

PRIVATE IMPROVEMENT PLANS

of

SUMMERLINN APARTMENTS

for

SHOW TIMBER COMPANY

DEVELOPER:

SHOW TIMBER COMPANY
 2525 Lorinda Drive.
 West Linn, Oregon 97068
 ATTN: NEIL B. NEDELISKY
 PHONE: (503) 658-2767
 FAX: (503) 658-5416

ARCHITECT

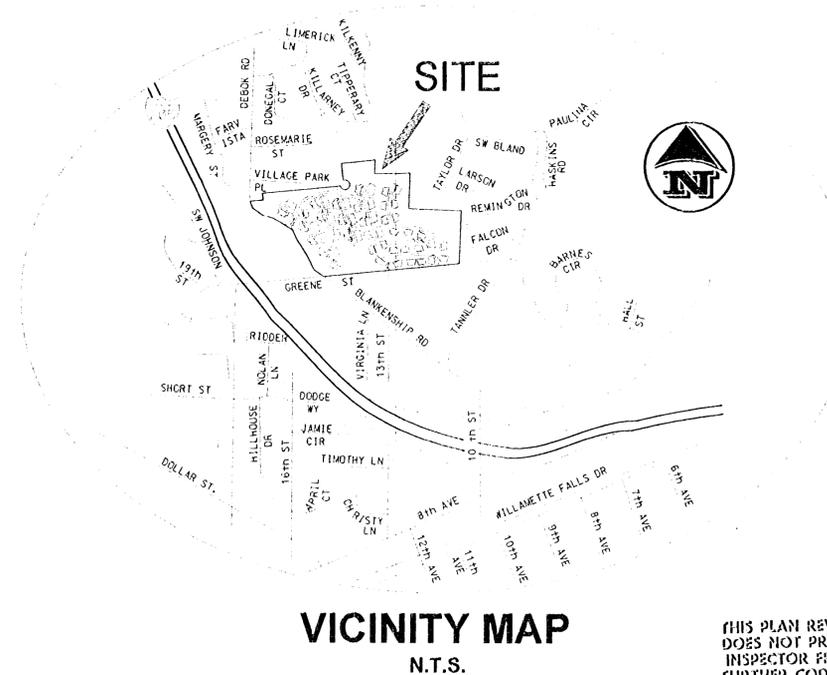
MARK E. HUMPHREYS ARCHITECT
 4370 N.E. HALSEY STREET, SUITE 235
 PORTLAND, OREGON 97213
 PHONE: (503) 249-6860

AND

L R S ARCHITECTS
 1121 S.W. SALMON STREET, SUITE 100
 PORTLAND, OREGON 97205
 PHONE: (503) 221-1121

ENGINEER:

W.B. WELLS AND ASSOC. INC.
 4230 NE FREMONT ST.
 PORTLAND, OR. 97213
 PHONE: (503) 284-5896
 FAX: (503) 284-8530



THIS PLAN REVIEW APPROVAL
 DOES NOT PREVENT THE FIELD
 INSPECTOR FROM REQUIRING
 FURTHER CODE CORRECTIONS.

SECTION 35, T. 2 S., R. 1 E., W.M.
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

**APPROVED PLANS
 NOT AS-BUILT DRAWINGS 5/7/1999**

APPROVED PLAN
 City of West Linn
 Building Department
 DATE: 5-7-99
 BY: *Thomas R. Wells*

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COVER SHEET

SUMMERLINN APARTMENTS
 CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



W.B. WELLS and associates inc.
 ENGINEERS • SURVEYORS • PLANNERS
 4230 NE FREMONT STREET
 PORTLAND, OREGON 97213
 PHONE (503) 284-5896
 FAX (503) 284-8530



GENERAL NOTES:

- ALL CONSTRUCTION AND MATERIALS SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE CITY OF WEST LINN, THE OREGON ADMINISTRATIVE RULES AND OTHER APPLICABLE CODES.
- THE CONTRACTOR AND/OR SUB-CONTRACTOR SHALL HAVE A MINIMUM OF ONE (1) SET OF APPROVED CONSTRUCTION PLANS ON THE JOB SITE AT ALL TIMES DURING CONSTRUCTION.
- THE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS AND LICENSES BEFORE STARTING CONSTRUCTION.
- NO CONSTRUCTION SHALL COMMENCE UNTIL PLANS ARE APPROVED AND ISSUED BY THE CITY OF WEST LINN.
- EXISTING UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE. THE EXISTENCE OR NON-EXISTENCE OF THE UTILITY, THE CONTRACTOR SHALL VERIFY THE EXISTENCE, TYPE AND LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION. IN CASES WHERE ACTUAL LOCATION OR INFORMATION VARIES FROM THE PLANS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF THE VARIANCE AND STOP WORK IN THE AFFECTED AREA UNTIL DIRECTED.
- THE CONTRACTOR SHALL COORDINATE WITH ALL OF THE UTILITY COMPANIES THE MARKING OF ALL EXISTING UTILITIES. COMPLIANCE WITH OREGON REVISED STATUTES 757.541 TO 757.571 IS REQUIRED. THE CITY OF WEST LINN IS A MEMBER OF ONE CALL. FOR EXISTING SANITARY SEWER AND STORM LINE, CALL (503) 216-6693.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL UTILITY SERVICES TO THE SITE: GAS, ELECTRICITY, PHONE, CABLE, ETC. ALL UTILITIES SHALL BE PLACED UNDERGROUND.
- THE CONTRACTOR SHALL DO ALL OF THE WORK SHOWN ON THE DRAWINGS AND ALL INCIDENTAL WORK CONSIDERED NECESSARY TO COMPLETE THE PROJECT IN AN ACCEPTABLE MANNER.
- STREET LIGHTING SHALL BE MADE COMPLETE AND OPERATIONAL ALONG THE PRIVATE ACCESS DRIVE. I.E. POLES INSTALLED AND WIRED. SEE STREET LIGHTING PLANS (BY OTHERS) FOR DETAILS.
- ALL MATERIAL USED SHALL BE PER THE APPROVED PLANS. ANY ALTERNATES SHALL BE APPROVED BY THE ENGINEER AND THE CITY OF WEST LINN PRIOR TO ITS USE. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO SUPPLY ALL NECESSARY INFORMATION FOR THE PROPER EVALUATION OF SUBMITTED ALTERNATIVES. ALLOW A MINIMUM OF TWO WEEKS FOR APPROVAL.
- ANY PLAN CHANGES SHALL BE SUBMITTED TO THE ENGINEER AND APPROVED BY THE CITY PRIOR TO ANY WORK. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO SUPPLY ALL NECESSARY INFORMATION FOR THE PROPER EVALUATION OF PLAN CHANGES. ALLOW A MINIMUM OF TWO WEEKS FOR APPROVAL.
- THE CONTRACTOR SHALL KEEP THE CITY, AND W. B. WELLS & ASSOC., INC. ADVISED OF HIS/HER PROGRESS SO THAT INSPECTION CAN BE PERFORMED IN AN EFFICIENT MANNER. A MINIMUM OF TWENTY-FOUR (24) HOUR NOTICE MUST BE GIVEN FOR TESTS OR INSPECTIONS.
- THE CONTRACTOR SHALL KEEP AN ACCURATE RECORD OF AS-BUILTS. SAID AS-BUILTS SHALL BE SUBMITTED TO W. B. WELLS & ASSOC., INC. UPON REQUEST.
- THE CONTRACTOR SHALL, UPON REQUEST, SUBMIT TO THE CITY AND/OR W. B. WELLS & ASSOC., INC. PROOF FROM THE MATERIAL SUPPLIERS THAT THE MATERIAL HAS BEEN TESTED AND FOUND IN COMPLIANCE WITH THE ABOVE SPECIFICATIONS. THEY SHALL CERTIFY THAT ALL MATERIALS TO BE DELIVERED TO THE JOB SITE MEET OR EXCEED SAID SPECIFICATIONS PRIOR TO DELIVERY. ANY MATERIAL NOT CERTIFIED BY THE SUPPLIER SHALL BE REMOVED FROM THE JOB SITE AT NO ADDITIONAL COST TO THE OWNER.
- THE OWNER, CITY AND W. B. WELLS & ASSOC., INC. SHALL RESERVE THE RIGHT TO CALL FOR COMPACTION TESTS. IF THE TESTS DO NOT SHOW SUITABLE RESULTS, THE CONTRACTOR SHALL CORRECT IT AT NO COST TO THE OWNER. THE COMPACTION TESTS IN TRENCHES MUST OBTAIN 95% OF AASHTO T-99 TEST METHOD.
- THE CONTRACTOR SHALL COMPLY WITH THE EROSION CONTROL (NPDES) PERMIT PER D.E.Q. REQUIREMENTS. EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY WORK. NON-COMPLIANCE IS THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR IS RESPONSIBLE FOR MANAGING CONSTRUCTION ACTIVITIES TO INSURE ADJACENT PUBLIC STREETS ARE KEPT CLEAR OF MUD, DEBRIS, AND DUST TO THE SATISFACTION OF THE CITY OF LAKE OSWEGO. THE CONTRACTOR SHALL LEAVE ALL AREAS OF THE PROJECT FREE OF DEBRIS AND ANY UNUSED CONSTRUCTION MATERIALS.
- DURING SUMMER TIME CONSTRUCTION, THE CONTRACTOR SHALL CONTROL DUST FROM THE CONSTRUCTION ACTIVITIES.
- TRAFFIC DURING CONSTRUCTION SHALL BE CONTROLLED IN ACCORDANCE WITH THE M.U.T.C.D. AND THE CITY OF WEST LINN REQUIREMENTS.
- CONTRACTOR SHALL REMOVE ALL EXCESS MATERIAL FROM THE SITE AFTER APPROVAL FROM THE OWNER OR THE OWNER'S REPRESENTATIVE. SUCH MATERIALS SHALL BE DISPOSED OF AT AN APPROVED SITE.
- CONTRACTOR IS RESPONSIBLE FOR REMOVAL OF ALL GRUBBINGS, BRUSH, TREES (AS SPECIFIED), STUMPS, DEBRIS AND TRASH FROM THE SITE. ON-SITE BURIAL IS NOT ALLOWED. CONTRACTOR SHALL DISPOSE OF ALL SUCH MATERIALS AT AN APPROVED DISPOSAL LOCATION.
- CONTRACTOR SHALL NOT REMOVE ANY TREES OVER 5 INCHES IN DIAMETER, AT 4.5 FEET ABOVE GROUND. OTHERWISE A TREE CUTTING PERMIT SHALL BE SECURED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL OBTAIN A GRADING PERMIT FROM THE CITY AND ADHERE TO THE PROVISIONS THEREIN. ALL STRIPPINGS AND WASTE MATERIALS SHALL BE REMOVED FROM THE PROJECT SITE UNLESS OTHERWISE AUTHORIZED BY THE OWNER.
- ALL EARTHWORK AND GRADING SHALL COMPLY WITH SECTION 204 OF THE A.P.W.A. SPECIFICATIONS AND CHAPTER 70 OF THE UNIFORM BUILDING CODE.
- ANY STRUCTURAL FILLS UNDER BUILDINGS, PARKING, STREETS, ETC. SHALL BE CONSTRUCTED OF MATERIAL FREE FROM ORGANICS, FROZEN MATERIAL, CONCRETE, ASPHALTIC CONCRETE AND OTHER DEBRIS. FILLS SHALL CONSIST OF AN APPROVED ALL-WEATHER MATERIAL (I.E. ROCK, CLEAN NATIVE SOIL).

- FILLS SHALL BE CONSTRUCTED IN SUCCESSIVE HORIZONTAL LAYERS NOT EXCEEDING EIGHT (8) INCHES IN LOOSE THICKNESS. THE USE OF LOADED HAULING EQUIPMENT IN ALL BUT THE LAST TWO (2) FEET OF FILL WILL BE ACCEPTABLE FOR COMPACTION, PROVIDED THE REQUIRED DENSITY IS OBTAINED THROUGHOUT THE WIDTH AND DEPTH OF EACH LAYER. THE TOP TWO (2) FEET SHALL BE COMPACTED USING SUITABLE MECHANICAL COMPACTORS.
- THE SUBGRADE FOR ALL PRIVATE PARKING AREAS, TRAVEL WAYS, AND BUILDING PADS SHALL BE PREPARED AND BROUGHT TO GRADE BY THE CONTRACTOR TO MEET 95% OF AASHTO T-180. SUBGRADE UNDER PUBLIC IMPROVEMENTS SHALL MEET THE ACCEPTABLE RATE OF 95% OF AASHTO T-180.
- ALL BUT THE TOP TWO (2) FEET OF FILL UNDER PRIVATE IMPROVEMENTS SHALL MEET 90% OF AASHTO T-180 FOR COMPACTION. THE TOP TWO (2) FEET SHALL MEET 95% OF AASHTO T-180. ALL EXCAVATED CUT AREAS AND FILLS UNDER PUBLIC IMPROVEMENTS SHALL MEET THE ACCEPTABLE RATE OF 95% T-180.
- THE MATERIAL IN EACH LAYER SHALL BE OF A MOISTURE CONTENT TO MEET THE ABOVE COMPACTION REQUIREMENT. WETTING OR DRYING OF THE MATERIAL AND MANIPULATION TO SECURE A UNIFORM MOISTURE CONTENT THROUGHOUT THE LAYER MAY BE REQUIRED. SHOULD THE MATERIALS BE TOO WET TO PERMIT PROPER COMPACTION BY ROLLING, ALL WORK ON ALL PORTIONS OF THE FILL THUS AFFECTED SHALL BE DELAYED UNTIL THE MATERIAL HAS DRIED TO AN ACCEPTABLE MOISTURE CONTENT. WHERE APPLICABLE THE SITE GEOTECHNICAL ENGINEER SHALL VERIFY.
- ALL PIPE THAT IS INSTALLED AT A SLOPE GREATER THAN 20% WILL HAVE CONCRETE PIPE ANCHORS INSTALLED PER APWA DRAWING NO. 303 ON SHEET 11 OF 12.

STORM SEWER NOTES:

- STORM SEWER PIPES SHALL BE ADS N-12 POLYETHYLENE PIPES OR OTHER APPROVED EQUIVALENT BY THE CITY. ALL PIPES SHALL HAVE WATER-TIGHT CONNECTORS, EXCEPT WHERE DESIGNATED ON THE PLANS.
- PRIVATE STORM SEWER CONSTRUCTION AND MATERIALS SHALL CONFORM TO THE UNIFORM PLUMBING CODE AND ALL OTHER APPLICABLE SPECIFICATIONS.
- CATCH BASINS, INLETS, PIPE BEDDING AND TRENCH BACKFILL SHALL BE AS SHOWN ON THE DETAIL SHEETS AND MEET U.P.C.
- MAINTAIN A MINIMUM OF 2.5 FEET OF COVER OR AS PER PLAN.
- PRIVATE SEWER REQUIRES A CLEANOUT EVERY 100 FEET, AND AS SHOWN ON THE PLANS.
- ALL BUILDINGS SHALL DRAIN TO THE PRIVATE STORM SYSTEM AS SHOWN. STUB OUT SUPPLIED TO WITHIN 5' OF FOUNDATION.
- ALL LINES SHALL BE TESTED AND ACCEPTED PER CITY REQUIREMENTS.
- ROCK PROTECTION AT OUTFALLS SHALL BE TYPE 1 REASONABLY WELL GRADED WITH ROCK GRADATION AS FOLLOWS: MAX. STONE SIZE=6", MIN. STONE SIZE=2".
- STORM SEWER MANHOLES AND DETENTION PIPES MAY BE CONCRETE OR C.M.P. MATERIAL TO BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- CONTRACTOR TO COORDINATE STORM SEWER INSTALLATION WITH BUILDING ARCHITECTURAL AND MECHANICAL DESIGN PLANS.

