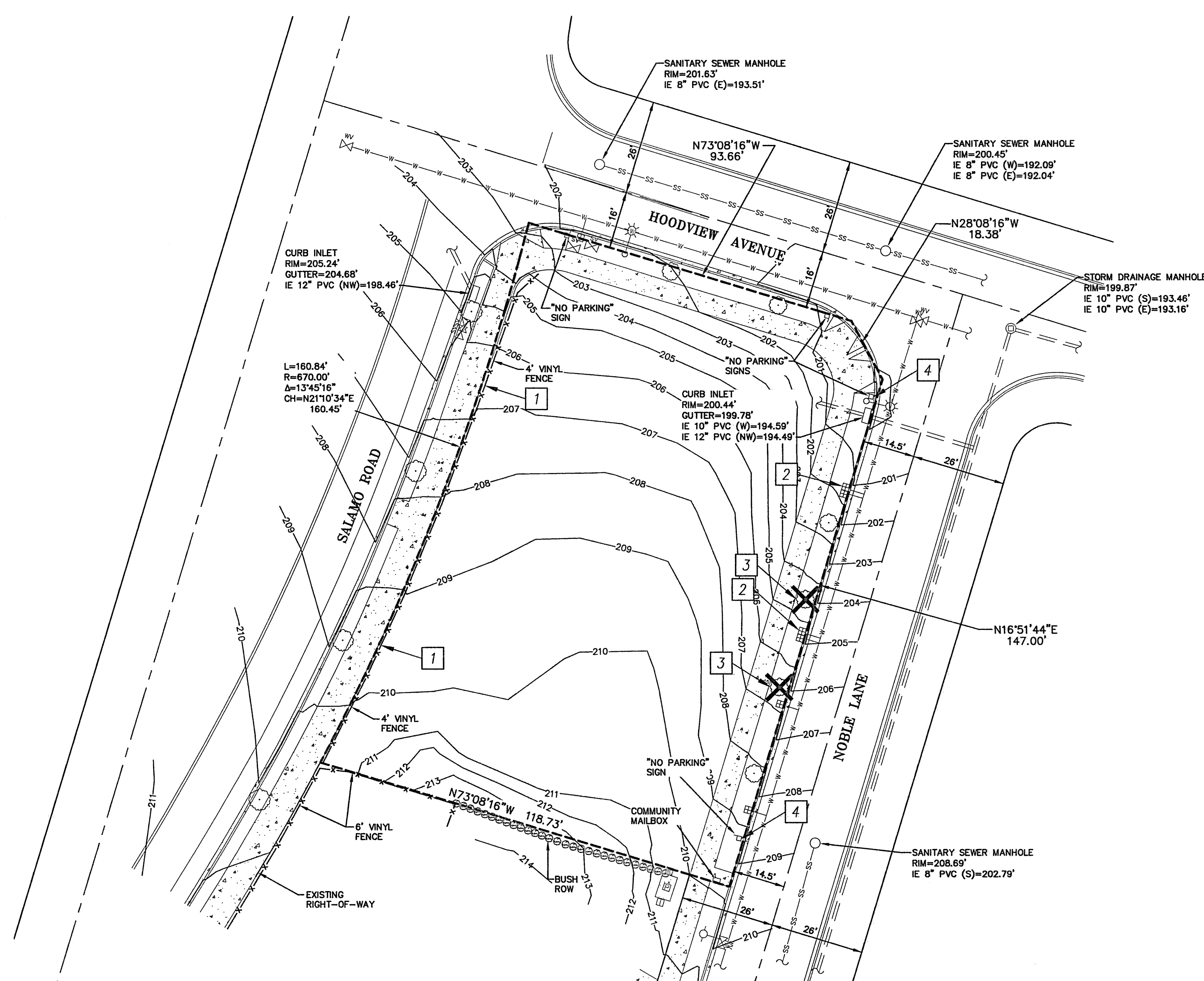
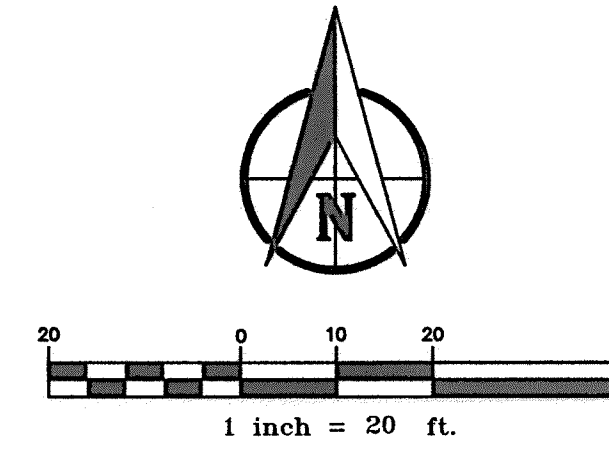
Conclusions

- Storm water will be managed per City of West Linn's storm water standards.

Appendix A:

Existing Conditions Plan

Preliminary Stormwater Plan



EXISTING CONDITIONS / DEMO PLAN
1"=20'

LEGEND

- 101--- - EXISTING 1' CONTOUR
- 105--- - EXISTING 5' CONTOUR
- ✕✕ - TREE TO BE REMOVED
- ☆ - CONIFEROUS TREE
- - DECIDUOUS TREE
- [Pattern] - EXISTING GRAVEL
- [Pattern] - EXISTING ASPHALT
- [Pattern] - EXISTING CONCRETE
- XSD- - EXIST STORM DRAIN LINE
- XOH- - EXIST OVERHEAD WIRE
- X- - EXIST FENCE LINE
- XSS- - EXIST SANITARY LINE
- XG- - EXIST GAS LINE
- - GUARDRAIL
- ⊗ - SPRINKLER VALVE
- - STANDPIPE
- ⊗ - WATER VALVE
- - FENCE POST
- ⊗ - TELEPHONE RISER
- ⊗ - POWER POLE
- ⊗ - GUY ANCHOR
- ⊗ - SIGN
- [Pattern] - CATCH BASIN
- - SANITARY MANHOLE
- ⊗ - POWER METER
- ⊗ - LIGHT POLE
- ⊗ - STORM MANHOLE
- ⊗ - WATER METER

DEMOLITION NOTES

- 1 EXISTING 4' VINYL FENCE TO BE REMOVED
- 2 EXISTING WATER METER TO BE ABANDONED
- 3 EXISTING STREET TREE TO BE REMOVED
- 4 NO PARKING SIGN TO BE REMOVED



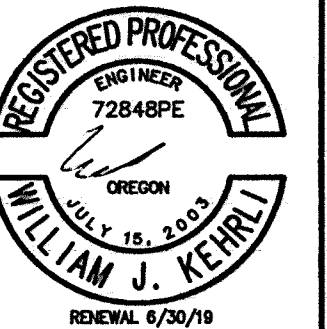
CWK2 LAND DEVELOPMENT CONSULTANTS
10700 SW BEAV. HILLSDALE HWY., SUITE 533
BEAVERTON, OR 97005
TEL: (503) 620-7182
FAX: (503) 620-7283

EXISTING CONDITIONS / DEMO PLAN

HOODVIEW SUMMIT

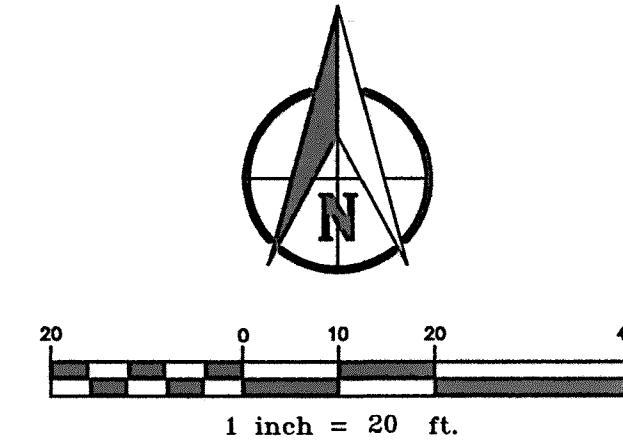
ALI LADHA

9334 SE HUNTERS BLUFF AVE. (503) 788-4401



DATE: 7/13/2018
DRAWN BY: WJK
PROJ. MGR: WJK
CHECKED BY: WJK
PROJECT NUMBER
BRT.001
CASE FILE NUMBER

SHEET NUMBER
2
OF
7

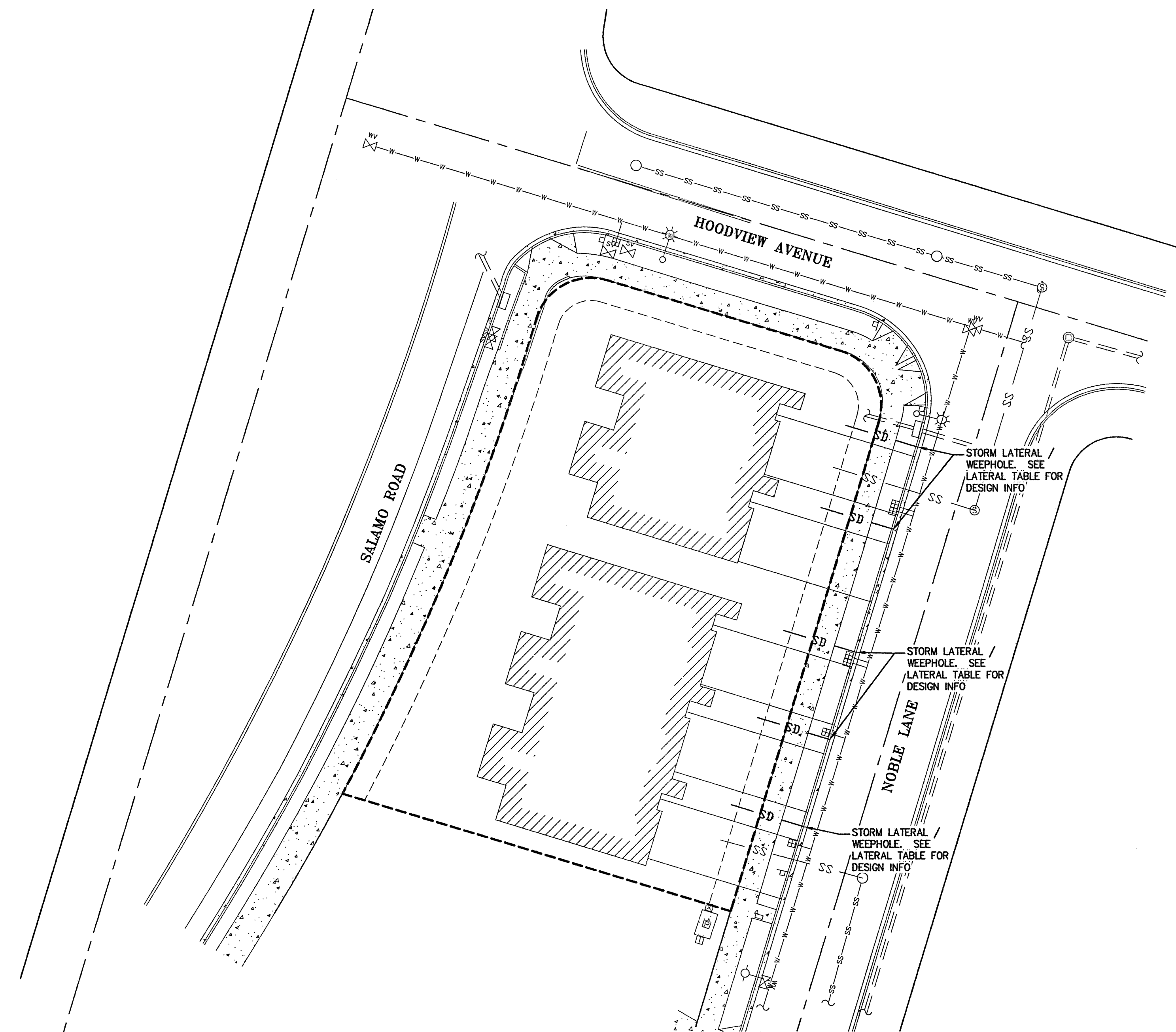


LEGEND

- PROPOSED SIDEWALK
- PROPOSED CURB
- SAWCUT LINE
- EXISTING PAVEMENT
- PROPOSED PAVEMENT

UTILITY LEGEND

- PROPOSED STORM LINE & MANHOLE
- PROPOSED STORM LINE & CLEANOUT
- PROPOSED SANITARY LINE & CLEANOUT
- PROPOSED SANITARY LINE & MANHOLE
- PROPOSED WATERLINE & VALVE
- AREA / DITCH INLET CATCH BASIN
- CG-30 STYLE CATCH BASIN



STORM SEWER PLAN
1"=20'

STORM LATERAL DATA								
LAT NO.	STREET STATION	LENGTH	SIZE	MATERIAL	INV. EL. CURB	INV. EL. PLUG	SLOPE	DEPTH PLUG
1	1+44.43	19.80'	4"	ASTM C-900	200.39	200.79	0.0200	2.2'
2	1+66.98	19.91'	4"	ASTM C-900	202.01	202.41	0.0200	2.1'
3	2+01.01	19.94'	4"	ASTM C-900	204.46	204.86	0.0200	2.1'
4	2+25.01	19.95'	4"	ASTM C-900	206.18	206.58	0.0200	1.9'
5	2+49.01	19.98'	4"	ASTM C-900	207.64	208.04	0.0200	1.5'

BACKFILL NOTE:
PIPES UNDER PAVED SURFACES REQUIRE GRANULAR BACKFILL. FOR PIPES OUTSIDE PAVEMENT, NATIVE BACKFILL IS PERMITTED, UNLESS OTHERWISE NOTED.

4" LATERALS ARE TO BE 4" PVC WITH A MINIMUM SLOPE OF 2% UNLESS OTHERWISE NOTED.

NOTE: ALL 2"x 4" STORM SERVICE CONNECTION MARKERS TO BE COLOR CODED WHITE

THE CONTRACTOR SHALL FIELD VERIFY THE SIZE, LOCATION & DEPTH OF EXISTING UTILITIES PRIOR TO CONSTRUCTION

STORMWATER MANAGEMENT
WATER QUALITY WILL BE MANAGED VIA THE EXISTING STORM WATER FACILITY WHICH WAS CONSTRUCTED FOR THIS NEIGHBORHOOD AT THE SOUTH END OF VIEWPOINT DRIVE.

CWK2 LAND DEVELOPMENT CONSULTANTS
10700 SW BEAV. HILLSDALE HWY., SUITE 533
BEAVERTON, OR 97005
TEL: (503) 620-7182
FAX: (503) 620-7283

STORM SEWER PLAN
HOODVIEW SUMMIT
ALI LADHA
9334 SE HUNTERS BLUFF AVE. (503) 788-4401

DATE: 7/13/2018
DRAWN BY: WJK
PROJ. MGR: WJK
CHECKED BY: WJK
PROJECT NUMBER
BRT.001
CASE FILE NUMBER

SHEET NUMBER
5
OF
7

P: \BRT.001 HoodView Subdivision\dwg\Eng\Set\BRT.001_05p-STORM.dwg
KehrlB, Plotted: Jul 11, 2018 - 9:21pm.

PC-4 Applicant's Resubmittal

DEVELOPMENT REVIEW APPLICATION

For Office Use Only		
STAFF CONTACT	PROJECT NO(S).	
NON-REFUNDABLE FEE(S)	REFUNDABLE DEPOSIT(S)	TOTAL

Type of Review (Please check all that apply):

- | | | |
|--|---|--|
| <input type="checkbox"/> Annexation (ANX) | <input type="checkbox"/> Historic Review | <input type="checkbox"/> Subdivision (SUB) |
| <input type="checkbox"/> Appeal and Review (AP) * | <input type="checkbox"/> Legislative Plan or Change | <input type="checkbox"/> Temporary Uses * |
| <input type="checkbox"/> Conditional Use (CUP) | <input type="checkbox"/> Lot Line Adjustment (LLA) */** | <input type="checkbox"/> Time Extension * |
| <input checked="" type="checkbox"/> Design Review (DR) CLASS II | <input type="checkbox"/> Minor Partition (MIP) (Preliminary Plat or Plan) | <input checked="" type="checkbox"/> Variance (VAR) CLASS I |
| <input type="checkbox"/> Easement Vacation | <input type="checkbox"/> Non-Conforming Lots, Uses & Structures | <input type="checkbox"/> Water Resource Area Protection/Single Lot (WAP) |
| <input type="checkbox"/> Extraterritorial Ext. of Utilities | <input type="checkbox"/> Planned Unit Development (PUD) | <input type="checkbox"/> Water Resource Area Protection/Wetland (WAP) |
| <input type="checkbox"/> Final Plat or Plan (FP) | <input type="checkbox"/> Pre-Application Conference (PA) */** | <input type="checkbox"/> Willamette & Tualatin River Greenway (WRG) |
| <input type="checkbox"/> Flood Management Area | <input type="checkbox"/> Street Vacation | <input type="checkbox"/> Zone Change |
| <input type="checkbox"/> Hillside Protection & Erosion Control | | |

Home Occupation, Pre-Application, Sidewalk Use, Sign Review Permit, and Temporary Sign Permit applications require different or additional application forms, available on the City website or at City Hall.

Site Location/Address: 0 NOBLE LANE, SITE IS UNADDRESSED BECAUSE IT IS VACANT.	Assessor's Map No.: T2S, R1E, SEC. 26D
	Tax Lot(s): 0402
	Total Land Area: 17,517 SF

Brief Description of Proposal:
DEVELOPMENT OF FIVE (5) RESIDENTIAL UNITS IN TWO (2) BUILDINGS ON VACANT SITE.

Applicant Name: (please print) ALI & NINA LAOHA, INVESTED REALTY GROUP	Phone: 503-720-0021
Address: 9334 SE HUNTERS BLUFF AVE.	Email:
City State Zip: HAPPY VALLEY, OR 97086	



Owner Name (required): (please print) ALI & NINA LAOHA, INVESTED REALTY GROUP	Phone: 503-720-0021
Address: 9334 SE HUNTERS BLUFF AVE.	Email:
City State Zip: HAPPY VALLEY, OR 97086	

Consultant Name: (please print) ROBERT PRICE	Phone: 503-807-4009
Address: 3935 NE 72 ND AVE.	Email: rprice1145@gmail.com
City State Zip: PORTLAND, OR 97213	

1. All application fees are non-refundable (excluding deposit). Any overruns to deposit will result in additional billing.
2. The owner/applicant or their representative should be present at all public hearings.
3. A denial or approval may be reversed on appeal. No permit will be in effect until the appeal period has expired.
4. Three (3) complete hard-copy sets (single sided) of application materials must be submitted with this application.
One (1) complete set of digital application materials must also be submitted on CD in PDF format.
If large sets of plans are required in application please submit only two sets.

* No CD required / ** Only one hard-copy set needed

The undersigned property owner(s) hereby authorizes the filing of this application, and authorizes on site review by authorized staff. I hereby agree to comply with all code requirements applicable to my application. Acceptance of this application does not infer a complete submittal. All amendments to the Community Development Code and to other regulations adopted after the application is approved shall be enforced where applicable. Approved applications and subsequent development is not vested under the provisions in place at the time of the initial application.

	7/30/18		7/30/18
Applicant's signature	Date	Owner's signature (required)	Date

I. Introduction

Ali and Nina Ladha, doing business as Investco Realty Group, submit the accompanying application for a Class II Design Review for development of a 5-unit attached multi-family housing project located at 0 Noble Lane, adjacent to 20322 Noble Lane, and at the intersection of Noble Lane, Hoodview Avenue, and Salamo Road. Also, the applicants are applying for a Class I Variance because the subject site will be less than 15,000 net square feet after dedications, and such Variance is required to develop five (5) units on a site of less than 15,000 square feet. The legal description of the site is T2S, R1E, Section 26D, Tax Lot 0402. The site is 17,517 square feet in total area, and is in the Parker Crest Neighborhood. Zoning is R-3 (Single Family and Multiple Family Residential/3,000 square foot minimum lot size). Preliminarily, the project carries the name "Hoodview Summit", but that could change by the time of final platting.

The site is currently vacant and appears never to have been developed for any purpose. Existing vegetation is comprised of grasses, with no trees or shrubs on the site. The site is basically flat and almost level, with some minor slope to the north and east. Gross size of the subject site is 17,517 square feet. After required dedications for additional right-of-way on both Noble Lane and Hoodview Avenue, the net site size will be 14,628 square feet. There will be no dedication on Salamo Road, as the finished street is in place, including sidewalks, and no widening of the right-of-way will be required.

The applicants propose to develop five (5) units in two structures on the site. One structure will contain three (3) units, while the second structure will contain two (2) units. All five units will be approximately the same size (approximately 2,000 square feet) and configuration, with each unit having three (3) bedrooms. Each unit will provide three (3) onsite parking spaces, two in the drive-in garage and one on the driveway. There will be no common parking area, or any area where visitors might park other than curbside parking on local streets.

The local vicinity surrounding the site is almost completely developed with similar attached housing along Hoodview Avenue and Noble Lane. According to the Parker Crest Neighborhood Association, homes in the Parker Crest neighborhood are 53% owner occupied. The applicants propose that all five units will be rental units. They will be managed by the applicants, who have their own property management organization, managing several properties in West Linn and in other urban locations.

A Pre-Application Conference Meeting was held on April 6, 2017, and the Summary Notes of that meeting are included in the application package. Based on those Summary Notes, the following sections of the West Linn Community Development Code are applicable to these two applications:

- Chapter 15: R-3
- Chapter 55: Design Review, Class II
- Chapter 75: Variances and Special Waivers – Class I Variance
- Chapter 99.060(B)

As required by the Code, a neighborhood meeting was held on June 6, 2018 at the West Linn Adult Community Center. Approximately 10 persons attended that neighborhood meeting. A complete mailing list is included with this application package, plus a sign in sheet for the meeting, written meeting notes taken by the applicants, and a recording of the meeting.

A traffic study has been completed by Lancaster Engineering, the results of which are included in their report that is included with this application package. In addition, the project engineer, Bill Kehrl, P.E. of CWK2 Consultants, has prepared a complete set of preliminary engineering drawings and calculations which are also part of the application package.

A public hearing is required for these applications before the West Linn Planning Commission. The decision of the Planning Commission is final, unless an appeal of the decision is filed in a timely manner. Any appeal goes to the West Linn City Council.

The applicants request approval of these two applications from the City of West Linn. Once approved, the applicants anticipate moving forward promptly to begin site preparation and ultimate development.

II. West Linn Community Development Code

In accordance with the items listed in the Summary Notes taken at the Pre-Application Conference Meeting held on April 6, 2017, several chapters of the West Linn Community Development Code must be addressed in the application narrative to demonstrate compliance with the various requirements, criteria, standards, and guidelines set forth in the Notes. The following is an addressing of those appropriate and applicable chapters of the Code.

- **Chapter 15 – Single-Family and Multiple-Family Residential, R-3**

15.010 PURPOSE

Comment: The intent of the R-3 zoning district is “to provide for a choice in housing types and is intended to implement the policies and locational criteria in the Comprehensive Plan for high density residential housing.” The development of the subject site will complement the housing that already exists in the immediate vicinity of the local neighborhood. The subject site is one of the few sites of this character that remains undeveloped in this area of the Parker Crest Neighborhood. The development of five (5) units on the site fits the city’s purpose to see higher density residential development in this area of the city.

15.030 PERMITTED USES

Comment: Listed among the “PERMITTED USES” is “4. Single family attached residential units”. Because the applicants propose to develop five (5) single-family attached units in two (2) structures on the subject site, the proposed land use fits within the category to allow single-family attached residential units on the subject site.

15.040 ACCESSORY USES

Comment: There will be no accessory uses built on the site. As such, this standard does not apply.

15.050 USES AND DEVELOPMENT PERMITTED UNDER PRESCRIBED CONDITIONS

Comment: The proposed use of the site is for five (5) single-family attached residential units in two buildings. This use is not one of the uses listed under this section. Therefore, this section does not apply to the current application.

15.060 CONDITIONAL USES

Comment: The proposed use of the site is for five (5) single-family attached residential units in two (2) buildings. This use is not one of the uses listed under this section. Therefore, this section does not apply to the current application.

15.070 DIMENSIONAL REQUIREMENTS, USES PERMITTED
OUTRIGHT AND USES PERMITTED UNDER PRESCRIBED
CONDITIONS

Comment: The subject site, at 17,517 square feet in gross area, and 14,628 square feet in net area, after deductions.

- In accordance with A.2, this gross lot size meets the standard for minimum lot size of 3,000 square feet for each single-family unit.
- The minimum front lot line or the minimum lot width at the front lot line shall be 35 feet, and the subject site will exceed this stated minimum (B.).
- D.1. requires the front yard be 15 feet, or 20 feet for a garage. Lot depths for this project will be in excess of 30 feet for all units.
- D.2 requires interior side yards to be five (5) feet, and the site plan illustrates that the minimum side yard setback will be 5 feet at the southerly property line. The side yard area between the two buildings will be 10 feet, and the side yard at the northerly property line, adjacent to Hoodview Avenue, will be about 15 feet to the property line and approximately 25 feet to the curbface of Hoodview Avenue. If this is considered a side yard abutting a street (because Hoodview Avenue bounds the site on the north side), required setback is 15 feet according to D.3.
- In accordance with D.4, rear yards must be 15 feet. As shown on the site plan, all rear yards will be at least 20 feet, with the rear yard for Unit #1 being the least at about 20 feet.
- In E., maximum building height is to be 35 feet for attached single family units. With each unit being three stories (including the lower garage/recreation room, maximum height will be less than 35 feet.
- F. requires that maximum lot coverage be 50 percent. With a net lot size of 14,628 square feet, allowable coverage would be 7,300+ feet. Based on the site plan, lot coverage is approximately 5,000 square feet, or 2,300 square feet less than the allowable limit.
- G. requires a floor area ration of not more than 0.45, nor less than 0.30. The site is not either Type I or Type II lands. The floor area ratio (FAR) for this site is between the minimum of 0.30 and the maximum of 0.45.
- H. requires that side walls of the structures comply with the side walk requirements set forth in this description. The basic design prepared by Mascord portrays side walls that are not exempt from this Chapter (43.040.C) because all side walls are less than 20 feet from the side lot line, the site is not steeply sloped, the basic lot (site) is more than 50 feet in width, structures are not contiguous to open space, unbuildable area, or non-residentially zoned lands. As such, the design is required to meet either A or B of 43.040. A. requires that the side elevation must be divided into "distinct planes of 700 square feet or less", while B. requires that the sidewall shall be no higher than 22 feet measured from grade. While Mascord has designed the structures with sidewalls higher than 22 feet, meaning that B. will not apply, the sidewall can be separated into areas of 700 square feet or less through the placement of windows in the sidewalls. By doing

this, the design allows for separation into smaller spaces of less than 700 square feet.

15.080 DIMENSIONAL REQUIREMENTS, CONDITIONAL USES

Comment: Because the proposed development of five (5) attached single family dwellings in two buildings on the subject site is not a conditional use, this standard does not apply to this project.

15.090 OTHER APPLICABLE DEVELOPMENT STANDARDS

Comment: After review of the 11 standards listed under this particular standard, the applicant finds that the basic design either meets each of the individual sections of the appropriate and applicable Chapters set forth in 15.090.A., as addressed later in this narrative.

In 15.090.B., the requirement is that Chapter 55, Design Review, applies *“to all uses except detached single-family dwellings, residential homes and residential facilities.”* Because this project involves five (5) single-family attached dwellings, Chapter 55, Design Review, is applicable. The requirements, standards, and criteria of Chapter 55, Design Review, is addressed later in this application narrative under 55.100.

• **Chapter 55 – Design Review, Class II**

Comment: Based on the Summary Notes from the Pre-Application Conference Meeting held on April 6, 2017, one of the requirements for this application is a Class II Design Review. Design Review is encompassed in Chapter 55 of the West Linn Community Development Code (CDC), and the appropriate and applicable portions of that Chapter are addressed as follows:

• **55.010 Purpose and Intent – General**

Comment: As stated in the opening paragraph of this section, the purpose of Design Review is *“to conserve and enhance the appearance of the City and to promote functional, safe, and innovative site development.”* Design Review seeks to ensure compatibility between adjoining uses, the vehicular access and circulation are safe, and areas of the City are safe, attractive and welcoming. The Design Review process also seeks to reduce reliance on autos by encouraging other modes of transportation.

The proposed five (5) unit development on the subject site seeks to achieve all of the goals and objectives of Design Review and to make the development a good addition to the City’s residential framework. By blending in with the other already developed areas of the City, and especially of the Parker Crest Neighborhood, this project will achieve the purposes of the Design Review process.

• **55.020 Classes of Design Review**

Comment: As noted in the Summary Notes, this project will be subject to the Class II process of Design Review. This class is set forth in 55.020.B. Because the project is a “major” multi-family construction project, the Class II Design Review process will apply.

- **55.025 Exemptions**

Comment: Exemptions to the Design Review process are listed under A. through D. of this section. This proposed project does not fit any of the exemptions listed and, therefore, is not exempt from the Design Review process.

- **55.030 Administration and Approval Process**

Comment: Under the provisions of 55.030.A., a pre-application conference is required for a project as is proposed. A Pre-Application Conference Meeting was held on April 6, 2017. The Summary Notes for PA-17-13 are included in this application package, thus fulfilling 55.030.A. 55.030.B. requires that the record owner(s) of the property or the authorized agent. Ali and Nina Ladha (Investco Realty Group) are the record owners of the site and have signed the required application. Robert Price, Consultant, is the planning and development consultant for the property owners, and is authorized to sign for the owners, and to represent the project on behalf of the owners.

- **55.060 Staged or Phased Development**

Comment: This project is not planned to be developed in stages.

- **55.070 Submittal Requirements**

Comment: As required under A., B., and C. the proper application form, signed by the applicants is included, as are the Summary Notes from the Pre-Application Conference Meeting held on April 6, 2017, and documentation from the Parker Crest Neighborhood Association. In accordance with 55.070.D.2, the application package includes a site analysis, a site plan, a grading plan, architectural drawings of the proposed two (2) structures that will contain the five (5) units, a utility plan, and addressing of the tree issues in the Tree Technical Manual, where necessary. Certainly, all of the items set forth in the section are included, except any tree issues because there are no trees on the site at the present time. In addition, samples of all exterior building materials and colors are also included.

- **55.100 Approval Standards – Class II Design Review**

Comment: As required for this section in 55.100.A., various other Chapters of the CDC must be addressed.

1. Chapter 34 – Accessory Structures, Dwelling Units, and Uses

Comment: There are no accessory structures, units, or uses planned for this site and the five (5) attached single family residential dwellings.

2. Chapter 38 – Additional Yard Area Required, etc.

Comment: There is no structure on the site at the present time, resulting in 38.020, No Yard Required not being applicable to this site. Also, 38.030, Setback from Street Centerline Required is not applicable to this site because the three streets that bound the site (i.e., Salamo Road, Hoodview Avenue, and Noble Lane) are finished streets and do not

required widening. The fourth side of the site (south) is adjacent to established dwellings.

Section 38.040, Exceptions to Yard Requirements, does not apply because the two (2) structures that will contain five (5) dwelling units are designed specifically for the subject site and in compliance with all development standards. As a result, no Exceptions are necessary.

There will be no storage in the front yard of any of the five (5) units, in accordance with 38.050, Storage in Front Yard.

As designed by Mascord, any projections into any required yards are within the limits of allowable projections. There will be no easements to allow projections into any yards.

3. Chapter 40, Building Height Limitations

Comment: This section does not apply because it has been repealed by Ordinance 1604.

4. Chapter 42, Clear Vision Areas

Comment: Clear vision areas are required at all corners of the property that is adjacent to an intersection. As illustrated by the Site Plan, the location and placement of the buildings on the site will allow for any needed and required clear vision areas at the intersection of Hoodview Avenue and Noble Lane, or the intersection of Hoodview Avenue and Salamo Road. This site is not within the Willamette area of the City, and, as such, any allowance for exceptions does not apply.

5. Chapter 44, Fences

Comment: The applicants do not plan for any fences along extended building lines between units, or around the perimeter of the site. However, any fences constructed during or after site development will meet the requirements of this section, particularly 44.030 and 44.050.

6. Chapter 46, Off-Street Parking, Loading and Reservoir Areas.

Comment: The Site Plan illustrates that there is no "parking area" within the subject site. Instead, all parking will be on the site for all residents of the five (5) units. Each unit will have two (2) off-street parking spaces, two in the garage that is within the building, and a third space on the driveway. Because each driveway is approximately 35 feet in length (garage door to street), up to two vehicles could be parked on the driveway, making for a total of four (4) vehicles parked at each unit. However, the applicants are only planning for three vehicles for each unit. There will be no loading area(s) for dwellings on this site, there are no "reservoir" area(s) on the subject site, and there are no other uses on the site other than single family attached residential.

7. Chapter 48, Access, Egress, and Circulation

Comment: The site is bounded by three (3) public streets, Salamo Road to the west, Hoodview Avenue to the north, and Noble Lane to the east. Access will be taken only from Noble Lane. Each of the five (5)

attached dwelling units will have its own access point (driveway) with Noble Lane. This will result in Option 3 being the only access option available to the site (48.025.B.3.c). Each driveway will be separate, and there will be no shared driveways. The site has no potential access points on either Salamo Road or Hoodview Avenue. Noble Lane is the only public street that will serve as access to the subject site.

8. Chapter 52, Signs

Comment: There will be no signs on the subject site, except for address signs. Depending on the requirements of the U.S. Postal Service, a gang mailbox may be placed on Noble Lane for use by all five units.

9. Chapter 54, Landscaping

Comment: At the present time, there is no landscaping on the subject site. There are no trees on the site, and the only vegetation is grasses that grow naturally. There is a line of arborvitae along the southerly property line, but those appear to be located on the adjacent property. This line of arborvitae may be useful in screening view between Unit #5 and the existing dwellings to the south.

Each unit will be landscaped by the applicants, and some individual landscaping may be done by residents of individual units after they move in. Street trees already exist along Noble Lane. With little area on the site for extended landscaping, the applicants do not believe that a landscape plan will illustrate anything more than the final construction plans for the two structures. Any nuisance plants will be removed, and no new nuisance plants will be planted on the site. The site will be planted in grass for the yard areas. There are no parking areas as part of this project that require landscaping. Overall, approximately 25% of the site (3,657 sf of the net site area of 14,628 sf) will be landscaped in that they will have grassy lawns, gardens, and some perimeter landscaping.

• 55.100.B Relationship to the natural and physical environment

Comment: Section 55.100.B. requires that a relationship be created between the proposed structures and dwelling units and the natural and physical environment. Because the site has no trees on it at the present time, there are no heritage trees that exist. There are no tree clusters or groves. As such, items B.1. and 2.(a and b) do not apply to the site. Again, because there are no trees on the site, there is no need for determination of driplines.

As stated previously, "*The site is basically flat and almost level, with some minor slope to the north and east.*" Site grading will do little to change that topographic character. Onsite drainage, and all site-based stormwater management, has been engineered by the project engineer to reflect that site and preserve the basic character (55.100.B.3)

The subject site is not one that is subject to slumping, sliding, and hazardous movement (55.100.B.4). Therefore, no specific mitigation measures need to be taken to protect the proposed development.

Because the site is vacant, separation between buildings is not an issue. When the site is developed with two (2) structures containing a total of five (5) dwelling units, separation of buildings, both on-site and off-site, has been taken into account. There will be five (5) feet between the southerly property line and the southerly property line of the site. Combined with the setback for the existing dwelling structure to the south, there will be adequate separation between buildings. Similarly, the two (2) structures to be built on the site will have 10 feet of separation between them. With the setback of over 15 feet between the second structure (containing 2 dwelling units) and the right-of-way of Hoodview Avenue, there is no issue of separation in this instance. These planned separations will serve to provide for light, air circulation, and fire protection (55.100.B.5).

Based on the preliminary design prepared by Mascord, the two structures will complement the architectural style of the existing residential structures on adjacent properties to the north, south and east. To the north is Salamo Road, and Rosemont Middle School across Salamo Road. While the design of the two structures reflects today's styles and trends, it will be both complementary and compatible with the styles that have already been established in the Parker Crest Neighborhood. The two structures reflect the "step down" transition from development around it by going from larger structures to smaller ones, and finally to the Hoodview Avenue right-of-way. The extended setback on the north side of the site promotes the transition to a more open setting. Thus, the transition between structures along Noble Lane and across Hoodview Avenue is promoted by the design of the structures.

The design of the structures is not a contrasting style of architecture, but one that is contemporary and complementary through distance between structures, screening (on the south side along the property line), and some minor grade variation. The size of these structures creates a scale in relationship between the environment and the humans who will occupy the site. The windows and the main entries reflect this scale and the attempt to make the structures more warm and welcoming. The east facing orientation of the structures will aid in capturing solar energy to aid in heating of the units earlier in the day, and to promote use of shade later in the day. Building orientation will protect from the prevalent wind direction (from the northwest in summer and from the southwest in winter), making the development sensitive to the local climatic conditions (55.100.B.6).