SANITARY SEWER NOTES:

- ALL SANITARY SEWER LATERAL PIPES WITHIN THE STREET RIGHT-OF-WAY SHALL BE 3034 SDR 35 P.V.C. PIPE OR AN APPROVED EQUIVALENT BY THE CITY TO MEET ASTM P-3034-80 FOR MATERIAL INSTALLATION.
- ALL SERVICE LATERALS ARE TO BE 6" OR AS SHOWN, 3034 P.V.C. AT A MIN. SLOPE OF 2% UNLESS OTHERWISE SHOWN ON THE PLANS. LATERALS SHALL BE EXTENDED TO WITHIN 5' OF THE BUILDING FOUNDATION.
- SANITARY SEWER LATERALS SHALL BE IDENTIFIED BY A 2"x4" MARKER EXTENDING TO A POINT AT LEAST ONE (1) FOOT ABOVE THE FINISHED GROUND SURFACE. THE 2"x4" MARKER SHALL BE PAINTED GREEN, AND THE DEPTH NOTED ON THE MARKER.
- 14 GAUGE TRACER WIRE AND LOCATION TAPE WITH PROPER COLOR MUST BE LAID ON ALL P.V.C. AND/OR A.D.S. PIPES WHEN INSTALLING LINE, AND TO BELOW FRAMES OF MANHOLES OR CLEANOUTS WHEN LAYING NEW LINES.
- PIPE BEDDING SHALL CONFORM TO THE MATERIAL REQUIREMENTS OF THE CITY OF WEST LINN STANDARD.
- BACKFILL FOR TRENCHES WITHIN THE RIGHT-OF-WAY OR PAVED AREAS SHALL CONFORM TO THE MATERIAL REQUIREMENTS OF THE CITY OF WEST LINN STANDARD. BACKFILL OF THE TRENCHES SHALL BE GRANULAR (CLASS 4) AND COMPACTED AS SPECIFIED FOR THE SUBGRADE AND BASE. BACKFILL SHALL BE CONSIDERED A PART OF THE TRENCH CONSTRUCTION.
- TRENCHES OUTSIDE OF THE RIGHT-OF-WAY OR PAVED AREAS MAY BE BACKFILLED WITH SELECT NATIVE MATERIAL EXCAVATED FROM THE TRENCH IN ACCORDANCE WITH THE CITY OF WEST LINN STANDARD. NO WATER SETTLING OF TRENCH BACKFILL MATERIAL SHALL BE ALLOWED.
- WEST LINN REQUIRES THAT LINE AND GRADE OF ALL SANITARY SEWER LINES MEET A.P.W.A. SPECIFICATION 303.3.01 "LINE AND GRADE".
- THE CONTRACTOR SHALL PRESSURE TEST ALL LINES PER CITY PROCEDURES OR AN APPROVED EQUIVALENT. ALL SEWER LINES SHALL AIR TESTED PER A.P.W.A. DIVISION III, SECTION 503.5.16. A MANDRAL WILL BE REQUIRED TO CHECK ALL NEW LINES. TESTS SHALL BE OBSERVED BY THE ENGINEER, INSPECTOR AND CITY INSPECTORS.
- PRIVATE SEWER REQUIRES A CLEANOUT EVERY 100 FEET, AND AS SHOWN ON THE PLANS.
- CONTRACTOR TO COORDINATE SAN. SEWER INSTALLATION WITH BUILDING ARCHITECTURAL AND MECHANICAL DESIGN PLANS.

WATERLINE NOTES:

- WATER LINE / SERVICE SPECIFICATIONS & INFORMATION FOR WATER SYSTEM CONSTRUCTION (CITY WATER)
- WATER MAINS AND SERVICE LATERALS SHALL BE C900-81 PIPE CONFORMING TO AWMA CLASS 150. JOINTS SHALL BE GASKETED. PIPE FITTINGS SHALL BE CAST DUCTILE IRON, CONFORMING TO AWMA C110 OR C153. PIPE SHALL BE LAID IN SUCH A WAY THAT IT IS SUPPORTED ALONG ITS FULL LENGTH, INCLUDING DIGGING OF BELL HOLES.
 - WATER MAINS AND SERVICE LATERALS SHALL HAVE A MINIMUM COVER OF 36".
 - THRUST BLOCKS SHALL BE PROVIDED AT ALL CHANGES IN DIRECTION AND BRANCHES. THRUST BLOCK CONCRETE STRENGTH SHALL BE 2000 PSI. POUR THRUST BLOCKS AGAINST UNDISTURBED EARTH.
 - TAPPING SLEEVES SHALL ALL BE STAINLESS STEEL WITH STAINLESS STEEL BOLTS OF CAST IRON (DUCTILE IRON) MECHANICAL JOINT SLEEVES.
 - GRANULAR MATERIAL USED FOR PIPE BASE, PIPE ZONE AND TRENCH BACKFILL SHALL BE 3/4"-0 CRUSHED ROCK. PIPE BASE SHALL BE A MINIMUM OF 4" DEEP AT THE PIPE BARREL, AND NOT LESS THAN 3" DEEP AT THE BELL. GRANULAR BACKFILL SHALL BE COMPACTED TO 95% MAXIMUM DENSITY PER AASHTO T99 TEST METHOD AND NATIVE MATERIAL SHALL BE COMPACTED TO 85% OF IN-PLACE DENSITY OF SURROUNDING SOIL. BACKFILL UNDER STREETS SHALL BE IN ACCORDANCE WITH CLASS "A" BACKFILL AS INDICATED ON THE PLANS. NO WATER SETTLING OF TRENCH BACKFILL SHALL BE ALLOWED.
 - SERVICE LATERALS 1" OR SMALLER SHALL HAVE HIGH MOLECULAR PVC COMPRESSION FITTINGS.
 - INSPECTION OF THE WATER IMPROVEMENTS WILL BE MADE BY THE CITY PERSONNEL OR OTHERS DESIGNATED BY THE CITY. CONTRACTOR IS RESPONSIBLE TO CONTACT THE PROPER INSPECTOR A MINIMUM OF 48 HOURS PRIOR TO INSPECTION.
 - ALL MATERIALS, INSTALLATION, TESTS AND CHLORINATION SHALL BE IN STRICT ACCORDANCE WITH THE STANDARDS AND CODES OF THE CITY OF WEST LINN AND THE OREGON STATE HEALTH DIVISION ADMINISTRATIVE RULES, CHAPTER 333.
 - THE CITY HAS NOT PROVIDED THE ENGINEER WITH ADEQUATE INFORMATION FOR PROPERLY DESIGNING THE WATER SYSTEM. IF THE SYSTEM DOES NOT PROVIDE ADEQUATE PRESSURE OR VOLUME BOOSTER PUMPS WILL BE REQUIRED. THESE PLANS WILL BE MODIFIED AS MORE INFORMATION IS PROVIDED BY THE CITY.
 - THE FIRE PROTECTION WATER SYSTEM AND HYDRANTS WILL BE COMPLETED, TESTED AND APPROVED PRIOR TO BRINGING ANY COMBUSTIBLE MATERIALS ON SITE. PER THE FIRE MARSHALL, LOCATION OF THE HYDRANTS SHALL BE REVIEWED AND APPROVED BY THE MARSHALL.
 - CONTRACTOR TO COORDINATE WATER INSTALLATION WITH BUILDING ARCHITECTURAL AND MECHANICAL DESIGN PLANS.

BENCH MARK:

- THE EXISTING CONTOURS SHOWN ON THESE PLANS ARE DRAWN AT A TOLERANCE OF PLUS OR MINUS ONE FOOT. TOPOGRAPHIC SURVEY PROVIDED BY W. B. WELLS & ASSOCIATES, INC.
- THE BASIS OF DATUM IS PER O.S.H.D. BENCH MARK RM2 "L631", BRASS DISK LOCATED OVER THE SOUTH BOUND LANES OF I-205, IN THE N'LY SIDE OF THE OVERPASS STRUCTURE FOR BLANKENSHIP ROAD, AT THE BASE OF THE HANDRAIL. THE ELEVATION IS 175.76'

ABBREVIATION:

A.C.	ASPHALTIC CONCRETE	MIN.	MINIMUM
A.P.	ANGLE POINT	M.S.L.	MEAN SEA LEVEL
APPROX.	APPROXIMATE	N.T.S.	NOT TO SCALE
APT.	APARTMENT	PAV.	PAVEMENT
AV.	AVENUE	P.C.	POINT OF CURVATURE
C.B.	CATCH BASIN	P.C.C.	POINT OF COMPOUND CURVATURE
CH	CHORD	PRC	POINT OF REVERSE CURVATURE
CHB	CHORD BEARING	P.T.	POINT OF TANGENCY
C/L	CENTERLINE	P.T.	POINT
CONC.	CONCRETE	PVC	POLYVINYL CHLORIDE PIPE
DIA.	DIAMETER	R	RADIUS
DR.	DRIVE	RD.	ROAD
DWG.	DRAWING	RT.	RIGHT
EL	ELEVATION	S	SLOPE
ELEV.	ELEVATION	SAN.	SANITARY
E/P	EDGE OF PAVEMENT	ST.	STREET
ESMT	EASEMENT	STA.	STATION
EXTG.	EXISTING	STM.	STORM
F.G.	FINISH GRADE	SVC.	SERVICE
G.L.	GROUND LINE	S/W	SIDEWALK
GUT	GUTTER	T/C	TOP OF CURB
H.P.	HIGH POINT	TL	TAX LOT
HORIZ.	HORIZONTAL	TYP.	TYPICAL
I.E.	INVERT ELEVATION	U.G.	UNDERGROUND
L	LENGTH/ARC LENGTH	VERT.	VERTICAL
LF	LINEAR FEET	W/	WITH
LOC.	LOCATION OR LOCATED	X/G	EXISTING GROUND
LT.	LEFT	Δ	INCLUDED ANGLE (DELTA)

CONSULTANT NOTES:

THE CONTRACTOR IS RESPONSIBLE OF OBTAINING AND BEING FAMILIAR WITH ALL PLANS, REPORTS, SPECIFICATIONS AND RECOMMENDATIONS FROM THE FOLLOWING CONSULTANTS FOR THIS PROJECT. THEY SHALL INCORPORATE ALL CONSULTANT RECOMMENDATIONS FROM THE PREVIOUSLY MENTIONED SAID INFORMATION INTO THE CONSTRUCTION PRACTICES FOR THIS PROJECT. ALL INFORMATION IS AVAILABLE TO THE CONTRACTOR UPON REQUEST VIA:

FINAL SITE PLAN AND ARCHITECTURAL BUILDING PLANS BY:
MARK E. HUMPHREYS ARCHITECT - BUILDINGS 24 - 51
4370 N.E. HALSEY STREET, SUITE 235
PORTLAND OR, 97213
TELE: (503) 249-6860

L R S ARCHITECTS - BUILDINGS 1 - 21
1121 S.W. SALMON STREET, SUITE 100
PORTLAND, OR, 97205
TELE: 503-221-1121 ATTN: MIKE GORMAN

GEOTECHNICAL SITE EVALUATION AND REPORT PREPARED BY:
GEOTECHNICAL RESOURCES INCORPORATED
9725 S.W. BEAVERTON-HILLSDALE HWY, SUITE 140
BEAVERTON, OR, 97005
TELE: 503-641-3478 ATTN: DAVE DRISCOLL, P.E.

THE LANDSCAPE PLANS PREPARED BY:
CHRISTOPHER FRESHLEY LANDSCAPE ARCHITECT - BUILDINGS 1 - 21
1020 S.W. TAYLOR STREET, SUITE 355
PORTLAND, OR, 97205
TELE: 503-222-9881 ATTN: CHRIS FRESHLEY

SITE STRUCTURAL ENGINEERING PLANS PREPARED BY:
CONLEE ENGINEERS, INC.
1509 SW BERTHA BLVD.
PORTLAND OR, 97219
TELE: 503-244-0579 ATTN: DOUG GANNETT

THE WETLANDS DELINEATION AND MITIGATION PLANS PREPARED BY:
PACIFIC HABITAT SERVICES, INC.
9450 SW COMMERCE CIRCLE
WILSONVILLE, OR, 97070
TELE: 503-570-0800 ATTN: JOHN VAN STAVERN

LEGEND:

	EXISTING ASPHALT PAVEMENT		PROPOSED CONCRETE DRIVEWAY
	EXISTING CONCRETE SIDEWALK		PROPOSED CONCRETE SIDEWALK
	EXISTING MAIL BOX		PROPOSED MAIL BOX
	EXISTING ROAD SIGN		PROPOSED ROAD SIGN
	EXISTING ROAD CENTERLINE		PROPOSED ROAD CENTERLINE
	EXISTING LOT LINE		PROPOSED LOT LINE
	EXISTING GUY ANCHOR		PROPOSED GUY ANCHOR
	EXISTING OVERHEAD POWER LINE		PROPOSED OVERHEAD POWER LINE
	EXISTING POWER POLE		PROPOSED POWER POLE
	EXISTING SANITARY MANHOLE		PROPOSED SANITARY MANHOLE
	EXISTING SANITARY SEWER LINE		PROPOSED SANITARY SEWER LINE
	EXISTING DITCH INLET		PROPOSED DITCH INLET
	EXISTING STORM SEWER LINE		PROPOSED STORM SEWER LINE
	EXISTING GAS VALVE		PROPOSED GAS VALVE
	EXISTING UNDERGROUND GAS LINE		PROPOSED UNDERGROUND GAS LINE
	EXISTING FIRE HYDRANT		PROPOSED FIRE HYDRANT
	EXISTING WATER LINE		PROPOSED WATER LINE
	EXISTING WATER METER		PROPOSED WATER METER
	EXISTING CONIFEROUS TREE		PROPOSED CONIFEROUS TREE
	EXISTING DECIDUOUS TREE		PROPOSED DECIDUOUS TREE
	EXISTING LANDSCAPING		PROPOSED LANDSCAPING
	EXISTING CLEANOUT		PROPOSED CLEANOUT

CONTACT TELEPHONE NUMBERS:

NORTHWEST NATURAL GAS	(503) 220-2415
PORTLAND GENERAL ELECTRIC	(503) 671-1234
T.C.I. CABLEVISION	(503) 605-4985
U.S. WEST TELEPHONE	(503) 242-4325
CITY OF WEST LINN	(503) 656-4211

REVISITONS	3-20-99 ADDED GENERAL NOTE # 30
DESIGNED: BSS	CHECKED: BSS
DRAWN: BSS/JRT	DATE: 11-20-98
JOB NO.: 96-340	SCALE:

CONSTRUCTION NOTES

for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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and associates inc.
ENGINEERS - SURVEYORS - PLANNERS
4230 NE FREMONT STREET
PORTLAND, OREGON 97213
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FAX (503) 284-8580



BEFORE THE LAND USE HEARINGS OFFICER
OF THE CITY OF WEST LINN, OREGON
FINAL ORDER DR-97-36/ZC-97-04/MISC-97-60 1
(SUMMERLINN APARTMENTS)

REGARDING APPLICATIONS BY SHOW FIMBER COMPANY FOR
DESIGN REVIEW AND PLANNED DEVELOPMENT APPROVAL,
TREE REMOVAL AND NATURAL AREA DEVELOPMENT FOR A
438-UNIT APARTMENT IN THE R-2.1 ZONE NORTHEAST OF
DEBOK ROAD AT 15TH AND GREENE STREETS IN WEST LINN, OREGON

IV. CONCLUSION AND ORDER

BASED ON THE ABOVE FINDINGS, THE HEARINGS OFFICER CONCLUDES THESE
APPLICATIONS SHOULD BE APPROVED SUBJECT TO THE CONDITIONS RECOMMENDED
BY CITY STAFF AS AMENDED TO BE CONSISTENT WITH THE ABOVE DISCUSSION.

CASEFILE DR-97-36/ZC-97-04/MISC-97-60 IS HEREBY APPROVED,
SUBJECT TO THE CONDITIONS OF APPROVAL IN THE IN THE CITY OF WEST LINN
STAFF REPORT AND RECOMMENDATIONS TO THE HEARINGS OFFICER DATED
JANUARY 28, 1998 WITH THE FOLLOWING MODIFICATIONS:

1. CONDITION OF APPROVAL 1 IS HEREBY DELETED.

2. A. CONSTRUCT A MINIMUM FOUR-FOOT WIDE, HARD SURFACED PEDESTRIAN
PATH BETWEEN THE DEBAUCH ROAD ENTRANCE AND THE NORTH
END OF THE NORTH-SOUTH SECTION OF THE MAIN ACCESS
DRIVEWAY. THE PATH SHALL BE SUBSTANTIALLY PARALLEL TO THE
EAST-WEST SECTION OF THE MAIN ACCESS DRIVEWAY. IF THE
PATH ABUTS THE DRIVEWAY AND IS NOT GRADE-SEPARATED
(E.G., BY A FULL CURB), THE IT SHALL BE CONSTRUCTED WITH A
CONTRASTING PAVING MATERIAL AND DESIGNED AND BUILT TO
ACCOMMODATE WEIGHT LOADS OF EMERGENCY VEHICLES AND
DELIVERY TRUCKS.

B. EXTEND AT LEAST ONE OF THE NORTH-SOUTH WALKWAYS TO THE
NORTH BOUNDARY OF THE SITE TO PROVIDE ACCESS TO THE
PUBLIC PARK.

C. THE APPLICANT SHALL GRANT TO THE CITY AN EASEMENT FOR
PEDESTRIAN AND BICYCLE ACCESS PURPOSES (1) FROM THE
GREENE STREET FRONTAGE OF THE SITE TO THE ACCESS POINT TO
THE PARK NORTH OF THE SITE OVER AT LEAST THE WESTERLY OFF-
ROAD PATHWAY PARALLEL TO THE MAIN NORTH-SOUTH DRIVEWAY,
AND (2) FROM THE DEBAUCH ROAD FRONTAGE TO THE NORTH
END OF THE NORTH-SOUTH SECTION OF THE MAIN ACCESS
DRIVEWAY OVER THE EAST-WEST PATHWAY REQUIRED IN
CONDITION 2.A. THE EASEMENTS SHALL ALLOW PUBLIC ACCESS
ON A DAILY BASIS BETWEEN 6 A.M. AND 9 P.M. THE
APPLICANT SHALL INSTALL SIGNS OR OTHER READILY APPARENT
MARKERS IDENTIFYING THE PUBLIC PATHWAYS AS SUCH, AND
MAY REQUIRE THAT PUBLIC USERS KEEP TO THE PATHWAYS.

3. DEDICATE AND/OR DEED SUFFICIENT LAND TO THE CITY AT THE
NORTHEAST CORNER OF THE QUARRY AREA TO ACCOMMODATE A LINK
BETWEEN THE EXISTING TRAIL FROM TANNER CREST SUBDIVISION TO
THE EXISTING TRAIL FROM REMINGTON DRIVE. THE APPLICANT SHALL
CONSTRUCT AN ASPHALT PATH APPROVED BY THE CITY ENGINEER TO
LINK THESE TWO TRAILS SEGMENTS.

4. PREPARE AND SUBMIT AN ANALYSIS TO THE PLANNING DIRECTOR OF THE
POTENTIAL ADVERSE ENVIRONMENTAL IMPACTS OF THE PROPOSED
BRIDGE OVER THE DRAINAGEWAY. THE ANALYSIS SHALL IDENTIFY
MEASURES WARRANTED TO PREVENT, REDUCE OR MITIGATE SUCH
POTENTIAL ADVERSE EFFECTS. THE APPLICANT SHALL CONSTRUCT A SMALL
BRIDGE ACROSS THE DRAINAGEWAY TO PROVIDE A PEDESTRIAN CROSSING
FOR THE PUBLIC ACCESS TRAIL CONNECTION TO THE ADJACENT CITY PARK
AS REQUIRED BY CONDITION 2. THE APPLICANT SHALL INCORPORATE
INTO THE BRIDGE AND ITS CONSTRUCTION PROCESS THE MEASURES
IDENTIFIED IN THE ANALYSIS TO AVOID, REDUCE OR MITIGATE THE
ADVERSE ENVIRONMENTAL IMPACTS OF THE BRIDGE, EXCEPT AS
OTHERWISE APPROVED BY THE PLANNING DIRECTOR. THE APPLICANT
SHALL PREPARE ALL REQUIRED STUDIES AND MITIGATION PLANS PURSUANT
TO CDC CHAPTERS 30 AND 32 FOR APPROVAL BY THE PLANNING
DIRECTOR AND SHALL IMPLEMENT THOSE STUDIES AND PLANS AS
DIRECTED.

5. THE APPLICANT SHALL PETITION FOR VACATION OF THE PORTION OF THE
GREENE STREET RIGHT OF WAY ABUTTING THE SITE. THE CITY SHALL
NOT AUTHORIZE OCCUPANCY OF ANY DWELLING UNITS ON THE SITE UNTIL
THE VACATION IS APPROVED OR UNTIL THE PLANNING DIRECTOR FINDS
THE ISSUE OF GREENE STREET IS OTHERWISE RESOLVED. THE
APPLICANT SHALL CONSTRUCT A FOUR-FOOT WIDE GRAVEL PATH WITHIN
AN EASEMENT RETAINED BY THE CITY AS A CONDITION OF THE VACATION
OF THE RIGHT OF WAY. IF THE RIGHT OF WAY IS NOT VACATED, THE
APPLICANT SHALL INSTALL HALF-STREET IMPROVEMENTS CONSISTENT WITH
THE CDC OR APPLY FOR AND RECEIVE APPROVAL OF VARIANCE FROM
THE CITY.

6. THE APPLICANT SHALL SUBMIT FRONT ELEVATION DRAWINGS OF THE FIVE
BUILDING TYPES SHOWING A MINIMUM THREE-FOOT PROJECTION
ACROSS AT LEAST TWO-THIRDS OF THE GARAGE DOOR ELEVATION OF ALL
STREET-FACING GARAGES. SUCH PROJECTIONS MAY INCLUDE A
PROJECTING SECOND STORY, PROJECTING BALCONIES, TRELLISES,
AWNINGS OR SIMILAR FEATURES, ATTACHED TO BUILDING OR FREE
STANDING. THE SUBMITTED PLANS SHALL BE APPROVED BY THE
PLANNING DIRECTOR BEFORE THE CITY APPROVES BUILDING PERMITS
FOR THE STRUCTURES IN QUESTION. EXCEPT FOR THESE MODIFICATIONS,
ALL BUILDING TYPES SHALL BE CONSTRUCTED AS SHOWN IN THE
APPLICANT'S SUBMITTAL, INCLUDING THE AMENDMENT PROPOSED AT
THE JANUARY 28 HEARING (EXHIBIT HO-13). COLOR AND MATERIAL
BOARDS SHALL BE OF EQUAL OR SUPERIOR BUILDING MATERIALS AND
SHALL BE APPROVED BY THE PLANNING DIRECTOR.

7. THE TRANSITION AREA FOR NATURAL DRAINAGEWAY AND THE AREA NORTH
OF THE DRAINAGEWAY SHALL BE DEEDED OR DEDICATED TO THE CITY OF
WEST LINN.

8. CONDITION OF APPROVAL 8 IS HEREBY DELETED.

9. ALL SIGNIFICANT TREES, EXCEPT THOSE IDENTIFIED FOR REMOVAL BY THE
CITY ARBORIST, SHALL BE PROTECTED BY A SNOW FENCE 10 FEET
BEYOND THE DRIPLINE PRIOR TO SITE CLEARING AND GRUBBING.

10. THE NATURAL DRAINAGEWAY SHALL BE PROTECTED FROM
ENCROACHMENT BY A SNOW FENCE IN ADDITION TO ALL NECESSARY
EROSION CONTROL MEASURES.

11. THE APPLICANT SHALL PROVIDE A PLAN FOR FULL "INCH BY INCH"
MITIGATION FOR LOSS OF ANY SIGNIFICANT TREE(S) THE CITY ARBORIST
AUTHORIZES THE APPLICATION TO REMOVE. THIS PLAN SHALL BE
REVIEWED BY THE CITY ARBORIST.

12. THE APPLICANT SHALL RE-DESIGN PARKING STALLS SO THAT A 50/50
SPLIT OF COMPACT AND FULL SIZED SPACES IS PROVIDED. ANY AREAS
GAINED SHALL BE LANDSCAPED OR USED FOR PEDESTRIAN FACILITIES.
THIS NEW DESIGN SHALL BE APPROVED BY THE PLANNING DIRECTOR.

13. DESIGNATED VISITOR PARKING SHALL BE EVENLY DISTRIBUTED
THROUGHOUT THE PROJECT SITE, PURSUANT TO A PLAN APPROVED BY THE
PLANNING DIRECTOR. APPROVED GUEST PARKING SPACES MUST BE DESIGNATED
AS SUCH ON THE SITE CONSISTENT WITH CDC 46.150.A.16.

14. MINOR ADJUSTMENTS TO THE ORIENTATION OF BUILDINGS OR THEIR
DESIGN SHALL BE PERMITTED ONLY AFTER APPROVAL BY THE PLANNING
DIRECTOR. ADDING UNITS IS NOT PERMITTED. ELIMINATING UNITS IS
PERMITTED, BUT THE TOTAL NUMBER OF UNITS MAY NOT GO UNDER 421
UNITS (80% RULE). MINOR SHIFTS IN FOOTPRINT MAY BE PERMITTED IF
IT IS DEMONSTRATED TO THE PLANNING DIRECTOR THAT EITHER
TOPOGRAPHY OR VEGETATION IS BETTER PROTECTED, GRADING IS
REDUCED, OR SUPERIOR DESIGN IS ACHIEVED.

15. THE PROJECT SHALL BE CONSTRUCTED CONSISTENT WITH THE SUBMITTED
COMPOSITE SITE PLAN (SHEET 9 OF 30) OF THE APPLICATION WITH THE
AMENDMENT SHOWN IN EXHIBIT HO-13, UNLESS SPECIFICALLY
EXEMPTED UNDER CONDITIONS OF APPROVAL.

16. THE APPLICANT SHALL CONFORM TO ALL FEDERAL, STATE AND LOCAL
POLICIES AND CODES UNLESS GRANTED A WRITTEN WAIVER,
MODIFICATION AND/OR VARIANCE BY THE APPROPRIATE DECIDING
BODY.

17. STREETS- THE APPLICANT SHALL: 1) DEED OR DEDICATE, ALONG THE
DEVELOPMENT'S DEBOK ROAD FRONTAGE, HALF THE COLLECTOR STREET
60-FOOT RIGHT-OF-WAY; 2) CONSTRUCT HALF STREET IMPROVEMENTS
ALONG DEBOK AND BLANKENSHIP ROADS CONSISTENT WITH THE 10TH
STREET CORRIDOR STUDY BUILD-OUT PAVEMENT WIDTH REQUIREMENTS
AND CHAPTER 92 OF THE WEST LINN COMMUNITY DEVELOPMENT
CODE. (THE PLANTER STRIP AND SIDEWALK WIDTHS MAY BE ADJUSTED
IN ORDER TO PROTECT TREES, SAVE AN EXISTING STORM WATER
DRAINAGEWAY AND/OR AVOID NON-APPLICANT CONTROLLED STREET
RIGHT-OF-WAY CONSTRAINTS); 3) IMPROVE 13TH STREET FROM THE
DEVELOPMENT SITE TO BLANKENSHIP ROAD ACCORDING TO THE CITY
ENGINEER'S REQUIREMENTS; 4) SUBMIT AND PROCESS A VACATION
REQUEST FOR THE GREENE STREET RIGHT-OF-WAY; 5) DELINEATE AND
INSTALL A GRAVEL PEDESTRIAN TRAIL WITHIN THE CENTER 20 FEET OF THE
EXISTING GREENE STREET RIGHT-OF-WAY (THE APPLICANT SHALL SUBMIT
A PRIVATE-PUBLIC DELINEATION PLAN FOR THIS PATHWAY PER CDC
55.100.G TO THE CITY ENGINEER AND PLANNING DIRECTOR FOR
APPROVAL); 6) CONSTRUCT THE 10TH STREET CORRIDOR STREET
IMPROVEMENTS REQUIRED BY THE CITY TRAFFIC STUDY CURRENTLY
BEING DEVELOPED BY THE TRAFFIC ENGINEERING CONSULTANT,
KITTELSON & ASSOCIATES (MINIMUM IMPROVEMENTS FOR THE
SUMMERLINN APARTMENTS DEVELOPMENT SHALL BE THE
CONSTRUCTION OF THE TWO TRAFFIC SIGNAL LIGHTS AND ASSOCIATED
IMPROVEMENTS AT THE WEST BOUND 1-205 FREEWAY OFF-RAMP AND
10TH STREET, AND THE 10TH STREET/SALAMO ROAD/ BLANKENSHIP
ROAD INTERSECTIONS AND SIDEWALK BETWEEN RIVER FALLS
SHOPPING CENTER SIDEWALK AND 8TH AVENUE; 7) GRANT TOWING
AND TICKETING ENFORCEMENT RIGHTS ON THE FIRE, LIFE AND SAFETY
ACCESS CORRIDORS WITHIN THE DEVELOPMENT; 8) CONSTRUCT THE
PRIVATE PARKING/DRIVEWAY ISLES AND FIRE TURNAROUNDS TO NOT
EXCEED 15% AND 8% GRADES RESPECTIVELY; AND 9) DESIGN AND
CONSTRUCT A SIGNAGE PLAN (FOR THE PLANNING DIRECTOR'S AND THE
CITY ENGINEER'S APPROVAL) TO ADEQUATELY DELINEATE THE CHANGES
FROM PUBLIC ACCESS TO PRIVATE ACCESS FOR DRIVEWAY ISLES AND
PEDESTRIAN PATHS FOR THE DEVELOPMENT.