While this project is a single family attached residential housing development, there are no parking lots, or auxiliary parking areas. The front façade exhibits breaks in the building line by the recessed main entries and porch roofs, the second floor projections, and the minimization of the garage entry through use of a single garage door for the two indoor parking spots, lending credence to the residential nature of the front of the buildings.

The size of this project, five (5) units contained in two (2) separate buildings, is not so large that there are, or need to be accessways, courtyard type parking areas, or internal driveway and pedestrian circulation. On a site this size, these features may not even be possible. Because the site is only 14,628 square feet, or 0.34 acre, and the density is five (5) units, this is a small project that may be considered an infill project on a site that is basically surrounded by residential development. This is a site where some neighbors questioned when or if this site would be developed.

With the site being bounded on three sides by public rights-of-way, building orientation is problematic. No vehicle access is proposed on Salamo Road, for obvious reasons, meaning that access must be on either Hoodview Avenue or Noble Lane. Because the length of frontage on Hoodview Avenue is much less than the frontage on Noble Lane, trying to front on and have access to Hoodview Avenue does not make sense and would be difficult to accomplish. As such, the primary street for this development is Noble Lane. Orienting the development inward to Noble Lane, and being consistent with the existing development, is really the only practical and suitable option. The location of the project site is not conducive to public transit. Routes 35 and 154 serve the West Linn area, but neither route is close enough to the site to make transit use a practical consideration. Therefore, the project will be primarily auto oriented, as is most of the residential development in the Parker Crest Neighborhood.

The structures will be three stories in height, including the garage/recreation area on the ground level, or less than 35 feet. With front setback of about 35 feet, the height-to-width ratio is about 1:1. The ratio of the existing residential structures appears to be greater than 1:1, with two story structures and front setbacks of less than 30 feet. However, the difference between the ratio of the existing structures and that for the proposed structures is not significant and will not appear to be inappropriate or out of place (55.100.B.7).

- 55.100.C Compatibility between adjoining uses, buffering, and screening

Comment: According to 55.100.C.1, “no buffering is required between single family homes and duplexes or single-family attached units.” Although no buffer is required, an existing line of arborvitae along the southerly property line will serve to buffer and screen the proposed development from that existing residential development across the southerly property line. This stand of vegetation will provide some visual and noise protection between similar uses, even with a 5-foot setback. Characteristically, residential uses are not large noise generators, so this stand of vegetation combined with appropriate setbacks, should provide some degree of separation and protection for the two residential uses on different but adjoining properties in terms of visual barriers and absorption of air pollution and dust. This row of vegetation will be a good “built-in” barrier or separator.

- 55.100.D Privacy and Noise

Comment: Each of the five units will have a patio and rear yard area for outdoor recreation (both passive and active), and internal fences to separate the individual "rear yards" may be a possibility. This project area will be separated from existing development to the south by a line of vegetation that will add to the privacy of residents on both sides of the property line. Since residential uses tend to be fairly passive in terms of noise, there should be little to no conflict between the various units of this project, and between other development areas to the north, east and south. Lighting, both indoor and outdoor, for each unit will be minimized to the greatest extent possible in order to provide privacy and protection from direct lighting and glare.

- 55.100.E Private outdoor space

Comment: Each of the five units will have their own outdoor area. These are generally considered to be "back yards" that will face Salamo Road. These will not be shared outdoor recreation areas, nor will any part be indoors. These outdoor areas will be spacious and will be in areas that are fully usable in one way or another for the resident's enjoyment.

- 55.100.G Demarcation of public, semi-public and private spaces

Comment: The overall plan for the subject site will result in a very clear demarcation between public and private spaces. Being bounded by three public streets, it is in the best interests of the applicant, and the future residents, to keep a distinct separation between public areas on those three sides and the private nature of single family dwellings. Some interior fences may be constructed to separate "back yard areas", to provide an even greater demarcation between private spaces.

- 55.100.H Public transit

Comment: The subject site is not served directly by public transit. Unfortunately, the site is not that close to public transit. Lines 35 and 154 serve the West Linn area, and neither route serves this area of West Linn. As such, public transit should not be an issue for this application.

- 55.100.I Public facilities

Comment: This area of the City, and the Parker Crest Neighborhood, is mostly built out. As a result, public facilities such as streets, water, and sanitary sewer are already in place to serve the proposed development. Some dedication for additional right-of-way along Hoodview Avenue and Noble Lane may be necessary, but only for sidewalk and planter strip purposes. The streets are already in place and are constructed curb-to-curb. Water and sanitary sewer lines are in place and are of sufficient capacity to serve the project. Connections will be made on the Noble Lane side of the site. There will be no rebuild or realignment of the local streets, based on the nature of the proposed development. Storm detention and treatment has been designed by the project engineer to current City standards in order to provide for the needs of the site based on the type of development proposed. This design will not create any

adverse off-site impacts. Solid waste and recycling will be stored in the individual garages, and there will be no "common" storage areas for solid waste and recycling.

- 55.100.J Crime prevention and safety/defensible space

Comment: Windows on all exterior sides of the structures will provide good visibility in all appropriate directions. Laundry and service areas for each unit will be interior to each unit, and there will not be common areas that could require protection for crime prevention. Depending on the wishes of the U.S. Postal Service, there may be either individual mail delivery to each unit, or a "gang" mailbox structure somewhere along the frontage of the site on Noble Lane. If there is a gang mailbox situation, it will be located so as to be in an open area that is visible from all units, and from existing units across Noble Lane. Exterior lighting will be provided for and on each unit, but lighting levels, directions, and shielding will all be taken into account when considering the location of individual exterior lighting fixtures. It is not anticipated that there will be the need for security fencing for utilities because there likely will not be any "bulk" utility facilities that would be large enough for security fencing. There will be street lighting on the streets that bound the site, to provide better safety and crime prevention. The location of the street lights are illustrated on the engineer's technical plans.

- 55.100.K Provisions for persons with disabilities

Comment: The route between the main entry and the street will be provided to make access by persons with disabilities a bit easier and more convenient. This route to each unit will likely be up the driveway from the street to the front entry stairs. This hard surfaced route will make it somewhat easier for those with disabilities to access the units.

- 55.100.L Signs

Comment: There will be no signs on the site, other than address signs. No commercial activities will take place within this project site. At most, a resident might operate a home-based business, or home occupation, but no signs will be allowed to "advertise" or "identify" the home-based business. There will be no "monument" sign to announce this project, located anywhere on the site. Because the site is small, the development will largely identify itself by virtue of its unique location on three public streets.

- 55.100.M Utilities

Comment: Existing utilities in the local neighborhood are already located and sized to serve the proposed development of five (5) residential units. All utilities, whether public or private, will be buried underground.

- 55.100.N Wireless communication facilities (WCF)

Comment: There are no existing wireless communication facilities on the subject site, and it appears unlikely that there will ever be wireless communication facilities located anywhere on the subject site.

- **55.100.O Refuse and recycling standards**

Comment: Because solid waste and recycling will be managed by collection from each individual unit, receptacles will be kept in or near each garage for ease of access. The solid waste/recycling hauler for the local area will provide garbage and recycling bins or receptacles for use by the individual resident, and it will be incumbent on the residents to work within the hauler's schedule for service to provide the receptacles for collection or emptying. Keeping the receptacles "out in the open" on the driveway or in a side yard, will be prohibited by the applicant.

- **55.110 Site Analysis**

Comment: A Site Analysis map is required for each project such as this one. A Site Analysis has been prepared by the project engineer, and has all of the required elements listed in A. and B.1 through 10 included in the mapping.

- **55.120 Site Plan**

Comment: A Site Plan map is required for this project. The Site Plan map has been prepared by the project engineer and contains all of the elements listed under this subsection, from 55.120.A through H.

- **55.125 Transportation Analysis**

Comment: Referring to 48.025 of the CDC, it appears that this project may not require a formal traffic impact study. As noted previously, this project will have only one option for access control, specifically, 48.025.B.3.c. However, a Technical Memorandum, dated December 22, 2017, prepared by Lancaster Engineering has been completed for this project. Because this is a small project, only 5 units, site generated traffic should not exceed about 5+ vehicle trips per day, based on the latest edition of the ITE Manual. This will result in about 28 total daily trips. All five units will access Noble Lane via separate driveways, and no shared driveways will exist. With the exception of the occasional delivery van or truck, all vehicles associated with the proposed development will be personal vehicles. The anticipated site generated traffic volume of about 28 vehicles per day will be far beneath the threshold of 250 trips set forth in 85.170.B.2.c.1.C.1.

- **55.130 Grading and Drainage Plans**

Comment: The project engineer has prepared grading and drainage plans as part of the technical plan set for this project. These plans meet the requirements of the City of West Linn, including a sufficient scale, location and extent of grading, and the like as listed in 55.130.A. through F.

- **55.140 Architectural Drawings**

Comment: Architectural drawings for this project have been prepared by Mascord, and are included with the application. These graphic exhibits include, but may not be limited to, a Site Plan, building elevations

and sections, and material and color boards. These architectural drawings accurately illustrates what the project will look like.

- **55.150 Landscape Plan**

Comment: No site specific landscape plan has been prepared for this application because the site is a basic development involving only five units in two buildings.

- **55.170 Exceptions to Underlying Zone, Yard, Parking, Sign Provisions, and Landscaping Provisions**

Comment: The only "exception" being requested is a Class I variance to allow for five (5) units to be built on a size that is slightly less than 15,000 square feet (14,628 net square feet, after deductions). Otherwise, no exceptions are proposed to any uses in the R-3 zone, yard dimensions, signs, or landscaping provisions. While we doubt that any exception will be identified for this project under this section, it is likely that any exception will be less than 20% of any required setback, or 10% of required parking.

- **55.180 Maintenance**

Comment: Because these will be rental units, the applicant will be responsible for maintaining the site and any features, uses, etc. that are associated with it. As the owner of other rental properties in West Linn and other parts of the metro area, the applicant is fully aware of the need for maintenance and the property owner's obligation to keep up with maintenance.

- **Shared Open Space**

Comment: There will be no shared open space within this project site.

- **55.195 Annexation and Street Lights**

Comment: The subject site is already within the City of West Linn, so no new annexation activities are required. Therefore, the applicant (developer) is not required to pay expenses for street lighting energy and maintenance. In fact, street lights already exist on Hoodview Avenue and Noble Lane. Nothing more is required of the applicant as a result of this section.

- **Chapter 75, Variances and Special Waivers**

Comment: At 3,000 square feet per unit in the R-3 zone, the overall site with five (5) units should be 15,000 square feet. Although the site is currently a gross size of 17,517, in excess of 15,000 square feet, some deductions for right-of-way along Hoodview Avenue and Noble Lane will result in a net site size of 14,628 square feet, or just less than 15,000 square feet needed to avoid a Variance. This difference is only 372 square feet, or less than 2.5% square footage of the total required. The two alternatives are: 1.) reduce the number of units to 4; or, 2.) apply for a Class I Variance. The applicant has chosen to apply for a Class I Variance for the 372 square feet that is short of the required lot area of 15,000 square feet for five (5) units. Based on 75.020.A.1., the lot area

can be modified by up to 5% if the application meets certain requirements. These requirements are addressed below:

a. *Provides for a more efficient use of the site;*

Comment: The actual difference between 4 units on 14,628 square feet and 5 units as proposed on the same lot area is one where the land is better utilized, the use of public utilities and services is more efficient and those facilities are already constructed and installed, and will not be overburdened because of 1 additional unit (75.020.A.1.a). At 4 units on this site, the density would be 3,658 square feet per unit. If five (5) units are developed, the density will be 2,926 square feet per unit. It would appear that six (6) units could be accommodated on this site, but density is limited to five (5) units.

b. *Preserves and incorporates natural features into the overall design of the project;*

Comment: The site is a "left over" site that is relatively simple and featureless. There are no trees on the site, no structures, and no distinctive vegetation. Only grasses grow on the subject site at this time. While the design reflects the relatively few attributes of the site, the lack of natural features makes the site fairly bland and easy to work with (75.020.A.1.b).

c. *Does not adversely affect adjoining properties in terms of light, air circulation, noise levels, privacy, and fire hazards;*

Comment: The site is separated from the other development areas of the City and the Parker Crest Neighborhood, and needs to be made a part of the local fabric. The addition of 5 units on the subject site will not alter the lack of impacts created on adjoining properties in terms of light, air, circulation, noise levels, privacy and fire hazards. All setbacks will be satisfied, and the existence of a line of arborvitae along the southerly property line will add greatly to the privacy of the nearest adjacent dwelling, as well as air circulation, availability of light (both direct and indirect), internal circulation, and fire hazards. As long as the plan reflects the proper measures to insure fire protection, the site will be better used for 5 units than 4 units (75.020.A.1.c).

d. *Provides for safe vehicular and pedestrian access to the site and safe on-site vehicular and pedestrian circulation.*

Comment: In terms of vehicular and pedestrian circulation both on-site and off-site, there will be no difference in these access and circulation issues for either 4 units or 5. Because there will be no difference between any impacts for 4 units or 5 units, there is no reason under this requirement that the Class I Variance cannot be acceptable and approved (75.020.A.1.d). See Technical Memorandum prepared by Lancaster Engineering.

With regard to off-street parking and number of spaces, the applicant is proposing the full number of required spaces for 5 units, without adverse impact to either the site or the surrounding neighborhood. All required parking spaces will "fit" onto the subject site, resulting in no adverse

impacts on parking for 5 units. No modification of up to 10% is being requested, as allowed by this requirement (75.020.A.2).

No dimensional sign requirements are being varied or modified as a result of fitting 5 units on the subject site, as opposed to 4 units, because there will be no signs on the site other than address signage (75.020.A.3).

No landscaping plan has been prepared for the site as a whole. Rather, landscaping will be provided for each individual unit. There is relatively little space for larger scale landscaping. This "lot-by-lot" landscaping approach will continue to be useful and efficient and will have no adverse impact on adjoining property. And with regard to incorporating natural features into the overall design of the site, there are no distinctive natural features on the site that can be identified and incorporated into the site plan (75.020.A.4).

- 75.020.B Class II Variance Approval Criteria

Comment: None of this portion of the Chapter is applicable because the Variance needed is a Class I Variance, not a Class II Variance.

- 75.020.C Special Waivers

Comment: Because there are no "special waivers" required for this application, this section of the Chapter does not apply.

- 75.050 Application

Comment: In accordance with the requirements of this section as listed in A. through F., all needs have been satisfied. This variance request is filed by the applicant who is also the property owner. It should be noted that items B., D., and E. refer specifically to a Class II variance. Because this application is for a Class I Variance, these items do not apply. As required by C., a completed application form, signed by the applicant, has been prepared and includes a narrative addressing the approval criteria set forth in 75.020. The required site plan is also part of the application for the Class I Variance. This site plan is the same site plan as prepared for the Design Review application. Finally, as required by F., the applicant submits the required fee with this application.

- 75.060 Site Plans and Map

Comment: As required under this section, the site plan prepared for this project is submitted with the application. This site plan contains the information required by 75.060.B.1 and 2. 75.060.B.3 does not apply because there is no request for a variance to building height provisions.

- **Chapter 99 Procedures for Decision Making: Quasi Judicial**

Comment: Section 99.038, *Neighborhood Contact Required for Some Applications*, sets forth the applications that require a neighborhood contact. As noted in the Summary Notes of the April 6, 2017 Pre-Application Conference Meeting, "The application requires a neighborhood meeting per 99.038". A neighborhood meeting with the

Parker Crest Neighborhood Association was held on June 6, 2018. All of the requirements for this meeting (i.e., mailed notifications, sign-in sheet, notes from that meeting, etc. are included with this application.

Within this Chapter, 99.060.B specifies that the Planning Commission shall have authority "*to approve, deny or approve with conditions*" certain types of applications, including Design Review, Class II (99.060.B.2.h). The applicant is aware of this requirement, and agrees with the determination that this application must be reviewed in an advertise, noticed public hearing.

Section 99.070.A specifies that certain applications can be consolidated for review by the Planning Commission. This means that the two applications submitted by the applicant, Class II Design Review and Class I Variance, will be consolidated and heard jointly by the Planning Commission. The applicant understands this requirement, and accepts this process.

City of West Linn
PRE-APPLICATION CONFERENCE MEETING
SUMMARY NOTES
April 6, 2017

SUBJECT: Application for a 5 unit attached multi-family housing development and Class I variance for lot size at 0 Noble Lane (adjacent to 20322 Noble Lane).

FILE: PA-17-13

ATTENDEES: Applicants: Investco Realty Group: Ali & Nina Ladha
Staff: Jennifer Arnold (Planning), Amy Pepper (Engineering)

The following is a summary of the meeting discussion provided to you from staff meeting notes. Additional information may be provided to address any "follow-up" items identified during the meeting. These comments are PRELIMINARY in nature. Please contact the Planning Department with any questions regarding approval criteria, submittal requirements, or any other planning-related items. Please note disclaimer statement below.

SITE INFORMATION:

Site Address: 0 Noble Lane (adjacent to 20322 Noble Lane) (21E26D tax lot 0402)
Site Area: 17,517 square feet
Neighborhood: Parker Crest NA
Comp. Plan: High density residential
Zoning: R-3 (Single Family and Multiple-Family Residential / 3,000 square foot minimum lot size)

Applicable code: CDC Chapter 15: R-3
CDC Chapter 55: Design Review- Class II
CDC Chapter 75: Variances and Special Waivers-Class I variance
CDC Chapter 99.060(B)

PROJECT DETAILS:

The proposal is to develop multi-family housing similar to the surrounding development of Hoodview Estates. Development on this lot would require dedications along Hoodview Avenue and Noble Lane. This dedication would adjust the front and side property lines behind the existing sidewalks. The applicant proposes two options for this development. Option 1: develop one structure with 5 housing units on the one unit of land. This would require a Class I variance to allow for 5 units on less than 15,000 sq. ft. of land. Option 2: would be to construct one multi-family structure with 4 units to avoid the Class I variance because the site would be over 12,000 sq. ft. This development would require extension of the existing sewer line on Noble Lane to the proposed site.

Engineering/TVFR Comments: Contact Amy Pepper at apepper@westlinnoregon.gov for Engineering comments and Ty Darby at tdarby@tvfr.com for TVFR comments.

PROCESS:

The application requires a neighborhood meeting per CDC 99.038. Please follow those requirements very carefully. Contact the Parker Crest Neighborhood Association, at ParkerCrestNA@westlinnoregon.gov

Land use applications include a Class II Design Review (Chapter 55) and a Class I Variance (Chapter 75). The CDC is online at <http://westlinnoregon.gov/cdc>.

A traffic study may be required. The applicant should verify CDC Chapter 48.025 for requirements.

N/A is not an acceptable response to the approval criteria. The submittal requirements may be waived, but the applicant must first identify the specific submittal requirement and request, in letter form, that it be waived by the Planning Manager and must identify the specific grounds for that waiver. The waiver may or may not be granted by the Planning Director. Waivers may also be subsequently overruled by the decision making body.

Submit the application to the Planning Department with an application form signed by the property owner. The Class II Design Review permit application deposit/fee is as follows: Based on construction value (CV), less than \$100,000 of CV the deposit is 4% of CV (\$2,000 minimum deposit); CV of \$100,000<\$500,000 the deposit is 4% of CV (\$8,000 Maximum deposit); CV of \$500,000+ the deposit is \$4,000 plus 4% of CV (\$20,000 maximum deposit); an inspection fee is assessed for all projects with a fee of \$300. An application fee for the Class I variance is \$825.

Once the application and deposit/fee are submitted, the City has 30 days to determine if the application is complete or not. If the application is not complete, the applicant has 180 days to make it complete or provide written notice to staff that no other information will be provided. Once the submittal is deemed complete, staff will provide notice per CDC Chapter 99. For a standard design review application, staff will schedule a public hearing with the Planning Commission. Appeals of the Planning Commission's decision on a standard subdivision application are heard by City Council.

Typical land use applications can take 6-10 months from beginning to end.

DISCLAIMER: This summary discussion covers issues identified to date. It does not imply that these are the only issues. The burden of proof is on the applicant to demonstrate that all approval criteria have been met. These notes do not constitute an endorsement of the proposed application **or provide any assurance of potential outcomes.** Staff responses are based on limited material presented at this pre-application meeting. New issues, requirements, etc. could emerge as the application is developed. **A new pre-application conference would have to be scheduled one that period lapses and these notes would no longer be valid. Any changes to the CDC standards may require a different design or submittal.**

APPLICANT NOTE:

Upon completion, submit this form with your Current Planning development application Attachment E

NEIGHBORHOOD MEETING AFFIDAVIT OF MAILING

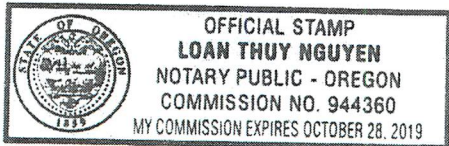
STATE OF OREGON)
COUNTY OF CLATSOP) ss
County of CLATSOP RP)
CITY OF WEST LINN)

I, ROBERT PRICE, being duly sworn, depose and say that on the 16th day of MAY, 2018 I caused to have mailed to each of the persons on the attached list a notice of a meeting to discuss a proposed development at NOBLE LAKE "HOODVIEW Summit", a copy of which notice so mailed is attached hereto and made a part hereof.

I further state that said notices were enclosed in envelopes plainly addressed to said persons and were deposited on the date indicated above in the United States Post Office with postage prepaid thereon.

Signature: [Handwritten Signature]

Subscribed and sworn to, or affirmed, before me this 30th day of July, 2018.



[Handwritten Signature]
Notary Public for the State of OREGON
County of MULTNOMAH

My Commission expires: 10/28/2019

APPLICANT NOTE:

Upon completion, submit this form with your Current Planning development application

NEIGHBORHOOD MEETING AFFIDAVIT OF POSTING NOTICE

Name of Applicant ALI & NINA LADHA, dba INVESTCO REALTY GROUP
Subject Property: Tax Lot(s) 0402 Tax Map(s) T2S, R15, SECTION 26D
Address or General Location: 0 NOBLE LANE

I, ALI LADHA, do swear or affirm that I am (~~represent~~) the party
initiating interest in a proposed FIVE (5) UNIT DEVELOPMENT
affecting the land located at 0 NOBLE LANE
and that pursuant to CDC 99.038 did on the 17th day of MAY, 2018
personally post the notice indicating that the site may be proposed for a CLASS II DESIGN REVIEW
AND A CLASS I VARIANCE application.

The sign was posted at ON WEST SIDE FACING SALAMO ROAD, ON NORTH SIDE FACING HODDUM, AND ON THE EAST SIDE FACING NOBLE LANE

 Map attached showing approximate sign notice location. (check if attached)

This July day of 30th, 2018

Ali Ladha
Signature

Subscribed and sworn to, or affirmed, before me this 30th day of July, 2018

Nancy Lea Gaylord
Notary Public for the State of WA
County of Clackamas
My Commission expires: 02, 01, 2021



Public Notice

This site will be the subject of a development proposal for a five unit attached housing project. Project sponsors are Ali and Nina Ladha of Investco Realty Group.

An informational meeting will be held to review the site plan and to discuss this project before an application is submitted to the City of West Linn

Date: Wednesday, June 6, 2018 at 6:00 PM
Place: West Linn Adult Community Center
1180 Rosemont in West Linn

For additional information please call Ali Nadha at 503-720-0021 or Robert Price at 503-807-4009

Minutes of the Meeting - Parker Crest Neighborhood

June 6, 2018 at the West Linn Adult Community Center
1180 SW Rosemont Road, West Linn

Attendance: See attached "Sign Up Sheet" – 12 people including
Ali and Nina Ladha, and Robert Price, Consultant

The meeting started at 6:00 PM on Wednesday, June 6th at the West Linn Adult Community Center. A sign-up sheet was provided and attendees were asked to sign in. The applicants, Ali and Nina Ladha, provided a 24" x 36" site plan, copies of the site survey, and renderings of the proposed design of the new residential units for review by all in attendance.

Ali Ladha explained the goal of the project, to provide 5 quality residential units in 2 structures on the site. Each unit will be approximately 2,000 square feet in floor area. Ali explained the layout of the units and the siting of the structures on the 17,500 sf (approx.) site. He also explained some of the history of the site, in which Ali and Nina are not the first to propose development of the site. For some reasons, none of the previous development proposals moved forward.

Robert Price, Consultant to Ali and Nina Ladha, explained the process in the City of West Linn for obtaining approval for the project. A public hearing will be held before the West Linn Planning Commission on this application when it is prepared, submitted, deemed complete and a hearing date is set by the city.

One of the first subjects to arise during the open discussion of the project was whether the units would be owner occupied or renter occupied. Lynne Saphir who, with her husband Bill, own and live in the property directly adjacent to the east, was concerned about renters occupying the units because she felt that renters might not be able to take care of the properties and may drive down home values. A discussion ensued about owner-occupied versus renter-occupied units. Ali pointed out that the goal is not to affect the existing community and they always look for up and coming professionals families who are looking for safe places with great schools to raise their families.

Lynne Saphir stated that she felt the site itself has not been "kept up" and she wondered if this is indicative of how the developed site would be managed. Nina Ladha provided a response, stating that they own several properties, not only in West Linn but in other locations, and she personally makes sure those rental properties are well maintained and occupied by qualified and responsible tenants.

On street parking was also an issue. Folks in general are worried about how the city dictates, manages and controls on street (curbside) parking. While the Ladha's are not able to control the on street parking, they recognize it as an issue

in the neighborhood. The current plan is for two full sized garages for each unit, plus a driveway of sufficient length to allow for parking of a third vehicle on the premises, off the street. Lynne Saphir is also worried about short term parking by contractors, workers, material suppliers, etc. and how parking would be managed during the construction period.

Angela Sullivan expressed a concern that the design of the new units, while current and contemporary in its design, may not fit with the rest of the neighborhood design. Gregg Creighton, a local resident and an architect in his own right, stated that the design reflected the current trends and styles.

Robert Price provided a brief discussion of public facilities and services to the site, and that all existing facilities and services are of adequate size, capacity, and general service levels to provide for the proposed 5 new residential units.

There were no other concerns for views expressed by others.

Nina Ladha wrapped up the meeting by stating that she would lead a short tour of local properties they owned, in order to demonstrate that the Ladha's are good, conscientious property owners and landlords. Lynne and Bill Saphir agreed that they would like to see Ali and Nina's properties.

The meeting concluded at approximately 7:10 PM.

Sign Up Sheet

Investco Realty Group

Meeting with Parker Crest Neighborhood Association

Date: Wednesday, June 3, 2018, 6:00PM - 7:00PM

Where: West Linn Adult Community Center

	NAME	Address	PHONE	EMAIL
1	KEVIN SEGURA	20367 NOBLE LN, WEST LINN	503 347-0766	kevurmelia@mail.com
2	Rita Segura	" "	503-867-2994	rita.segura@comcast.net
3	Greg Creighton	20413 S. Noble Ln.	503-635-0777	gregcreighton@comcast.net
4	HUNNE SAPHIR	20322 Noble Ln, WL	(503) 449-2944	LPsaphire@aol.com
5	BILL SAPHIR	" "	" "	" "
6	Angela Sullivan	20213 Hoodview	503-358-8182	angela.sullivan.s1@aol.com
7	Julie Warner	20371 Noble Ln	503-390-7160	606loiswarner@sprintmail.com
8	Candace Toyama	20315 Noble Ln	971-266-9172	569Kawai@gmail.com
9	Kimo Toyama	" "	" "	" "
10				
11				
12				
13				
14				
15				

ParkerCrestNA@westlinnoregon.gov

Gmail

Move to Inbox

More

COMPOSE

Inbox (3)

Starred

Sent Mail

Drafts (4)

Business

Registration

Wl. Development



INVESTCO

to me

Greetings,

I was at City Hall today to speak with the planners about the parcel of land on Hoodview Ave, that we have been discussing.

Based on our previous phone call, it's my understanding that this is the same parcel of land that the Parker Crest NA discussed different applicant. It is my understanding that if the application is substantially the same as the previous application and the 18 association meeting is sufficient and a new meeting is not needed.

The planner that I spoke with is Peter Spir.

Please let me know your thoughts on the matter.

Best regards, Bill R.

From: INVESTCO REALTY GROUP [mailto:investcorealtygroup@gmail.com]

Sent: Tuesday, October 17, 2017 11:38 AM

To: wrlyea <wrlyea@comcast.net>

Subject: Re: Parker Crest Neighborhood Association meeting



ALI LADHA <investcorealtygroup@gmail.com>

to William

Bill

Thanks for the info. I did confirm but they need documentation or written record of the previous meeting. Can you email me some

Thanks
Ali, Nina Ladha

No recent chats

Start a new one



William Relyea <wrlyea@comcast.net>

City Of West Linn
22500 Salamo Rd #600
West Linn, OR 97068

Roic Cascade Summit LLC
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San Diego, CA 92122

Thomas Finn
9010 Terrace Falls Dr
Soddy Daisy, TN 37379

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Scott Brandt
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West Linn, OR 97068

Julian Garcia
20573 Noble Ln
West Linn, OR 97068

Randi King
20575 Noble Ln
West Linn, OR 97068

Peter Storli
20579 Noble Ln
West Linn, OR 97068

Patricia Kool
20581 Noble Ln
West Linn, OR 97068

Susan Molnar
20589 Noble Ln
West Linn, OR 97068

Willem Beltman
20593 Noble Ln
West Linn, OR 97068

Kleth Miller
20595 Noble Ln
West Linn, OR 97068

B Garber
20464 Hoodview Ave
West Linn, OR 97068

Burton Levin
11989 SW Lausanne St
Wilsonville, OR 97070

Ball Jocelyn Jean Trustee
20440 Hoodview Ave
West Linn, OR 97068

Abolfazl Shahbazi
20438 Hoodview Ave
West Linn, OR 97068

Timothy Ferguson
20426 Hoodview Ave
West Linn, OR 97068

Tath Rautio
20414 Hoodview Ave
West Linn, OR 97068

Alexa Johnson
20406 Hoodview Ave
West Linn, OR 97068

Lori Pankratz
20394 Hoodview Ave
West Linn, OR 97068

Maureen Darrow
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Wilsonville, OR 97070

Tribou Thomas R Trustee
352 S Dubois Rd
Ariel, WA 98603

Gregg McElhinney
2425 SW Gregory Dr
West Linn, OR 97068

Jared Smith
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Washougal, WA 98671

Cindy Self
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Lake Oswego, OR 97034

Doug Gavrich
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Woodburn, OR 97071

Min Lu
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Portland, OR 97209

Pamela Dalmolin
20288 Hoodview Ave
West Linn, OR 97068

Cunha Marlene L Trustee
20282 Hoodview Ave
West Linn, OR 97068

Matthew Eriksen
20276 Hoodview Ave
West Linn, OR 97068

Salamo Terrace Homeowners Assn

Hoodview Estates LLC
3563 Vista Ridge
West Linn, OR 97068

Hoodview Estates LLC
3563 Vista Ridge
West Linn, OR 97068

Hoodview Estates LLC
3563 Vista Ridge
West Linn, OR 97068

Hoodview Estates LLC
3563 Vista Ridge
West Linn, OR 97068

Salamo Terrace Homeowners Assn

Retail Opportunity Investments Corp
8895 Towne Centre Dr Ste 113
San Diego, CA 92122

Kenneth Kuntz
20763 Viewpoint Rd
West Linn, OR 97068

Heather Bengtson
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West Linn, OR 97068

Jillian Smith
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West Linn, OR 97068

Chidi Odiah
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Lake Oswego, OR 97035

Ying Zhang
15 Masaryk St
Lake Oswego, OR 97035

David Lavares
20711 Viewpoint Rd
West Linn, OR 97068

Uzuegbunam Uju Miriam Trustee
413 Dewey St
Oregon City, OR 97045

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West Linn, OR 97068

Ralph Prelligera
20736 Viewpoint Rd
West Linn, OR 97068

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Happy Valley, OR 97086

Jeffrey Maiden
12690 SE Crest Dr
Happy Valley, OR 97086

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West Linn, OR 97068

Jordan Marona
20786 Viewpoint Rd
West Linn, OR 97068

Cameron McMinn
20790 Viewpoint Rd
West Linn, OR 97068

Suzanne Anderson
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West Linn, OR 97068

Hoodview Townhomes Owners Assn
PO Box 130
Lake Oswego, OR 97034

Koss Brod Goodrich & Assoc Inc
22400 Salamo Rd #204
West Linn, OR 97068

May 16, 2018

From: Ali and Nina Ladha
Investco Realty Group

Subject: Neighborhood Review Meeting for Proposed Development at 20590 Noble Lane in West Linn (T2S, R1E, Section 26D, Tax Lot 0402)

Dear Neighbor:

Ali and Nina Ladha of Investco Realty Group, owners of the property located at 20590 Noble Lane, in the R-3 District (Single Family Residential and Multiple Family Residential / 3,000 square foot min. lot size), are proposing to develop the property with a five (5) unit attached residential development. The existing 17,517 square foot (0.40 acre) property is currently undeveloped. The property is of a size to allow development of five (5) units in a row house configuration. This development proposal is similar to one that was previously considered by the Parker Crest Neighborhood a year or so ago. So, prior to submitting a current application to the City of West Linn, we wish to give you the opportunity to discuss this project with us.

The purpose of this meeting is to provide a forum for the applicant and surrounding property owners and local residents to review the proposal and to identify any issues that may need to be considered before the formal application is submitted to the City of West Linn. This meeting gives you the opportunity to share any special information you know about the property involved, or ask questions about the development review process. We will attempt to answer any questions which may be relevant to meeting the development standards consistent with West Linn's Community Development Code (CDC). A Variance may be necessary, depending on the final configuration and design of the proposed residential units.

Pursuant to Chapter 99.038 of the West Linn Community Development Code, you are invited to a meeting:

Date and Time: Wednesday, June 6, 2018, from 6:00 to 7:00 PM
Location: West Linn Adult Community Center
Address: 1180 Rosemont Road West Linn, OR 97068

Please note that this meeting will be an informational meeting on the preliminary development plan. This plan may ultimately be revised prior to submittal to the City. Depending on the type of land use action required for this particular application (a Type III process), you may receive official notice from the City of West Linn for you to participate with written or verbal comments provided to city staff, or a public hearing before the West Linn Planning Commission.

We look forward to more specifically discussing our proposal with you. Please feel free to call or e-mail us with any questions or comments at [503-720-0021](tel:503-720-0021), or our Planning Consultant Robert Price at [503-807-4009](tel:503-807-4009).

Sincerely,

Ali and Nina Ladha

=====
 PORTLAND AIRPORT
 7640 NE AIRPORT WAY
 PORTLAND
 OR
 97238-9099
 4067540039
 05/16/2018 (800)275-8777 3:54 PM
 =====

Product Description	Sale Qty	Final Price
First-Class Mail Letter (Domestic) (WEST LINN, OR 97068) (Weight:0 Lb 0.20 Oz) (Estimated Delivery Date) (Friday 05/18/2018) Certified (@@USPS Certified Mail #) (70180360000115189000)	1	\$0.50
First-Class Intl Letter (International) (Germany) (Weight:0 Lb 0.20 Oz)	1	\$1.15
Total		\$5.10

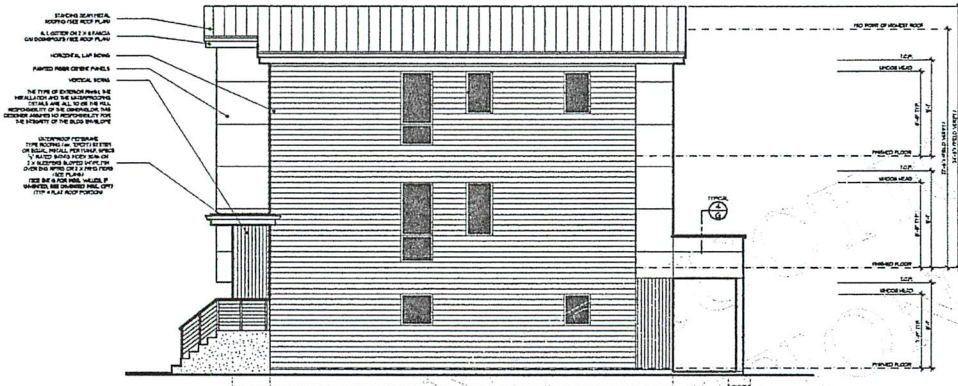
Debit Card Remit'd \$5.10
 (Card Name:VISA)
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 (ARC:00)
 (CVR:420000)
 (IAD:06010A03602400)
 (TSI:6800)
 (TVR:8000048000)

Text your tracking number to 28777
 (2USPS) to get the latest status.
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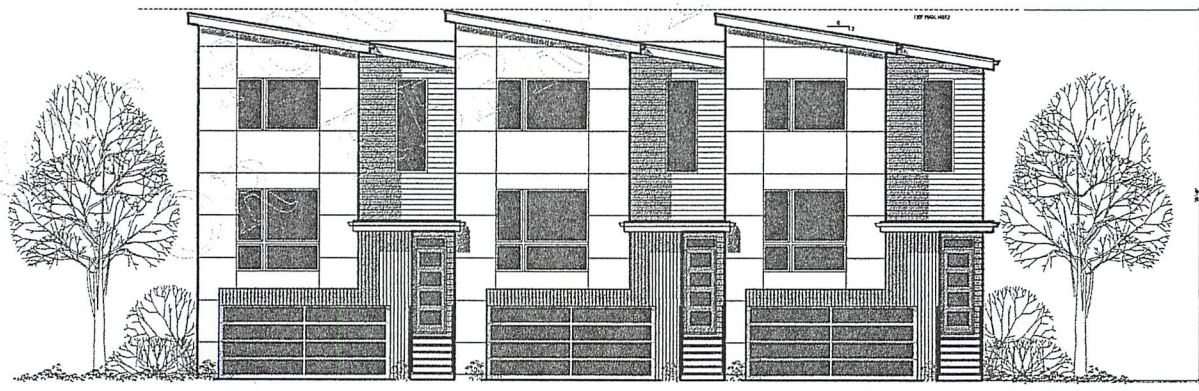
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Postage \$0.50 \$ Total Postage and Fees \$3.95	
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PS Form 3800, April 2015 PSN 7530-02-000-8047 See Reverse for Instructions	



RIGHT ELEVATION
SCALE: 1/4" = 1'-0"



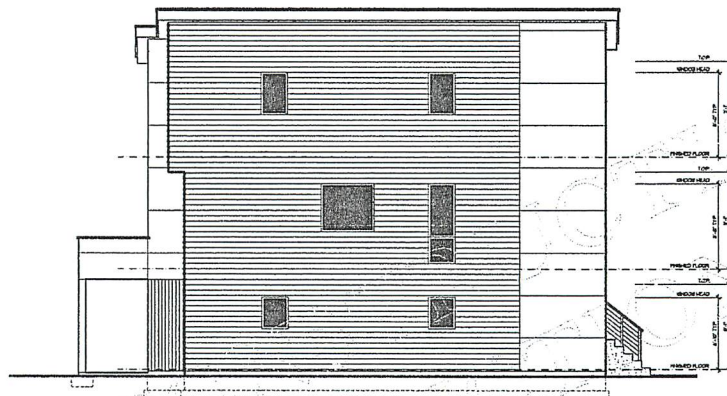
FRONT ELEVATION
SCALE: 1/4" = 1'-0"

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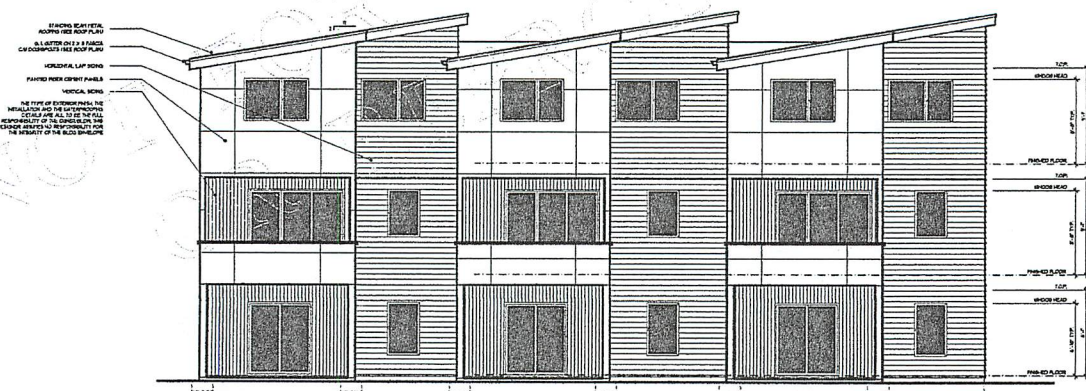
Harvard COLLECTION

INVESTCO REALTY GROUP

1



LEFT ELEVATION
SCALE: 1/4" = 1'-0"



REAR ELEVATION
SCALE: 1/4" = 1'-0"

BRICKS SEAMLESS,
ACROSS THE ROOF PLANE
& LAYERS ON A 3/4\"/>

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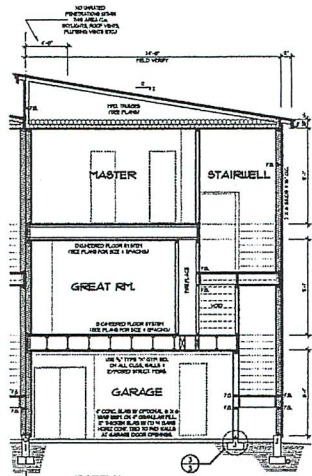
RESIDENCY FOR
INVESTCO REALTY GROUP

156 SNOW LEAD

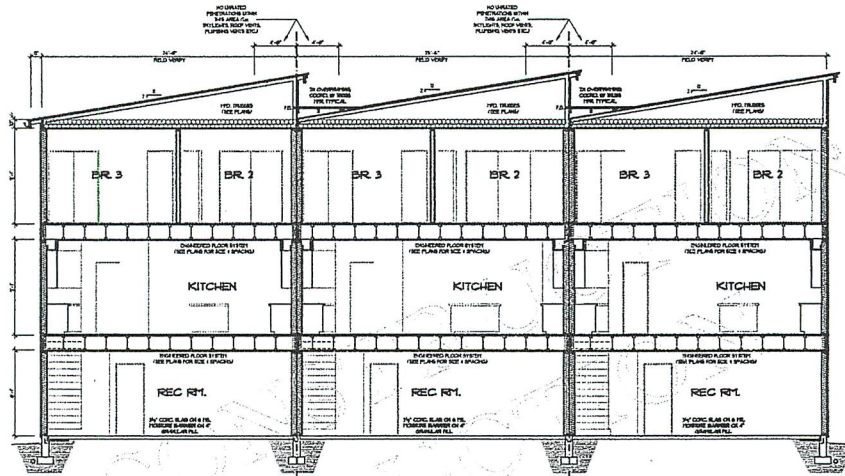
DATE	NO. OF SHEETS	TOTAL SHEETS	DATE	NO. OF SHEETS	TOTAL SHEETS
2018.03.15	1	1	2018.03.15	1	1
2018.03.15	1	1	2018.03.15	1	1
2018.03.15	1	1	2018.03.15	1	1
2018.03.15	1	1	2018.03.15	1	1
2018.03.15	1	1	2018.03.15	1	1

INVESTCO

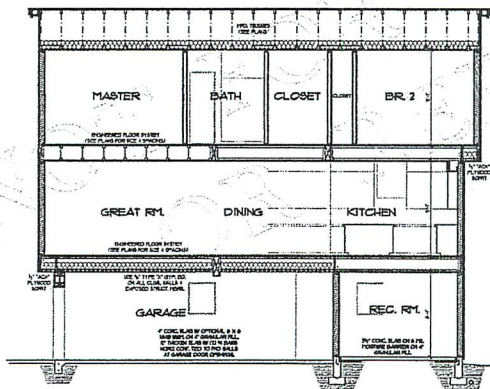
2



D PARTIAL BUILDING SECTION
SCALE: 1/4" = 1'-0"

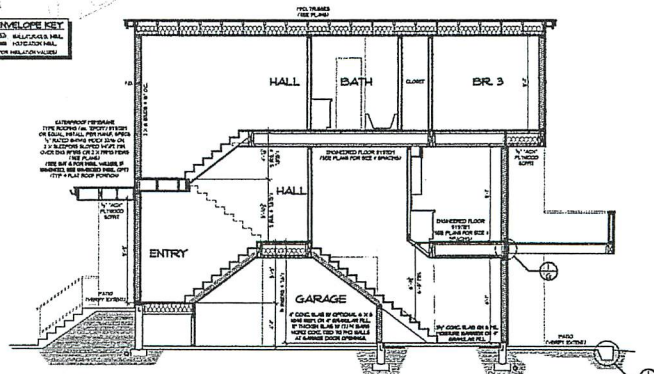


C BUILDING SECTION
SCALE: 1/4" = 1'-0"



B BUILDING SECTION
SCALE: 1/4" = 1'-0"

ENERGY ENVELOPE KEY
 UNFINISHED WALLS
 FINISHED WALLS
 UNFINISHED CEILING
 FINISHED CEILING
 UNFINISHED FLOOR
 FINISHED FLOOR
 UNFINISHED ROOF
 FINISHED ROOF



A BUILDING SECTION
SCALE: 1/4" = 1'-0"

INVESTCO REALTY GROUP
 6

Technical Memorandum

To: Ali Ladha
Investco Realty Group LLC

From: Daniel Stumpf, EI
William Farley, PE

Date: December 22, 2017

Subject: Hoodview Avenue Townhouses – Transportation Study



**LANCASTER
ENGINEERING**

321 SW 4th Ave., Suite 400
Portland, OR 97204
phone: 503.248.0313
fax: 503.248.9251
lancasterengineering.com

Introduction

This memorandum reports and evaluates the transportation impacts related to the proposed Hoodview Avenue Townhouses, located at 20110 Hoodview Avenue in West Linn, Oregon. The proposed development will include the partitioning and development of a single property into five townhomes.

The purpose of this memorandum is to examine the projected trip generation of the proposed development and assess the safety and operation of nearby intersections and vicinity roadways.

Location Description

Project Site Description

The project site is located south of Hoodview Avenue, east of Salamo Road, and west of Noble Lane in West Linn, Oregon. The site includes a single tax lot, lot 402, which encompasses an approximate total of 0.40 acres. The site is currently undeveloped; however, frontage improvements along adjacent roadway segments are in place. Access between the site and the greater transportation system will be provided via driveways along Noble Lane.

Vicinity Roadways

The proposed development is expected to predominantly impact the following four nearby vicinity roadways: Salamo Road, Parker Road, Hoodview Avenue, and Noble Lane. Table 1 provides a description of each of the vicinity roadways.

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Table 1 – Vicinity Roadway Descriptions

Roadway	Jurisdiction	Functional Classification	Cross-Section	Speed	On-street Parking	Bicycle Lanes	Curbs	Sidewalks
Salamo Road	West Linn	Minor Arterial	3 Lanes	35 mph Posted	Not Permitted	Both Sides	Both Sides	Partial Both Sides
Parker Road	West Linn	Minor Arterial	2 to 3 Lanes	35 mph Posted	Not Permitted	Partial Both Sides	Partial Both Sides	Partial Both Sides
Hoodview Avenue	West Linn	Local Street	2 Lanes	25 mph Posted	Partially Permitted	None	Both Sides	Both Sides
Noble Lane	West Linn	Local Street	2 Lanes	25 mph Posted	Partially Permitted	None	Both Sides	Both Sides

Vicinity Intersections

The intersection of Salamo Road at Hoodview Avenue is a three-legged intersection that is stop-controlled for the westbound approach of Hoodview Avenue. The northbound approach has one shared lane for all turning movements and a bicycle lane to the right of the standard travel lane. The southbound approach has one left-turn lane, one through lane, and a bicycle lane to the right of the outermost standard travel lane. The westbound approach has one shared lane for all turning movements. Crosswalks are unmarked across all three intersection legs.

The intersection of Parker Road at Noble Lane is a four-legged intersection that is stop-controlled for the northbound and southbound approaches. The northbound and southbound approaches each have one shared lane for all turning movements. The eastbound and westbound approaches of Parker Road have one left-turn lane, one shared through/right-turn lane, and a bicycle lane to the right of the outermost standard travel lane. Crosswalks are unmarked across all four intersection legs.

Figure 1 presents an aerial image of the nearby vicinity with the project site outlined in yellow.

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Figure 1: Aerial Photo of Site Vicinity (Image from Google Earth)

Trip Generation

The proposed Hoodview Avenue Townhouses includes the construction of five townhouses, each of which will have three floors. To estimate the number of trips that will be generated by the proposed development, trip rates from the *TRIP GENERATION MANUAL*¹ were used. Data from land-use code 221, *Multifamily Housing (Mid-Rise)*, was used to estimate the proposed development's trip generation of the site based on the number of dwelling units.

The trip generation calculations show that the proposed development is projected to generate 2 morning peak hour, 2 evening peak hour, and 28 average weekday site trips. The trip generation estimates are summarized in Table 2. Detailed trip generation calculations are included as an attachment to this study.

¹ Institute of Transportation Engineers (ITE), *TRIP GENERATION MANUAL*, 10th Edition, 2017.

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Table 2 – Trip Generation Summary

	ITE Code	Size	Morning Peak Hour			Evening Peak Hour			Weekday
			Enter	Exit	Total	Enter	Exit	Total	Total
Proposed Development	221	5 units	1	1	2	1	1	2	28

Safety Analysis

Using data obtained from the Oregon Department of Transportation’s (ODOT) Crash Analysis and Reporting Unit, a review was performed for the most recent five years of available crash data (January 2011 through December 2015) at the nearby intersections of Salamo Road at Hoodview Avenue and Parker Road at Noble Lane, and along the nearby roadways of Hoodview Avenue and Noble Lane. The crash data was evaluated based on the number of crashes, the type of collisions, and the severity of the collisions at the nearby transportation facilities.

The intersection of Salamo Road at Hoodview Avenue had one reported crash during the analysis period. The crash was a rear-end collision involving two northbound passenger cars. The crash was classified as “Property Damage Only” (PDO).

No other crashes were reported within the immediate site vicinity. Due to the low number of crashes and the low severity of collisions, there do not appear to be any significant safety hazards at the nearby transportation facilities. Furthermore, based on site observations, no design deficiencies which would impact the safety of the area roadways and intersections were noted. Accordingly, no safety mitigation is necessary or recommended.

Detailed information about crashes and crash reports for the nearby roadways and intersection are included as an attachment to this memorandum.

Operational Analysis

Intersection Capacity

To determine if the nearby transportation facilities can adequately accommodate future trips to and from the proposed development in addition to the existing uses within the site vicinity, peak hour observations of traffic conditions were conducted. Traffic observations were conducted at the intersections of Salamo Road

at Hoodview Avenue and at Parker Road at Noble Lane during the morning and evening peak periods on Thursday, December 7th, 2017, from 8:15 AM to 8:40 AM and from 4:30 PM to 4:55 PM, respectively.

The level of service (LOS) of an intersection can range from LOS A, which indicates little or no delay experienced by vehicles, to LOS F, which indicates a high degree of congestion and delay. The City of West Linn requires unsignalized intersections operate at LOS D or better along non-arterial facilities. Detailed LOS descriptions are included as an attachment to this memorandum.

The intersection of Salamo Road at Hoodview Avenue was observed having less than 5 vehicles enter the intersection from the minor-street approach of Hoodview Avenue over the course of approximately 15 minutes during the morning peak period and 10 minutes during the evening peak period. Observed queuing on the minor-street approach never exceeded more than 1 vehicle during either peak period. Traffic volumes along Salamo Road were observed to be at light to moderate levels and in a state of free-flow during both peak periods. Gaps to enter the major-street traffic stream from the minor-street approach were available within an average control delay of 10 to 15 seconds during either peak period. Accordingly, the intersection was estimated to currently operate at LOS C or better during both peak periods.

The intersection of Parker Road at Noble Lane was observed to have 5 or less vehicles enter the intersection from the minor-street approach of Noble Lane over the course of approximately 10 minutes during the morning and evening peak periods. It should be noted that the northbound driveway approach opposite of Noble Lane was observed to serve twice as many entering vehicles during the morning peak period and the same number of entering vehicles during the evening peak period as Noble Lane. Observed queuing on the minor-street approach of Noble Lane never exceeded more than 1 vehicle during either peak period while the northbound driveway approach was observed to have queues of up to 2 vehicles during the morning peak period and queues of 1 vehicle during the evening peak period. Traffic volumes along Parker Road were observed to be at light levels and in a state of free-flow during both peak periods. Gaps to enter the major-street traffic stream from the highest delay minor-street approach were available within an average control delay of 5 to 10 seconds during the morning peak period and 10 to 15 seconds during the evening peak period. Accordingly, the intersection was estimated to currently operate at LOS A during the morning peak period and at LOS C or better during the evening peak period.

The results of the capacity observations indicate that the intersections of Salamo Road at Hoodview Avenue and Parker Road at Noble Lane operate acceptably during the weekday morning and evening peak periods under existing conditions. Based on the low volumes of additional weekday morning and evening peak hour trips expected to be added to the transportation system, the intersection is projected to continue operating acceptably by the build-out year of the site. No mitigation is necessary or recommended with regard to intersection capacity or operation as part of the proposed development.

Roadway Capacity

To ensure the vicinity roadways of Hoodview Avenue and Noble Lane will operate acceptably upon build-out of the proposed development, an assessment of current and future roadway volumes was conducted. Typically, local residential streets are designed and expected to serve traffic volumes that are less than 1,500 vehicles per day. Volumes above this threshold can affect the safety and livability of the street and surrounding neighborhood.

Noble Lane and Hoodview Avenue currently serve approximately 81 two-story townhouses and 81 three-story townhouses. According to the *TRIP GENERATION MANUAL*, land-use code 220, *Multifamily Housing (Low-Rise)*, is described as "...apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have one or two levels (floors)" whereas land-use code 221 is described as "...apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have between three and 10 levels (floors)." Based on these land-use descriptions as well as the trip generation methodologies described in the *Trip Generation* section, the 162 existing townhouses are estimated to currently generate 1,032 weekday trips. Upon build-out of the proposed development, an additional 28 weekday trips will utilize these roadways, an increase of approximately 2.7 percent, for a total of 1060 trips. Since the total daily trips generated by the existing and proposed uses are below the desirable local street carrying capacity of approximately 1,500 vehicles per day for a single roadway, it is expected that the both roadways will operate safely, and the proposed development will not have a significant impact on the livability of the surrounding neighborhood.

Requirement of a Full Traffic Impact Analysis

Per the City of West Linn Community Code Section 85.170.B.2 – *Traffic Impact Analysis* (TIA), a TIA is required when the following conditions apply:

- 1) *The development application involves one or more of the following actions:*
 - (A) *A change in zoning or a plan amendment designation; or*
 - (B) *Any proposed development or land use action that ODOT states may have operational or safety concerns along a State highway; and*
 - (C) *The development shall cause one or more of the following effects, which can be determined by field counts, site observations, traffic impact analysis or study, field measurements, crash history, Institute of Transportation Engineers Trip Generation manual; and information and studies provided by the local reviewing jurisdiction and/or ODOT:*
 - (1) *An increase in site traffic volume generation by 250 average daily trips (ADT) or more (or as required by the City Engineer); or*
 - (2) *An increase in use of adjacent streets by vehicles exceeding the 20,000-pound gross vehicle weights by 10 vehicles or more per day; or*
 - (3) *The location of the access driveway does not meet minimum intersection sight distance requirements, or is located where vehicles entering or leaving the property are restricted, or such vehicles queue or hesitate on the State highway, creating a safety hazard; or*
 - (4) *The location of the access driveway does not meet the access spacing standard of the roadway on which the driveway is located; or*
 - (5) *A change in internal traffic patterns that may cause safety problems, such as backup onto the highway or traffic crashes in the approach area.*

Criterion A

The proposed development application does not change the zoning of the subject property or a plan amendment designation. Accordingly, Criterion A is not met.

Criterion B

The proposed development is projected to generate a low number of additional site trips and is located over a mile driving distance away from the nearest ODOT facility. Therefore, the proposed development is expected to have minimal impacts to ODOT facilities whereby Criterion B is not met.

Criterion C

Regarding Criterion C, the proposed development (1) generates less than 250 additional average daily trips, (2) is not expected to generate heavy vehicle traffic, (3) provides adequate sight distance at individual lot driveways and existing nearby public intersections, (4) meets adequate access spacing standards, (5) and will not change internal traffic patterns or the residential nature of the surrounding area. Accordingly, Criterion C is not meet.

Based on an evaluation of the City of West Linn's Community Code, a full TIA is not required for approval of the proposed development application.

Conclusions

Due to the low number of crashes and the low severity of collisions, there do not appear to be any significant safety hazards at the nearby transportation facilities. Furthermore, based on site observations, no design deficiencies which would impact the safety of the area roadways and intersections were noted. Accordingly, no safety mitigation is necessary or recommended.

The intersections of Salamo Road at Hoodview Avenue and Parker Road at Noble Lane currently operate acceptably during the weekday morning and evening peak periods. Based on the low volumes of additional weekday morning and evening peak hour trips expected to be added to the transportation system, the intersection is projected to continue operating acceptably by the build-out year of the site. No mitigation is necessary or recommended with regard to intersection capacity or operation as part of the proposed development.

The total daily trips generated by the existing and proposed uses are projected to be below the desirable local street carrying capacity of 1,500 for a single roadway. Accordingly, upon build-out of the proposed development it is expected that the two roadways of Hoodview Avenue and Noble Lane will operate safely and will not have a significant impact on the livability of the surrounding neighborhood.

Based on an evaluation of the City of West Linn's Community Code, a full Transportation Impact Analysis is not required for approval of the proposed development application.

If you have any questions regarding this technical memorandum, please don't hesitate to contact us.

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TRIP GENERATION CALCULATIONS Proposed Development

Land Use: Multifamily Housing (Mid-Rise)
Land Use Code: 221
Setting/Location: General Urban/Suburban
Variable: Dwelling Units
Variable Value: 5

AM PEAK HOUR

Trip Rate: 0.36

	Enter	Exit	Total
Directional Distribution	26%	74%	
Trip Ends	1	1	2

PM PEAK HOUR

Trip Rate: 0.44

	Enter	Exit	Total
Directional Distribution	61%	39%	
Trip Ends	1	1	2

WEEKDAY

Trip Rate: 5.44

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	14	14	28

SATURDAY

Trip Rate: 4.91

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	12	12	24

Source: TRIP GENERATION, Tenth Edition

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TRIP GENERATION CALCULATIONS
Existing Residential Uses

Land Use: Multifamily Housing (Low-Rise)
Land Use Code: 220
Setting/Location: General Urban/Suburban
Variable: Dwelling Units
Variable Value: 81

AM PEAK HOUR

Trip Rate: 0.46

	Enter	Exit	Total
Directional Distribution	23%	77%	
Trip Ends	9	28	37

PM PEAK HOUR

Trip Rate: 0.56

	Enter	Exit	Total
Directional Distribution	63%	37%	
Trip Ends	28	17	45

WEEKDAY

Trip Rate: 7.32

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	296	296	592

SATURDAY

Trip Rate: 8.14

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	330	330	660

Source: TRIP GENERATION, Tenth Edition

16

TRIP GENERATION CALCULATIONS
Existing Residential Uses

Land Use: Multifamily Housing (Mid-Rise)

Land Use Code: 221

Setting/Location: General Urban/Suburban

Variable: Dwelling Units

Variable Value: 81

AM PEAK HOUR

Trip Rate: 0.36

	Enter	Exit	Total
Directional Distribution	26%	74%	
Trip Ends	8	21	29

PM PEAK HOUR

Trip Rate: 0.44

	Enter	Exit	Total
Directional Distribution	61%	39%	
Trip Ends	22	14	36

WEEKDAY

Trip Rate: 5.44

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	220	220	440

SATURDAY

Trip Rate: 4.91

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	199	199	398

Source: TRIP GENERATION, Tenth Edition

CRASH SUMMARIES BY YEAR BY COLLISION TYPE

HOODVIEW AVE at SALAMO RD, City of West Linn, Clackamas County, 01/01/2011 to 12/31/2015

COLLISION TYPE	NON-PROPERTY		TOTAL CRASHES	PEOPLE KILLED	PEOPLE INJURED	TRUCKS	DRY SURF	WET SURF	DAY	DARK	INTER-SECTION RELATED		OFF-ROAD
	FATAL CRASHES	DAMAGE ONLY									INTER-SECTION	RELATED	
YEAR: 2011	0	0	1	0	0	0	1	0	1	0	0	1	0
REAR-END	0	0	1	0	0	0	1	0	1	0	0	1	0
YEAR 2011 TOTAL	0	0	1	0	0	0	1	0	1	0	0	1	0
FINAL TOTAL	0	0	1	0	0	0	1	0	1	0	0	1	0

Disclaimer: The information contained in this report is compiled from individual driver and police crash reports submitted to the Oregon Department of Transportation as required in ORS 811.720. The Crash Analysis and Reporting Unit is committed to providing the highest quality crash data to customers. However, because submittal of crash report forms is the responsibility of the individual driver, the Crash Analysis and Reporting Unit can not guarantee that all qualifying crashes are represented nor can assurances be made that all details pertaining to a single crash are accurate. Note: Legislative changes to DMV's vehicle crash reporting requirements, effective 01/01/2004, may result in fewer property damage only crashes being eligible for inclusion in the Statewide Crash Data File.

OREGON - DEPARTMENT OF TRANSPORTATION - TRANSPORTATION DEVELOPMENT DIVISION
TRANSPORTATION DATA SECTION - CRASH ANALYSIS AND REPORTING UNIT
URBAN NON-SYSTEM CRASH LISTING

CITY OF WEST LINN, CLACKAMAS COUNTY
HOODVIEW AVE at SALAMO RD, City of West Linn, Clackamas County, 01/01/2011 to 12/31/2015

1 - 1 of 1 Crash records shown.

SER#	P R S W DATE	CLASS	CITY STREET	INT-TYPE	INT-REL	OFFRD	WTHR	CRASH	SFCL USE	MOVE	PH TYPE	SVRTY	E X RES	LOC	ACT EVENT	CAUSE		
INVEST#	E A D C O DAY	DIST	FIRST STREET	(MEDIAN)	TRAFFIC	RNDPT	SURF	COLL	TRSR CTY	FROM	TO	INJ	G E LICNS	PED				
RD DFT	E L G H R TIME	FROM	SECOND STREET	LEGS	CONTL	DRVWY	LIGHT	SVRTY	VH TYPE	OWNER	STRTGHT	NO<5	F					
UNLOC?	D C S L K LAT	LONG	LRG	(#LANES)	3-LEG	N	CLR	S-1STOP	01 NONE	0	STRTGHT							
02025	N N N	06/08/2011	17 HOODVIEW AVE	N	UNKNOWN	N	DRY	REAR	PRVTE	S -N	01	DRVR	NONE	36	F	OR-Y	07	
NONE	WE	0	SALAMO RD	CN					PSNGR CAR						000	00	00	
N	9A	-122 38		02	0	N	DAY	PDO	PSNGR CAR						000	000	07	
N	45 22	50.1226463																
	2.5727668																	
									01 NONE	0	STRTGHT				000	00	00	
									PRVTE	S -N					000	00	00	
									PSNGR CAR		02	PSNG	NO<5	03	F			
															000	000	00	
									02 NONE	0	STOP							
									PRVTE	S -N					011	004	00	
									PSNGR CAR		01	DRVR	NONE	62	F	OR-Y	00	
															000	000	00	

Disclaimer: The information contained in this report is compiled from individual driver and police crash reports submitted to the Oregon Department of Transportation as required in ORS 811.720. The Crash Analysis and Reporting Unit is committed to providing the highest quality crash data to customers. However, because submittal of crash report forms is the responsibility of the individual driver, the Crash Analysis and Reporting Unit can not guarantee that all qualifying crashes are represented nor can assurances be made that all details pertaining to a single crash are accurate. Note: Legislative changes to DMV's vehicle crash reporting requirement, effective 01/01/2004, may result in fewer property damage only crashes being eligible for inclusion in the Standard Crash Data File.

CDS380
12/20/2017

OREGON... DEPARTMENT OF TRANSPORTATION - TRANSPORTATION DEVELOPMENT DIVISION
TRANSPORTATION DATA SECTION - CRASH ANALYSIS AND REPORTING UNIT

HOODVIEW AVE and Intersectional Crashes at HOODVIEW AVE, City of West Linn, Clackamas County, 01/01/2011 to 12/31/2015

CITY OF WEST LINN, CLACKAMAS COUNTY

URBAN NON-SYSTEM CRASH LISTING

1 - 1 of 1 Crash records shown.

SR#	DATE	CLASS	CITY STREET	RD CHAR	INT-TYPE	INT-REL	OFFRD	WTHR	CRASH	SPCL USE	VEH TYPE	VEH TYPE	SVRTY	PH TYPE	SVRTY	E X RES	LOC	ERROR	ACT EVENT	CAUSE	
02025	N N N	06/08/2011	HOODVIEW AVE	INTER	3-LEG	N	N	CLR	S-1STOP	01 NONE	0	STRGHT	01 NONE	0	STRGHT	004	000	000	00	07	
N	ME	0	SALAMO RD	CN	0	UNKNOWN	N	DRY	REAR	PRVTE	S -N	01 DRIVER	NONE	36 F	OR-Y	OR<25	026	000	000	07	
N	9A	-122 38		02	0		N	DAY	PPO	PSNGR CAR		01 NONE	0	STRGHT							
N	45 22	50.1226463								PRVTE	S -N	02 PSNGR	NO<5	03 F			000	000	000	00	
	2.5727668									PSNGR CAR		02 PSNGR	NO<5	03 F			000	000	000	00	
										02 NONE	0	STOP							011 004	00	
										PRVTE	S -N	01 DRIVER	NONE	62 F	OR-Y	OR<25	000	000	000	00	
										PSNGR CAR											

Disclaimer: The information contained in this report is compiled from individual driver and police crash reports submitted to the Oregon Department of Transportation as required in ORS 81.720. The Crash Analysis and Reporting Unit is committed to providing the highest quality crash data to customers. However, because submission of crash report forms is voluntary, the information may not be complete. The Oregon Department of Transportation can not guarantee that all qualifying crashes are represented nor can assurances be made that all details pertaining to a single crash are accurate. Note: Legislative changes to DMV's vehicle crash reporting requirement, effective 01/01/2004, may result in newer property damage only crashes being eligible for inclusion in the Statewide Crash Data File.

Preliminary Storm Drainage Report

*Hoodview Summit
July 17th, 2018*

Prepared For: Ali Ladha
9334 SE Hunters Bluff Ave.
Happy Valley, Oregon 97086

Prepared By: CWK2 Land Development Consultants
10700 SW Beaverton Hillsdale Hwy., Suite 533
Beaverton, Oregon 97005

Submitted To: City of West Linn



EXPIRATION DATE: 6/30/05

18



Table of Contents:

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Purpose.....3

Introduction/Project Overview.....3

Existing Conditions.....3

Proposed Conditions.....3

Conclusions.....4

PRELIMINARY STORM DRAINAGE REPORT

HOODVIEW SUMMIT

Purpose

The purpose of this analysis is to:

- Describe existing and proposed site conditions.

Introduction/Project Overview

This report addresses the storm drainage impacts associated with the Hoodview Summit development. This property occupies approximately 0.34 acres (after proposed right-of-way dedications) and resides in City of West Linn, Oregon. Private improvements are anticipated to include five attached single family residences, driveways, wet and dry utilities, and lot grading for the proposed residences.

Existing Conditions

The site is currently vacant and is covered by low quality landscaped grasses and low-lying shrubs. Well established street trees exist along Salamo Rd., and smaller street trees are present at the north and east end of the site along Hoodview Ave., and Noble Ln. The site is bordered to the north, south and east by existing single family attached residences. The site is further bordered to the west, north, and east by Salamo Rd, Hoodview Ave., and Noble Ln. which is fully built-out with sidewalks. The site currently slopes from the south to the north with an average slope of 5 percent. (See appendix A: Existing Conditions Plan). All existing storm water runoff appears to either infiltrate into the soil or runoff to Hoodview Ave. or Noble Ln.

Proposed Conditions

The proposed Hoodview Summit development consists of five new single new residences. No street frontage improvements are anticipated to be required since Salamo Rd., Hoodview Ave., and Noble Ln is fully built-out with sidewalks. Stormwater runoff associated with the new residences will be directed towards the public storm system in Noble Lane. (See appendix A: Storm Sewer Plan). Water quality will be managed via the existing storm water facility which was constructed for this neighborhood at the south end of Viewpoint Drive.

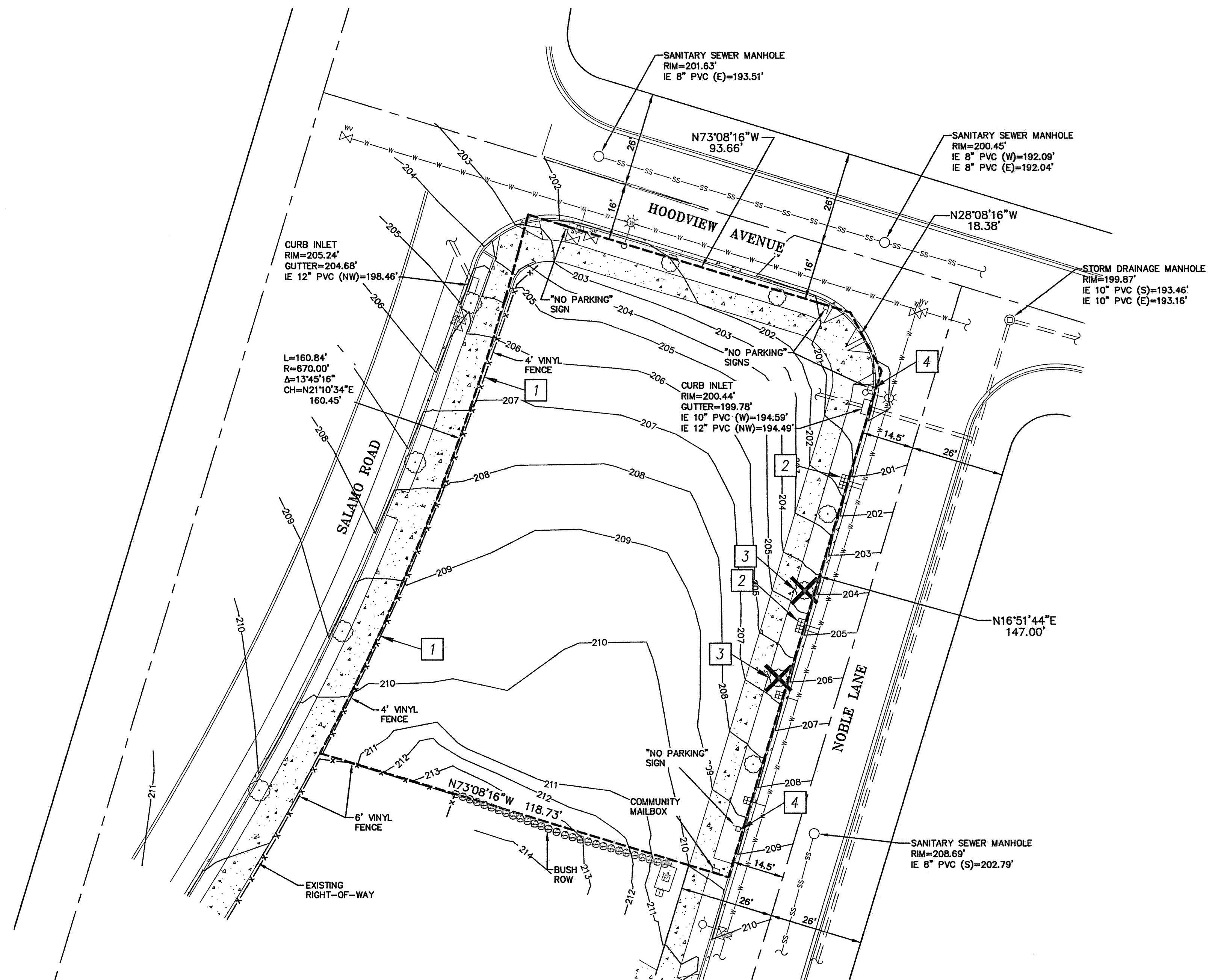
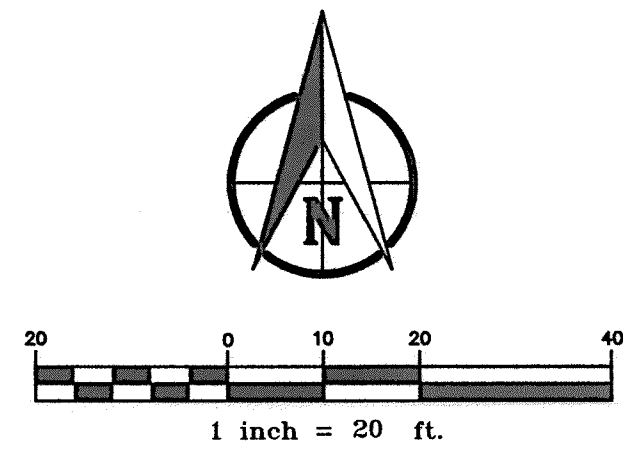
Conclusions

- Storm water will be managed per City of West Linn's storm water standards.