18. STORM. THE APPLICANT SHALL: 1) DESIGN THE DEBOK ROAD
IMPROVEMENTS AND PRIVATE BUILDING SITE GRADING TO RETAIN AS
MUCH AS POSSIBLE OF THE MAN-MADE DRAINAGE WAY DITCH ALONG
DEBOK ROAD (THIS MAY REQUIRE COMBINING THE DEVELOPMENT'S
PRIVATE WATER QUALITY SWALE AND THE EXISTING SWALE); 2) RECORD
WITH THE COUNTY AN AGREEMENT WITH THE CITY THAT REQUIRES THE
PROPERTY OWNER TO OPERATE AND MAINTAIN THE PRIVATE STORM
RETENTION AND WATER QUALITY FACILITIES AND PROVIDE THIRD PARTY
CERTIFICATION TO THE CITY THAT IT IS WORKING PROPERLY ON AN
ANNUAL BASIS; 3) DESIGN AND CONSTRUCT THE PRIVATE ON-SITE STORM
DRAINAGE FACILITIES TO MINIMIZE INTER-BASIN TRANSFERS OF STORM
WATER; 4) DETAIN THE DOWNSTREAM STORM DRAINAGE RUN-OFF WITH
PRIVATE STORM WATER DETENTION FACILITIES SO THAT THE 1-, 5- AND
10-YEAR POST DEVELOPMENT RELEASE RATES ARE EQUAL TO THE 1-, 5-
AND 10-YEAR PRE-DEVELOPMENT RELEASE RATES; 5) EXTEND THE 18-
INCH STORM DRAINAGE MAIN STUB-OUT LOCATED AT BLANKENSHIP
ROAD AND 13TH STREET TO THE PROPOSED PRIVATE STORM SYSTEM
OUTFALL AT THE TOP OF 13TH STREET; 6) CONSTRUCT THE STORM
DRAINAGE MASTER PLAN PROJECT 32-01A (10TH STREET CULVERT
CROSSING) OR, PROVIDE ADDITIONAL ON-SITE DETENTION OR OTHER
MITIGATION SUFFICIENT TO DEMONSTRATE THAT ADDITIONAL STORMWATER
RUNOFF GENERATED BY DEVELOPMENT ON THIS SITE WILL NOT EXCEED
THE CAPACITY OF THE 10TH STREET CULVERT CROSSING DURING A 100-
YEAR STORM EVENT.

19. WATER. THE APPLICANT SHALL: 1) FINANCE THE REVIEW OF THE
DEVELOPMENT'S FIRE AND DOMESTIC WATER SYSTEM DEMANDS WITH
THE CITY'S NEW WATER MASTER PLAN, MONTGOMERY WATSON, TO
ESTABLISH ALL NECESSARY OFF-SITE AND ON-SITE WATER
IMPROVEMENTS REQUIRED FOR THE DEVELOPMENT. THE MINIMUM
OFF-SITE MASTER PLAN WATER TRANSMISSION MAIN CONSTRUCTION
IMPROVEMENTS REQUIRED SHALL INCLUDE: A) PHASE II OF THE
WILLAMETTE FALLS DRIVE WATER TRANSMISSION MAIN, AND B) THE
WATERLINE ON BLANKENSHIP ROAD BETWEEN 13TH STREET AND
DEBOK; 2) PERFORM ACTUAL FIRE FLOW TESTS ON THE VARIOUS NEW
PRIVATE FIRE HYDRANTS, DURING AN INDUCED HIGH WATER DEMAND
DAY, THAT PROVIDE PROOF THAT THE FIRE FLOW IS ADEQUATE TO MEET
EACH OF THE BUILDINGS WITHIN ITS SERVICE AREA; 3) OBTAIN WRITTEN
APPROVAL FROM THE CITY ENGINEER AND THE CITY FIRE MARSHALL
THAT THE NECESSARY HYDRANT FIRE FLOW IS AVAILABLE PRIOR TO ANY
BUILDING-RELATED CONSTRUCTION WITH COMBUSTIBLE MATERIALS; 4)
RECORD WITH THE COUNTY AN AGREEMENT WITH THE CITY THAT
REQUIRES THE PROPERTY OWNER TO PROVIDE ANNUAL CERTIFICATION TO
THE CITY'S FIRE DEPARTMENT THAT THE PRIVATE FIRE SYSTEM IS
OPERATING PROPERLY; AND 5) DESIGN AND CONSTRUCT THE PRIVATE
DOMESTIC WATER SYSTEMS TO SERVICE THE BUILDINGS LOCATED IN
VARIOUS MASTER PLAN PRESSURE ZONES FROM THE SAME ZONE.
WATER FOR FIRE FLOW REQUIREMENTS MAY BE ALLOWED TO CROSS
PRESSURE ZONES. THE FIRE FLOW REQUIREMENTS OF THE BUILDINGS AT
THE TOP OF THE WILLAMETTE ZONE MAY REQUIRE THAT THESE
BUILDINGS USE FIREWALLS AND/OR SPRINKLERS TO MAKE UP ANY
PRIVATE HYDRANT FIRE FLOW DEFICIENCIES.

20. SANITARY SEWER. THE APPLICANT SHALL: A.) DESIGN AND CONSTRUCT
THE SEWER SYSTEM TO AVOID ANY INTER-BASIN TRANSFERS; OR B.) 1)
FINANCE A REVIEW (LESS THAN \$500) BY THE CITY'S SANITARY SEWER
MASTER PLAN CONSULTANT, MURRAY SMITH, OF THE IMPACT OF
PROPOSED INTER-BASIN TRANSFERS OF THE SEWAGE SYSTEM ON THE
NEED FOR ANY OFF-SITE SANITARY SEWER MAIN SYSTEM
IMPROVEMENTS; AND 2) CONSTRUCT ANY OFF-SITE IMPROVEMENTS
THAT ARE DETERMINED NECESSARY FROM THE REVIEW TO ALLOW THE
PROPOSED INTERBASIN TRANSFER.

21. THE APPLICANT SHALL CONSTRUCT ALL OF THE DEVELOPMENT'S 1)
PUBLIC STREET AND NECESSARY PRIVATE PARKING/DRIVEWAY ISLE
IMPROVEMENTS; 2) PUBLIC AND NECESSARY PRIVATE STORM DRAINAGE WATER QUALITY,
DETENTION AND OUF-FALL FACILITIES; 3) PUBLIC AND PRIVATE
(DOMESTIC AND FIRE FLOW) WATER TRANSMISSION IMPROVEMENTS;
AND 4) PUBLIC AND NECESSARY PRIVATE SANITARY SEWER MAIN
TRANSMISSION IMPROVEMENTS PRIOR THE BEING GRANTED ANY
BUILDING FINAL INSPECTION AND/OR OCCUPANCY OF ANY PHASE OF THE
DEVELOPMENT.

22. THE APPLICANT SHALL INSTALL A SIX-FOOT TALL SOLID WOOD FENCE OR
OTHER DESIGN THAT PROPERLY SCREENS GLARE ALONG THE MAIN
DRIVEWAY AISLE #1 WHEN IT IS ADJACENT TO THE NORTH PROPERTY
LINE.

3-20-99	DESIGNED: BSS	CHECKED: BSS
	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.:	SCALE:

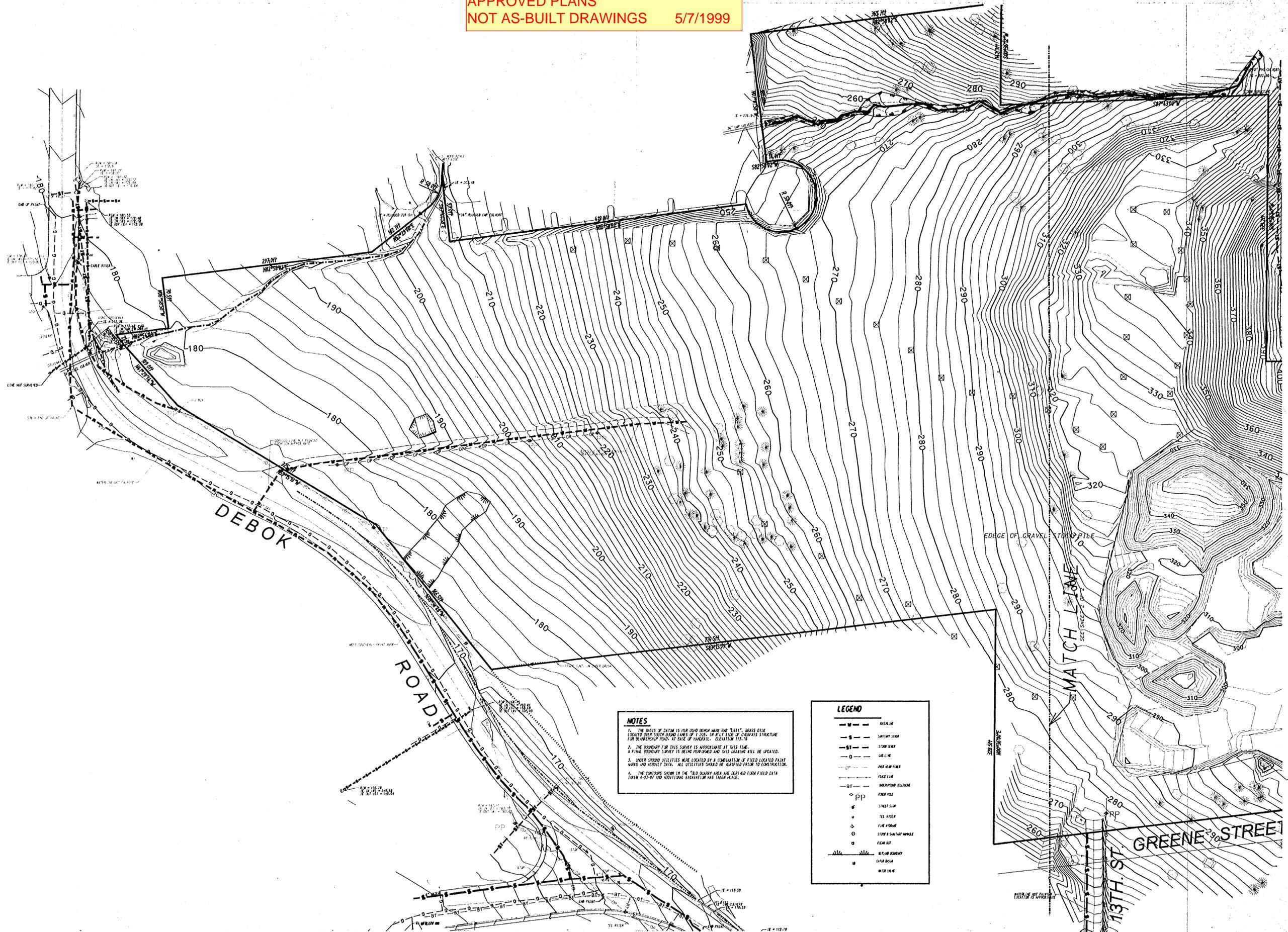
CONDITIONS OF APPROVAL
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



W.B. WELLS
and associates inc.
ENGINEERS • SURVEYORS • PLANNERS
4230 NE FREMONT STREET
PORTLAND, OREGON 97213
PHONE (503) 284-5896
FAX (503) 284-8580



APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



NO.	DATE	REVISIONS
3-20-99		

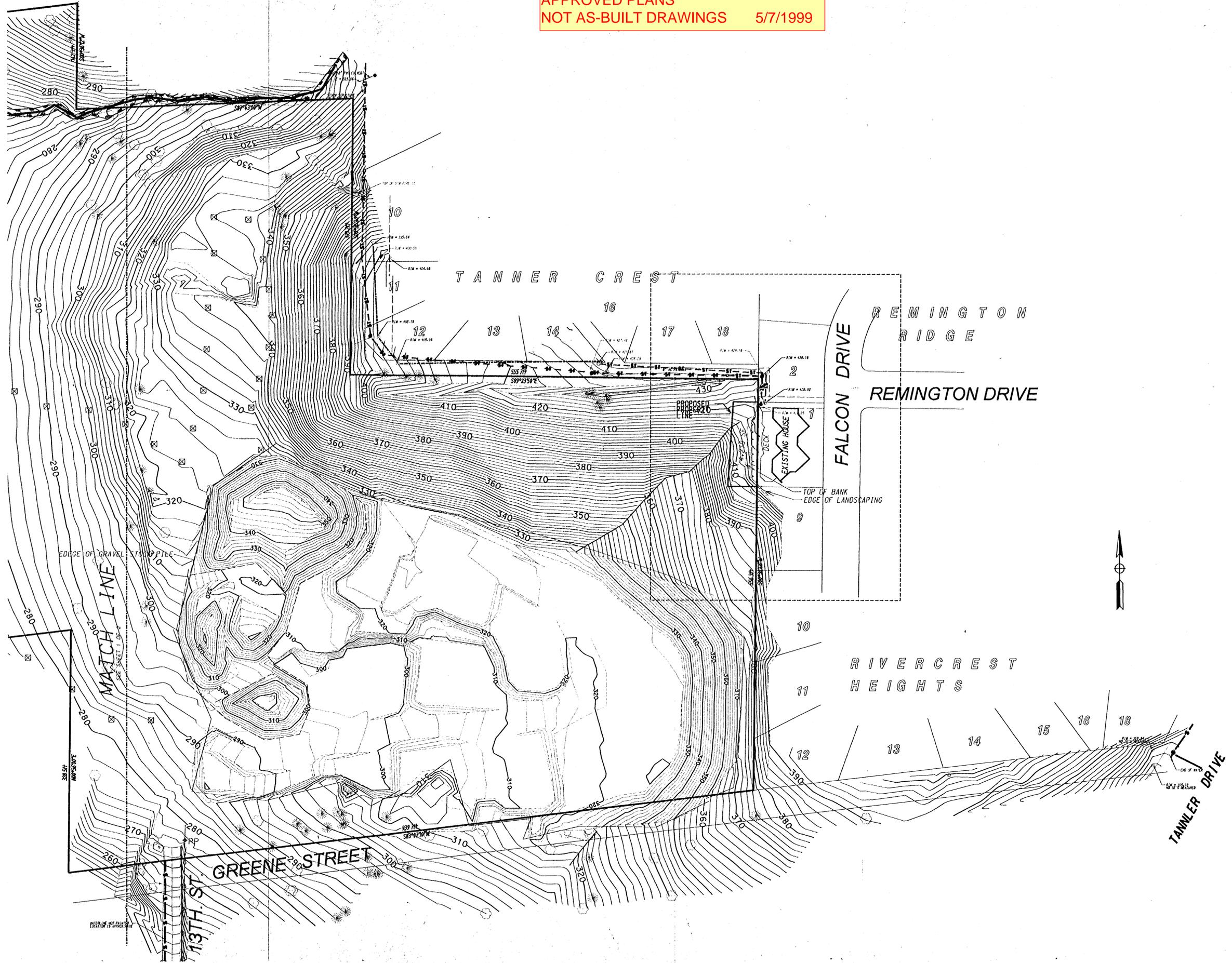
TOPOGRAPHIC SURVEY
for
SUMMERLIN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

REGISTERED
PROFESSIONAL
LAND SURVEYOR
David W. Mills
OREGON
JULY 19, 1990
DAVID W. MILLS
1915
EXPIRES: 12-31-00
4/14/99

W.B. WELLS
and associates inc.
ENGINEERS • SURVEYORS • PLANNERS
4250 NE FREMONT STREET
PORTLAND, OREGON 97213
PHONE (503) 284-5896
FAX (503) 284-8530



APPROVED PLANS
 NOT AS-BUILT DRAWINGS 5/7/1999



REVISIONS
3-20-99
10-01-97 RETOPO QUARRY AREA
DESIGNED: BSS
DRAWN: DMW
CHECKED: DMW
DATE: 6-24-97
JOB NO.: 96-340
SCALE: 1"=60'

TOPOGRAPHIC SURVEY
 for
SUMMERLIN APARTMENTS
 CITY OF WEST Linn, CLACKAMAS COUNTY, OREGON

REGISTERED
 PROFESSIONAL
 LAND SURVEYOR
David W. Wells
 OREGON
 JULY 19, 1980
 DAVID W. WELLS
 1915
 EXPIRES: 12-31-99
 416422

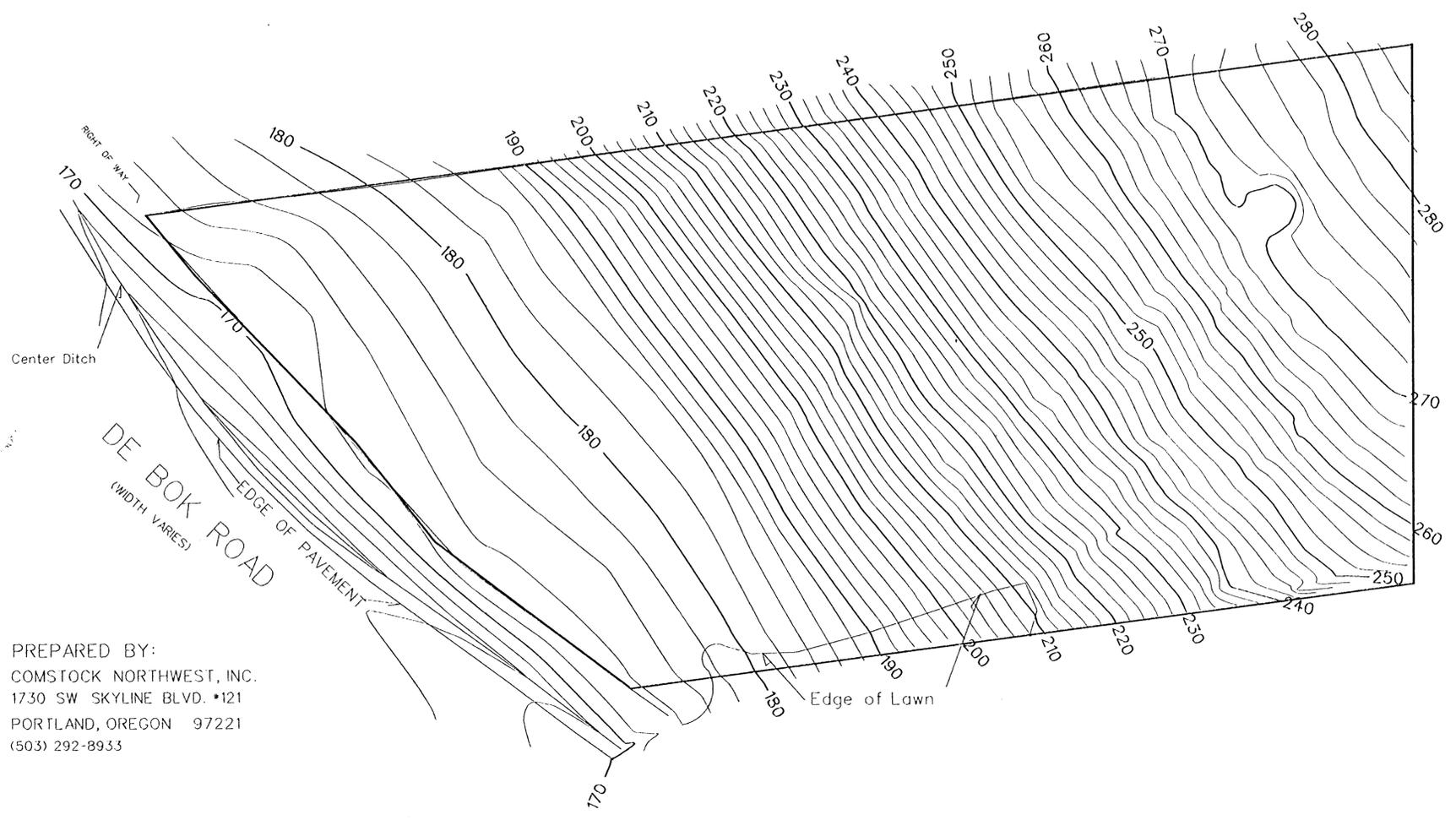
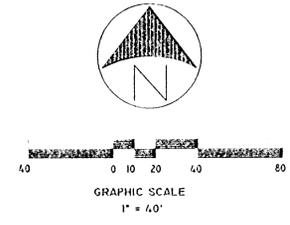
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SHEET
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APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

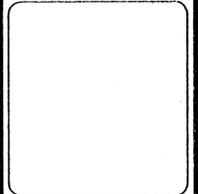
TOPOGRAPHIC SURVEY
OF TAX LOT 100 2S 1E 35CB
LOCATED IN THE SW 1/4 SEC.35, T2S, R1E, W.M.
CITY OF WEST LINN CLACKAMAS COUNTY OREGON
DECEMBER 4, 1995 BENCH MARK "L631" EL.=175.76'



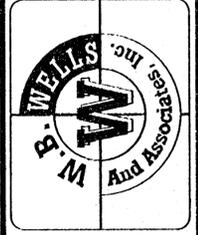
PREPARED BY:
COMSTOCK NORTHWEST, INC.
1730 SW SKYLINE BLVD. #121
PORTLAND, OREGON 97221
(503) 292-8933

3-20-99	REVISIONS
DESIGNED: BSS	CHECKED: BSS
DRAWN: BSS/RT	DATE: 11-20-98
JOB NO.: 96-340	SCALE: AS SHOWN

TOPOGRAPHIC SURVEY
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

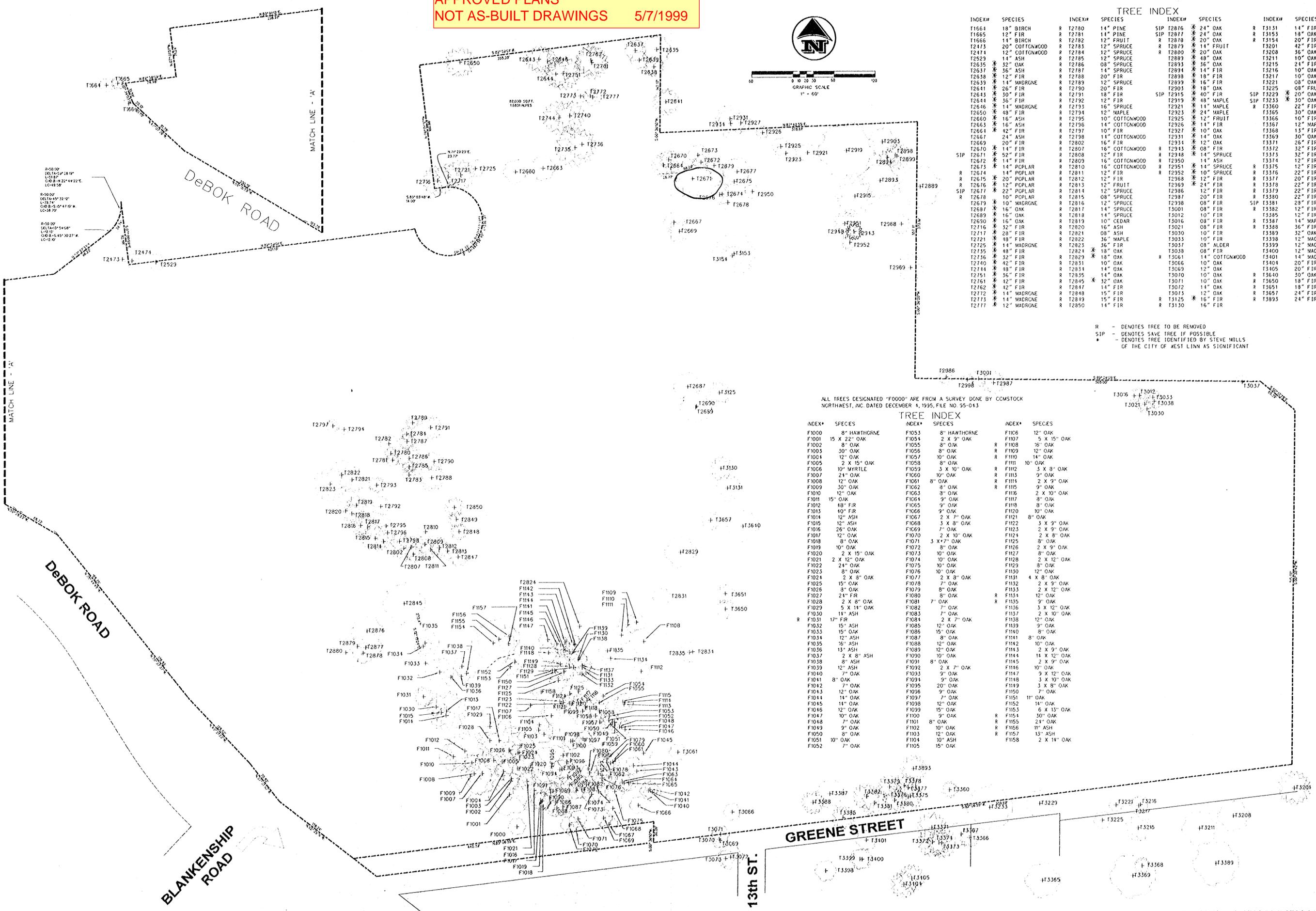


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SHEET
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APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



TREE INDEX

INDEX#	SPECIES	INDEX#	SPECIES	INDEX#	SPECIES	INDEX#	SPECIES
T1664	18" BIRCH	R T2780	14" PINE	SIP T2876	24" OAK	R T3131	14" FIR
T1665	12" FIR	R T2781	14" PINE	SIP T2877	24" OAK	R T3153	18" OAK
T1666	12" FRUIT	R T2782	12" FRUIT	R T2878	20" OAK	R T3154	20" OAK
T2473	20" COTTONWOOD	R T2783	12" SPRUCE	R T2879	14" FRUIT	R T3201	42" FIR
T2474	12" COTTONWOOD	R T2784	12" SPRUCE	R T2880	20" OAK	R T3208	36" OAK
T2529	14" ASH	R T2785	12" SPRUCE	T2889	48" OAK	R T3211	10" OAK
T2635	32" OAK	R T2786	08" SPRUCE	T2893	36" OAK	R T3215	24" FIR
T2637	36" ASH	R T2787	18" SPRUCE	T2894	14" FIR	R T3216	10" OAK
T2638	12" FIR	R T2788	20" FIR	T2898	18" FIR	R T3217	10" OAK
T2639	14" MADRONE	R T2789	12" SPRUCE	T2899	16" FIR	R T3221	08" OAK
T2641	26" FIR	R T2790	20" FIR	T2903	18" OAK	R T3225	08" FRUIT
T2643	30" FIR	R T2791	18" FIR	R T2915	10" FIR	SIP T3229	20" OAK
T2644	36" FIR	R T2792	12" FIR	T2919	48" MAPLE	SIP T3233	30" OAK
T2646	14" MADRONE	R T2793	16" SPRUCE	T2921	11" MAPLE	R T3236	22" FIR
T2650	48" FIR	R T2794	12" MAPLE	T2923	24" MAPLE	R T3255	30" OAK
T2660	16" ASH	R T2795	10" COTTONWOOD	T2925	12" FRUIT	R T3266	10" FIR
T2663	16" ASH	R T2796	14" COTTONWOOD	T2926	14" FIR	R T3267	12" MAPLE
T2664	42" FIR	R T2797	10" FIR	T2927	10" OAK	R T3268	15" FIR
T2667	24" ASH	R T2798	11" COTTONWOOD	T2931	14" OAK	R T3269	30" OAK
T2669	20" FIR	R T2802	16" FIR	T2934	12" OAK	R T3271	26" FIR
T2670	14" FIR	R T2801	16" COTTONWOOD	R T2935	08" FIR	R T3272	32" FIR
T2671	52" FIR	R T2808	12" FIR	R T2938	14" SPRUCE	R T3273	32" FIR
T2672	14" FIR	R T2809	16" COTTONWOOD	R T2950	14" ASH	R T3274	12" FIR
T2673	14" POPLAR	R T2810	16" COTTONWOOD	R T2951	14" SPRUCE	R T3275	12" FIR
T2674	14" POPLAR	R T2811	12" FIR	R T2952	10" SPRUCE	R T3276	22" FIR
T2675	20" POPLAR	R T2812	12" FIR	T2968	12" FIR	R T3277	20" FIR
T2676	12" POPLAR	R T2813	12" FRUIT	T2969	24" FIR	R T3278	22" FIR
T2677	22" POPLAR	R T2814	12" SPRUCE	T2986	12" FIR	R T3279	22" FIR
T2678	10" POPLAR	R T2815	08" SPRUCE	T2987	20" FIR	R T3280	22" FIR
T2679	32" MADRONE	R T2816	12" MADRONE	R T2988	08" FIR	SIP T3281	28" FIR
T2681	16" OAK	R T2817	14" SPRUCE	T3001	08" FIR	R T3282	12" FIR
T2689	16" OAK	R T2818	14" SPRUCE	T3012	10" FIR	R T3283	15" MAPLE
T2690	16" OAK	R T2819	10" CEDAR	T3016	08" FIR	R T3287	14" MAPLE
T2716	32" OAK	R T2820	16" ASH	T3021	08" FIR	R T3288	36" FIR
T2717	28" FIR	R T2821	08" ASH	T3021	10" FIR	R T3289	32" OAK
T2721	48" FIR	R T2822	36" MAPLE	T3033	10" FIR	R T3298	12" MADRONE
T2725	14" MADRONE	R T2823	36" FIR	T3037	08" ALDER	R T3299	12" MADRONE
T2735	48" FIR	R T2824	18" OAK	T3038	08" FIR	R T3400	12" MADRONE
T2736	48" FIR	R T2825	14" OAK	R T3061	14" COTTONWOOD	R T3401	14" MADRONE
T2744	42" FIR	R T2831	10" OAK	T3066	10" OAK	R T3404	20" FIR
T2744	48" FIR	R T2834	14" OAK	T3069	12" OAK	R T3405	20" FIR
T2751	36" FIR	R T2835	14" OAK	T3070	10" OAK	R T3406	30" OAK
T2761	42" FIR	R T2845	32" OAK	T3071	10" OAK	R T3450	18" FIR
T2762	12" MADRONE	R T2847	14" OAK	T3072	14" OAK	R T3451	18" FIR
T2772	14" MADRONE	R T2848	15" FIR	T3073	12" OAK	R T3457	24" FIR
T2773	14" MADRONE	R T2849	15" FIR	R T3125	16" FIR	R T3458	24" FIR
T2777	12" MADRONE	R T2850	14" FIR	R T3130	16" FIR	R T3459	24" FIR

R - DENOTES TREE TO BE REMOVED
SIP - DENOTES SAVE TREE IF POSSIBLE
* - DENOTES TREE IDENTIFIED BY STEVE MILLS OF THE CITY OF WEST LINN AS SIGNIFICANT