Appendix A:

Existing Conditions Plan

Preliminary Stormwater Plan



EXISTING CONDITIONS / DEMO PLAN
1"=20'

LEGEND

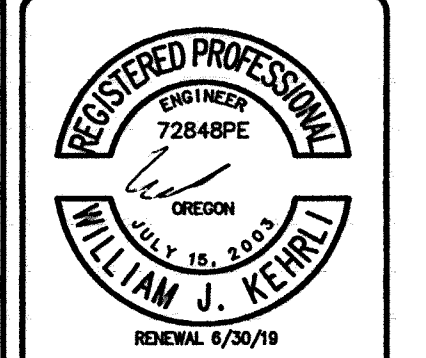
- 101--- - EXISTING 1' CONTOUR
- 105--- - EXISTING 5' CONTOUR
- TREE TO BE REMOVED
- CONIFEROUS TREE
- DECIDUOUS TREE
- EXISTING GRAVEL
- EXISTING ASPHALT
- EXISTING CONCRETE
- XSD - EXIST STORM DRAIN LINE
- XOH - EXIST OVERHEAD WIRE
- X - EXIST FENCE LINE
- XSS - EXIST SANITARY LINE
- XG - EXIST GAS LINE
- - GUARDRAIL
- SPRINKLER VALVE
- STANDPIPE
- WATER VALVE
- FENCE POST
- TELEPHONE RISER
- POWER POLE
- GUY ANCHOR
- SIGN
- CATCH BASIN
- SANITARY MANHOLE
- POWER METER
- LIGHT POLE
- STORM MANHOLE
- WATER METER

DEMOLITION NOTES

- 1 EXISTING 4' VINYL FENCE TO BE REMOVED
- 2 EXISTING WATER METER TO BE ABANDONED
- 3 EXISTING STREET TREE TO BE REMOVED
- 4 NO PARKING SIGN TO BE REMOVED

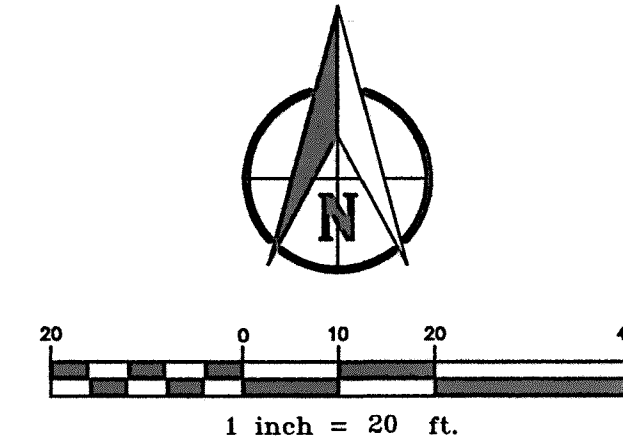
CWK2 LAND DEVELOPMENT CONSULTANTS
10700 SW BEAV. HILLSDALE HWY., SUITE 533
BEAVERTON, OR 97005
TEL: (503) 620-7182
FAX: (503) 620-7283

EXISTING CONDITIONS / DEMO PLAN
HOODVIEW SUMMIT
ALI LADHA
9334 SE HUNTERS BLUFF AVE. (503) 788-4401



DATE: 7/13/2018
DRAWN BY: WJK
PROJ. MGR: WJK
CHECKED BY: WJK
PROJECT NUMBER
BRT.001
CASE FILE NUMBER

SHEET NUMBER
2
OF
7

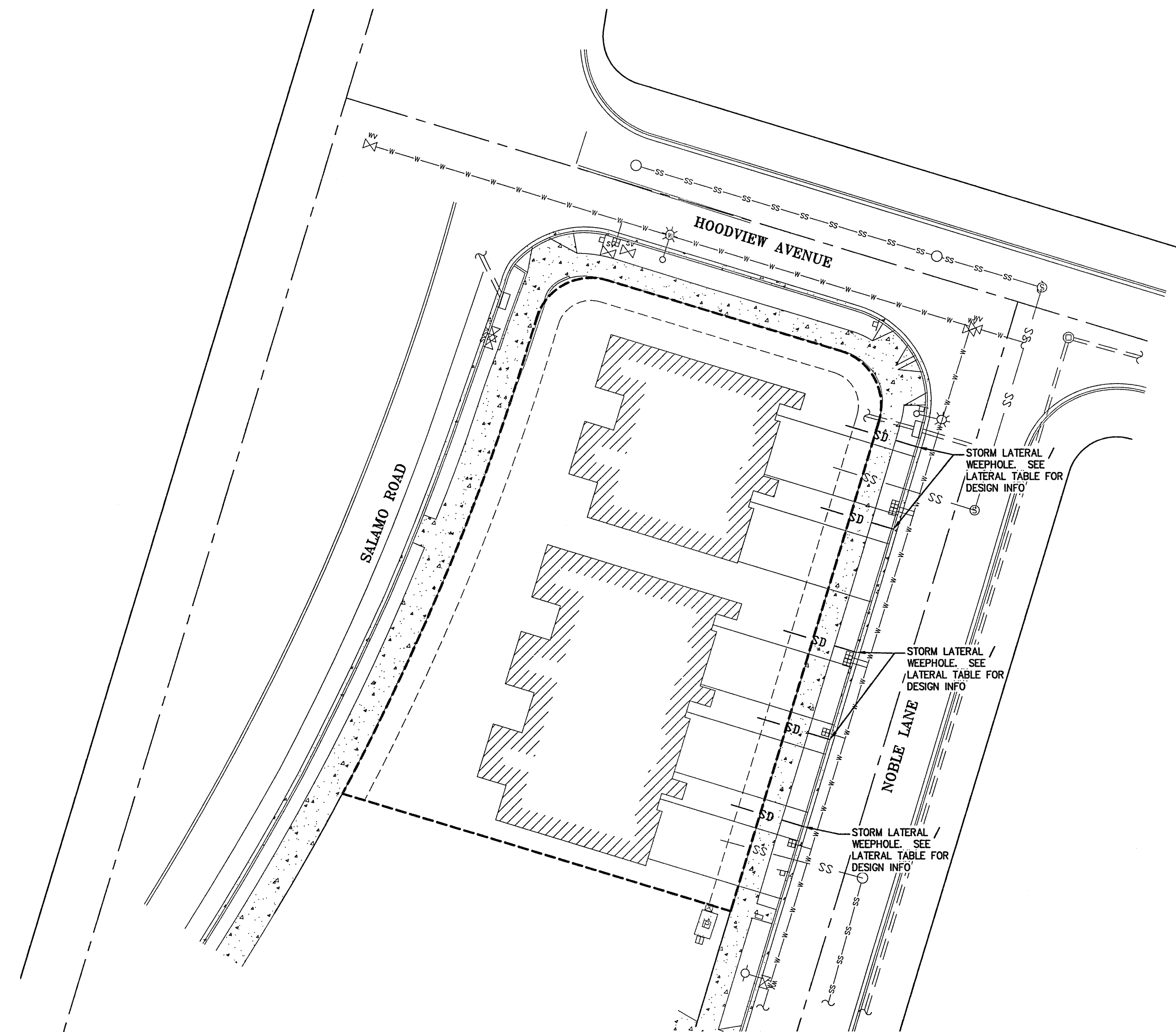


LEGEND

- PROPOSED SIDEWALK
- PROPOSED CURB
- SAWCUT LINE
- EXISTING PAVEMENT
- PROPOSED PAVEMENT

UTILITY LEGEND

- PROPOSED STORM LINE & MANHOLE
- PROPOSED STORM LINE & CLEANOUT
- PROPOSED SANITARY LINE & CLEANOUT
- PROPOSED SANITARY LINE & MANHOLE
- PROPOSED WATERLINE & VALVE
- AREA / DITCH INLET CATCH BASIN
- CG-30 STYLE CATCH BASIN



STORM SEWER PLAN
1"=20'

STORM LATERAL DATA								
LAT NO.	STREET STATION	LENGTH	SIZE	MATERIAL	INV. EL. @ CURB	INV. EL. @ PLUG	SLOPE	DEPTH @ PLUG
1	1+44.43	19.80'	4"	ASTM C-900	200.39	200.79	0.0200	2.2'
2	1+66.98	19.91'	4"	ASTM C-900	202.01	202.41	0.0200	2.1'
3	2+01.01	19.94'	4"	ASTM C-900	204.46	204.86	0.0200	2.1'
4	2+25.01	19.95'	4"	ASTM C-900	206.18	206.58	0.0200	1.9'
5	2+49.01	19.98'	4"	ASTM C-900	207.64	208.04	0.0200	1.5'

BACKFILL NOTE:
PIPES UNDER PAVED SURFACES REQUIRE GRANULAR BACKFILL. FOR PIPES OUTSIDE PAVEMENT, NATIVE BACKFILL IS PERMITTED, UNLESS OTHERWISE NOTED.

4" LATERALS ARE TO BE 4" PVC WITH A MINIMUM SLOPE OF 2% UNLESS OTHERWISE NOTED.

NOTE: ALL 2"x 4" STORM SERVICE CONNECTION MARKERS TO BE COLOR CODED WHITE

THE CONTRACTOR SHALL FIELD VERIFY THE SIZE, LOCATION & DEPTH OF EXISTING UTILITIES PRIOR TO CONSTRUCTION

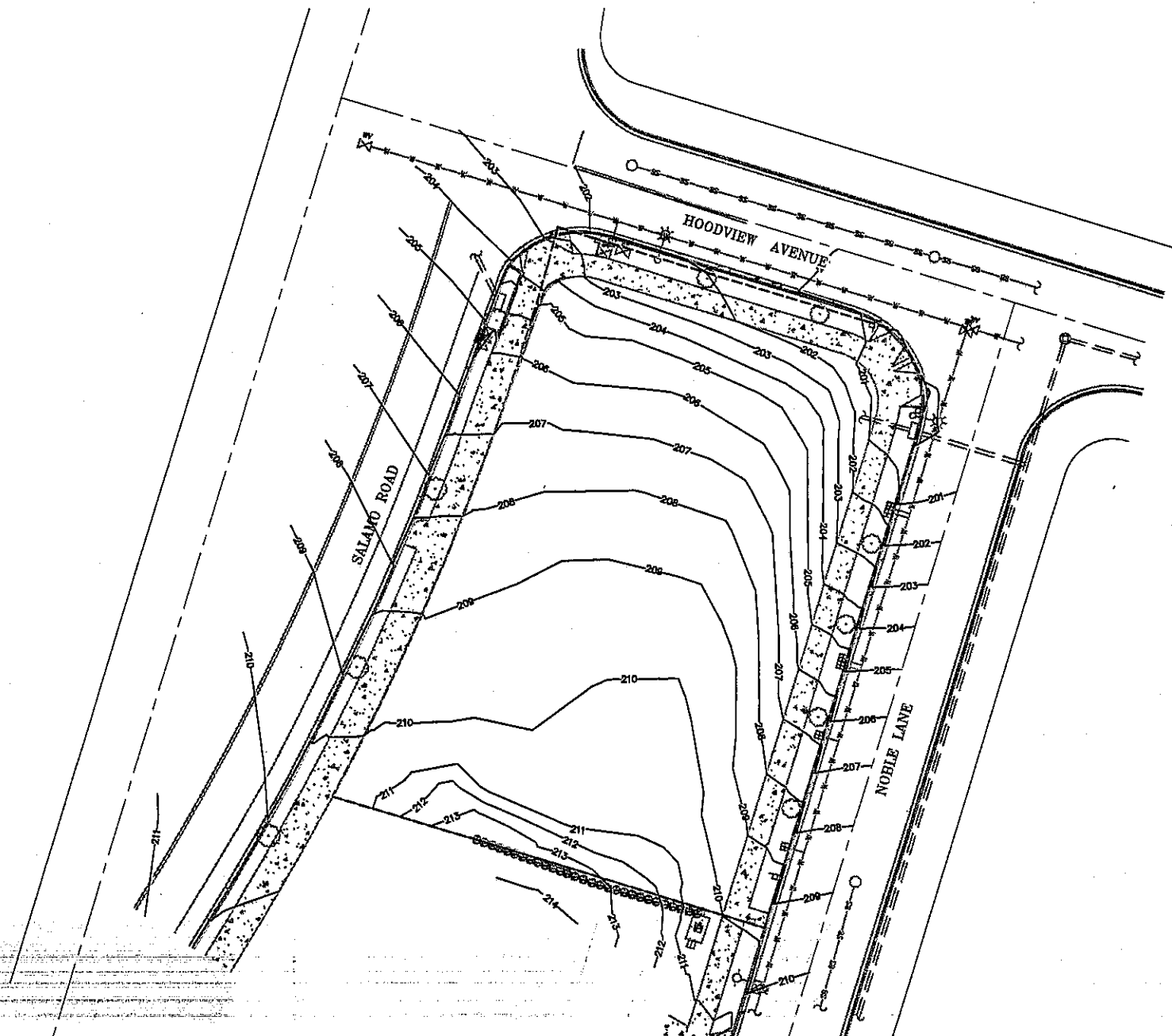
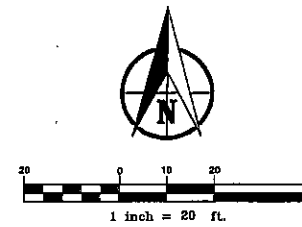
STORMWATER MANAGEMENT
WATER QUALITY WILL BE MANAGED VIA THE EXISTING STORM WATER FACILITY WHICH WAS CONSTRUCTED FOR THIS NEIGHBORHOOD AT THE SOUTH END OF VIEWPOINT DRIVE.

CWK2 LAND DEVELOPMENT CONSULTANTS
10700 SW BEAV. HILLSDALE HWY., SUITE 533
BEAVERTON, OR 97005
TEL: (503) 620-7182
FAX: (503) 620-7283

STORM SEWER PLAN
HOODVIEW SUMMIT
ALI LADHA
9334 SE HUNTERS BLUFF AVE. (503) 788-4401

DATE: 7/13/2018
DRAWN BY: WJK
PROJ. MGR: WJK
CHECKED BY: WJK
PROJECT NUMBER
BRT.001
CASE FILE NUMBER

SHEET NUMBER
5
OF
7



EXISTING CONDITIONS
T-20

LEGEND

- 101--- - EXISTING 1' CONTOUR
- 105--- - EXISTING 5' CONTOUR
- TREE TO BE REMOVED
- CONIFEROUS TREE
- DECIDUOUS TREE
- EXISTING GRAVEL
- EXISTING ASPHALT
- EXISTING CONCRETE
- XSD--- - EXIST STORM DRAIN LINE
- XOH--- - EXIST OVERHEAD WIRE
- X--- - EXIST FENCE LINE
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- SPRINKLER VALVE
- STANDPIPE
- WATER VALVE
- FENCE POST
- TELEPHONE RISER
- POWER POLE
- GUY ANCHOR
- SIGN
- CATCH BASIN
- SANITARY MANHOLE
- POWER METER
- LIGHT POLE
- STORM MANHOLE
- WATER METER

SITE SLOPE ANALYSIS

TOTAL SITE AREA = 17,431 SF
 TOTAL SITE AREA WITH SLOPE LESS THAN 10% = 9,581 SF (55%)
 TOTAL SITE AREA WITH SLOPE BETWEEN 10% AND 25% = 7,850 SF (45%)



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LAND CLASSIFICATION MAP

HOODVIEW SUMMIT

ALI LADHA

9334 SE HUNTERS BLUFF AVE. (503) 788-4401



DATE: 11/7/2018
 DRAWN BY: WJK
 PROJ. MGR: WJK
 CHECKED BY: WJK
 PROJECT NUMBER
 BRT.001
 CASE FILE NUMBER

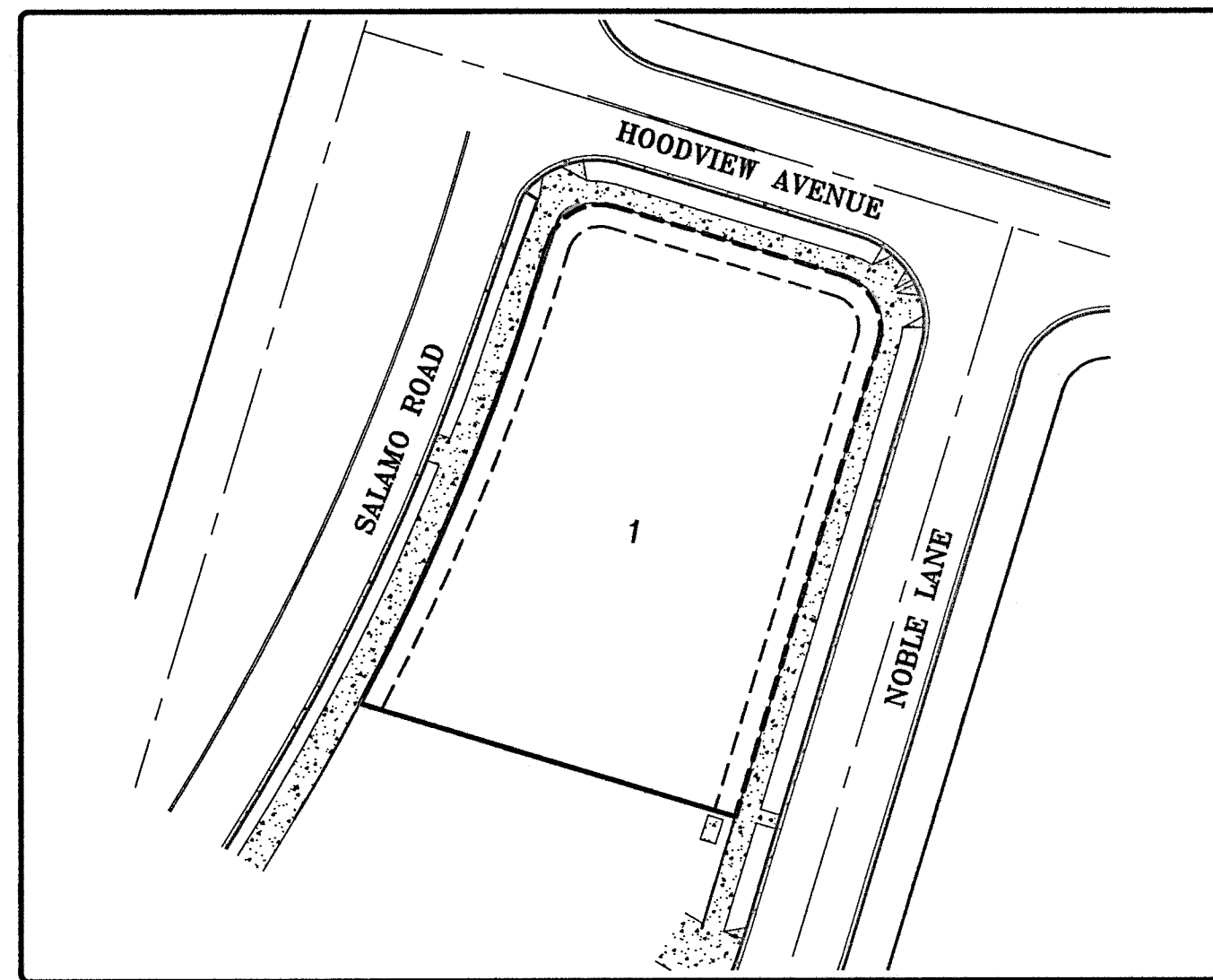
SHEET NUMBER
 1
 OF 1

KeirliB, Plotted: Nov 07, 2018 - 2:59pm, P: BRT.001 HoodView Subdivision.dwg\Eng\Exhibit\BRT.001_Avea-Exhibit.dwg

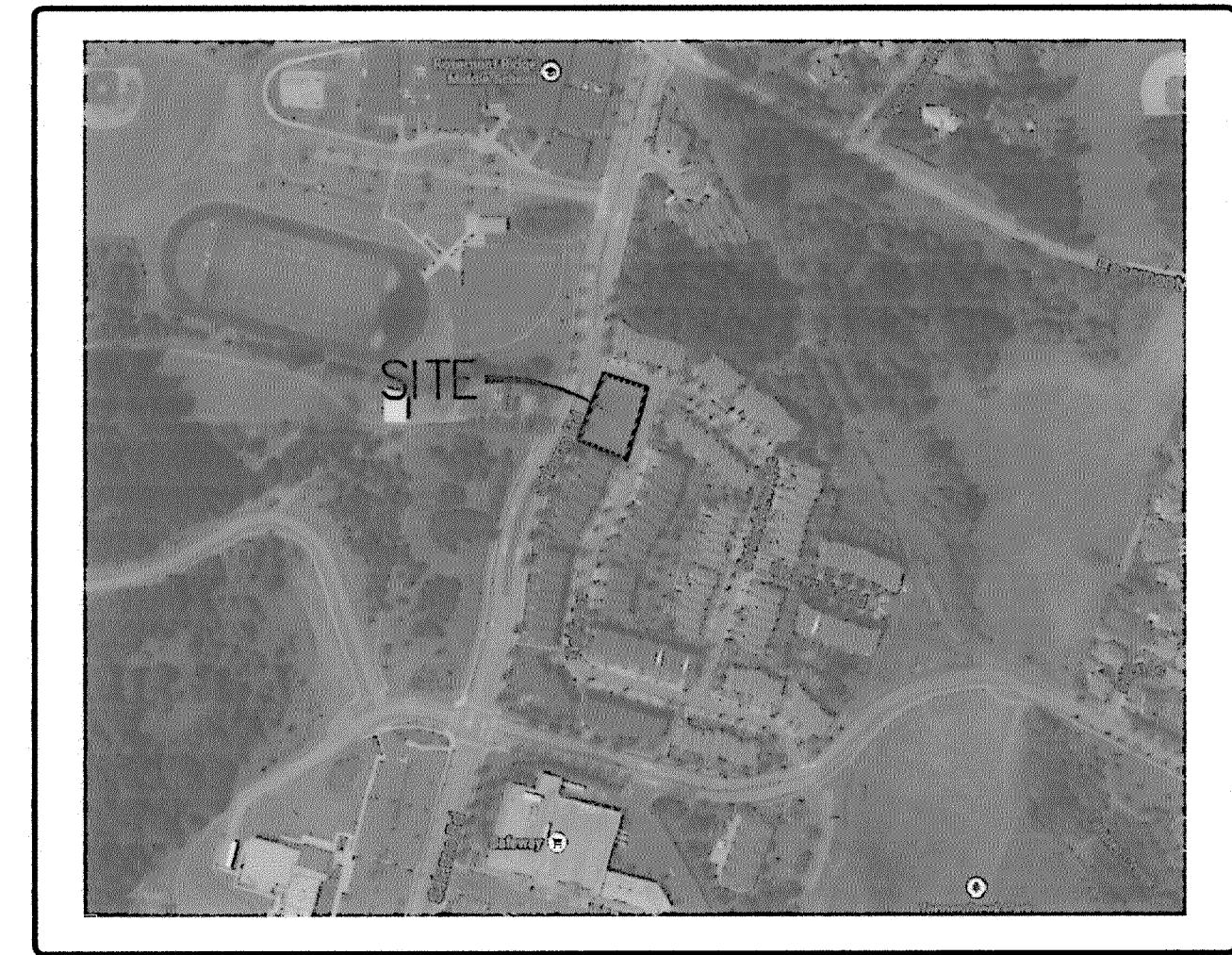
HOODVIEW SUMMIT SUBDIVISION

CONSTRUCTION DOCUMENT PLAN SET

WEST LINN, OREGON
 LOCATED IN THE SOUTHEAST 1/4 OF SECTION 26
 TOWNSHIP 2 SOUTH, RANGE 1 EAST, WILLAMETTE MERIDIAN



LOCATION MAP
NOT TO SCALE



VICINITY MAP
NOT TO SCALE



LEGEND

—XSD—⊙	EXISTING MANHOLE & STORM SEWER	—COM—	EXISTING COMMUNICATION LINE
—FSD—⊙	PROPOSED MANHOLE & STORM SEWER	—XG—	EXISTING GAS LINE
—XSD—□	EXISTING INLET & STORM SEWER	—XV—	EXISTING GAS VALVE
—	PROPOSED CATCH BASIN & STORM SEWER	—XT—	EXISTING UNDERGROUND TELEPHONE LINE
—	PROPOSED DITCH INLET & STORM SEWER	—	EXISTING TELEPHONE PEDESTAL
—	PROPOSED CG-48 MH & STORM SEWER	—XC—	EXISTING UNDER GROUND CABLE TV LINE
—	PROPOSED CG-48 CB & STORM SEWER	—	EXISTING CABLE TV PEDESTAL
—	PROPOSED CG-30 CB & STORM SEWER	—XE—	EXISTING UNDERGROUND ELECTRIC LINE
—XSS—⊙	EXISTING MANHOLE & SANITARY SEWER	—E—	EXISTING ELECTRIC VAULT
—FSS—⊙	PROPOSED MANHOLE & SANITARY SEWER	—	EXISTING UTILITY POLE
—XSS—⊙	FUTURE MANHOLE & STORM SEWER	—	EXISTING STREET LIGHT
—XSS—⊙	EXISTING CLEANOUT & SANITARY SEWER	—	PROPOSED STREET LIGHT
—	PROPOSED CLEANOUT & SANITARY SEWER	—	PROPOSED COBRA STREET LIGHT
—	SANITARY OR STORM LATERAL	—	EXISTING SIGN
—XW—	EXISTING WATERLINE	—	PROPOSED SIGN
—FW—	PROPOSED WATERLINE	—	RETAINING WALL (ROCKERY)
—	FUTURE WATERLINE	—	RETAINING WALL (KEYSTONE)
—	EXISTING WATER VALVE & BOX	—	EXISTING STREET BARRICADE
—	PROPOSED WATER VALVE & BOX	—	PROPOSED STREET BARRICADE
—	EXISTING REDUCER	—	CURB RETURN NUMBER
—	PROPOSED REDUCER	—	CENTERLINE CURVE NUMBER
—	EXISTING BLOWOFF	—	HANDICAP RAMP TO BE CONSTRUCTED WITH THIS CONTRACT
—	PROPOSED BLOWOFF	—	SIDEWALK TO BE CONSTRUCTED WITH THIS CONTRACT
—	PROPOSED C.A.R.V.	—	SIDEWALK TO BE CONSTRUCTED BY HOMEBUILDER
—	EXISTING WATER SERVICE	—	INSTALL SURVEY MONUMENT, FRAME AND COVER PER WASHINGTON COUNTY DETAIL M404. MONUMENTS SHALL BE INSTALLED AT ALL CENTERLINE INTERSECTIONS, PC'S, PT'S, PI'S (WHERE IN PAVEMENT), AND EYEBROW RADIUS POINTS.
—	PROPOSED WATER SERVICE	—	
—	EXISTING FIRE HYDRANT	—	
—	PROPOSED FIRE HYDRANT	—	

DEVELOPMENT TEAM

CIVIL ENGINEER

CWK2 LAND DEVELOPMENT CONSULTANTS
 10700 SW BEAVERTON-HILLSDALE HWY., SUITE 533
 BEAVERTON, OREGON 97005
 ATTN: WILLIAM KEHRLI, P.E.
 PH: (503) 620-7182

OWNER

ALI LADHA
 9334 SE HUNTERS BLUFF AVE.
 HAPPY VALLEY, OR 97086
 PH: (503) 788-4401

SURVEYOR

FRONTIER LAND SURVEYING
 2207B PORTLAND RD.
 NEWBERG, OR 97132
 ATTN: DARREN HARR, PLS
 PH: (503) 538-4600

LOCATES (48 HOURS NOTICE REQUIRED PRIOR TO EXCAVATION)

THE CONTRACTOR MUST COMPLY WITH THE REGULATIONS OF O.R.S. 757.541 TO 757.571 IN LOCATION AND PROTECTION OF UNDERGROUND UTILITIES. OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER.



ENGINEER'S NOTE TO CONTRACTOR:

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITIES OR STRUCTURES SHOWN ON THESE PLANS ARE OBTAINED BY A SEARCH OF AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE, THERE ARE NO EXISTING UTILITIES EXCEPT THOSE SHOWN ON THESE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN ON THESE DRAWINGS. THE CONTRACTOR FURTHER ASSUMES ALL LIABILITY AND RESPONSIBILITY FOR THE UTILITY PIPES, CONDUITS OR STRUCTURES SHOWN OR NOT SHOWN ON THESE DRAWINGS.

THE CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR THE ENGINEER.

CONTRACTOR SHALL VERIFY ALL CONDITIONS AND DIMENSIONS AND SHALL REPORT ALL DISCREPANCIES TO THE ENGINEER PRIOR TO THE COMMENCEMENT OF WORK.

NOTE:

THIS DESIGN COMPLIES WITH ORS 92.044 (7) IN THAT NO UTILITY INFRASTRUCTURE IS DESIGNED TO BE WITHIN ONE FOOT OF A SURVEY MONUMENT LOCATION SHOWN ON A SUBDIVISION OR PARTITION PLAT. NO DESIGN MODIFICATION NOR FIELD LOCATION CHANGES SHALL BE PERMITTED IF IT WOULD CAUSE ANY UTILITY INFRASTRUCTURE TO BE PLACED WITHIN THE PROHIBITED AREA.

DRAWING INDEX

1. COVER SHEET
2. EXISTING CONDITIONS AND DEMOLITION PLAN
3. TENTATIVE PLAN
4. GRADING EROSION CONTROL PLAN
5. STORM SEWER PLAN
6. SANITARY SEWER AND WATER PLAN AND PROFILE
7. NEIGHBORING PROPERTIES / ZONING MAP

BENCHMARK

BENCHMARK: AJ8196
DATUM: NAVD 88
DESCRIPTION: FOUND BRONZE CAP INSIDE MONUMENT BOX IN THE CENTERLINE OF SALAMO ROAD, APPROXIMATELY 50' SOUTH OF THE SOUTHERLY LINE OF THE SUBJECT PROPERTY.

ELEVATION: 211.10'



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 10700 SW BEAV. HILLSDALE HWY., SUITE 533
 BEAVERTON, OR 97005
 TEL: (503) 620-7182
 FAX: (503) 620-7283

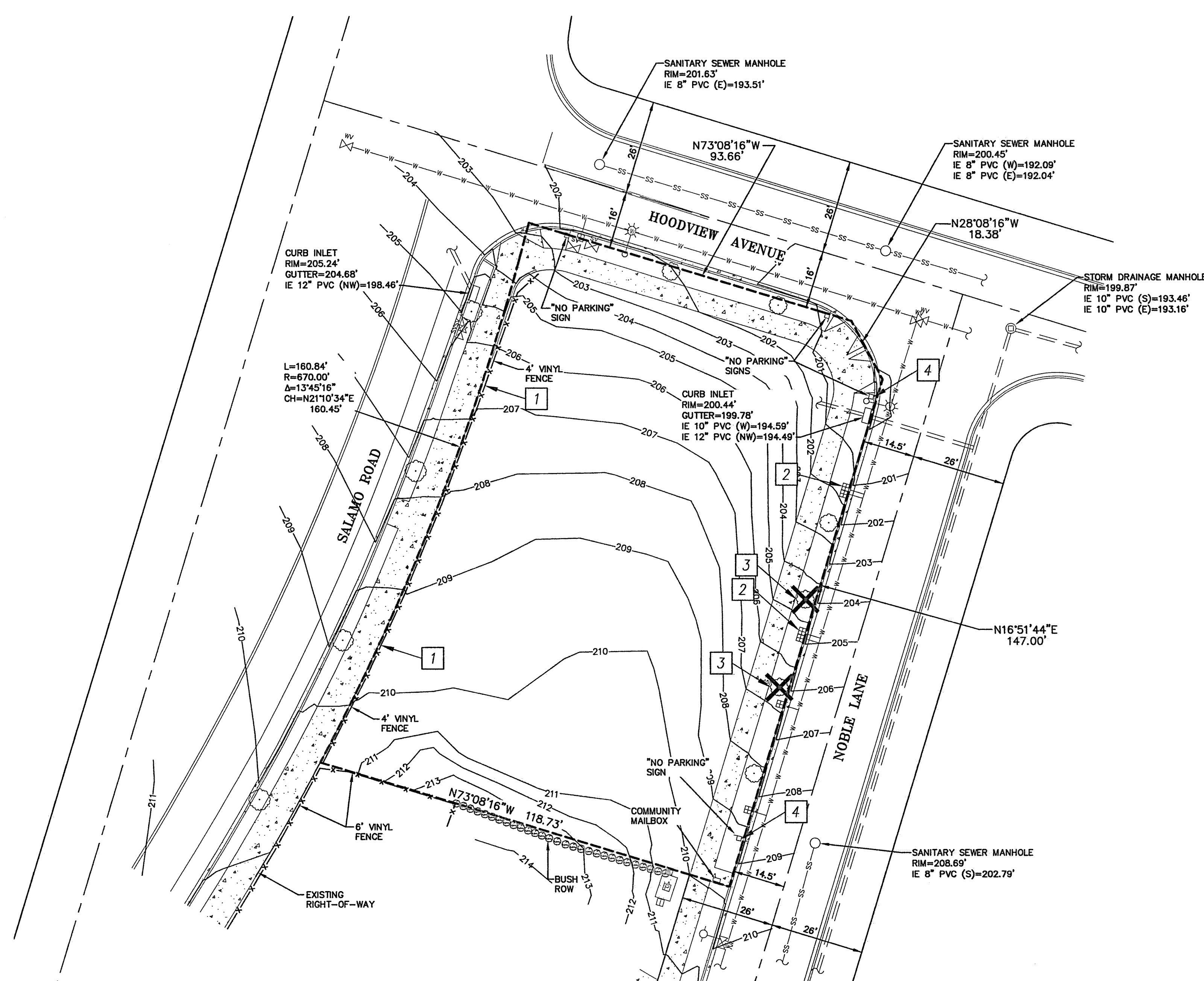
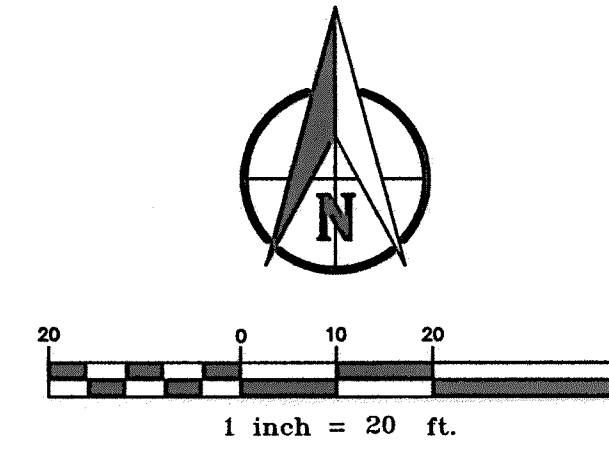
COVER SHEET
 HOODVIEW SUMMIT
 ALI LADHA

9334 SE HUNTERS BLUFF AVE. (503) 788-4401



DATE: 7/13/2018
 DRAWN BY: WJK
 PROJ. MGR: WJK
 CHECKED BY: WJK
 PROJECT NUMBER
 BRT.001
 CASE FILE NUMBER

SHEET NUMBER
 1
 OF 7



EXISTING CONDITIONS / DEMO PLAN
1"=20'

LEGEND

- 101--- - EXISTING 1' CONTOUR
- 105--- - EXISTING 5' CONTOUR
- TREE TO BE REMOVED
- CONIFEROUS TREE
- DECIDUOUS TREE
- EXISTING GRAVEL
- EXISTING ASPHALT
- EXISTING CONCRETE
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- SANITARY MANHOLE
- POWER METER
- LIGHT POLE
- STORM MANHOLE
- WATER METER

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- 2 EXISTING WATER METER TO BE ABANDONED
- 3 EXISTING STREET TREE TO BE REMOVED
- 4 NO PARKING SIGN TO BE REMOVED



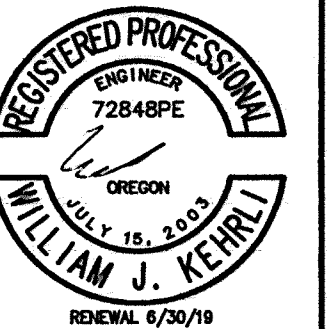
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TEL: (503) 620-7182
FAX: (503) 620-7283

EXISTING CONDITIONS / DEMO PLAN

HOODVIEW SUMMIT

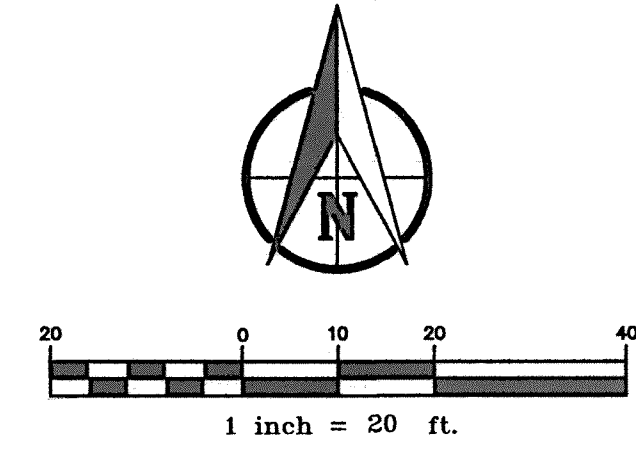
ALI LADHA

9334 SE HUNTERS BLUFF AVE. (503) 788-4401



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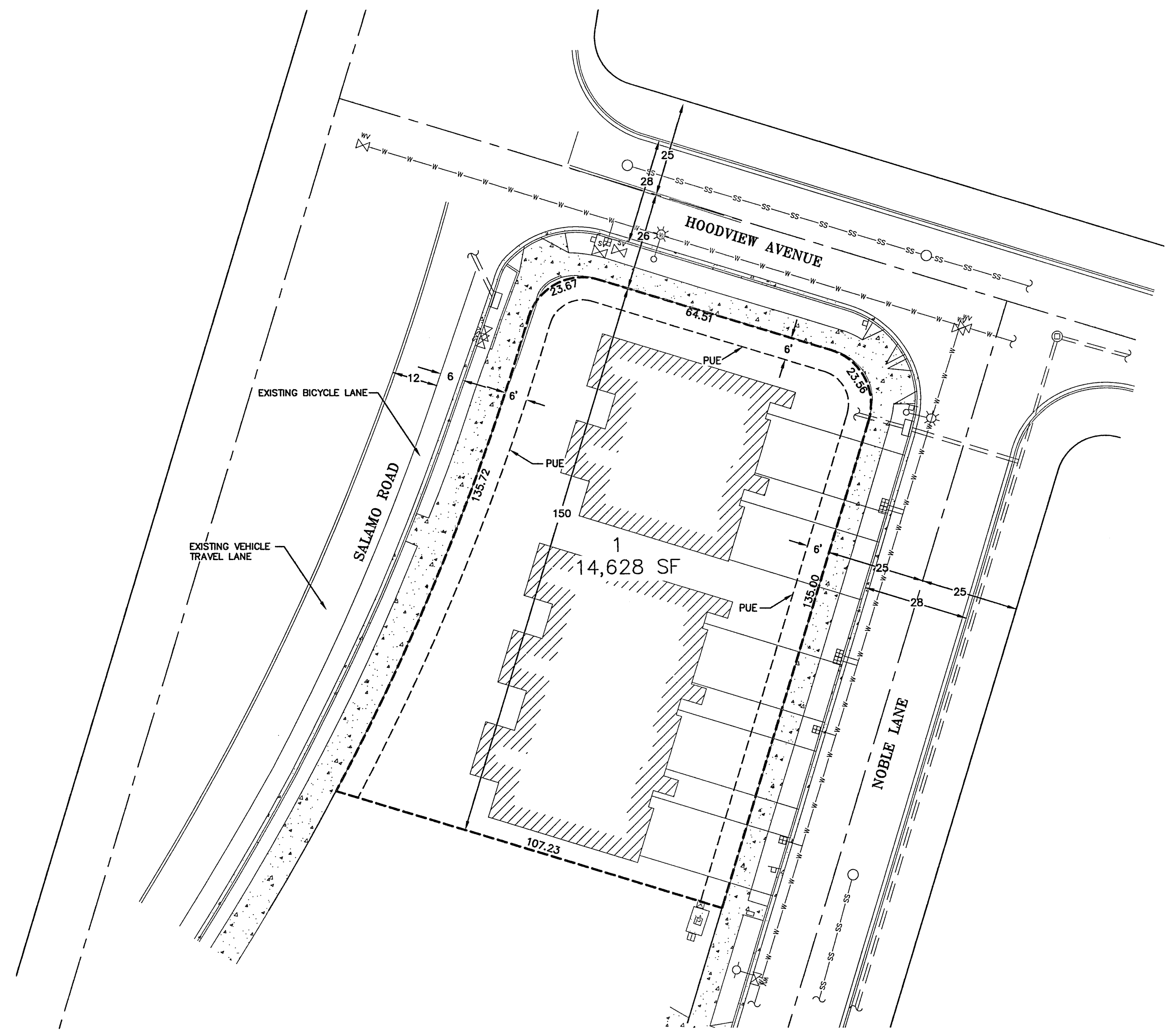


LEGEND

- PROPOSED SIDEWALK
- PROPOSED CURB
- SAWCUT LINE
- EXISTING PAVEMENT
- PROPOSED PAVEMENT

UTILITY LEGEND

- PROPOSED STORM LINE & MANHOLE
- PROPOSED STORM LINE & CLEANOUT
- PROPOSED SANITARY LINE & CLEANOUT
- PROPOSED SANITARY LINE & MANHOLE
- PROPOSED WATERLINE & VALVE
- AREA / DITCH INLET CATCH BASIN
- CG-30 STYLE CATCH BASIN

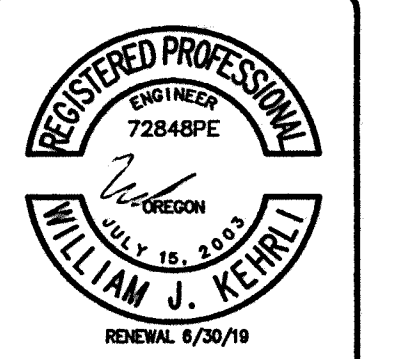


TENTATIVE PLAN
1"=20'



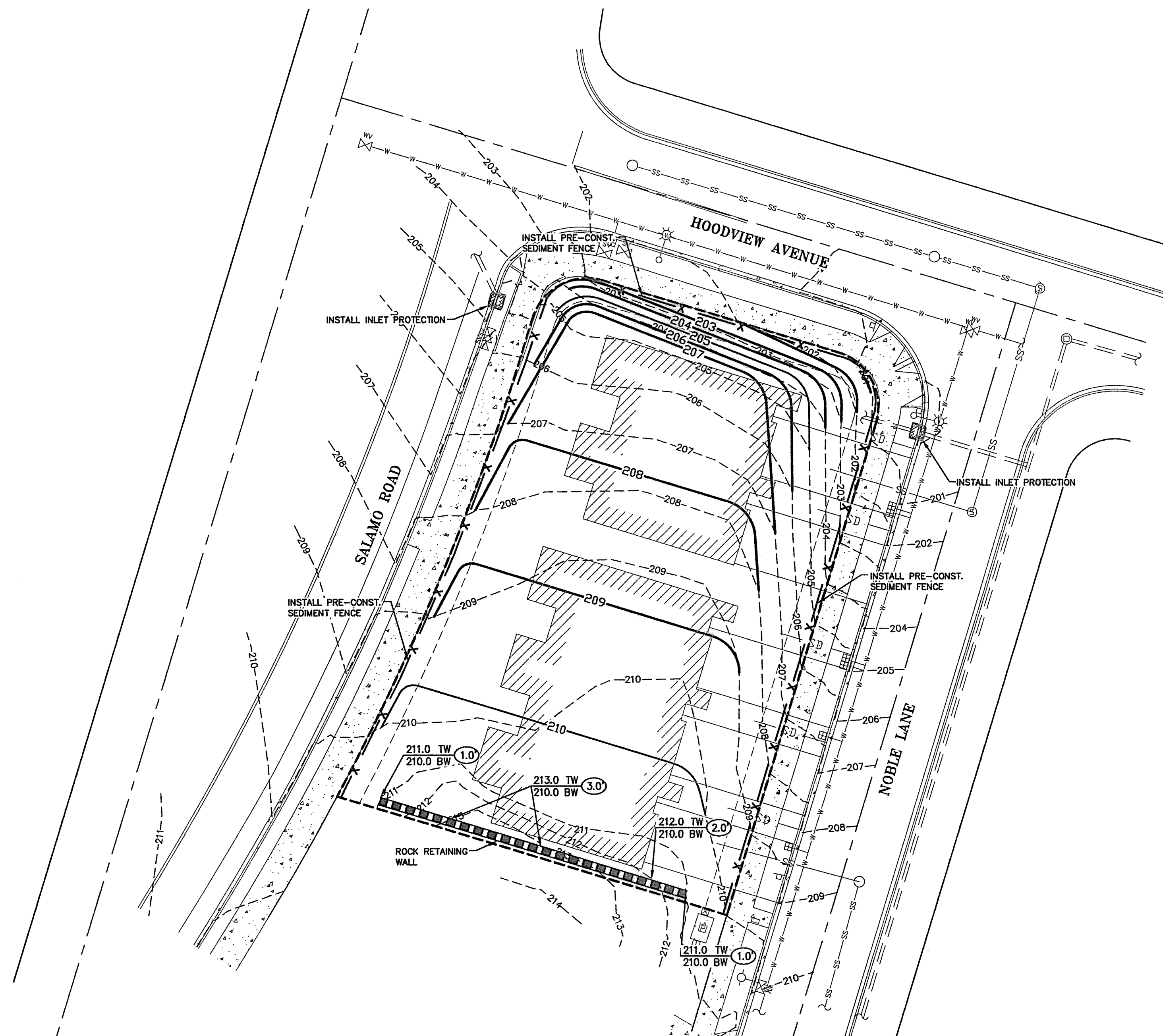
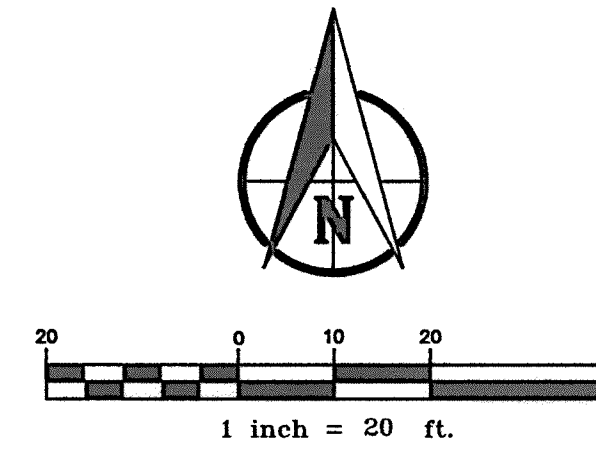
CWK2 LAND DEVELOPMENT CONSULTANTS
10700 SW BEAV. HILLSDALE HWY., SUITE 533
BEAVERTON, OR 97005
TEL: (503) 620-7182
FAX: (503) 620-7283

TENTATIVE PLAN
HOODVIEW SUMMIT
ALI LADHA
9334 SE HUNTERS BLUFF AVE. (503) 788-4401



DATE: 7/13/2018
DRAWN BY: WJK
PROJ. MGR: WJK
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PROJECT NUMBER
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3
OF 7



GRADING / EROSION CONTROL PLAN
1"=20'

GRADING & EROSION CONTROL LEGEND

	BIOFILTER BAG TYPE INLET PROTECTION		EXISTING 1' CONTOUR
	DIRECTION OF STORMWATER FLOW		EXISTING 5' CONTOUR
	PROPOSED PRE-CONSTRUCTION SEDIMENT FENCE		PROPOSED 1' CONTOUR
	ROCK RETAINING WALL		PROPOSED 5' CONTOUR
	TOP OF WALL		
	BOTTOM OF WALL - ELEVATION AT TOP OF FOOTING		

GRADING, STREET AND UTILITY EROSION AND SEDIMENT CONTROL NOTES:

1. SEED USED FOR TEMPORARY OR PERMANENT SEEDING SHALL BE COMPOSED OF ONE OF THE FOLLOWING MIXTURES, UNLESS OTHERWISE AUTHORIZED:
 - A. DWARF GRASS MIX (MIN. 100 LB./AC.)
 1. DWARF PERENNIAL RYEGRASS (80 % BY WEIGHT)
 2. CREEPING RED FESCUE (20% BY WEIGHT)
 - B. STANDARD HEIGHT GRASS MIX (MIN. 100LB./AC.)
 1. ANNUAL RYEGRASS (40% BY WEIGHT)
 2. TURF-TYPE FESCUE (60% BY WEIGHT)
2. SLOPE TO RECEIVE TEMPORARY OR PERMANENT SEEDING SHALL HAVE THE SURFACE ROUGHENED BY MEANS OF TRACK-WALKING OR THE USE OF OTHER APPROVED IMPLEMENTS. SURFACE ROUGHENING IMPROVES SEED BEDDING AND REDUCES RUN-OFF VELOCITY.
3. LONG TERM SLOPE STABILIZATION MEASURES SHALL INCLUDE THE ESTABLISHMENT OF PERMANENT VEGETATIVE COVER VIA SEEDING WITH APPROVED MIX AND APPLICATION RATE.
4. TEMPORARY SLOPE STABILIZATION MEASURES SHALL INCLUDE: COVERING EXPOSED SOIL WITH PLASTIC SHEETING, STRAW MULCHING, OR OTHER APPROVED MEASURES.
5. STOCKPILED SOIL OR STRIPPINGS SHALL BE PLACED IN A STABLE LOCATION AND CONFIGURATION. DURING "WET WEATHER" PERIODS, STOCKPILES SHALL BE COVERED WITH PLASTIC SHEETING OR STRAW MULCH. SEDIMENT FENCE IS REQUIRED AROUND THE PERIMETER OF THE STOCKPILE.
6. EXPOSED CUT OR FILL AREAS SHALL BE STABILIZED THROUGH THE USE OF TEMPORARY SEEDING AND MULCHING, EROSION CONTROL BLANKETS OR MATS, MID-SLOPE SEDIMENT FENCES OR WATTLES, OR OTHER APPROPRIATE MEASURES. SLOPES EXCEEDING 25% MAY REQUIRE ADDITIONAL EROSION CONTROL MEASURES.
7. AREAS SUBJECT TO WIND EROSION SHALL USE APPROPRIATE DUST CONTROL MEASURES INCLUDING THE APPLICATION OF A FINE SPRAY OF WATER, PLASTIC SHEETING, STRAW MULCHING, OR OTHER APPROVED MEASURES.
8. CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES INCLUDING, BUT NOT LIMITED TO, TIRE WASHES, STREET SWEEPING, AND VACUUMING MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
9. ACTIVE INLETS TO STORM WATER SYSTEMS SHALL BE PROTECTED THROUGH THE USE OF APPROVED INLET PROTECTION MEASURES. ALL INLET PROTECTION MEASURES ARE TO BE REGULARLY INSPECTED AND MAINTAINED AS NEEDED.
10. SATURATED MATERIALS THAT ARE HAULED OFF-SITE MUST BE TRANSPORTED IN WATER-TIGHT TRUCKS TO ELIMINATE SPILLAGE OF SEDIMENT AND SEDIMENT-LADEN WATER.
11. AN AREA SHALL BE PROVIDED FOR THE WASHING OUT OF CONCRETE TRUCKS IN A LOCATION THAT DOES NOT PROVIDE RUN-OFF THAT CAN ENTER THE STORM WATER SYSTEM. IF THE CONCRETE WASH-OUT AREA CAN NOT BE CONSTRUCTED GREATER THAN 50' FROM ANY DISCHARGE POINT, SECONDARY MEASURES SUCH AS BERMS OR TEMPORARY SETTLING PITS MAY BE REQUIRED. THE WASH-OUT SHALL BE LOCATED WITHIN SIX FEET OF TRUCK ACCESS AND BE CLEANED WHEN IT REACHES 50% OF THE CAPACITY.
12. SWEEPINGS FROM EXPOSED AGGREGATE CONCRETE SHALL NOT BE TRANSFERRED TO THE STORM WATER SYSTEM. SWEEPINGS SHALL BE PICKED UP AND DISPOSED IN THE TRASH.
13. AVOID PAVING IN WET WEATHER WHEN PAVING CHEMICALS CAN RUN-OFF INTO THE STORM WATER SYSTEM.
14. USE BMPs SUCH AS CHECK-DAMS, BERMS, AND INLET PROTECTION TO PREVENT RUN-OFF FROM REACHING DISCHARGE POINTS.
15. COVER CATCH BASINS, MANHOLES, AND OTHER DISCHARGE POINTS WHEN APPLYING SEAL COAT, TACK COAT, ETC. TO PREVENT INTRODUCING THESE MATERIALS TO THE STORM WATER SYSTEM.

EROSION AND SEDIMENT CONTROL BMP IMPLEMENTATION:

1. ALL BASE ESC MEASURES (INLET PROTECTION, PERIMETER SEDIMENT CONTROL, GRAVEL CONSTRUCTION ENTRANCES, ETC.) MUST BE IN PLACE, FUNCTIONAL, AND APPROVED IN AN INITIAL INSPECTION, PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES.
 2. ALL "SEDIMENT BARRIERS (TO BE INSTALLED AFTER GRADING)" SHALL BE INSTALLED IMMEDIATELY FOLLOWING ESTABLISHMENT OF FINISHED GRADE AS SHOWN ON THESE PLANS.
 3. LONG TERM SLOPE STABILIZATION MEASURES "INCLUDING MATTING" SHALL BE IN PLACE OVER ALL EXPOSED SOILS BY OCTOBER 1.
 4. INLET PROTECTION SHALL BE IN-PLACE IMMEDIATELY FOLLOWING PAVING ACTIVITIES.
- THESE EROSION AND SEDIMENT CONTROL PLANS ASSUME "DRY WEATHER" CONSTRUCTION. "WET WEATHER" CONSTRUCTION MEASURES NEED TO BE APPLIED BETWEEN OCTOBER 1ST AND MAY 31ST.



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GRADING / EROSION CONTROL PLAN

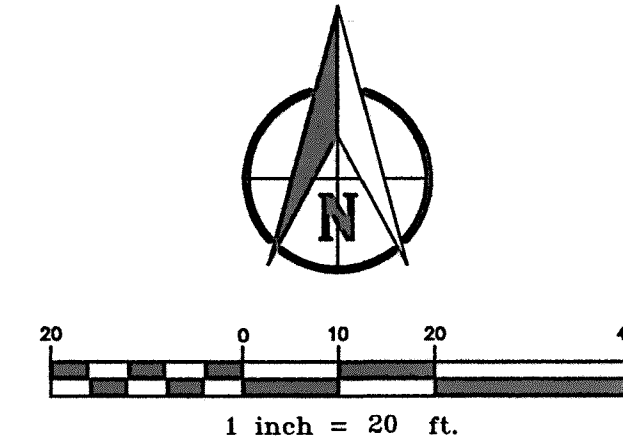
HOODVIEW SUMMIT
ALI LADHA

9334 SE HUNTERS BLUFF AVE. (503) 788-4401



DATE: 7/13/2018
DRAWN BY: WJK
PROJ. MGR: WJK
CHECKED BY: WJK
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SHEET NUMBER
4
OF 7

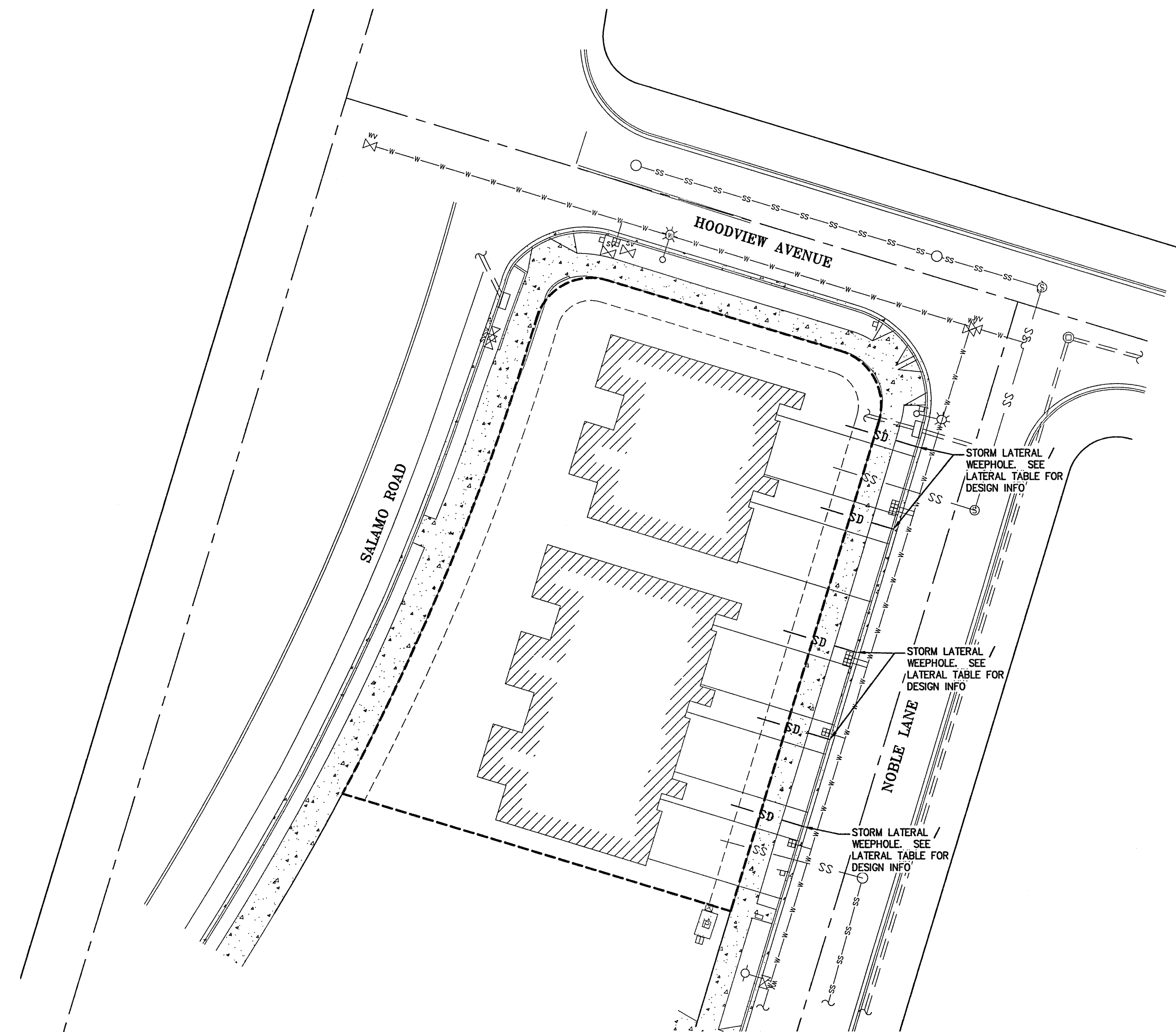


LEGEND

- PROPOSED SIDEWALK
- PROPOSED CURB
- SAWCUT LINE
- EXISTING PAVEMENT
- PROPOSED PAVEMENT

UTILITY LEGEND

- PROPOSED STORM LINE & MANHOLE
- PROPOSED STORM LINE & CLEANOUT
- PROPOSED SANITARY LINE & CLEANOUT
- PROPOSED SANITARY LINE & MANHOLE
- PROPOSED WATERLINE & VALVE
- AREA / DITCH INLET CATCH BASIN
- CG-30 STYLE CATCH BASIN



STORM SEWER PLAN
1"=20'

STORM LATERAL DATA								
LAT NO.	STREET STATION	LENGTH	SIZE	MATERIAL	INV. EL. CURB	INV. EL. PLUG	SLOPE	DEPTH PLUG
1	1+44.43	19.80'	4"	ASTM C-900	200.39	200.79	0.0200	2.2'
2	1+66.98	19.91'	4"	ASTM C-900	202.01	202.41	0.0200	2.1'
3	2+01.01	19.94'	4"	ASTM C-900	204.46	204.86	0.0200	2.1'
4	2+25.01	19.95'	4"	ASTM C-900	206.18	206.58	0.0200	1.9'
5	2+49.01	19.98'	4"	ASTM C-900	207.64	208.04	0.0200	1.5'

BACKFILL NOTE:
PIPES UNDER PAVED SURFACES REQUIRE GRANULAR BACKFILL. FOR PIPES OUTSIDE PAVEMENT, NATIVE BACKFILL IS PERMITTED, UNLESS OTHERWISE NOTED.

4" LATERALS ARE TO BE 4" PVC WITH A MINIMUM SLOPE OF 2% UNLESS OTHERWISE NOTED.

NOTE: ALL 2"x 4" STORM SERVICE CONNECTION MARKERS TO BE COLOR CODED WHITE

THE CONTRACTOR SHALL FIELD VERIFY THE SIZE, LOCATION & DEPTH OF EXISTING UTILITIES PRIOR TO CONSTRUCTION

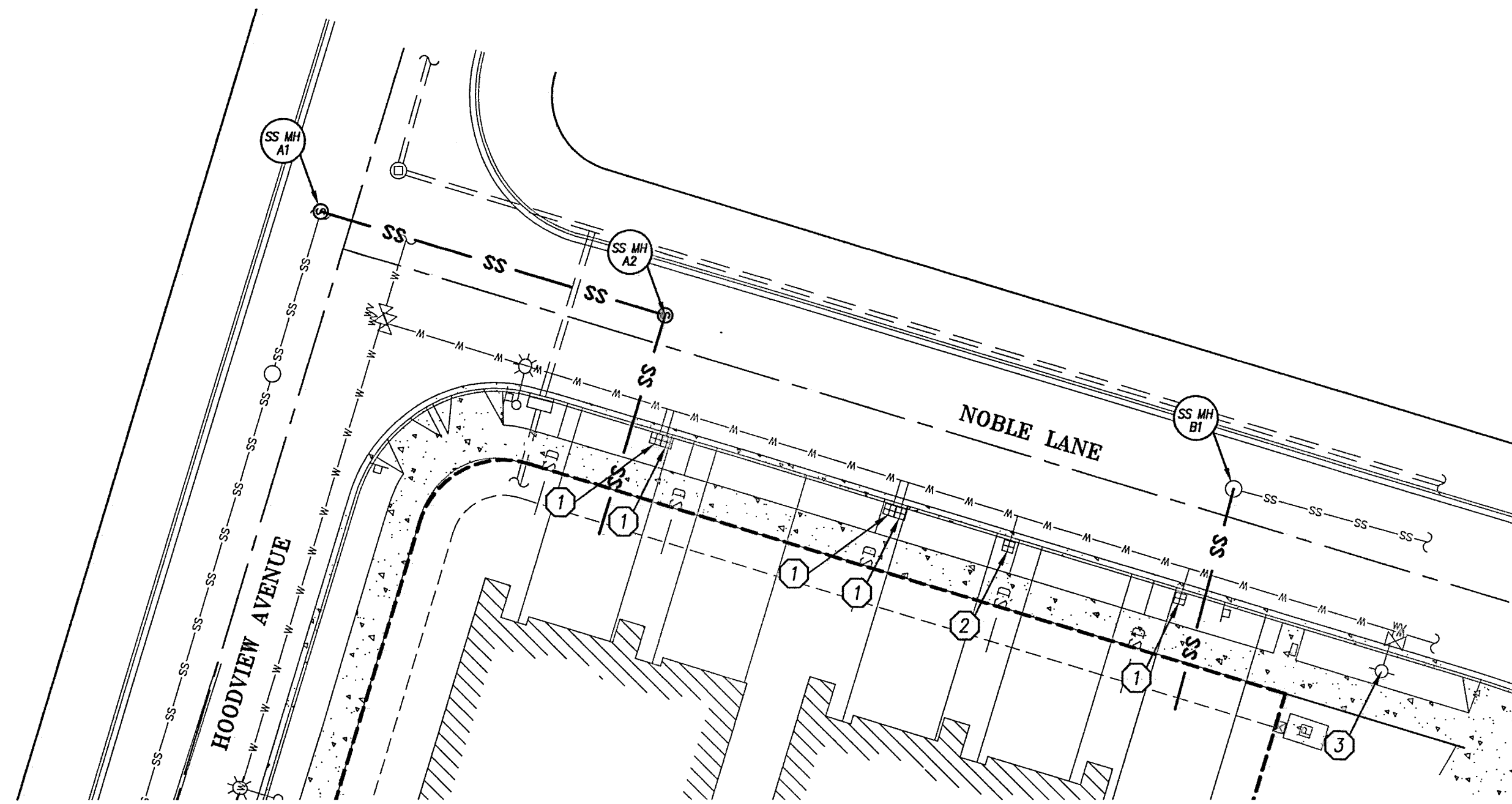
STORMWATER MANAGEMENT
WATER QUALITY WILL BE MANAGED VIA THE EXISTING STORM WATER FACILITY WHICH WAS CONSTRUCTED FOR THIS NEIGHBORHOOD AT THE SOUTH END OF VIEWPOINT DRIVE.

CWK2 LAND DEVELOPMENT CONSULTANTS
10700 SW BEAV. HILLSDALE HWY., SUITE 533
BEAVERTON, OR 97005
TEL: (503) 620-7182
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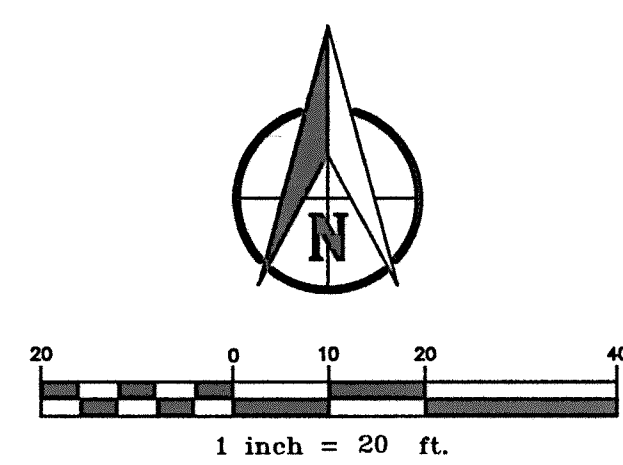
STORM SEWER PLAN
HOODVIEW SUMMIT
ALI LADHA
9334 SE HUNTERS BLUFF AVE. (503) 788-4401

DATE: 7/13/2018
DRAWN BY: WJK
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CHECKED BY: WJK
PROJECT NUMBER
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SHEET NUMBER
5
OF
7



SANITARY SEWER / WATER PLAN
1"=20'



LEGEND

- PROPOSED SIDEWALK
- PROPOSED CURB
- SAWCUT LINE
- EXISTING PAVEMENT
- PROPOSED PAVEMENT

UTILITY LEGEND

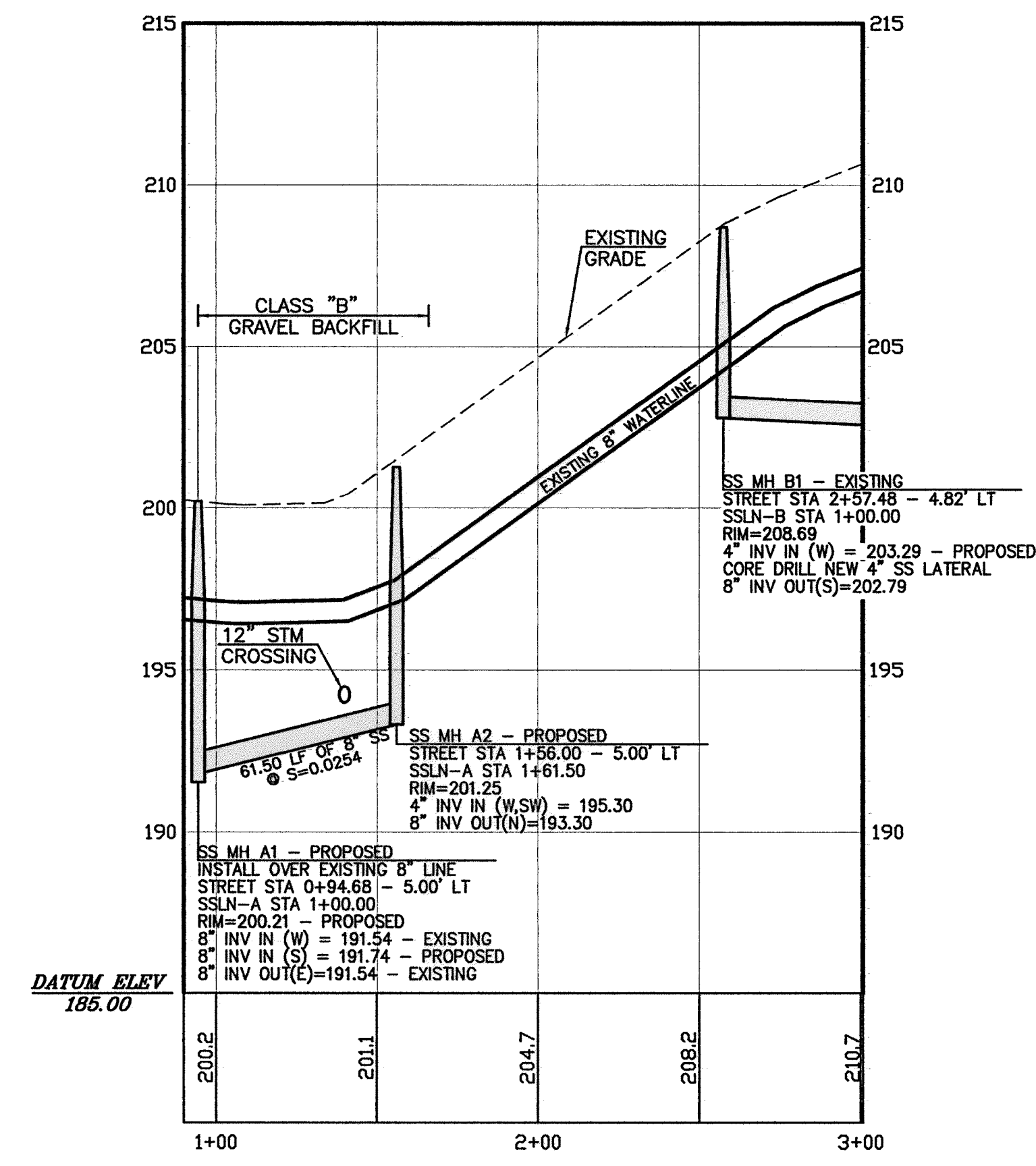
- PROPOSED STORM LINE & MANHOLE
- PROPOSED STORM LINE & CLEANOUT
- PROPOSED SANITARY LINE & CLEANOUT
- PROPOSED SANITARY LINE & MANHOLE
- PROPOSED WATERLINE & VALVE
- AREA / DITCH INLET CATCH BASIN
- CG-30 STYLE CATCH BASIN

SANITARY LATERAL DATA									
BLDG NO.	D.S. M.H.	DIST. FROM D.S. MH	LENGTH	SIZE	MATERIAL	INV. EL. @ MANHOLE	INV. EL. @ PLUG	SLOPE	DEPTH @ PLUG
1	A2	0.0'	39.00'	4"	ASTM D 3034 SDR-35	195.30	197.50	0.0564	6.0'
2	B1	0.0'	38.84'	4"	ASTM D 3034 SDR-35	203.29	204.07	0.0200	5.9'

NOTE: 8"x4" TAPS TO EXISTING MAINLINE SHALL BE ASTM D 3034 SADDLE TEES

WATER LINE NOTES

- ① EXISTING 3/4" DOMESTIC WATER METER TO REMAIN
- ② EXISTING 3/4" DOMESTIC WATER METER TO BE ABANDONED
- ③ EXISTING FIRE HYDRANT TO REMAIN IN PLACE



SANITARY SEWER / WATER PLAN
1"=40' HORIZONTAL
1"=4' VERTICAL

BACKFILL NOTE:
PIPES UNDER PAVED SURFACES REQUIRE GRANULAR BACKFILL. FOR PIPES OUTSIDE PAVEMENT, NATIVE BACKFILL IS PERMITTED, UNLESS OTHERWISE NOTED.