ALL TREES DESIGNATED "F0000" ARE FROM A SURVEY DONE BY COMSTOCK NORTHWEST, INC. DATED DECEMBER 4, 1995, FILE NO. 95-043

TREE INDEX

INDEX#	SPECIES	INDEX#	SPECIES	INDEX#	SPECIES
F1000	8" HAWTHORNE	F1053	8" HAWTHORNE	F1106	12" OAK
F1001	15 X 22" OAK	F1054	2 X 9" OAK	F1107	5 X 15" OAK
F1002	8" OAK	F1055	8" OAK	R F1108	16" OAK
F1003	30" OAK	F1056	8" OAK	R F1109	12" OAK
F1004	12" OAK	F1057	10" OAK	R F1110	14" OAK
F1005	2 X 15" OAK	F1058	8" OAK	F1111	10" OAK
F1006	10" MYRTLE	F1059	3 X 10" OAK	R F1112	3 X 8" OAK
F1007	24" OAK	F1060	10" OAK	R F1113	9" OAK
F1008	12" OAK	F1061	8" OAK	R F1114	2 X 12" OAK
F1009	30" OAK	F1062	8" OAK	R F1115	9" OAK
F1010	12" OAK	F1063	8" OAK	F1116	2 X 10" OAK
F1011	15" OAK	F1064	9" OAK	F1117	8" OAK
F1012	48" FIR	F1065	9" OAK	F1118	8" OAK
F1013	40" FIR	F1066	9" OAK	F1120	10" OAK
F1014	12" ASH	F1067	2 X 7" OAK	F1121	8" OAK
F1015	12" ASH	F1068	3 X 8" OAK	F1122	3 X 9" OAK
F1016	26" OAK	F1069	7" OAK	F1123	2 X 9" OAK
F1017	12" OAK	F1070	2 X 10" OAK	F1124	2 X 8" OAK
F1018	8" OAK	F1071	3 X 7" OAK	F1125	8" OAK
F1019	10" OAK	F1072	8" OAK	F1126	2 X 9" OAK
F1020	2 X 15" OAK	F1073	10" OAK	F1127	8" OAK
F1021	2 X 12" OAK	F1074	10" OAK	F1128	2 X 12" OAK
F1022	24" OAK	F1075	10" OAK	F1129	8" OAK
F1023	8" OAK	F1076	10" OAK	F1130	12" OAK
F1024	2 X 8" OAK	F1077	2 X 8" OAK	F1131	4 X 8" OAK
F1025	15" OAK	F1078	7" OAK	F1132	2 X 9" OAK
F1026	8" OAK	F1079	8" OAK	F1133	2 X 12" OAK
F1027	24" FIR	F1080	8" OAK	R F1134	12" OAK
F1028	2 X 8" OAK	F1081	7" OAK	R F1135	9" OAK
F1029	5 X 14" OAK	F1082	7" OAK	F1136	3 X 12" OAK
F1030	11" ASH	F1083	7" OAK	F1137	2 X 10" OAK
R F1031	17" FIR	F1084	2 X 7" OAK	F1138	12" OAK
F1032	15" ASH	F1085	12" OAK	F1139	9" OAK
F1033	15" OAK	F1086	15" OAK	F1140	8" OAK
F1034	12" ASH	F1087	8" OAK	F1141	8" OAK
F1035	16" ASH	F1088	12" OAK	F1142	10" OAK
F1036	15" ASH	F1089	12" OAK	F1143	2 X 9" OAK
F1037	2 X 8" ASH	F1090	10" OAK	F1144	14 X 12" OAK
F1038	8" ASH	F1091	8" OAK	F1145	2 X 9" OAK
F1039	12" ASH	F1092	2 X 7" OAK	F1146	10" OAK
F1040	7" OAK	F1093	9" OAK	F1147	9 X 12" OAK
F1041	8" OAK	F1094	9" OAK	F1148	3 X 10" OAK
F1042	7" OAK	F1095	20" OAK	F1149	3 X 8" OAK
F1043	12" OAK	F1096	9" OAK	F1150	7" OAK
F1044	14" OAK	F1097	7" OAK	F1151	11" OAK
F1045	14" OAK	F1098	12" OAK	F1152	14" OAK
F1046	12" OAK	F1099	15" OAK	F1153	6 X 13" OAK
F1047	10" OAK	F1100	9" OAK	R F1154	30" OAK
F1048	8" OAK	R F1101	8" OAK	R F1155	24" OAK
F1049	9" OAK	R F1102	10" OAK	R F1156	11" ASH
F1050	8" OAK	R F1103	12" OAK	R F1157	13" ASH
F1051	10" OAK	F1104	10" ASH	F1158	2 X 14" OAK
F1052	7" OAK	F1105	15" OAK		

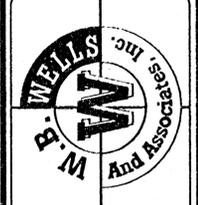
REVISIONS

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	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.: 96-340	SCALE:

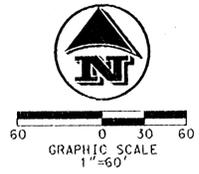
TREE SURVEY for SUMMERLINN APARTMENTS CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



W.B. WELLS and associates inc. ENGINEERS - SURVEYORS - PLANNERS 4230 NE FREMONT STREET PORTLAND, OREGON 97213 PHONE: (503) 284-5896 FAX: (503) 284-8590



APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

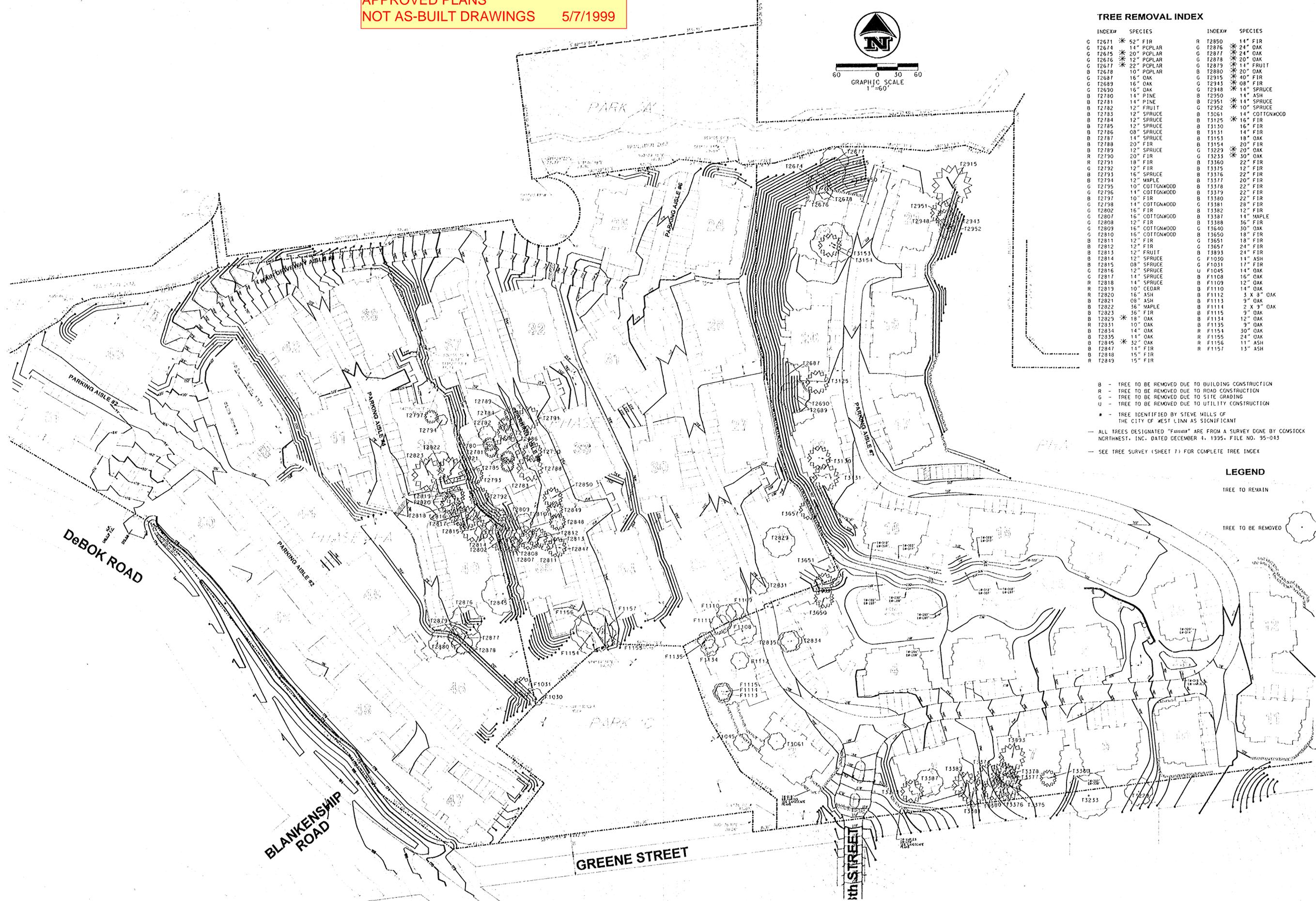
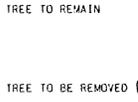


TREE REMOVAL INDEX

INDEX#	SPECIES	INDEX#	SPECIES
G T2671	* 52" FIR	R T2850	14" FIR
G T2674	14" POPLAR	G T2876	* 24" OAK
G T2675	* 20" POPLAR	G T2877	* 24" OAK
G T2676	* 12" POPLAR	G T2878	* 20" OAK
G T2677	* 22" POPLAR	G T2879	* 20" FRUIT
B T2678	10" POPLAR	B T2880	* 20" OAK
G T2687	16" OAK	G T2915	* 40" FIR
G T2689	16" OAK	G T2943	* 08" FIR
G T2690	15" OAK	G T2948	* 14" SPRUCE
B T2700	14" PINE	B T2950	14" ASH
B T2701	14" PINE	B T2951	* 14" SPRUCE
B T2702	12" FRUIT	G T2952	* 10" SPRUCE
B T2703	12" SPRUCE	B T3061	* 14" COTTONWOOD
B T2704	12" SPRUCE	B T3125	16" FIR
B T2705	12" SPRUCE	B T3130	16" FIR
B T2706	08" SPRUCE	B T3131	14" FIR
B T2707	14" SPRUCE	B T3153	18" OAK
B T2788	20" FIR	B T3154	20" FIR
B T2789	12" SPRUCE	G T3229	* 20" OAK
R T2790	20" FIR	G T3235	* 30" OAK
R T2791	18" FIR	B T3360	22" FIR
G T2792	12" FIR	B T3375	12" FIR
B T2793	16" SPRUCE	B T3376	22" FIR
B T2794	12" MAPLE	B T3377	20" FIR
G T2795	10" COTTONWOOD	B T3378	22" FIR
G T2796	14" COTTONWOOD	B T3379	22" FIR
B T2797	10" FIR	B T3380	22" FIR
G T2798	14" COTTONWOOD	G T3381	28" FIR
G T2802	16" FIR	B T3382	12" FIR
G T2807	16" COTTONWOOD	B T3387	14" MAPLE
G T2808	12" FIR	B T3388	36" FIR
G T2809	16" COTTONWOOD	G T3640	30" OAK
G T2810	16" COTTONWOOD	B T3650	18" FIR
B T2811	12" FIR	G T3651	18" FIR
B T2812	12" FIR	G T3657	24" FIR
B T2813	12" FRUIT	B T3835	24" FIR
B T2814	12" SPRUCE	G F1030	11" ASH
B T2815	08" SPRUCE	G F1031	17" FIR
G T2816	12" SPRUCE	U F1045	14" OAK
G T2817	14" SPRUCE	B F1108	16" OAK
R T2818	14" SPRUCE	B F1109	12" OAK
R T2819	10" CEDAR	B F1110	14" OAK
R T2820	16" ASH	B F1112	3 X 8" OAK
B T2821	08" ASH	B F1113	9" OAK
B T2822	36" MAPLE	B F1114	2 X 9" OAK
B T2823	36" FIR	B F1115	9" OAK
B T2829	* 18" OAK	B F1134	12" OAK
R T2831	10" OAK	B F1135	9" OAK
B T2834	14" OAK	R F1154	30" OAK
B T2835	14" OAK	R F1155	24" OAK
B T2845	* 32" OAK	B F1156	11" ASH
B T2847	14" FIR	R F1157	13" ASH
B T2848	15" FIR		
R T2849	15" FIR		

B - TREE TO BE REMOVED DUE TO BUILDING CONSTRUCTION
 R - TREE TO BE REMOVED DUE TO ROAD CONSTRUCTION
 G - TREE TO BE REMOVED DUE TO SITE GRADING
 U - TREE TO BE REMOVED DUE TO UTILITY CONSTRUCTION
 * - TREE IDENTIFIED BY STEVE WILLS OF THE CITY OF WEST LINN AS SIGNIFICANT
 — ALL TREES DESIGNATED "FURNISH" ARE FROM A SURVEY DONE BY CONSTOCK NORTHWEST, INC. DATED DECEMBER 4, 1995. FILE NO. 95-043
 — SEE TREE SURVEY (SHEET 7) FOR COMPLETE TREE INDEX

LEGEND



REVISIONS

3-20-99	DESIGNED: BSS	CHECKED: BSS
	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.: 96-340	SCALE:

TREE REMOVAL PLAN
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



W.B. WELLS
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ENGINEERS • SURVEYORS • PLANNERS
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FAX (503) 284-8530



APPROVED PLANS
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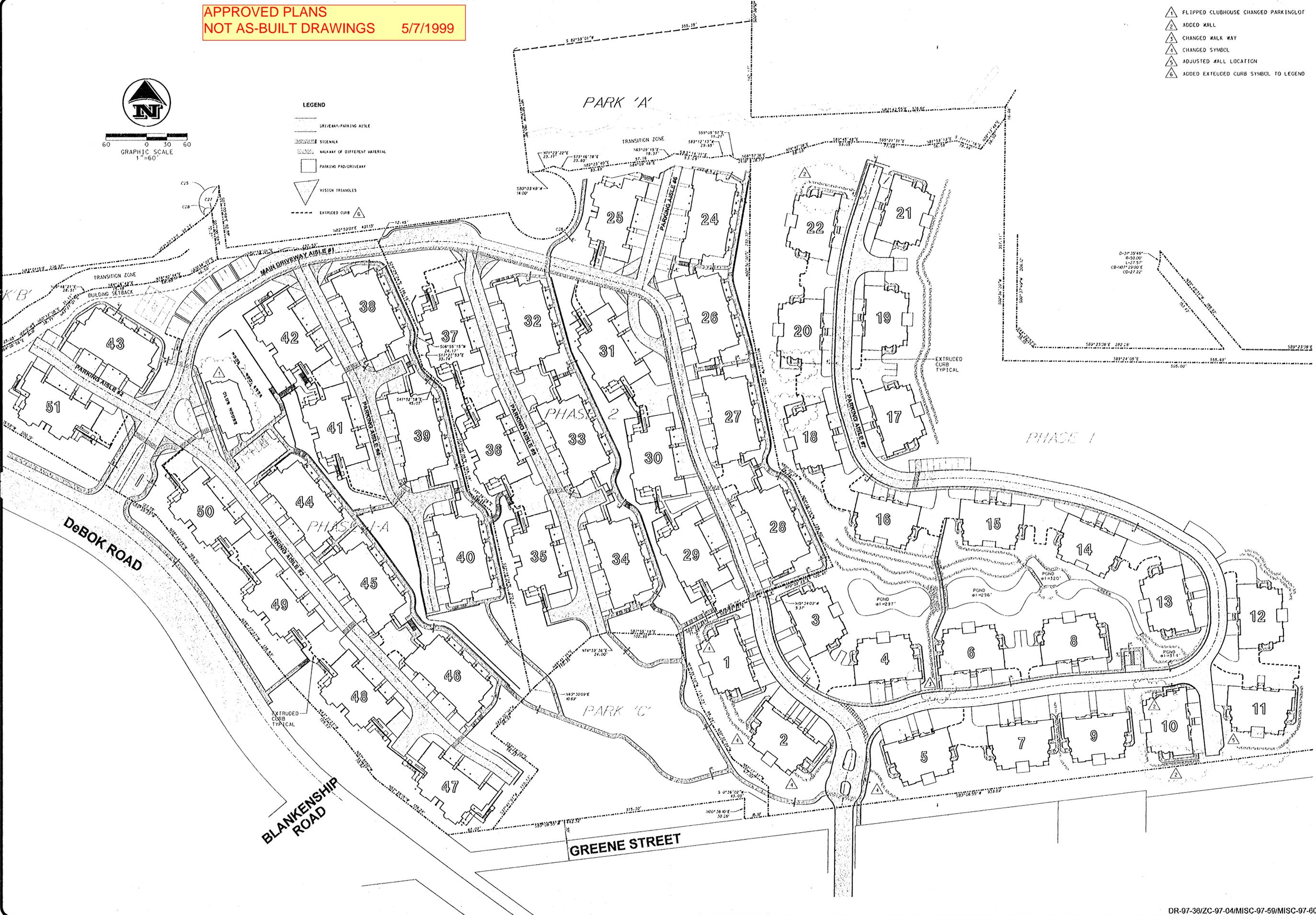