UNLESS OTHERWISE NOTED, ALL LATERALS ARE TO BE 4" PVC WITH A MINIMUM SLOPE OF 2% AND LATERAL CONNECTIONS TO MAIN

NOTE: ALL 2"x 4" SANITARY SERVICE CONNECTION MARKERS TO BE COLOR CODED GREEN

THE CONTRACTOR SHALL FIELD VERIFY THE SIZE, LOCATION & DEPTH OF EXISTING UTILITIES PRIOR TO CONSTRUCTION

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FAX: (503) 620-7283

SANITARY SEWER AND WATER
PLAN AND PROFILE
HOODVIEW SUMMIT
ALI LADHA
9334 SE HUNTERS BLUFF AVE.
(503) 788-4401

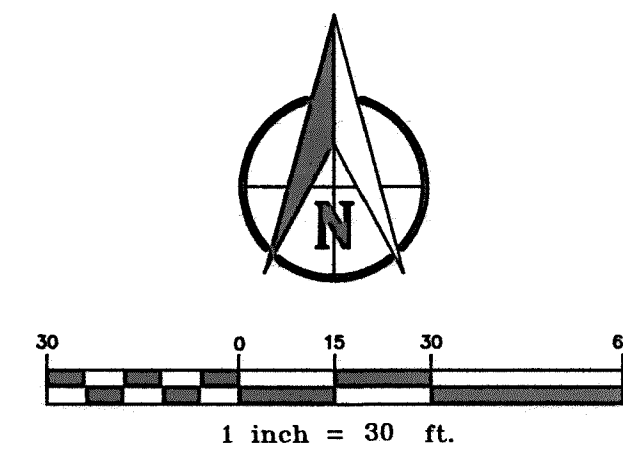
REGISTERED PROFESSIONAL ENGINEER
72848PE
WILLIAM J. KEHRLI
RENEWAL 6/30/19

DATE: 7/13/2018
DRAWN BY: WJK
PROJ. MGR: WJK
CHECKED BY: WJK
PROJECT NUMBER
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OF 7



NEIGHBORING PROPERTIES / ZONING MAP
1'-30'



LEGEND

- PROPOSED SIDEWALK
- PROPOSED CURB
- SAWCUT LINE
- EXISTING PAVEMENT
- PROPOSED PAVEMENT

UTILITY LEGEND

- PROPOSED STORM LINE & MANHOLE
- PROPOSED STORM LINE & CLEANOUT
- PROPOSED SANITARY LINE & CLEANOUT
- PROPOSED SANITARY LINE & MANHOLE
- PROPOSED WATERLINE & VALVE
- AREA / DITCH INLET CATCH BASIN
- CG-30 STYLE CATCH BASIN

CWK2
LAND DEVELOPMENT CONSULTANTS

CWK2 LAND DEVELOPMENT CONSULTANTS
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BEAVERTON, OR 97005
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FAX: (503) 620-7283

NEIGHBORING PROPERTIES / ZONING MAP

HOODVIEW SUMMIT
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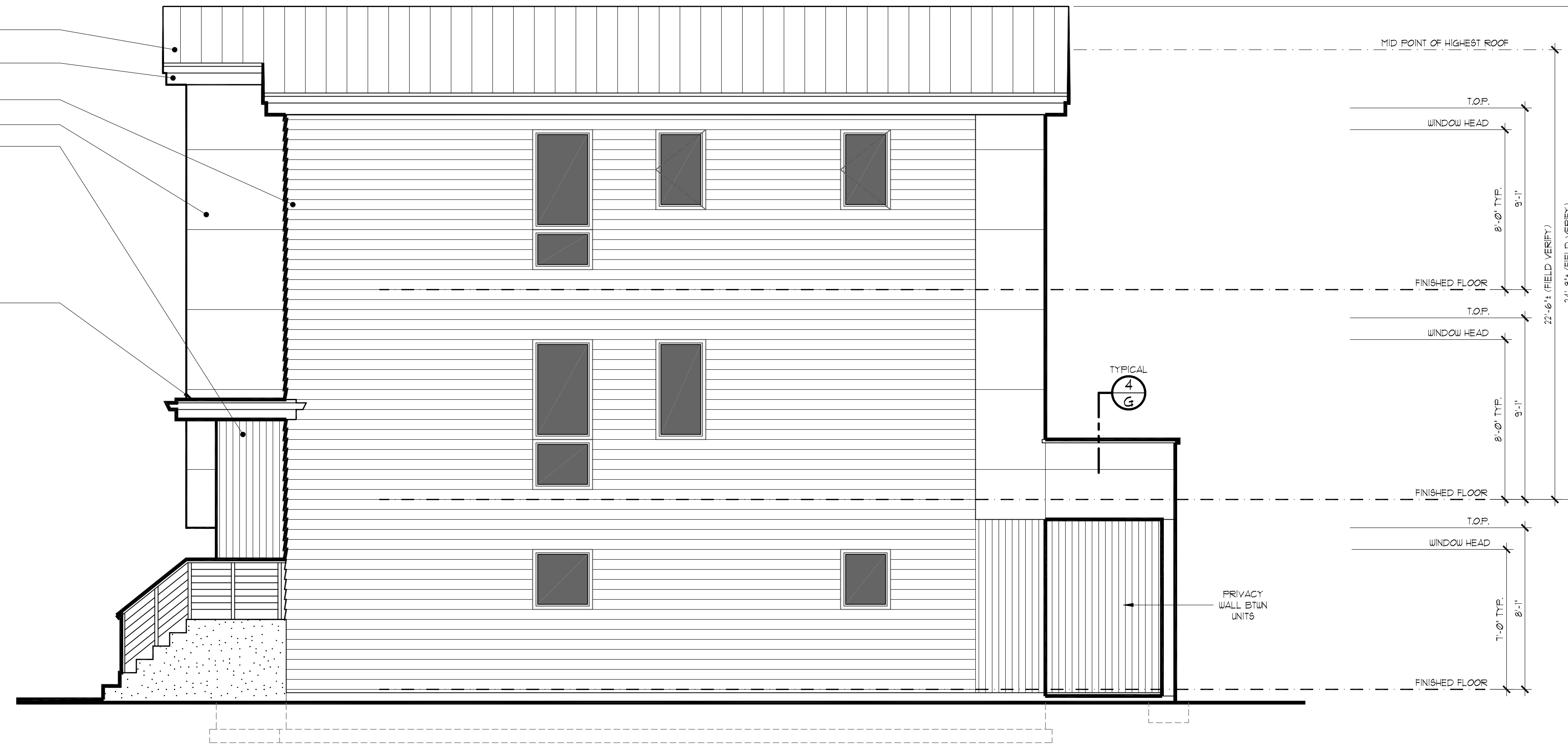
REGISTERED PROFESSIONAL
ENGINEER
72848PE
William J. Kehrl
WILLIAM J. KEHRL
RENEWAL 6/30/19

DATE: 7/13/2018
DRAWN BY: WJK
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OF
7

STANDING SEAM METAL ROOFING (SEE ROOF PLAN)
 G. I. GUTTER ON 2 X 8 FASCIA C/W DOWNSPOUTS (SEE ROOF PLAN)
 HORIZONTAL LAP SIDING
 PAINTED FIBER CEMENT PANELS
 VERTICAL SIDING
 THE TYPE OF EXTERIOR FINISH, THE INSTALLATION AND THE WATERPROOFING DETAILS ARE ALL TO BE THE FULL RESPONSIBILITY OF THE OWNER/BLDR. THIS DESIGNER ASSUMES NO RESPONSIBILITY FOR THE INTEGRITY OF THE BLDG ENVELOPE

WATERPROOF MEMBRANE TYPE ROOFING (ex. 'EPDM') SYSTEM OR EQUAL. INSTALL PER MANUF. SPECS 1/2" RATED SHING INDEX 32/16 ON 2 X SLEEPERS SLOPED 1/4" FT. MIN OVER ENG RETRS OR 2 X FRNG MEMBS (SEE PLANS)
 (SEE SHT G FOR INSL VALUES, IF UNVENTED, SEE (UNVENTED INSL OPT) (TYP. @ FLAT ROOF PORTION)



RIGHT ELEVATION

SCALE: 1/4" = 1'-0"

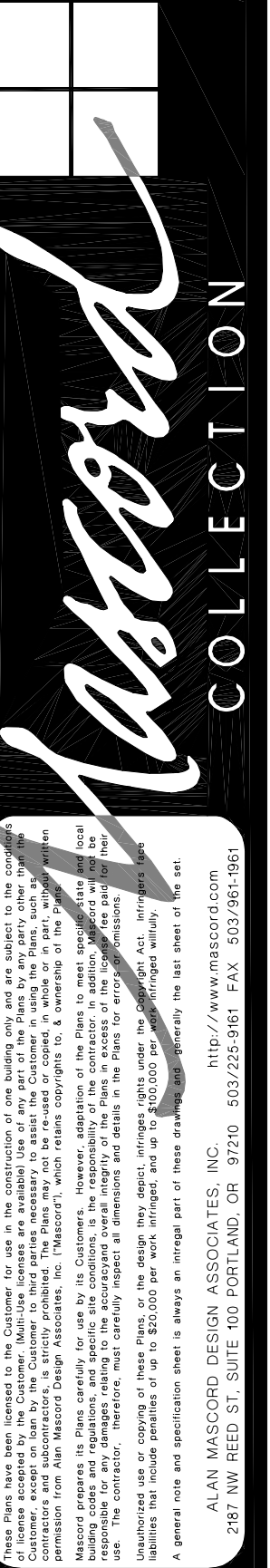


FRONT ELEVATION

SCALE: 1/4" = 1'-0"

THE CONTRACTOR ASSUMES FULL RESPONSIBILITY FOR THE CORRECT INSTALLATION OF ALL EXTERIOR FINISHES AND WEATHERPROOFING.

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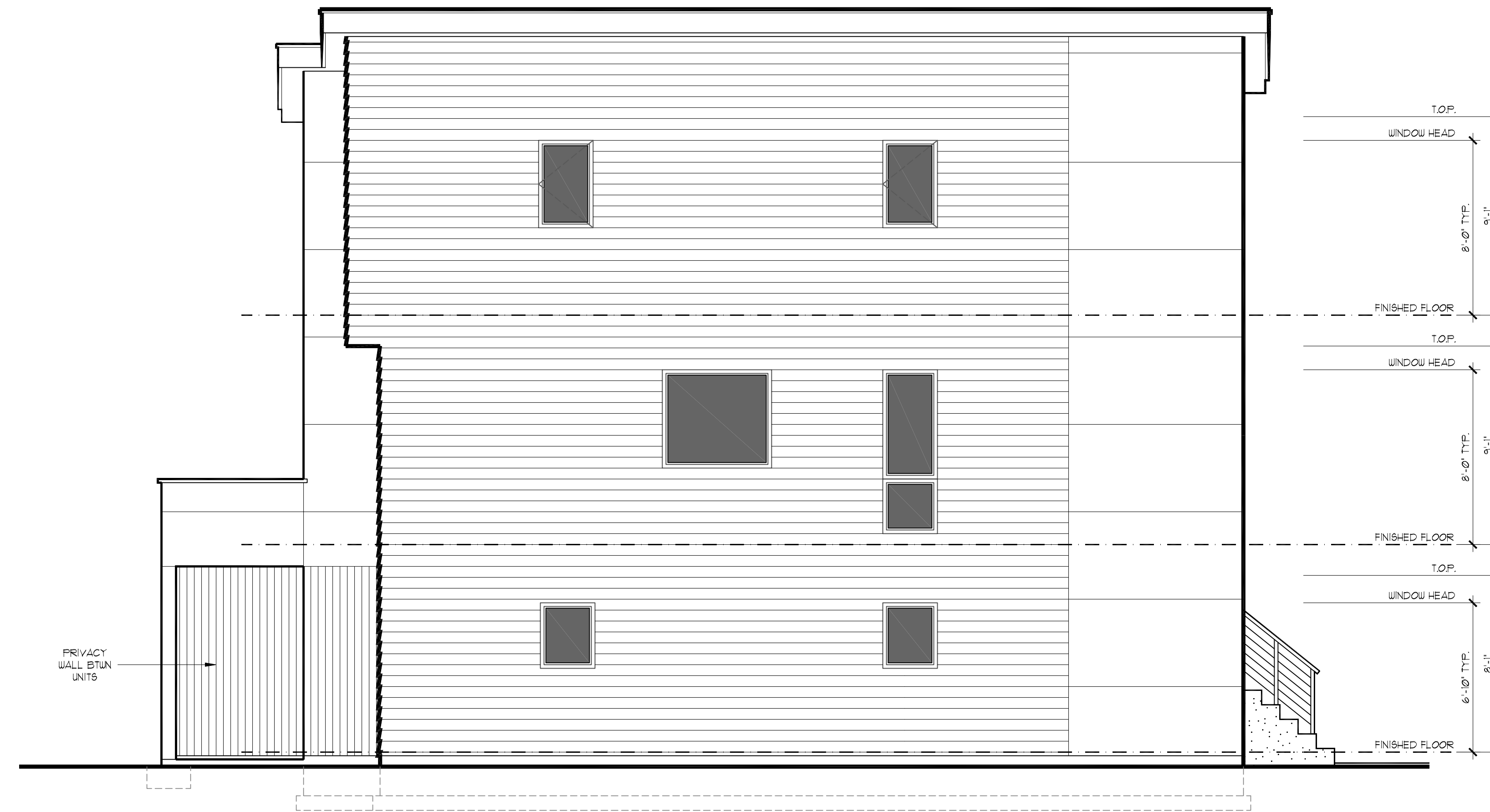
INVESTCO-DUPLEX-1 PROJECT MANAGERS DRAWN 02/02/18 LAW

RESIDENCE FOR
INVESTCO REALTY GROUP
 HOODVIEW ZONING SUBDIVISION
 WCT-LIND-OR-EGON

25# SNOW LOAD

UNITS A & C	878 SQ. FT.
UPPER FLOOR	854 SQ. FT.
MAIN FLOOR	286 SQ. FT.
LOWER FLOOR	286 SQ. FT.
TOTAL AREA	2020 SQ. FT.
GARAGE AREA	488 SQ. FT.





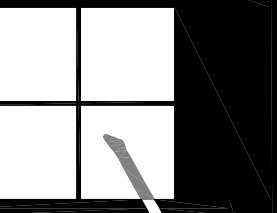
LEFT ELEVATION

SCALE: 1/4" = 1'-0"



REAR ELEVATION

SCALE: 1/4" = 1'-0"



Mascord
COLLECTION

ALAN MASCORD DESIGN ASSOCIATES, INC.
1116 S. MAIN ST. PORTLAND, OR 97204
TEL: 503.225.9051 FAX: 503.981.1851

INVESTCO-DUPLEX-2
PROJECT MANAGER/AS
DRAWN 02/02/18 LAW

RESIDENCE FOR
INVESTCO REALTY GROUP
HOODVIEW ZONING SUBDIVISION
WETLAND REGION

25# SNOW LOAD

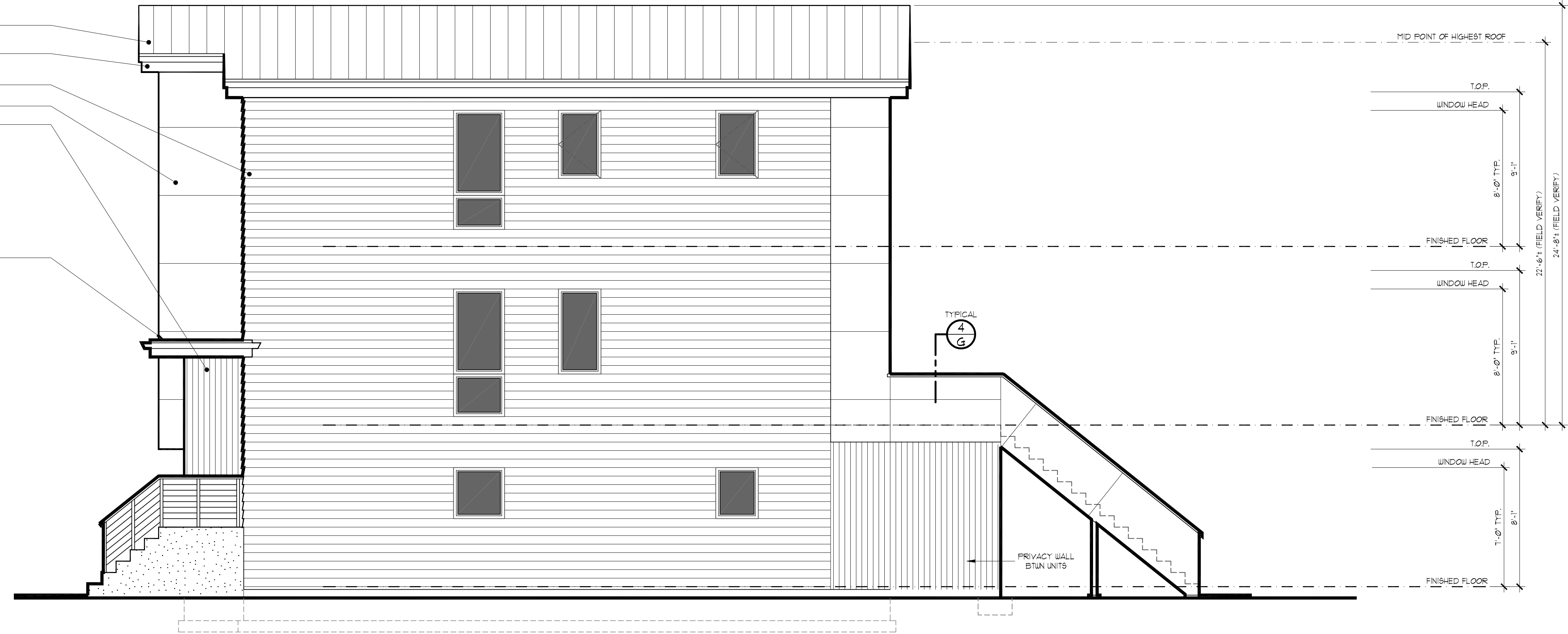
UNITS A & C	878 SQ. FT.
UPPER FLOOR	854 SQ. FT.
MAIN FLOOR	286 SQ. FT.
LOWER FLOOR	286 SQ. FT.
TOTAL AREA	2020 SQ. FT.
GARAGE AREA	1,583 SQ. FT.

INVESTCO

2

THE CONTRACTOR ASSUMES FULL RESPONSIBILITY FOR THE CORRECT INSTALLATION OF ALL EXTERIOR FINISHES AND WEATHERPROOFING.

STANDING SEAM METAL ROOFING (SEE ROOF PLAN)
 G. I. GUTTER ON 2 X 8 FASCIA C/W DOWNSPOUTS (SEE ROOF PLAN)
 HORIZONTAL LAP SIDING
 PAINTED FIBER CEMENT PANELS
 VERTICAL SIDING
 THE TYPE OF EXTERIOR FINISH, THE INSTALLATION AND THE WATERPROOFING DETAILS ARE ALL TO BE THE FULL RESPONSIBILITY OF THE OWNER/BLDG. THIS DESIGNER ASSUMES NO RESPONSIBILITY FOR THE INTEGRITY OF THE BLDG ENVELOPE
 WATERPROOF MEMBRANE TYPE ROOFING (ex. EPDM) SYSTEM OR EQUAL, INSTALL PER MANUF. SPECS
 1/2" RATED SHING INDEX 32/16 ON 2 X SLEEPERS SLOPED 1/4" FT. MIN. OVER ENG. RETRS OR 2 X FRNG MBRG (SEE PLANS)
 (SEE SHT G FOR INSL. VALUES, IF UNVENTED, SEE (INVENTED INSL. OPT) (TYP. @ FLAT ROOF PORTION)



RIGHT ELEVATION

SCALE: 1/4" = 1'-0"

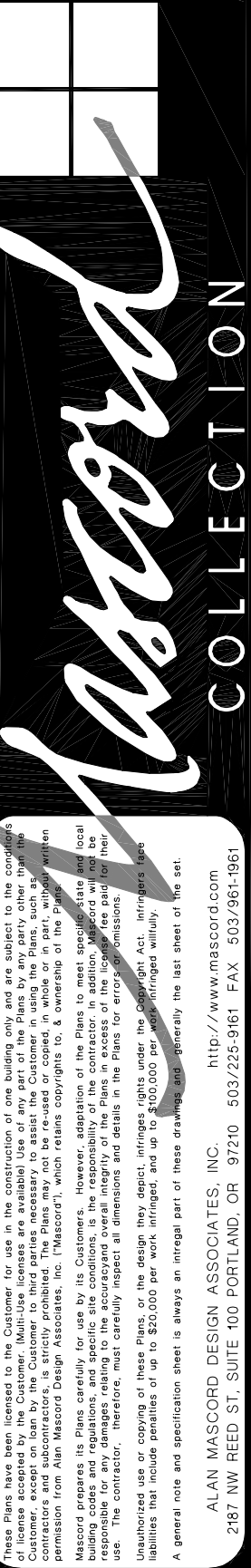


FRONT ELEVATION

SCALE: 1/4" = 1'-0"

THE CONTRACTOR ASSUMES FULL RESPONSIBILITY FOR THE CORRECT INSTALLATION OF ALL EXTERIOR FINISHES AND WEATHERPROOFING.

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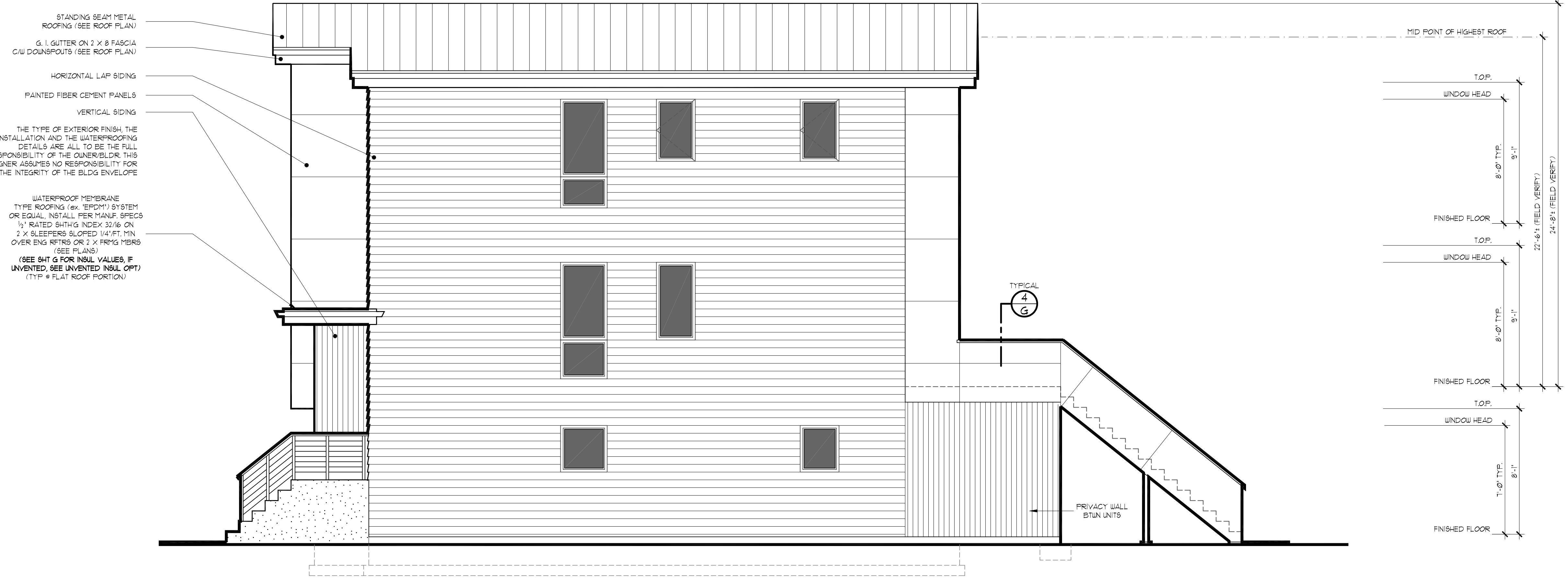
INVESTCO-DUPLEX-1
 PROJECT MANAGERS
 DRAWN 02/02/18 LAW

RESIDENCE FOR
INVESTCO REALTY GROUP
 HOODVIEW ZONING SUBDIVISION
 W.C.T. LIND - ORIGINATOR

25# SNOW LOAD

UNITS A & C	878 SQ. FT.
UPPER FLOOR	854 SQ. FT.
MAIN FLOOR	286 SQ. FT.
LOWER FLOOR	286 SQ. FT.
TOTAL AREA	2020 SQ. FT.
GARAGE AREA	488 SQ. FT.





RIGHT ELEVATION
SCALE: 1/4" = 1'-0"



FRONT ELEVATION
SCALE: 1/4" = 1'-0"

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COLLECTION

ALAN MASCORD DESIGN ASSOCIATES, INC.
1116 S. MAIN ST. PORTLAND, OR 97204
TEL: 503.225.4961 FAX: 503.981.1815

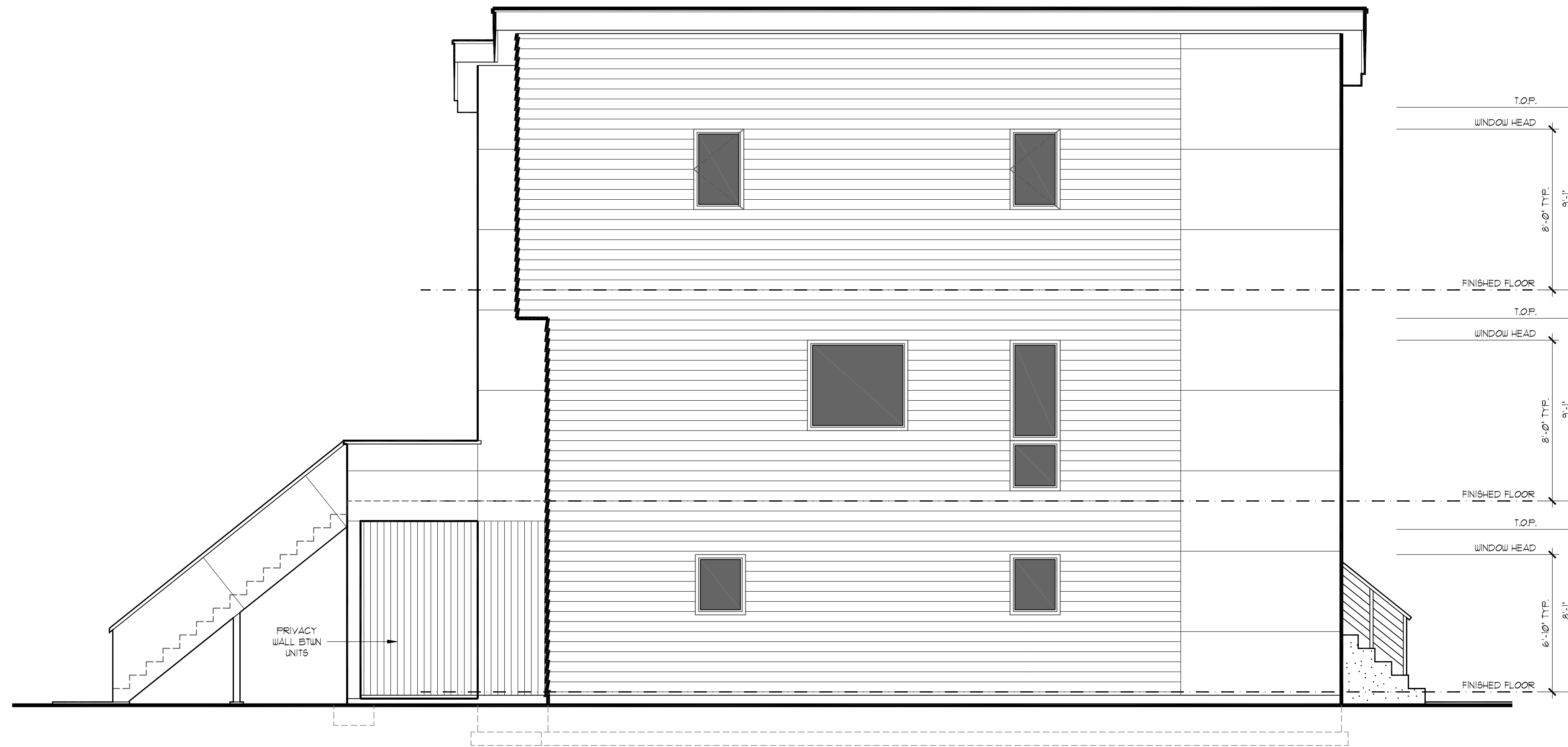
INVESTCO-TRIPLEX-1
PROJECT MANAGERS
DRAWN 02/02/18 LAW

RESIDENCES FOR
INVESTCO REALTY GROUP
HOODVIEW / JARRIT / SUBDIVISION
WEST Linn, Oregon

25# SNOW LOAD

UNITS A & C	UNITS B
UPPER FLOOR	UPPER FLOOR
878 SQ. FT.	939 SQ. FT.
MAIN FLOOR	MAIN FLOOR
854 SQ. FT.	913 SQ. FT.
LOWER FLOOR	LOWER FLOOR
286 SQ. FT.	306 SQ. FT.
TOTAL AREA	TOTAL AREA
2020 SQ. FT.	2158 SQ. FT.
GARAGE AREA	GARAGE AREA
• 683 SQ. FT.	• 622 SQ. FT.

INVESTCO
1



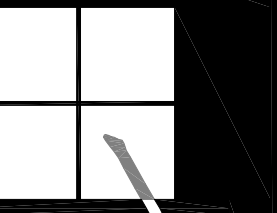
LEFT ELEVATION

SCALE : 1/4" = 1'-0"



REAR ELEVATION

SCALE : 1/4" = 1'-0"



Mascord
COLLECTION

ALAN MASCORD DESIGN ASSOCIATES, INC.
1116 S. W. HAYDEN AVENUE
PORTLAND, OREGON 97205
TEL: 503.225.9051 FAX: 503.981.1851

INVESTCO-DUPLEX-2
PROJECT MANAGER/AS
DRAWN 02/02/18 LAW

RESIDENCE FOR
INVESTCO REALTY GROUP
HOODVIEW ZONING SUBDIVISION
WETLAND REGION

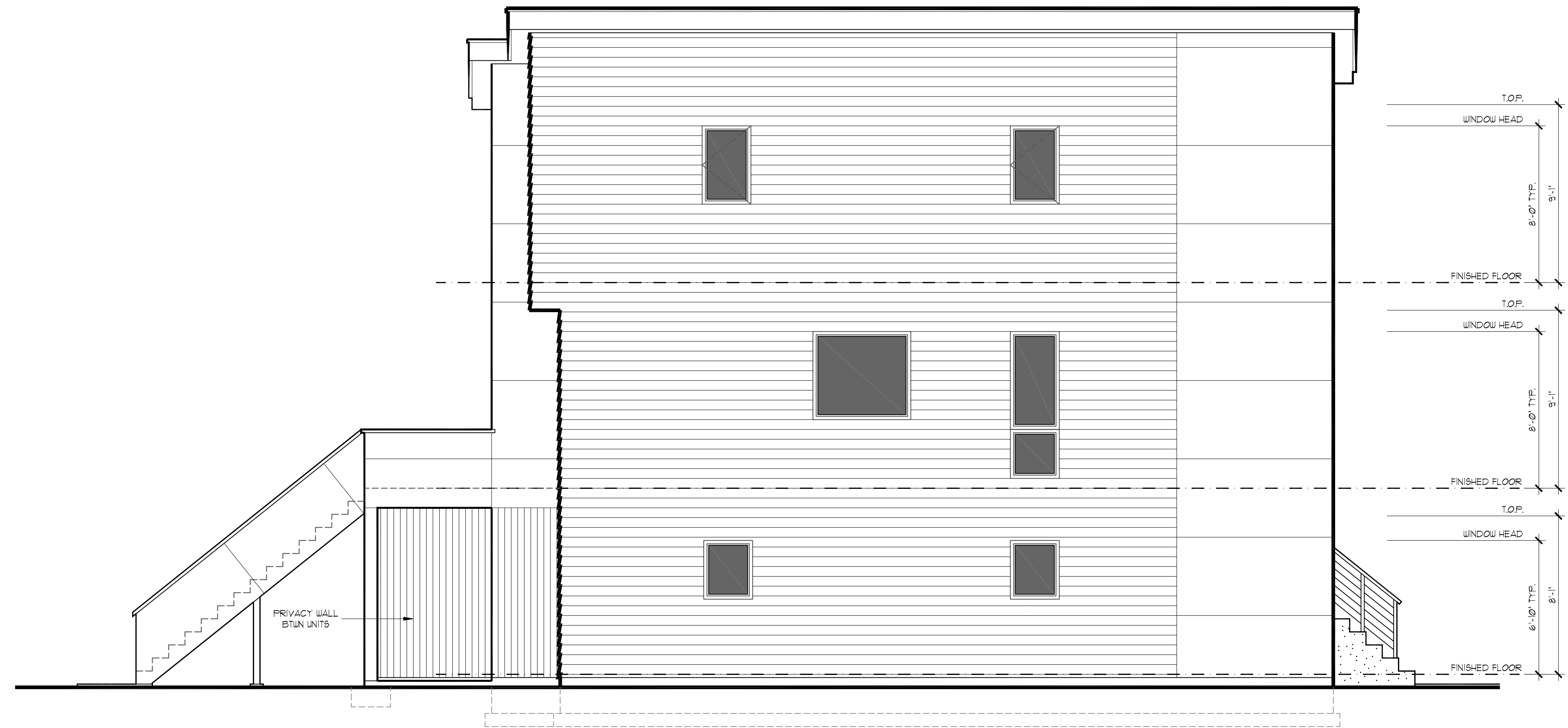
25# SNOW LOAD

UNITS A & C	878 SQ. FT.
UPPER FLOOR	854 SQ. FT.
MAIN FLOOR	286 SQ. FT.
LOWER FLOOR	286 SQ. FT.
TOTAL AREA	2020 SQ. FT.
GARAGE AREA	1,583 SQ. FT.

INVESTCO

2

THE CONTRACTOR ASSUMES FULL RESPONSIBILITY FOR THE CORRECT INSTALLATION OF ALL EXTERIOR FINISHES AND WEATHERPROOFING.



LEFT ELEVATION

SCALE: 1/4" = 1'-0"

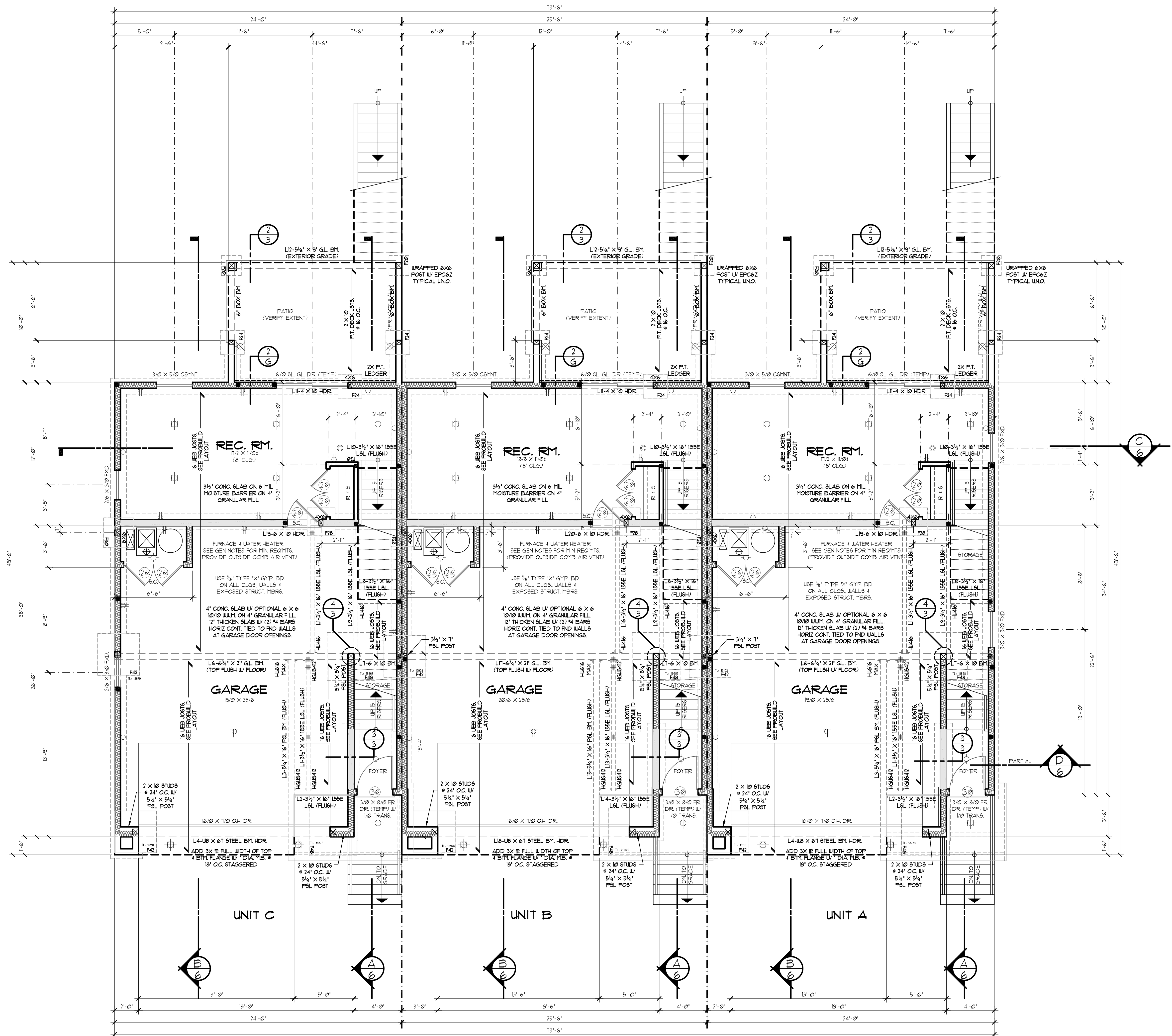


REAR ELEVATION

SCALE: 1/4" = 1'-0"

UNITS A & C	UNITS B	GARAGE AREA
UPPER FLOOR	UPPER FLOOR	UPPER FLOOR
878 SQ. FT.	939 SQ. FT.	622 SQ. FT.
MAIN FLOOR	MAIN FLOOR	
884 SQ. FT.	913 SQ. FT.	
LOWER FLOOR	LOWER FLOOR	
288 SQ. FT.	306 SQ. FT.	
TOTAL AREA	TOTAL AREA	
2020 SQ. FT.	2168 SQ. FT.	

THE CONTRACTOR ASSUMES FULL RESPONSIBILITY FOR THE CORRECT INSTALLATION OF ALL EXTERIOR FINISHES AND WEATHERPROOFING.



LOWER FLOOR / FOUNDATION PLAN

SCALE: 1/4" = 1'-0"

REFER TO ENGINEERING SHEETS FOR LATERAL SPECIFICATIONS PRIOR TO CONSTRUCTION

25# SNOW LOAD

UNITS A & C	UNIT B	GARAGE AREA
UPPER FLOOR	UPPER FLOOR	UPPER FLOOR
878 SQ. FT.	939 SQ. FT.	622 SQ. FT.
MAIN FLOOR	MAIN FLOOR	MAIN FLOOR
884 SQ. FT.	945 SQ. FT.	628 SQ. FT.
LOWER FLOOR	LOWER FLOOR	LOWER FLOOR
288 SQ. FT.	306 SQ. FT.	268 SQ. FT.
TOTAL AREA	TOTAL AREA	TOTAL AREA
2020 SQ. FT.	2020 SQ. FT.	2020 SQ. FT.

LEGEND

- RECESSED LIGHT
- RECESSED DIRECTIONAL LIGHT FIXTURE
- WALL-MOUNT LIGHT
- SURFACE-MOUNT LIGHT
- FLOOD LIGHT
- SURFACE MOUNTED FLUORESCENT
- RECESSED EXHAUST FAN VENTED TO THE EXTERIOR
- CEILING FAN
- DUPLEX OUTLET
- CEILING MOUNTED DUPLEX OUTLET
- 220V OUTLET
- FLUSH FLOOR MOUNTED OUTLET (VERIFY LOC.)
- DATA OUTLET
- TELEPHONE OUTLET
- TELEVISION OUTLET
- SPEAKER LOCATION
- SMOKE / CO DETECTOR (SEE GENERAL NOTES FOR OTHER SPECS.)
- BEARING POINT LOCATION (PROVIDE SOLID BEARG. MIN. OF MEMBER WIDTH UNO.)
- POINT LOAD FROM ABOVE
- 4 x 4 POST FROM ROOF HIP VALLEY OR RIDGE DOWN TO BEARING POINT ON WALL BELOW (MAX. OF 45" FROM VERT.)
- BEARING WALL SUPPORTING STRUCTURE ABOVE
- 4 x 10 HDR. BEARING WALL INT. DOOR 4 OPENINGS W/ MIN (2) x SUPPORT EA END (UNO.)
- DROPPED STRUCT. MEMBER BEARING WALL

S-FLR.LND 07/18/16

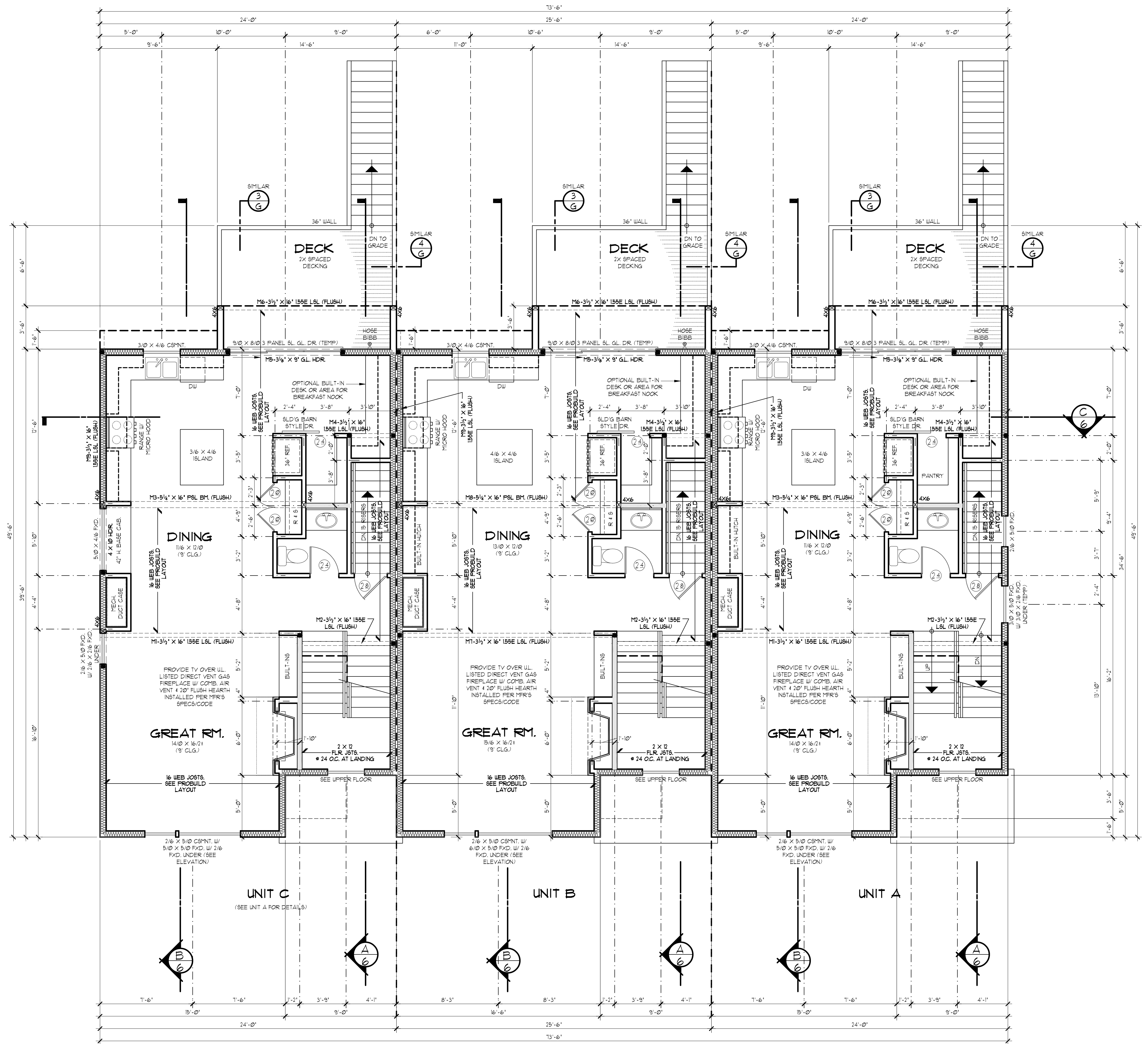
ENERGY ENVELOPE KEY

- WALL, FLR., CLG. INSUL.
- FOUNDATION INSUL.

(SEE SHEET 'S' FOR INSULATION VALUES)

CO. DET LOCATION

CARBON MONOXIDE ALARMS SHALL BE LOCATED IN EA. BEDROOM OR WITHIN 5 FEET OUTSIDE OF EA. BEDROOM DOOR, AT EVERY FLOOR LEVEL W/ BEDROOMS (SEE SHEET 'G' FOR ADDL. INFO)



MAIN FLOOR PLAN
SCALE: 1/4" = 1'-0"

THIS FLOOR REFLECTS FLOOR LAYOUT AND DESIGN BY PROBUILD - JOB #2000339. SEE MAIN FLOOR LAYOUT DRAWINGS AND ENGINEERING FOR ADDITIONAL SPECIFICATIONS AND INSTALLATION REQUIREMENTS.

REFER TO ENGINEERING SHEETS FOR LATERAL SPECIFICATIONS PRIOR TO CONSTRUCTION

ALAN MASCORD DESIGN ASSOCIATES, INC. 1116 W. MAIN ST. PORTLAND, OR 97201
 TEL: 503.255.4961 FAX: 503.981.1851

INVESTCO-TRIPLEX-A
PROJECT MANAGER/AS
DRAWN 12/28/17 LAW

INVESTCO REALTY GROUP

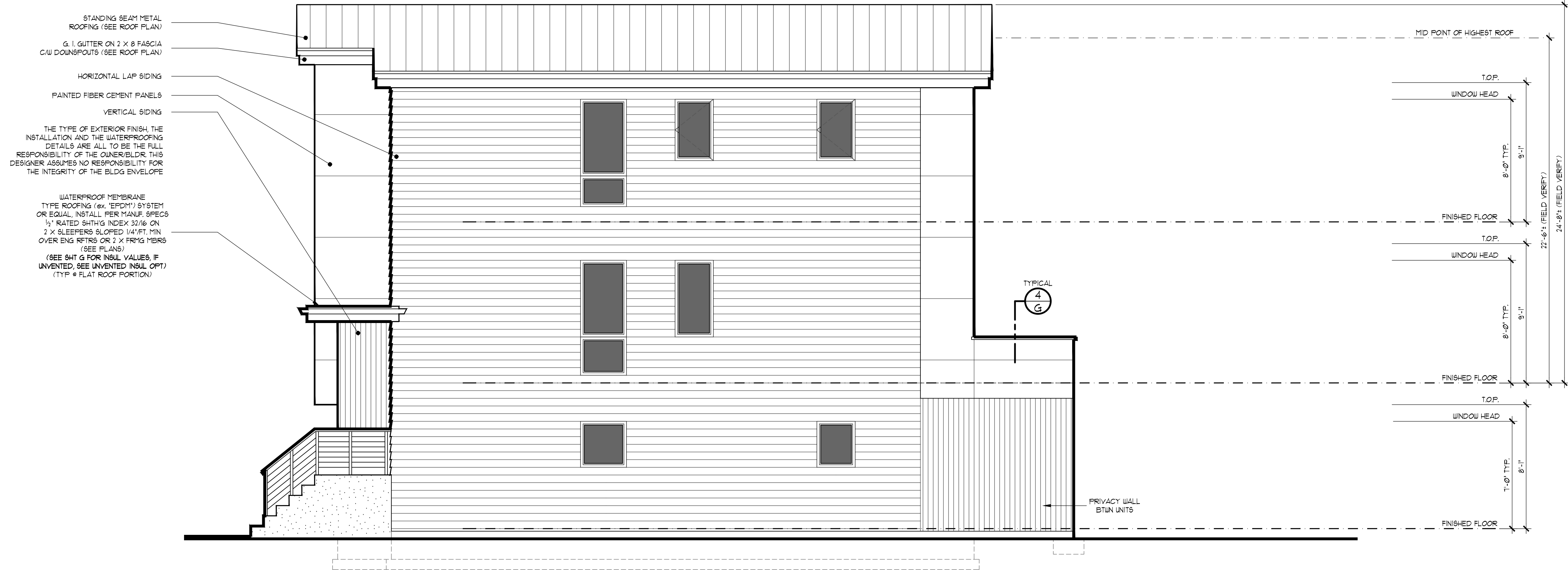
HOODVIEW/JURRIT/SUBDIVISION
 W.E.T. LTRM 08/08

25# SNOW LOAD

UNITS A & C	UNIT B
UPPER FLOOR	UPPER FLOOR
878 SQ. FT.	939 SQ. FT.
MAIN FLOOR	MAIN FLOOR
884 SQ. FT.	913 SQ. FT.
LOWER FLOOR	LOWER FLOOR
288 SQ. FT.	306 SQ. FT.
TOTAL AREA	TOTAL AREA
2050 SQ. FT.	2158 SQ. FT.
GARAGE AREA	GARAGE AREA
• 683 SQ. FT.	• 622 SQ. FT.

INVESTCO

4



STANDING SEAM METAL ROOFING (SEE ROOF PLAN)
 G. I. GUTTER ON 2 X 8 FASCIA C/W DOWNSPOUTS (SEE ROOF PLAN)
 HORIZONTAL LAP SIDING
 PAINTED FIBER CEMENT PANELS
 VERTICAL SIDING
 THE TYPE OF EXTERIOR FINISH, THE INSTALLATION AND THE WATERPROOFING DETAILS ARE ALL TO BE THE FULL RESPONSIBILITY OF THE OWNER/BUILDER. THIS DESIGNER ASSUMES NO RESPONSIBILITY FOR THE INTEGRITY OF THE BLDG ENVELOPE

WATERPROOF MEMBRANE
 TYPE ROOFING (ex. EPDM) SYSTEM OR EQUAL, INSTALL PER MANUF. SPECS
 1/2" RATED 9HT/G INDEX 32/16 ON 2 X SLEEPERS SLOPED 1/4"FT. MIN OVER ENG RTNRS OR 2 X FR'G MBRS (SEE PLANS)
 (SEE 8HT G FOR INSL VALUES, IF UNVENTED, SEE UNVENTED INSL OPT) (TYP = FLAT ROOF PORTION)

RIGHT ELEVATION

SCALE: 1/4" = 1'-0"



FRONT ELEVATION

SCALE: 1/4" = 1'-0"

THE CONTRACTOR ASSUMES FULL RESPONSIBILITY FOR THE CORRECT INSTALLATION OF ALL EXTERIOR FINISHES AND WEATHERPROOFING.

Mascord
 COLLECTION

ALAN MASCORD DESIGN ASSOCIATES, INC.
 245 NW 103RD ST, SUITE 100, PORTLAND, OR 97228
 TEL: 503.222.9981 FAX: 503.981.0100
 WWW.MASCORD.COM

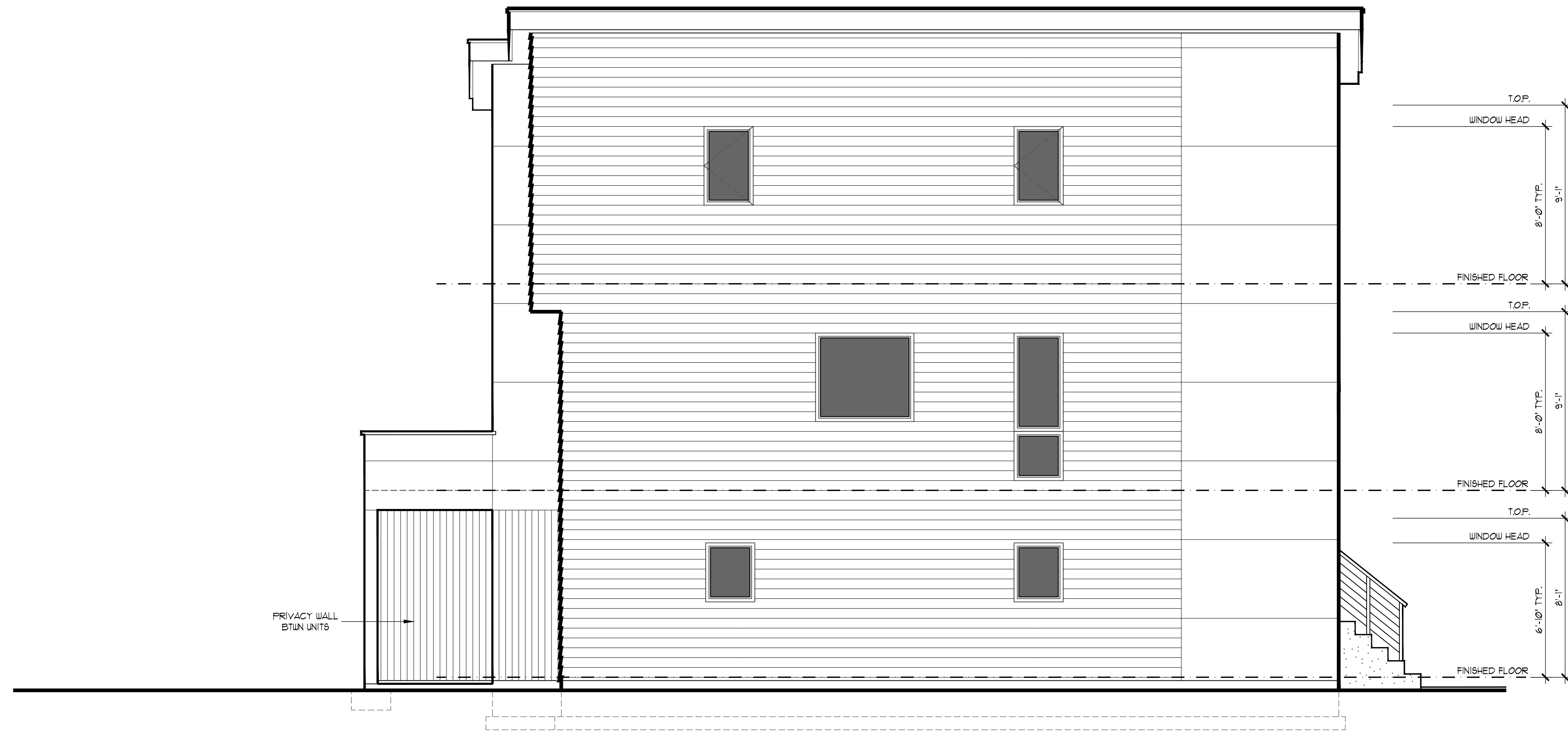
INVESTCO, TRIPLEX-1
 PROJECT MANAGER/ARCHITECT
 DRAWN 02/02/18 LAW

RESIDENCES FOR:
INVESTCO REALTY GROUP
 TOODVIEW QUARTZ / SUBDIVISION
 W. E. LIND, ORG & CO.

25# SNOW LOAD

UNIT	UPPER FLOOR	LOWER FLOOR	TOTAL AREA	GARAGE AREA
UNITS A & C	878 SQ. FT.	288 SQ. FT.	2020 SQ. FT.	583 SQ. FT.
UNIT B	938 SQ. FT.	308 SQ. FT.	2156 SQ. FT.	622 SQ. FT.

INVESTCO
 1



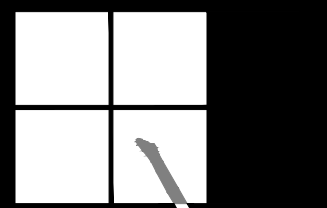
LEFT ELEVATION

SCALE: 1/4" = 1'-0"



REAR ELEVATION

SCALE: 1/4" = 1'-0"



Mascord
 COLLECTION

ALAN MASCORD DESIGN ASSOCIATES, INC.
 PROJECT MANAGER/ARCHITECT
 245 NW 103rd St, Suite 100, Portland, OR 97228-3961 FAX: 503.981.8180
 WWW.MASCORD.COM

INVESTCO-TRIPLEX-2
 PROJECT MANAGER/ARCHITECT
 DRAWN 02/02/18 LAW

RESIDENCES FOR:
INVESTCO REALTY GROUP
 TO OBTAIN PLANS, SUBMITTALS, W. P. LIND, OR G. S. O.

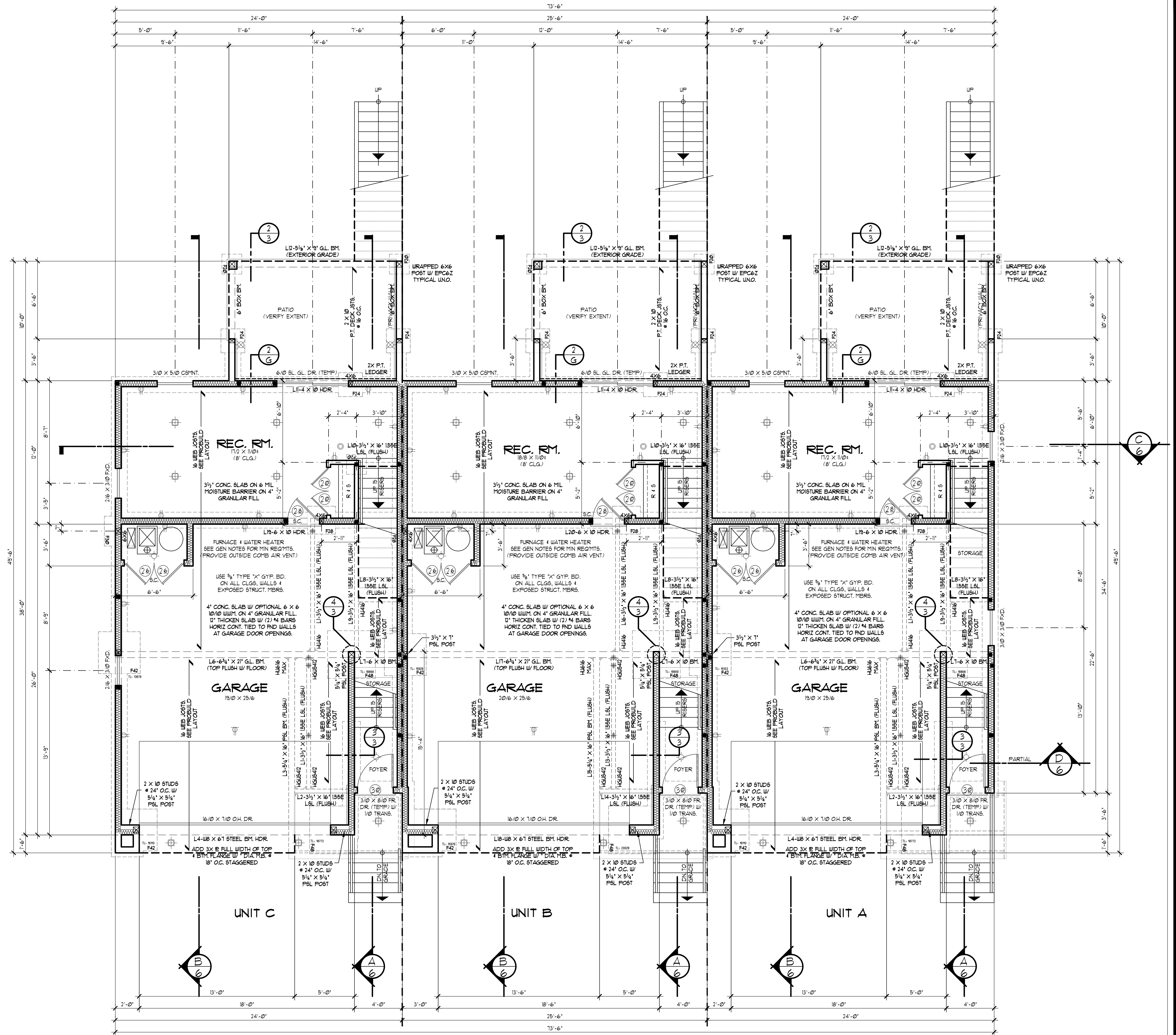
25# SNOW LOAD

UNIT	UPPER FLOOR	LOWER FLOOR	TOTAL AREA	GARAGE AREA
UNITS A & C	878 SQ. FT.	288 SQ. FT.	2020 SQ. FT.	583 SQ. FT.
UNIT B	938 SQ. FT.	306 SQ. FT.	2156 SQ. FT.	622 SQ. FT.

THE CONTRACTOR ASSUMES FULL RESPONSIBILITY FOR THE CORRECT INSTALLATION OF ALL EXTERIOR FINISHES AND WEATHERPROOFING.

INVESTCO

2



LOWER FLOOR / FOUNDATION PLAN

SCALE: 1/4" = 1'-0"

REFER TO ENGINEERING SHEETS FOR LATERAL SPECIFICATIONS PRIOR TO CONSTRUCTION

ALAN MASCORD DESIGN ASSOCIATES, INC.
 245 NW 103RD ST, SUITE 100 PORTLAND, OR 97228
 TEL: 503.252.9961 FAX: 503.252.9910
 WWW.MASCORD.COM

INVESTCO-TRIPLEX-3
 PROJECT MANAGER
 DRAWN 12/28/17 LAW

RESIDENCES FOR:
INVESTCO REALTY GROUP
 25# SNOW LOAD
 TO OBTAIN PLANS, SUBDIVISION
 W.P. LIND, O.R.C.S.O.

UNIT	UPPER FLOOR	LOWER FLOOR	TOTAL AREA	GARAGE AREA
UNITS A & C	878 SQ. FT.	288 SQ. FT.	2020 SQ. FT.	583 SQ. FT.
UNIT B	938 SQ. FT.	308 SQ. FT.	2198 SQ. FT.	622 SQ. FT.

INVESTCO
 3

LEGEND

- RECESSED LIGHT
- RECESSED DIRECTIONAL LIGHT FIXTURE
- WALL-MOUNT LIGHT
- SURFACE-MOUNT LIGHT
- FLOOD LIGHT
- SURFACE MOUNTED FLUORESCENT
- RECESSED EXHAUST FAN VENTED TO THE EXTERIOR
- CEILING FAN
- DUPLEX OUTLET
- CEILING MOUNTED DUPLEX OUTLET
- 220V OUTLET
- FLUSH FLOOR MOUNTED OUTLET (VERIFY LOC.)
- DATA OUTLET
- TELEVISION OUTLET
- SPEAKER LOCATION
- SMOKE / CO DETECTOR (SEE 'GENERAL NOTES' FOR OTHER SPECS)
- BEARING POINT LOCATION (PROVIDE SOLID BEARING - MIN. OF MEMBER WIDTH UNO.)
- POINT LOAD FROM ABOVE
- 4 X 4 POST FROM ROOF HIP, VALLEY OR RIDGE DOWN TO BEARING POINT ON WALL BELOW (MAX. OF 48" FROM VERT.)
- BEARING WALL SUPPORTING STRUCTURE ABOVE
- 4 X 10 HDR. BEARING WALL INT. DOOR. 4 OPENINGS W/ MIN (2) 2 X SUPPORT EA END (UNO.)
- DROPPED STRUCT. MEMBER BEARING WALL

S-FLOORING 07/18/18

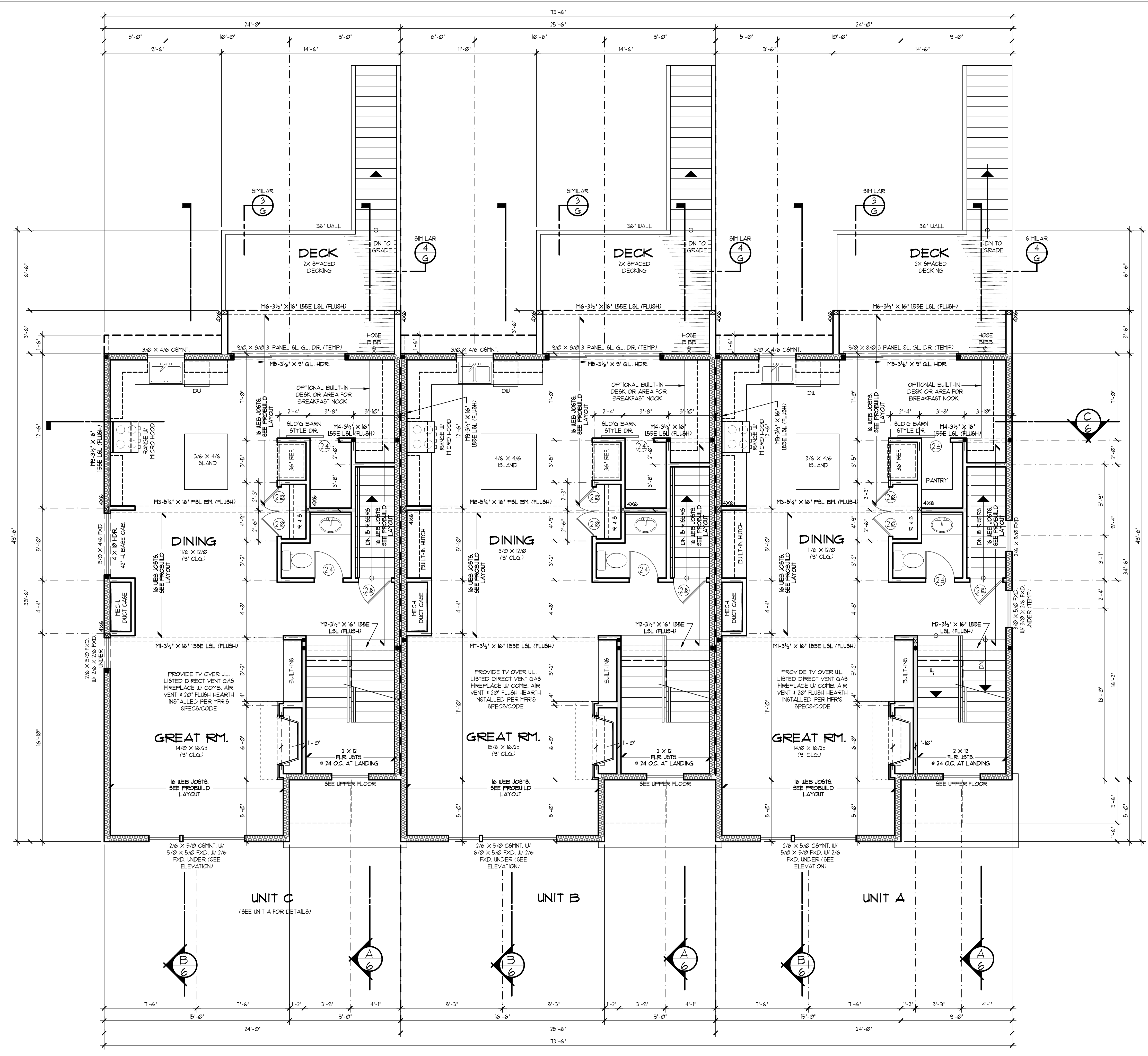
ENERGY ENVELOPE KEY

- WALL, FLR./CLG. INSUL.
- FOUNDATION INSUL.

(SEE SHEET 'G' FOR INSULATION VALUES)

CO. DET LOCATION

CARBON MONOXIDE ALARMS SHALL BE LOCATED IN EA. BEDROOM OR WITHIN 15 FEET OUTSIDE OF EA. BEDROOM DOOR, AT EVERY FLOOR LEVEL W/ BEDROOMS (SEE SHEET 'G' FOR ADD'L INFO)



MAIN FLOOR PLAN
SCALE: 1/4" = 1'-0"

THIS FLOOR REFLECTS FLOOR LAYOUT AND DESIGN BY PROBUILD - JOB #1909039. SEE MANUF. LAYOUT DRAWINGS AND ENGINEERING FOR ADDITIONAL SPECIFICATIONS AND INSTALLATION REQUIREMENTS.

REFER TO ENGINEERING SHEETS FOR LATERAL SPECIFICATIONS PRIOR TO CONSTRUCTION

ALAN MASCORD DESIGN ASSOCIATES, INC.
245 NW 100th St., Suite 100, Portland, OR 97228
TEL: 503.222.9961 FAX: 503.981.0100
WWW.ALMASCORD.COM

INVESTCO TRIPLE X
PROJECT MANAGER/EA
DRAWN 12/28/17 LAW

RESIDENCES FOR:

INVESTCO REALTY GROUP

TOO VIEW QUARTZ / SUBDIVISION
W. E. LIND ORSON

25# SNOW LOAD

UNIT	UPPER FLOOR	LOWER FLOOR	TOTAL AREA	GARAGE AREA
UNIT A & C	878 SQ. FT.	288 SQ. FT.	2020 SQ. FT.	583 SQ. FT.
UNIT B	938 SQ. FT.	308 SQ. FT.	2158 SQ. FT.	622 SQ. FT.

4

LEGEND

- RECESSED LIGHT
- RECESSED DIRECTIONAL LIGHT FIXTURE
- WALL-MOUNT LIGHT
- SURFACE-MOUNT LIGHT
- FLOOD LIGHT
- SURFACE MOUNTED FLUORESCENT
- RECESSED EXHAUST FAN VENTED TO THE EXTERIOR
- CEILING FAN
- DUPLEX OUTLET
- CEILING MOUNTED DUPLEX OUTLET
- 220V OUTLET
- FLUSH FLOOR MOUNTED OUTLET (VERIFY LOC.)
- TELEPHONE OUTLET
- DATA OUTLET
- TELEVISION OUTLET
- SPEAKER LOCATION
- SMOKE / CO DETECTOR (SEE GENERAL NOTES FOR OTHER SPEC'S)
- BEARING POINT LOCATION (PROVIDE SOLID BEARING MIN. OF MEMBER WIDTH UNO.)
- POINT LOAD FROM ABOVE
- 4 X 4 POST FROM ROOF HIP, VALLEY OR RIDGE DOWN TO BEARING POINT ON WALL BELOW (MAX. OF 45' FROM VERT.)
- BEARING WALL SUPPORTING STRUCTURE ABOVE
- 4 X 10 HDR, # BEARING WALL INT. DOOR 4 OPENINGS W/ MIN (2) 2 X SUPPORT EA END (UNO.)
- DROPPED STRUCT. MEMBER BEARING # WALL

S-FULLCOND 07/18/16

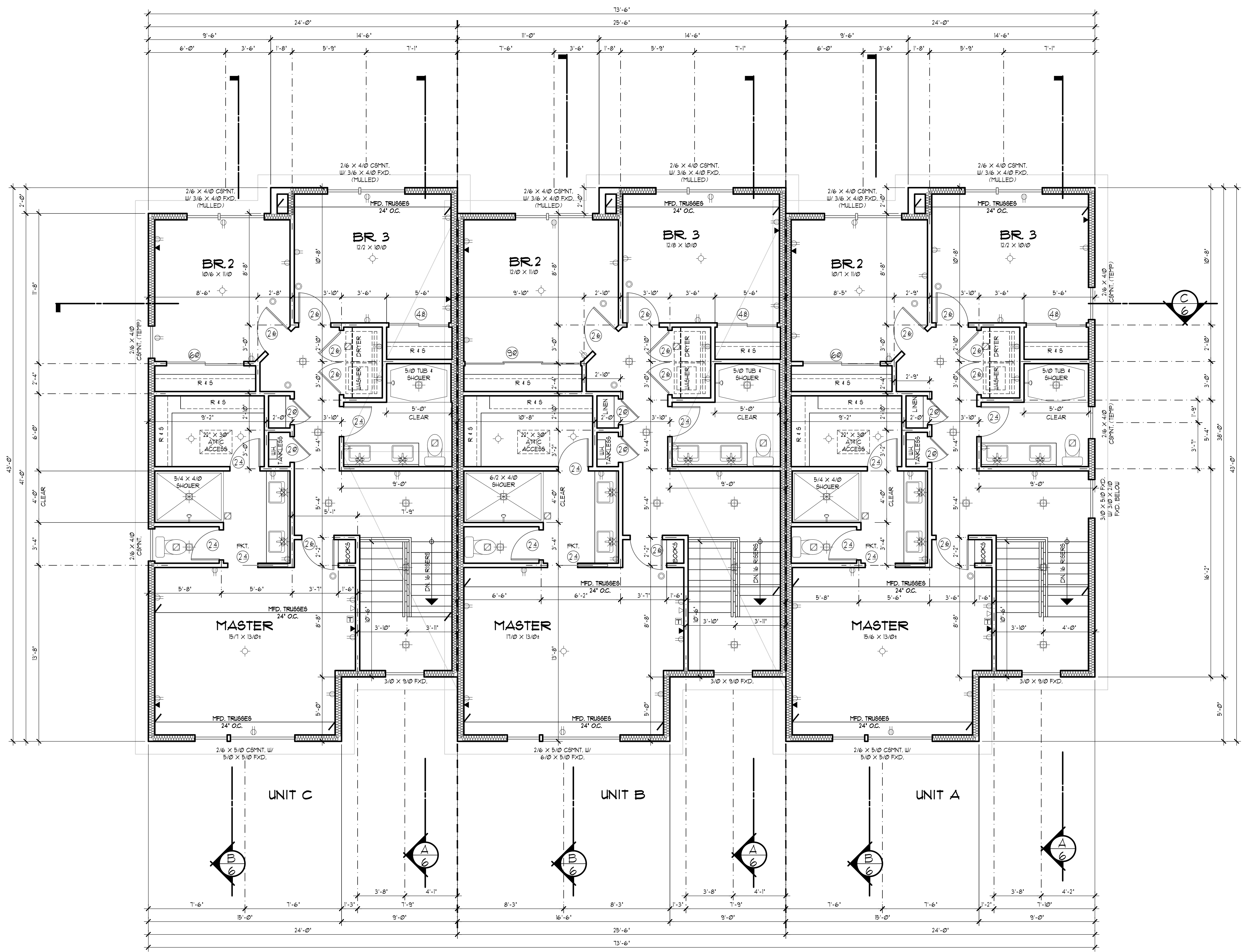
ENERGY ENVELOPE KEY

- WALL/FLR/CLG. INSUL.
- FOUNDATION INSUL.

(SEE SHEET 'G' FOR INSULATION VALUES)

CO. DET LOCATION

CARBON MONOXIDE ALARMS SHALL BE LOCATED IN EA. BEDROOM OR WITHIN 5 FEET OUTSIDE OF EA. BEDROOM DOOR. AT EVERY FLOOR LEVEL W/ BEDROOMS. (SEE SHEET 'G' FOR ADD'L INFO.)



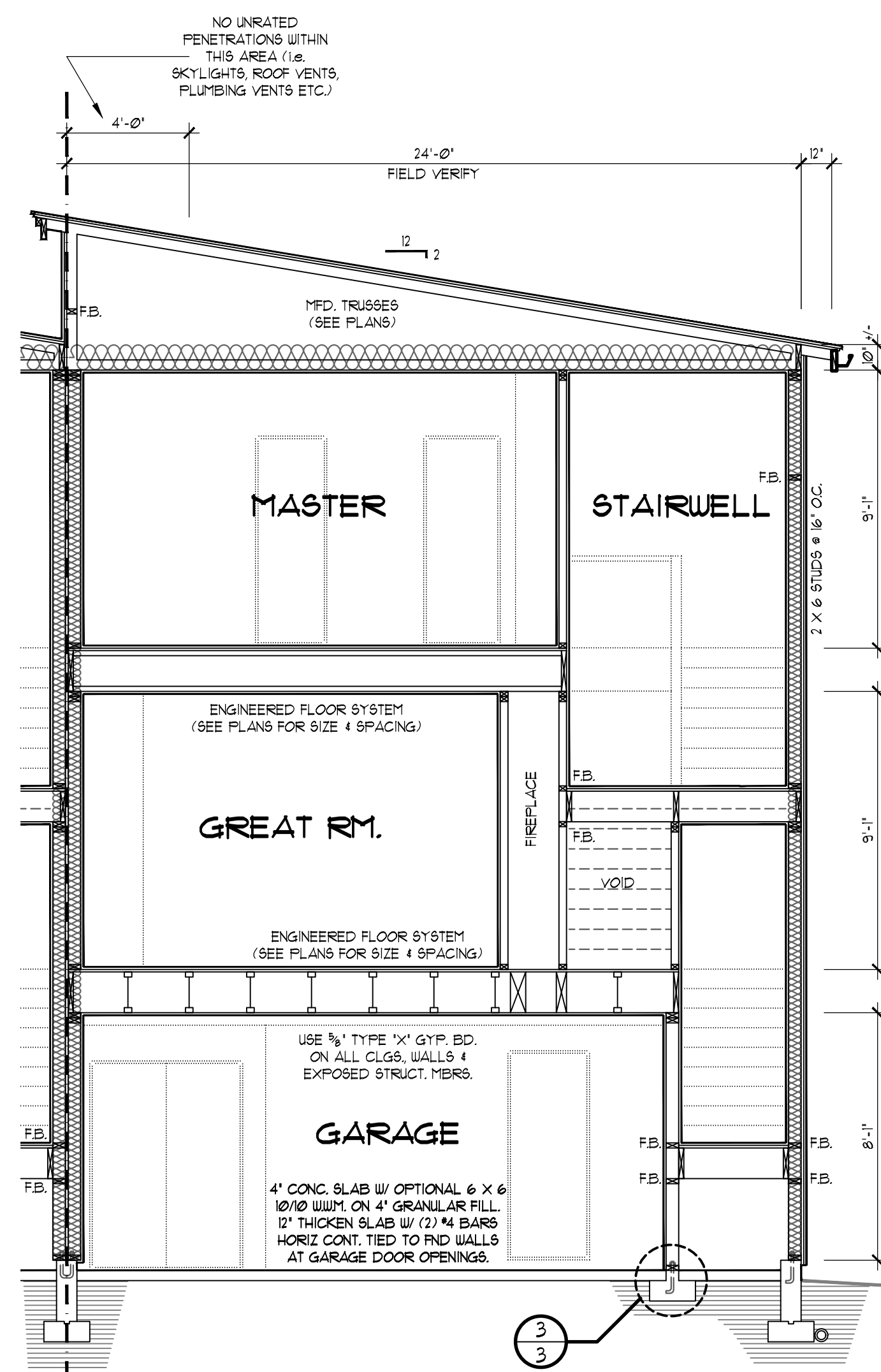
UPPER FLOOR PLAN

SCALE: 1/4" = 1'-0"

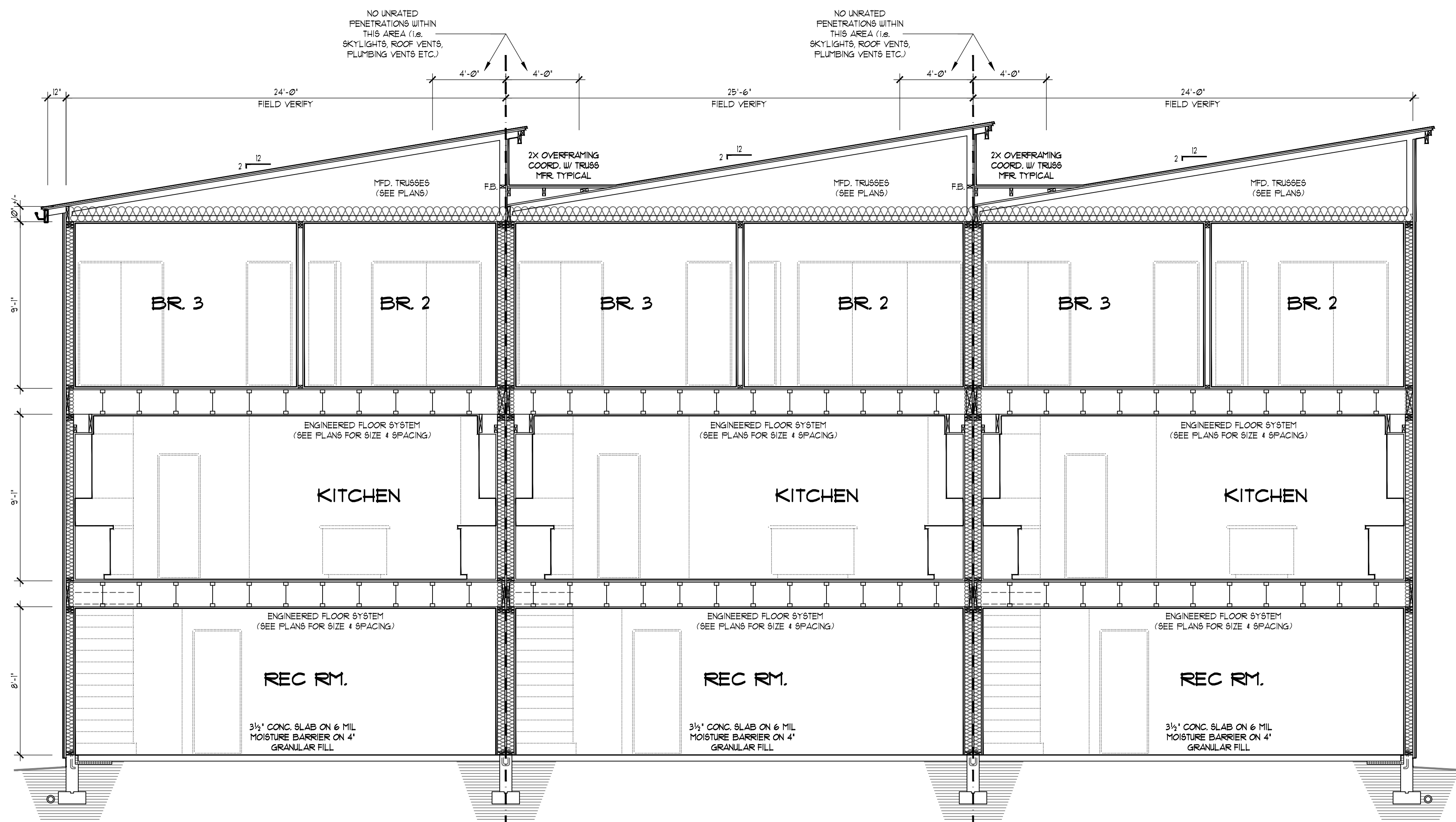
REFER TO ENGINEERING SHEETS FOR LATERAL SPECIFICATIONS PRIOR TO CONSTRUCTION

25# SNOW LOAD

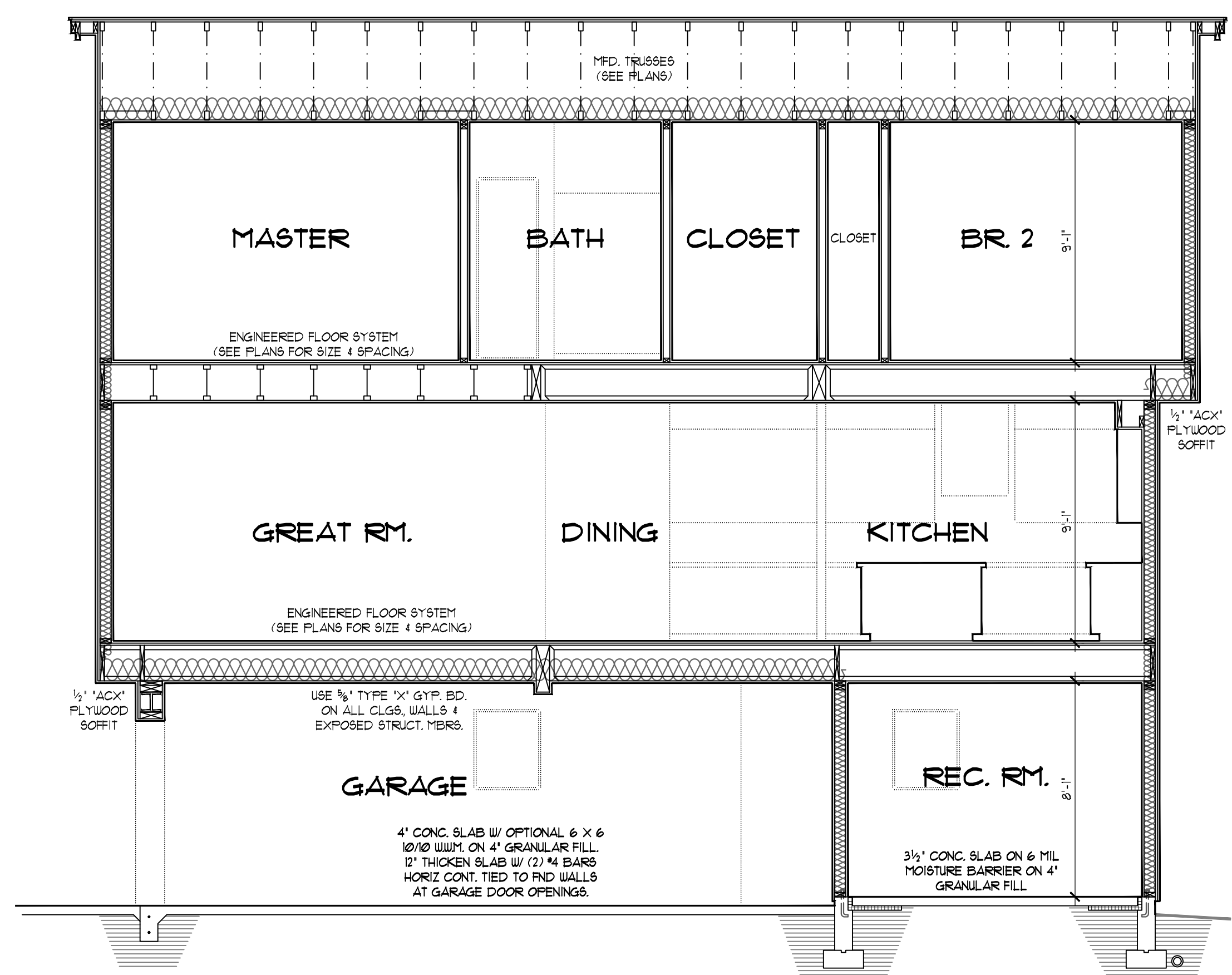
UNIT	UPPER FLOOR	LOWER FLOOR	TOTAL AREA	GARAGE AREA
UNIT A & C	878 SQ. FT.	288 SQ. FT.	2020 SQ. FT.	583 SQ. FT.
UNIT B	938 SQ. FT.	308 SQ. FT.	2156 SQ. FT.	622 SQ. FT.



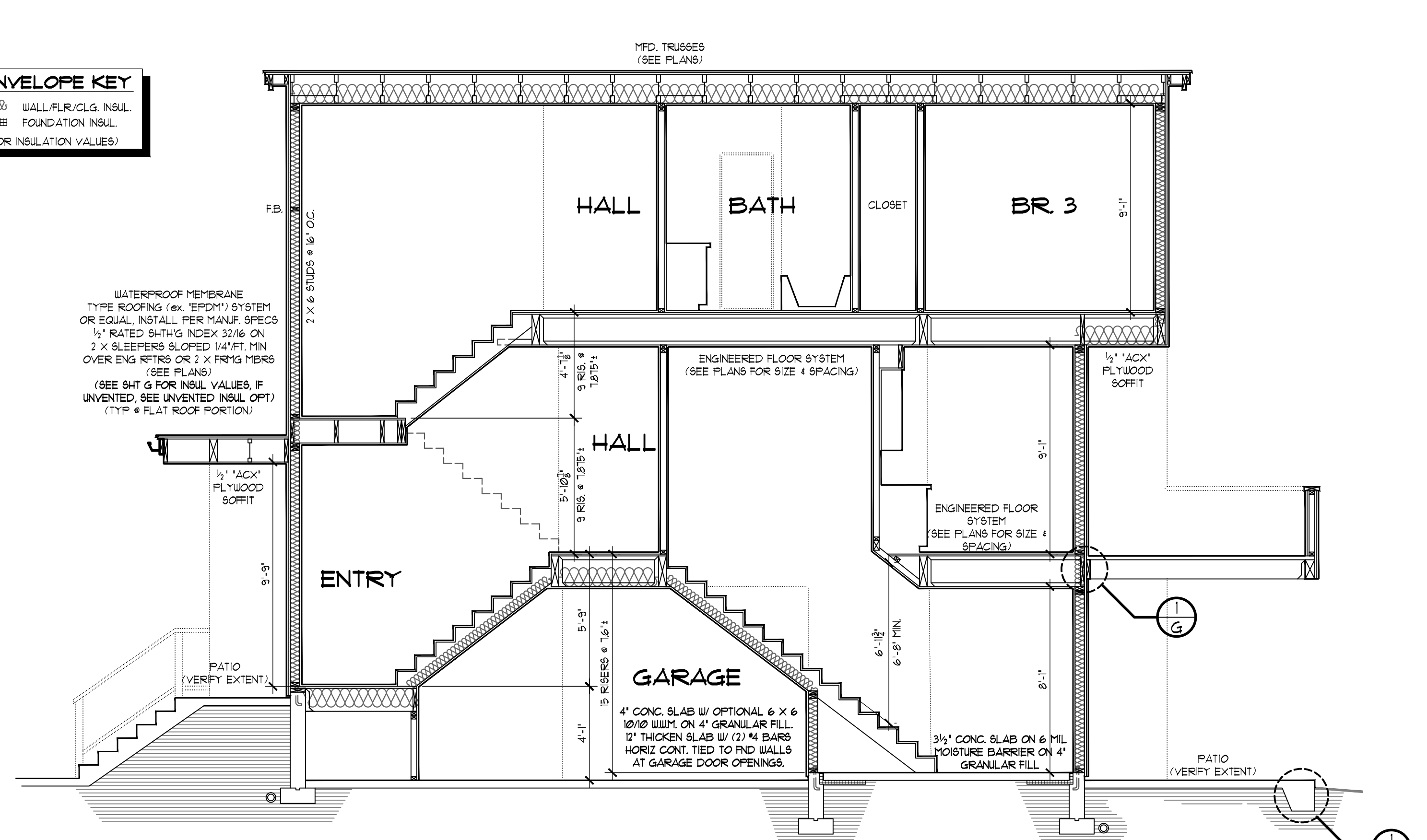
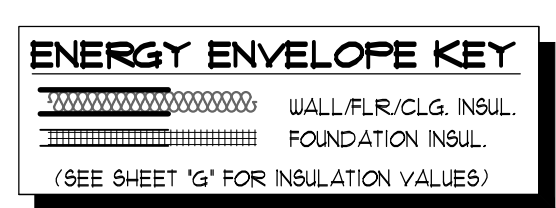
D PARTIAL BUILDING SECTION
SCALE: 1/4" = 1'-0"



C BUILDING SECTION
SCALE: 1/4" = 1'-0"



B BUILDING SECTION
SCALE: 1/4" = 1'-0"



A BUILDING SECTION
SCALE: 1/4" = 1'-0"

Mascord COLLECTION

ALAN MASCORD DESIGN ASSOCIATES, INC.
245 NW REED ST., SUITE 100 PORTLAND, OR 97209 503.225.9961 FAX 503.241.0100
WWW.MASCORD.COM

INVESTCO-TRIPLEX-6
PROJECT MANAGER
DRAWN 02/19/18 LAW

RESIDENCES FOR:
INVESTCO REALTY GROUP
TOOBY NEW QUARTER SUBDIVISION
W.E. LIND, OREGON

25# SNOW LOAD

UNITS A & C	UNITS B
UPPER FLOOR 878 SQ. FT.	UPPER FLOOR 938 SQ. FT.
LOWER FLOOR 288 SQ. FT.	LOWER FLOOR 308 SQ. FT.
TOTAL AREA 2020 SQ. FT.	TOTAL AREA 2158 SQ. FT.
GARAGE AREA - 583 SQ. FT.	GARAGE AREA - 622 SQ. FT.

INVESTCO
6

ROOF DESIGN NOTES

THIS ROOF HAS BEEN DESIGNED TO SUPPORT CEDAR SHAKE ROOFING MATERIALS AND COMPOSITION ROOFING OF VARIOUS TYPES. THE TABLE BELOW DESCRIBES IN DETAIL THE ASSUMPTIONS MADE IN THE DESIGN OF THE ROOF STRUCTURE OF THIS BUILDING.

ROOF LIVE LOAD (SNOW)	25.0 PSF	3.25 PSF	AVE/(LET)
FRAMING MATERIALS:	2.0 PSF	3.25 PSF	ACTUAL REQ'D
SHEATHING MATERIALS:	15 PSF	6.75 PSF	SAFETY FACTOR
MISC. MATERIALS:	15 PSF		
ROOFING TYPE	DRY / WET	40.0	PSF TL

GYPSUM MATERIALS: ADD 3.0 PSF FOR VAULTED AREAS (COVERED IN SAFETY FACTOR)

NOTE: HIPS, VALLEYS & RIDGES SHALL NOT BE LESS IN DEPTH THAN THE END OUT OF THE RAFTERS (FIELD VERIFY ALL CONDITIONS)

LEGEND

4 X 4 WOOD POST FROM RIDGE (HIP OR VALLEY) TO WALL BELOW (MIN. (2) 2 X 4 REQ'D AT WALL BEARING POINT; NOTE: SPLICES IN HIPS & VALLEYS CAN ONLY OCCUR @ POST DOWN LOCATIONS)

49 SQ. IN. ROOF VENTS (SEE VENT TABLE FOR QTY. - 50%/50% SHOWN)

2X4 PURLINS TO BR1 OR WALL BELOW (FRAMING AT 24" O.C.)

SHADED AREA DENOTES ROOF FRAMED OVER RAFTERS BELOW

DOWNSPUTS

STANDING SEAM METAL ROOF

MAXIMUM SPANS PER 2004 U.M.P.A. TEL. RR-28 1" D.F. L/240 25" LL 4 15" DL

SIZE	SPACING	SPAN
2X6	12" O.C.	14'-0"
	16" O.C.	12'-1"
	24" O.C.	9'-0"
2X8	12" O.C.	17'-8"
	16" O.C.	15'-4"
	24" O.C.	12'-6"
2X10	12" O.C.	21'-1"
	16" O.C.	18'-9"
	24" O.C.	15'-3"
2X12	12" O.C.	25'-1"
	16" O.C.	21'-8"
	24" O.C.	17'-9"

ROOF VENTS EACH UNIT

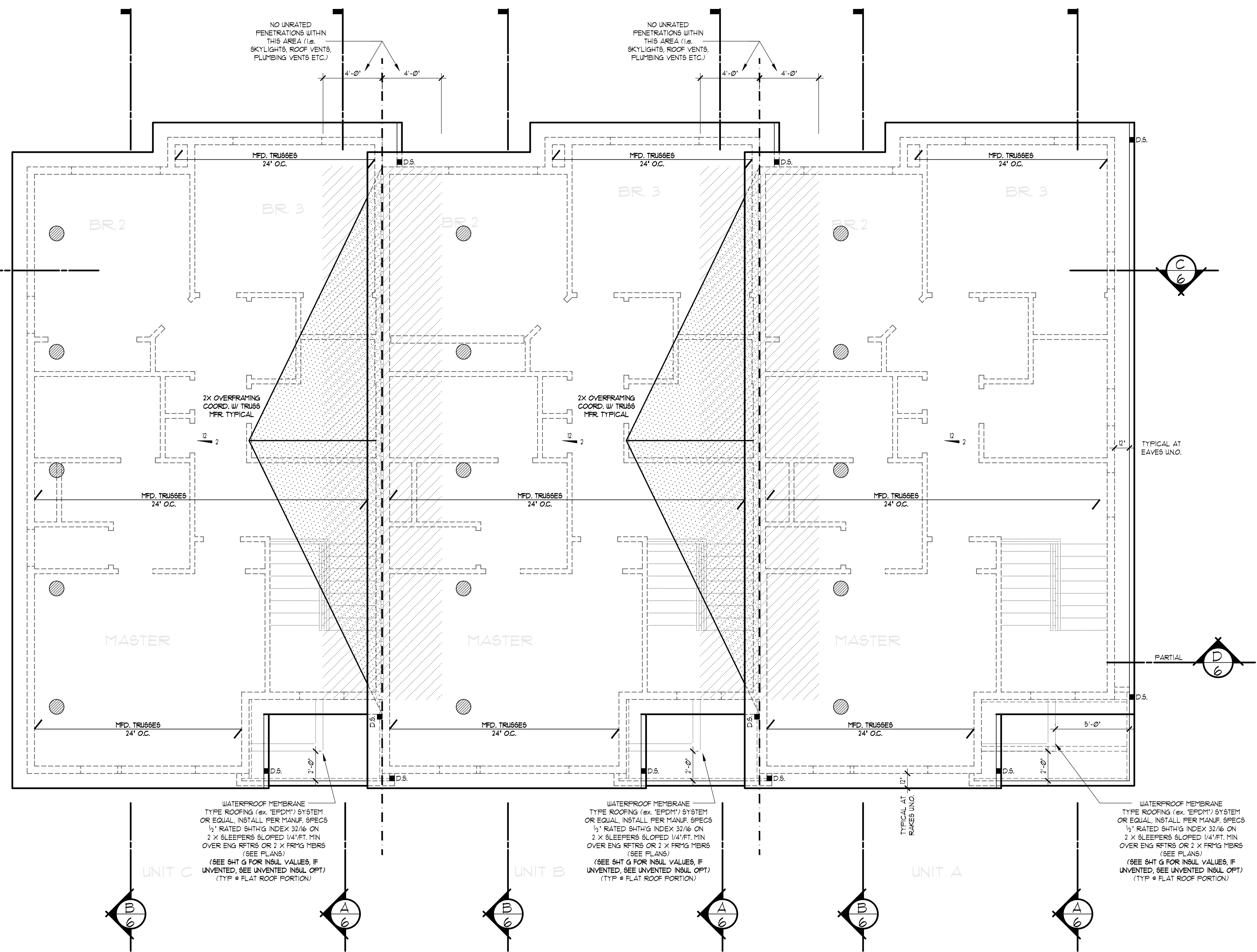
ROOF AREA (ft ²)	% EAVE	AREA (ft ²)	% ROOF	AREA (ft ²)	3-VENT	4-VENT	49 in ²
60	303.8	40	202.6	32	24	4	
56.7	281.1	43.3	219.3	30	23	4	
53.3	266.9	46.7	236.9	29	21	5	
50	253.2	50	253.2	21	20	5	

PER 2015 I.R.C. - R806.2 MINIMUM AREA: THE TOTAL NET FREE VENTILATING AREA SHALL NOT BE LESS THAN 1/600 OF THE AREA OF THE SPACE VENTILATED, PROVIDED AT LEAST 40% AND NOT MORE THAN 50% OF THE REQUIRED VENTILATING AREA IS PROVIDED WITH VENT OPENINGS LOCATED IN THE UPPER PORTION OF THE SPACE TO BE VENTILATED AT LEAST 3 FEET ABOVE THE EAVE OR CORNICE VENTS WITH THE BALANCE OF THE REQUIRED VENTILATION PROVIDED BY EAVE OR CORNICE VENTS. IN CLIMATE ZONE 6, 1, 1.8, A CLASS 1 OR 2 VAPOR RETARDER SHALL BE INSTALLED ON THE WARM-N-INTERIOR SIDE OF THE CLG.

EAVE BLOCKING

3-VENT BLOCK (13 2/4" HOLES (9.43 in² EA))

4-VENT BLOCK (14 1/4" HOLES (12.56 in² EA))

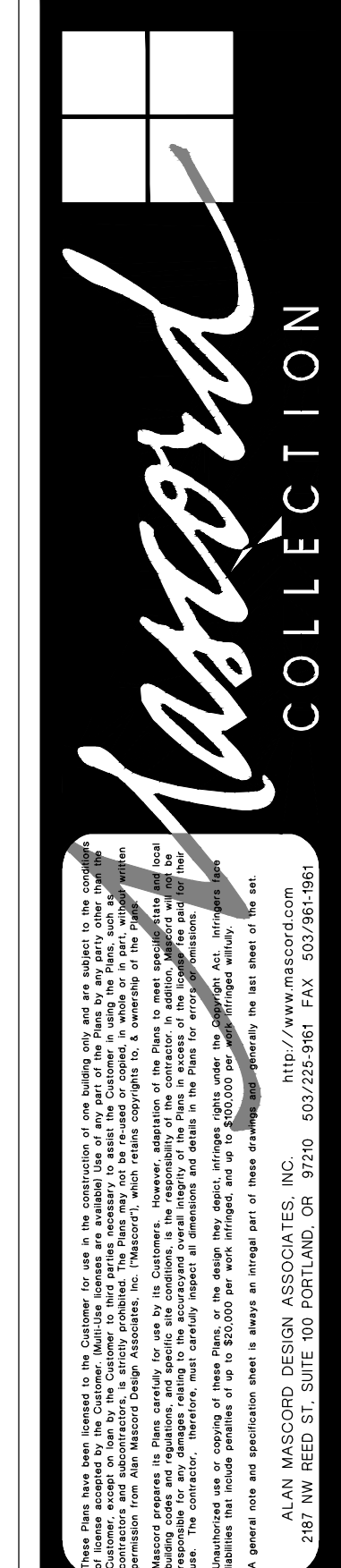


ROOF FRAMING PLAN

SCALE: 1/4" = 1'-0"

THIS ROOF REFLECTS TRUSS LAYOUT AND DESIGN BY PROBUILD - JOB 151900073. SEE MANUF. LAYOUT, TRUSS DRAWINGS AND ENGINEERING FOR ADDITIONAL SPECIFICATIONS AND INSTALLATION REQUIREMENTS.

REFER TO ENGINEERING SHEETS FOR LATERAL SPECIFICATIONS PRIOR TO CONSTRUCTION



ALAN MASCORD DESIGN ASSOCIATES, INC. 245 NW 100th St, Suite 100, Portland, OR 97228-9901 FAX: 503.941.8100

RESIDENCES FOR: **INVESTICO REALTY GROUP**
 PROJECT MANAGER: [Name] DRAWN 12/28/17 LAW

25# SNOW LOAD

UNIT	UPPER FLOOR	LOWER FLOOR	TOTAL AREA	GARAGE AREA
UNIT B	938 SQ. FT.	308 SQ. FT.	2956 SQ. FT.	622 SQ. FT.
UNIT A & C	878 SQ. FT.	288 SQ. FT.	2020 SQ. FT.	583 SQ. FT.



Minutes of the Meeting - Parker Crest Neighborhood

June 6, 2018 at the West Linn Adult Community Center
1180 SW Rosemont Road, West Linn

Attendance: See attached "Sign Up Sheet" – 12 people including
Ali and Nina Ladha, and Robert Price, Consultant

The meeting started at 6:00 PM on Wednesday, June 6th at the West Linn Adult Community Center. A sign-up sheet was provided and attendees were asked to sign in. The applicants, Ali and Nina Ladha, provided a 24" x 36" site plan, copies of the site survey, and renderings of the proposed design of the new residential units for review by all in attendance.

Ali Ladha explained the goal of the project, to provide 5 quality residential units in 2 structures on the site. Each unit will be approximately 2,000 square feet in floor area. Ali explained the layout of the units and the siting of the structures on the 17,500 sf (approx.) site. He also explained some of the history of the site, in which Ali and Nina are not the first to propose development of the site. For some reasons, none of the previous development proposals moved forward.

Robert Price, Consultant to Ali and Nina Ladha, explained the process in the City of West Linn for obtaining approval for the project. A public hearing will be held before the West Linn Planning Commission on this application when it is prepared, submitted, deemed complete and a hearing date is set by the city.

One of the first subjects to arise during the open discussion of the project was whether the units would be owner occupied or renter occupied. Lynne Saphir who, with her husband Bill, own and live in the property directly adjacent to the east, was concerned about renters occupying the units because she felt that renters might not be able to take care of the properties and may drive down home values. A discussion ensued about owner-occupied versus renter-occupied units. Ali pointed out that the goal is not to affect the existing community and they always look for up and coming professionals families who are looking for safe places with great schools to raise their families.

Lynne Saphir stated that she felt the site itself has not been "kept up" and she wondered if this is indicative of how the developed site would be managed. Nina Ladha provided a response, stating that they own several properties, not only in West Linn but in other locations, and she personally makes sure those rental properties are well maintained and occupied by qualified and responsible tenants.

On street parking was also an issue. Folks in general are worried about how the city dictates, manages and controls on street (curbside) parking. While the Ladha's are not able to control the on street parking, they recognize it as an issue

in the neighborhood. The current plan is for two full sized garages for each unit, plus a driveway of sufficient length to allow for parking of a third vehicle on the premises, off the street. Lynne Saphir is also worried about short term parking by contractors, workers, material suppliers, etc. and how parking would be managed during the construction period.

Angela Sullivan expressed a concern that the design of the new units, while current and contemporary in its design, may not fit with the rest of the neighborhood design. Gregg Creighton, a local resident and an architect in his own right, stated that the design reflected the current trends and styles.

Robert Price provided a brief discussion of public facilities and services to the site, and that all existing facilities and services are of adequate size, capacity, and general service levels to provide for the proposed 5 new residential units.

There were no other concerns for views expressed by others.

Nina Ladha wrapped up the meeting by stating that she would lead a short tour of local properties they owned, in order to demonstrate that the Ladha's are good, conscientious property owners and landlords. Lynne and Bill Saphir agreed that they would like to see Ali and Nina's properties.

The meeting concluded at approximately 7:10 PM.

Preliminary Storm Drainage Report

**Hoodview Summit
July 17th, 2018
Revised October 16th, 2018**

Prepared For: Ali Ladha
9334 SE Hunters Bluff Ave.
Happy Valley, Oregon 97086

Prepared By: CWK2 Land Development Consultants
10700 SW Beaverton Hillsdale Hwy., Suite 533
Beaverton, Oregon 97005

Submitted To: City of West Linn



EXPIRATION DATE: 6/30/05

13



CWK2 Project # BRT.001

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PRELIMINARY STORM DRAINAGE REPORT

HOODVIEW SUMMIT

Purpose

The purpose of this analysis is to:

- Describe existing and proposed site conditions.

Introduction/Project Overview

This report addresses the storm drainage impacts associated with the Hoodview Summit development. This property occupies approximately 0.34 acres (after proposed right-of-way dedications) and resides in City of West Linn, Oregon. Private improvements are anticipated to include five attached single family residences, driveways, wet and dry utilities, and lot grading for the proposed residences.

Existing Conditions

The site is currently vacant and is covered by low quality landscaped grasses and low-lying shrubs. Well established street trees exist along Salamo Rd., and smaller street trees are present at the north and east end of the site along Hoodview Ave., and Noble Ln. The site is bordered to the north, south and east by existing single family attached residences. The site is further bordered to the west, north, and east by Salamo Rd, Hoodview Ave., and Noble Ln. which is fully built-out with sidewalks. The site currently slopes from the south to the north with an average slope of 5 percent. (See appendix A: Existing Conditions Plan). All existing storm water runoff appears to either infiltrate into the soil or runoff to Hoodview Ave. or Noble Ln.

Proposed Conditions

The proposed Hoodview Summit development consists of five new attached single family residences. No street frontage improvements are anticipated to be required since Salamo Rd., Hoodview Ave., and Noble Ln is fully built-out with sidewalks. New impervious area on-site is anticipated to equal 7,690 sf (Which includes driveways, as well as building impervious area) Stormwater runoff associated with the new residences will be directed towards the public storm system in Noble Lane. (See appendix A: Storm Sewer Plan). A public water quality and detention facility was constructed with the Hoodview Subdivision in 2001 (As-Builts Dated 5/4/01), and modified for the Hoodview II Subdivision in 2002 (As-Builts Dated 10/14/02). The preliminary stormwater drainage report prepared

by WRG with the land use application identified this 0.34 acre undeveloped parcel as a "Future Water Tower" and assumed it's area as impervious with their detention and water quality calculations. With this proposed development, the existing facilities have already been adequately sized and water quality and detention will be provided once built out. If during the engineering phase it's determined that this development needs to provide for water quality/quantity on-site, another approvable option would be to install LIDA facilities which will detail and treat all new impervious area.

Conclusions

- Storm water will be managed per City of West Linn's storm water standards.

Appendix A:

Existing Conditions Plan

Preliminary Stormwater Plan



Bob Price <rprice1145@gmail.com>

Hoodview Preliminary Drainage Report

5 messages

William Kehrli <kehrlib@cwk2.com>

Tue, Oct 16, 2018 at 10:31 AM

To: "Lais, Erich" <elais@westlinnoregon.gov>

Cc: Bob Price <rprice1145@gmail.com>, ALI LADHA <investcorealtygroup@gmail.com>

Erich,

I've updated our preliminary stormwater drainage report based on the as-built files that both you and Teresa were able to send over to me – thanks again for your assistance. Attached is an updated copy of the report which the land use planner will be re-submitting to the City so that our application can be deemed complete. If you have any additional input I'd appreciate it

Thanks!

Bill

From: Lais, Erich [mailto:elais@westlinnoregon.gov]

Sent: Wednesday, October 10, 2018 12:12 PM

To: William Kehrli

Subject: Drainage Report

Bill,

I got your voicemail and I'm not going to have anything other than what Teresa got you. My files don't go back that far. I did find these asbuilts though for hoodview townhomes.

Sorry!

Erich Lais

Assistant City Engineer

Public Works Support

22500 Salamo Road
West Linn, Oregon 97068
elais@westlinnoregon.gov
westlinnoregon.gov
503-722-3434



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This e-mail is subject to the State Retention Schedule and may be made available to the public

 **AR-M450_20000719_003750.pdf**
189K

William Kehrli <kehrlib@cwk2.com>
To: Bob Price <rprice1145@gmail.com>
Cc: ALI LADHA <investcorealtygroup@gmail.com>

Tue, Oct 16, 2018 at 10:32 AM

Bob,

I'll send you an original copy of this report so that you can include it with your completeness packet back to the City

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From: William Kehrli [mailto:kehrlib@cwk2.com]
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To: 'Lais, Erich'
Cc: 'Bob Price'; 'ALI LADHA'
Subject: Hoodview Preliminary Drainage Report

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Hoodview Preliminary Drainage Report

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Robert Price

Planning Consultant

3935 N.E. 72nd Avenue
Portland, OR 97213-5711

503-281-1037
Cell 503-807-4009
rprice1145@gmail.com

Supplemental Narrative

This Supplemental Narrative is designed to address the issues raised in the city's letter dated August 27, 2018. In a point-by-point process, the following explanations and discussions cover everything contained in the city's letter.

1. *55.100.E: Private outdoor area*

Comment: The private outdoor area is shown on the architectural plan graphics that are part of this re-submittal. Based on these plans prepared by the project architect, the required private outdoor area meets the city's requirements. It should be noted that the private outdoor area is in the rear yards of each of the units. Out of necessity, the units are oriented to Noble Lane on the south side of the site. Salamo Road is not a viable route for main access to the site, so all access must be from Noble Lane. The use of fences on the site as a means to separate the yard areas help to provide some screening and privacy for the residents. On the landscape plan is shown the use of plantings along the fence lines which will also assist in screening and privacy.

2. *55.100.I(2): Storm detention and treatment and geologic hazards*

Comment: The project engineer, Bill Kerhli of CWK2, has been in contact with Erich Lais, Assistant City Engineer, regarding the issues raised for the storm detention and treatment. As is shown by the "*Preliminary Storm Drainage Report, Revised October 16, 2018*", and the attached e-mail exchange between Bill Kerhli and Erich Lais, "*it looks like you've addressed my questions and that your parcel is accounted for.*" The Revised Report and the e-mail thread show that this issue has been successfully addressed.

3. *55.110.B(10): Land classification*

Comment: The attached map illustrates the Land Classification(s) for this site. As such, this issue has been addressed.

4. *55.130: Grading and Drainage Plans*

Comment: Grading and Drainage Plans (*Hoodview Summit Subdivision – Construction Document Plan Set*) were initially submitted with the original application package. There have been no changes to the Plans themselves, only to the "*Preliminary Storm Drainage Report*". Therefore, no new or revised Grading and Drainage Plans have been submitted.

5. *55.140: Architectural Drawings*

Comment: New and revised architectural drawings to illustrate some of the details of the proposed style and construction of the five (5) units are included herein. These architectural drawings should meet the city's requirements.

6. 55.150: *Landscape Plan*

Comment: The Landscape Plan for the site, not included in the original submittal package, is included herein. Prepared by Harmony Design Northwest, this Landscape Plan should address all requirements and encompasses all issues discussed. Therefore, this issue has been addressed.

PC-5 Public Comments

**No public comments have been received as of the publication of this Staff
Report**