GRAPHIC SCALE
 1"=60'

LEGEND

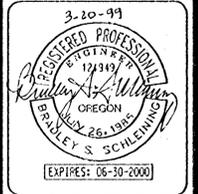
- DRIVEWAY/PARKING AISLE
- SIDWALK
- WALKWAY OF DIFFERENT MATERIAL
- PARKING PAD/DRIVEWAY
- VISION TRIANGLES
- EXTRUDED CURB

- FLIPPED CLUBHOUSE CHANGED PARKINGLOT
- ADDED WALL
- CHANGED WALK WAY
- CHANGED SYMBOL
- ADJUSTED WALL LOCATION
- ADDED EXTRUDED CURB SYMBOL TO LEGEND

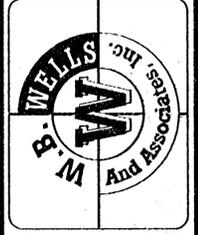


NO.	DATE	BY	DESCRIPTION
1	11-20-98	BSS	DESIGNED
2	11-20-98	BSS/RT	DRAWN
3	96-340	BSS	CHECKED
4	96-340	BSS	DATE
5	96-340	BSS	SCALE

COMPOSITE SITE PLAN
 for
SUMMERLINN APARTMENTS
 CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



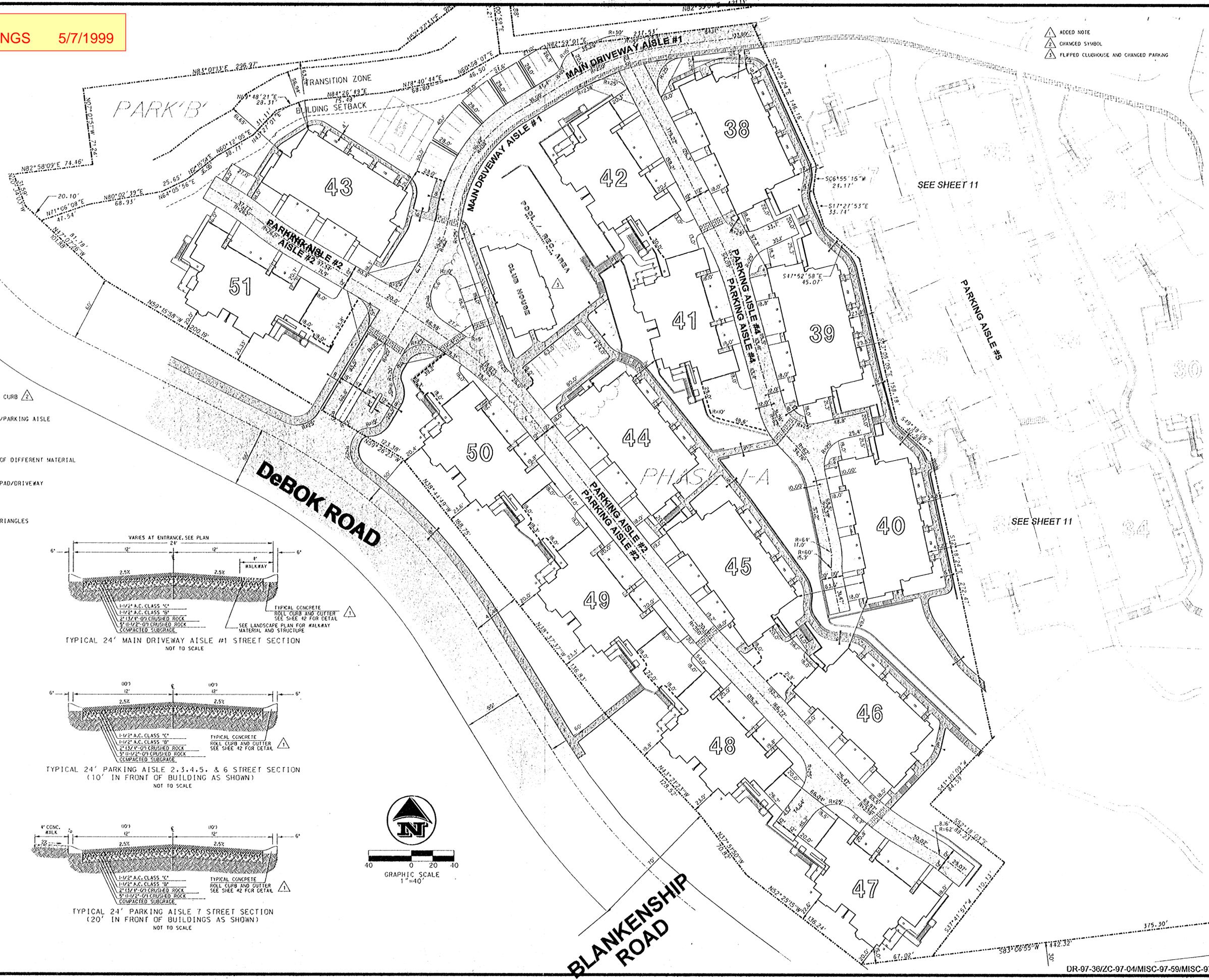
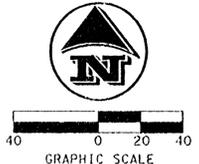
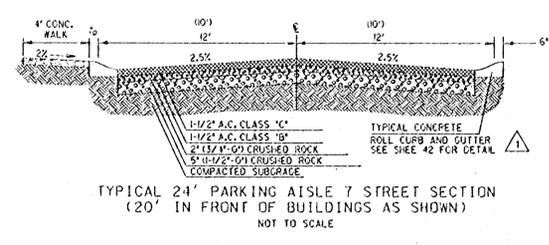
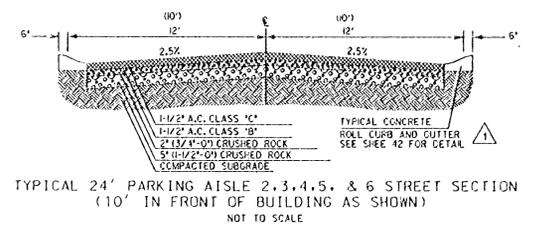
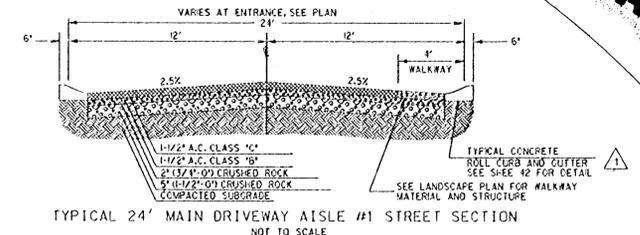
W.B. WELLS
 and associates inc.
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 FAX (503) 284-8530



APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

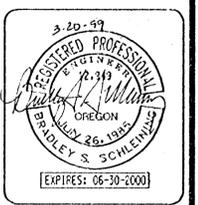
- ▲ ADDED NOTE
- ▲ CHANGED SYMBOL
- ▲ FLIPPED CLUBHOUSE AND CHANGED PARKING

- LEGEND**
- EXTRUDED CURB ▲
 - DRIVEWAY/PARKING AISLE
 - SIDEWALK
 - WALKWAY OF DIFFERENT MATERIAL
 - PARKING PAD/DRIVEWAY
 - ▲ VISION TRIANGLES



REVISIONS	DATE	BY	CHKD	SCALE
3-20-99				

SITE PLAN SHEET 1 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

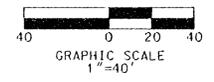


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APPROVED PLANS
 NOT AS-BUILT DRAWINGS 5/7/1999

- ▲ ADDED WALL
- ▲ CHANGED WALL SYMBOL
- ▲ CHANGED SYMBOL



LEGEND

- EXTRUDED CURB ▲
- DRIVEWAY/PARKING AISLE
- SIDEWALK
- WALKWAY OF DIFFERENT MATERIAL
- PARKING PAD/DRIVEWAY
- ▲ VISION TRIANGLES



SEE SHEET 10

SEE SHEET 10

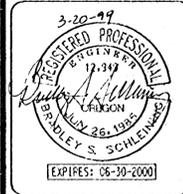
SEE SHEET 12

REVISIONS	DESIGNED BY	CHECKED BY
3-20-99	BSS	BSS
	BSS/RT	BSS

DATE: 11-20-98
 JOB NO.: 96-340
 SCALE:

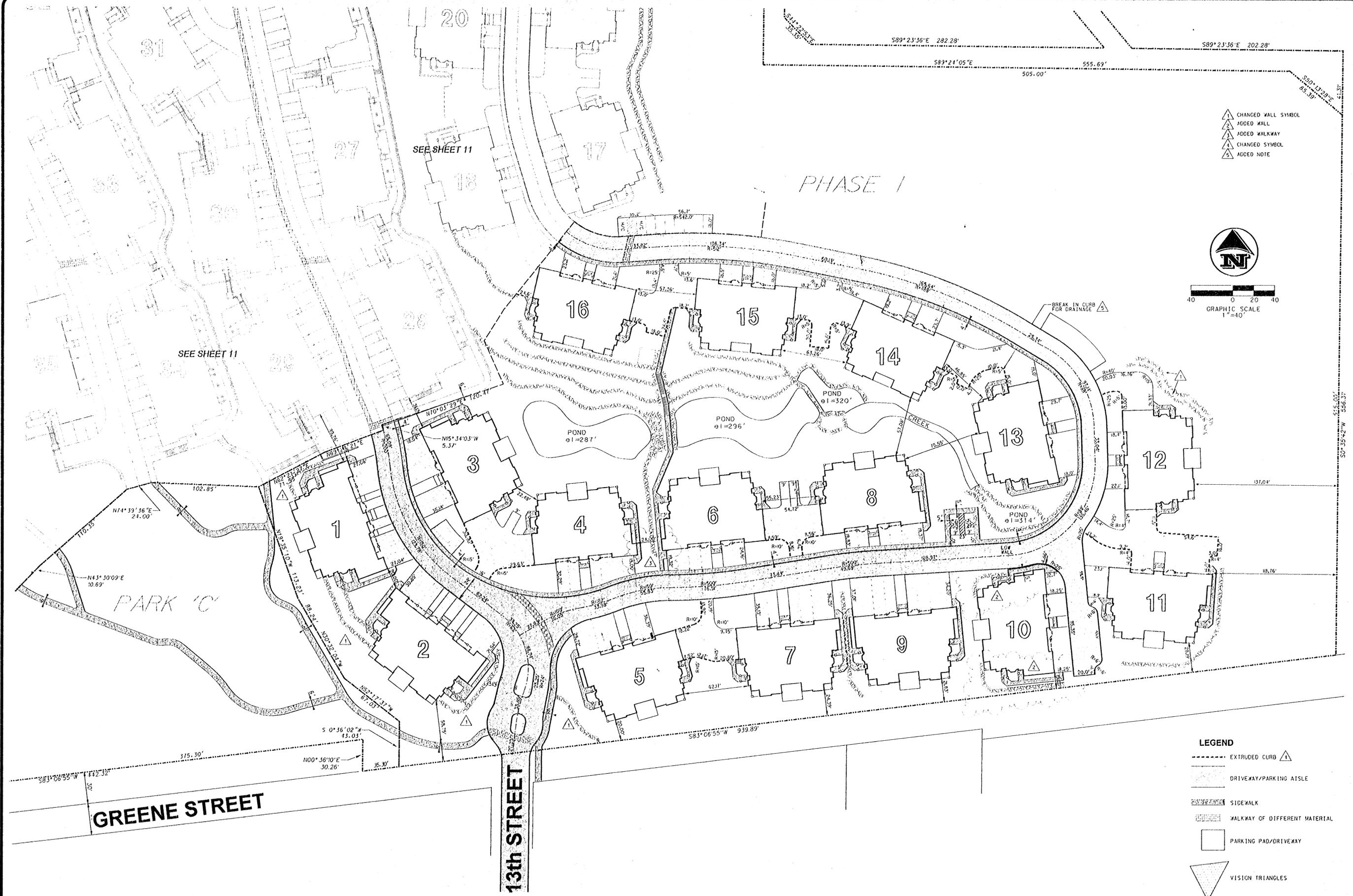
SITE PLAN SHEET 2 OF 3

for
SUMMERLINN APARTMENTS
 CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

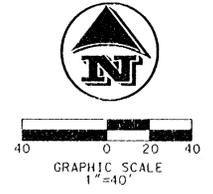


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- ▲ CHANGED WALL SYMBOL
- ▲ ADDED WALL
- ▲ ADDED WALKWAY
- ▲ CHANGED SYMBOL
- ▲ ADDED NOTE



LEGEND

- ▲ EXTRUDED CURB
- DRIVEWAY/PARKING AISLE
- SIDEWALK
- WALKWAY OF DIFFERENT MATERIAL
- PARKING PAD/DRIVEWAY
- ▲ VISION TRIANGLES

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

REVISIONS	DESIGNED: BSS	CHECKED: BSS
3-20-99	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.: 98-340	SCALE:

SITE PLAN SHEET 3 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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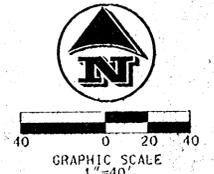
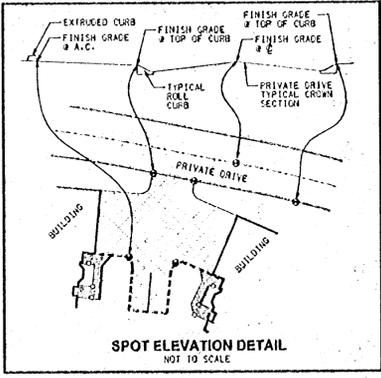
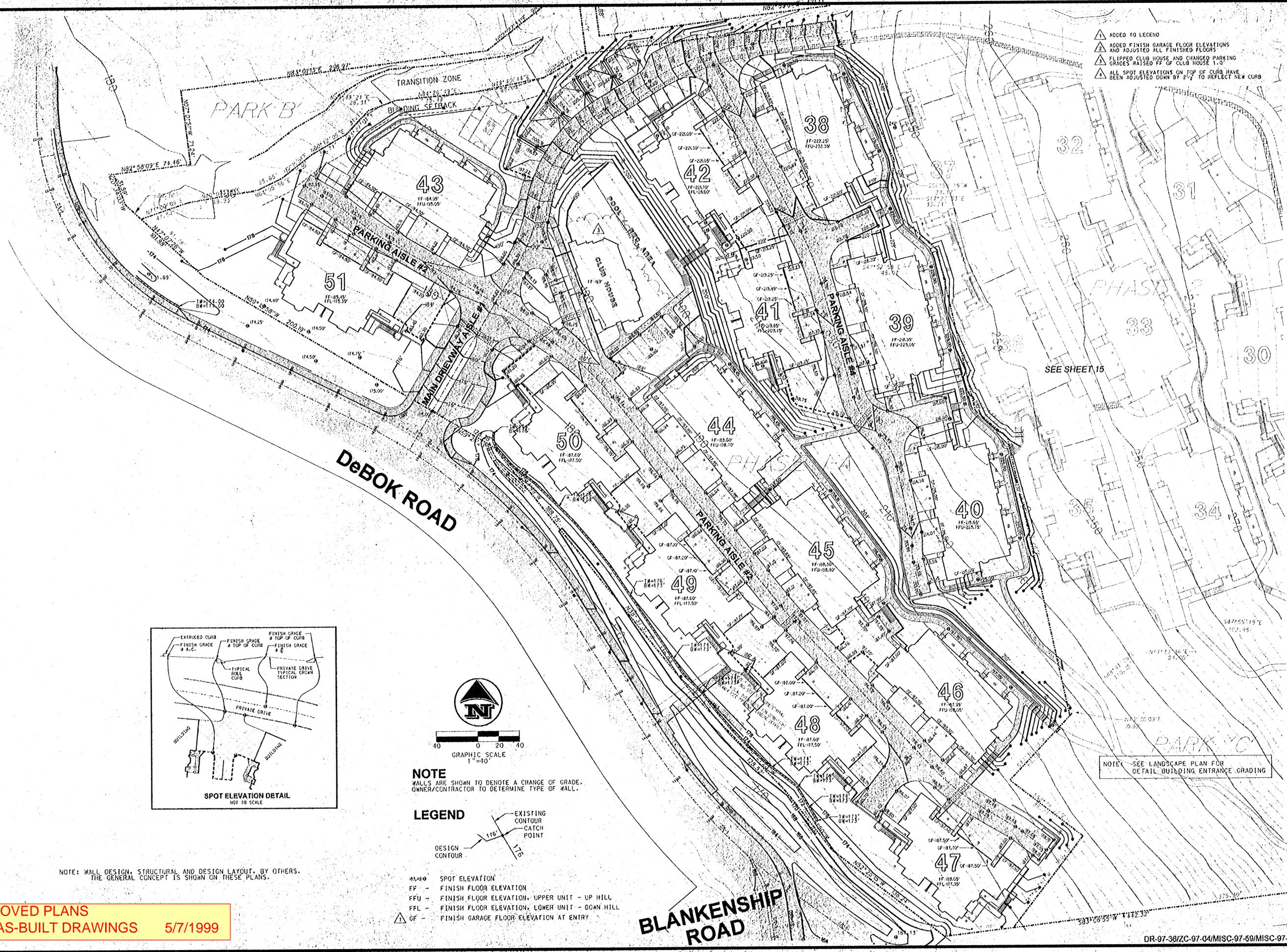
- ▲ ADDED TO LEGEND
- ▲ ADDED FINISH GARAGE FLOOR ELEVATIONS AND ADJUSTED ALL FINISHED FLOORS
- ▲ FLIPPED CLUB HOUSE AND CHANGED PARKING GRADES RAISED FF OF CLUB HOUSE 1.0'
- ▲ ALL SPOT ELEVATIONS ON TOP OF CURB HAVE BEEN ADJUSTED DOWN BY 2 1/2" TO REFLECT NEW CURB

REVISIONS	DESIGNED: BSS	CHECKED: BSS
3-20-99	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.: 96-940	SCALE:

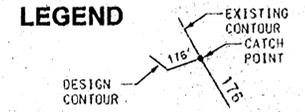
GRADING PLAN - SHEET 1 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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FAX: (503) 284-8530



NOTE
WALLS ARE SHOWN TO DENOTE A CHANGE OF GRADE. OWNER/CONTRACTOR TO DETERMINE TYPE OF WALL.



- SPOT ELEVATION
- FF - FINISH FLOOR ELEVATION
- FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
- FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
- GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY

NOTE: SEE LANDSCAPE PLAN FOR DETAIL BUILDING ENTRANCE GRADING

NOTE: WALL DESIGN, STRUCTURAL AND DESIGN LAYOUT, BY OTHERS. THE GENERAL CONCEPT IS SHOWN ON THESE PLANS.

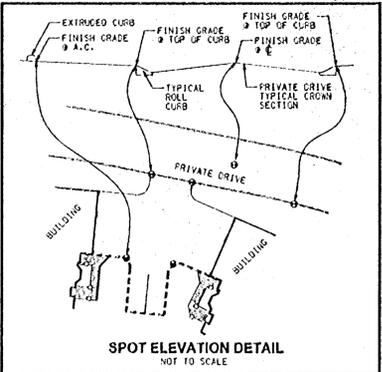
APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

BLANKENSHIP ROAD

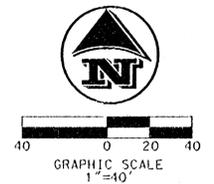
LEGEND

- EXISTING CONTOUR
- CATCH POINT
- DESIGN CONTOUR
- SPOT ELEVATION
- FF - FINISH FLOOR ELEVATION
- FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
- FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
- GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY

NOTE: WALL DESIGN, STRUCTURAL AND DESIGN LAYOUT, BY OTHERS. THE GENERAL CONCEPT IS SHOWN ON THESE PLANS.



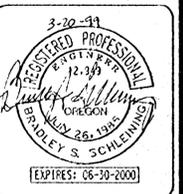
- ADD NOTE TO LEGEND
- ADJUST FINISH FLOOR ELEVATIONS. GARAGE FLOORS STAYED THE SAME
- ADDED WALLS
- CHANGED WALL SYMBOL
- ADDED GARAGE FLOOR ELEVATIONS TO ALL BLDGS.
- ADJUST GRADES OF FINISH FLOORS BLDGS. 24-37



NOTE: SEE LANDSCAPE PLAN FOR DETAIL BUILDING ENTRANCE GRADING.

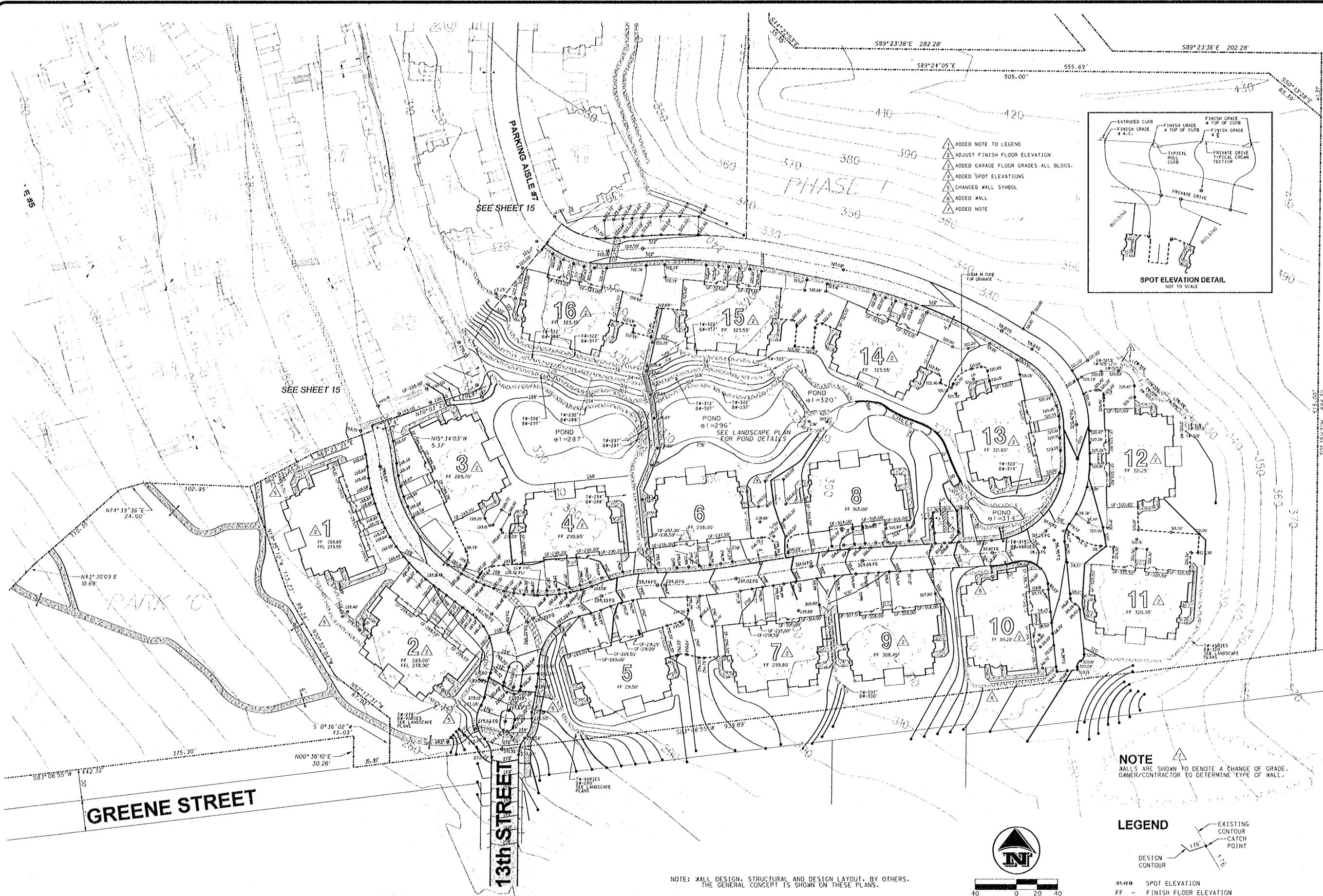
REVISIONS	
3-20-99	
DESIGNED: BSS	CHECKED: BSS
DRAWN: BSS/RT	DATE: 11-20-98
JOB NO.: 96-340	SCALE:

GRADING PLAN - SHEET 2 OF 3
 for
SUMMERLINN APARTMENTS
 CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

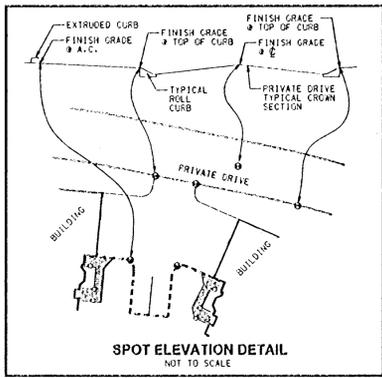


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 FAX (503) 284-8530





- ▲ ADDED NOTE TO LEGEND
- ▲ ADJUST FINISH FLOOR ELEVATION
- ▲ ADDED GARAGE FLOOR GRADES ALL BLDGS.
- ▲ ADDED SPOT ELEVATIONS
- ▲ CHANGED WALL SYMBOL
- ▲ ADDED WALL
- ▲ ADDED NOTE



NOTE
WALLS ARE SHOWN TO DENOTE A CHANGE OF GRADE. OWNER/CONTRACTOR TO DETERMINE TYPE OF WALL.

LEGEND

- ▲ 85.489 SPOT ELEVATION
- FF - FINISH FLOOR ELEVATION
- FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
- FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
- GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY

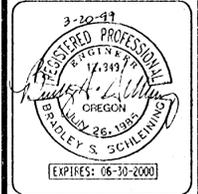


NOTE: WALL DESIGN, STRUCTURAL AND DESIGN LAYOUT, BY OTHERS. THE GENERAL CONCEPT IS SHOWN ON THESE PLANS.

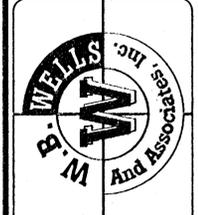
APPROVED PLANS
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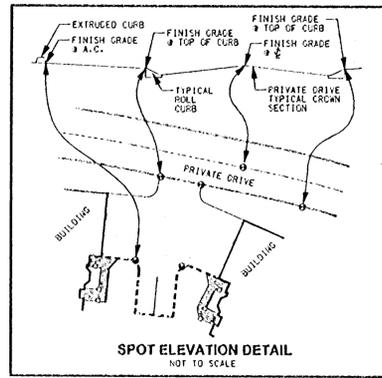
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3-20-99	DESIGNED: BSS
	DRAWN: BSS/RT
	DATE: 11-20-98
	JOB NO.: 96-340
	SCALE:

GRADING PLAN - SHEET 3 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



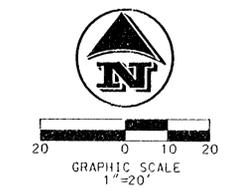
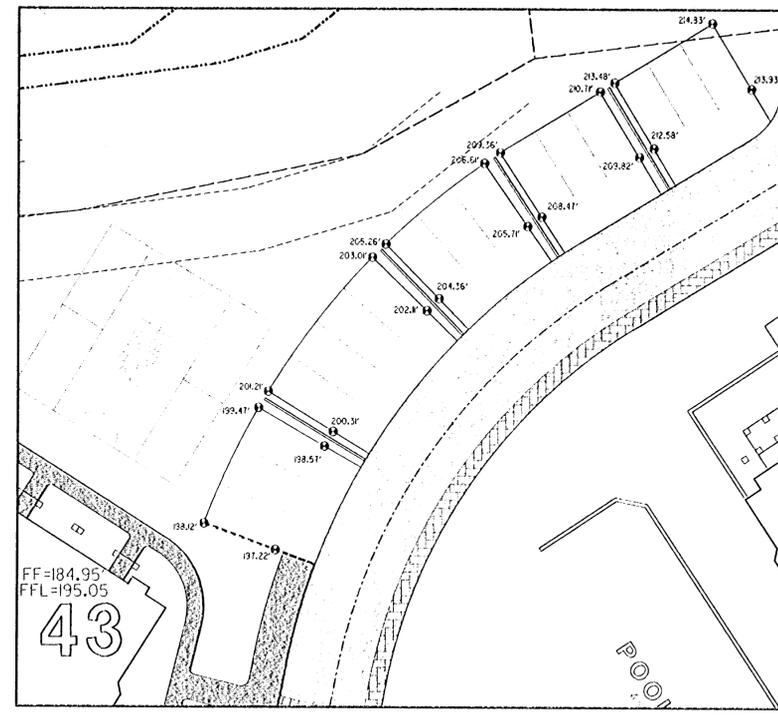
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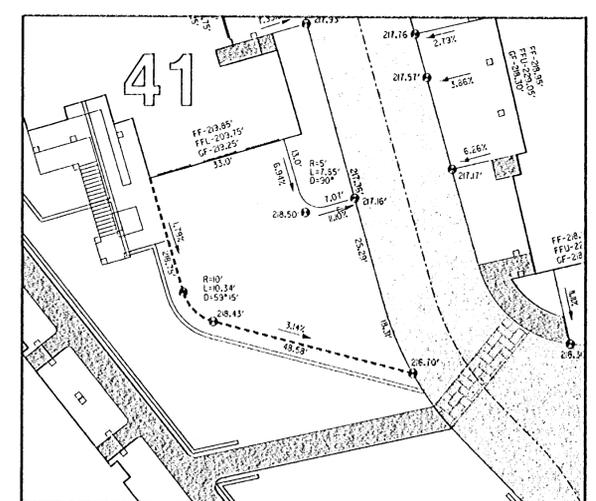
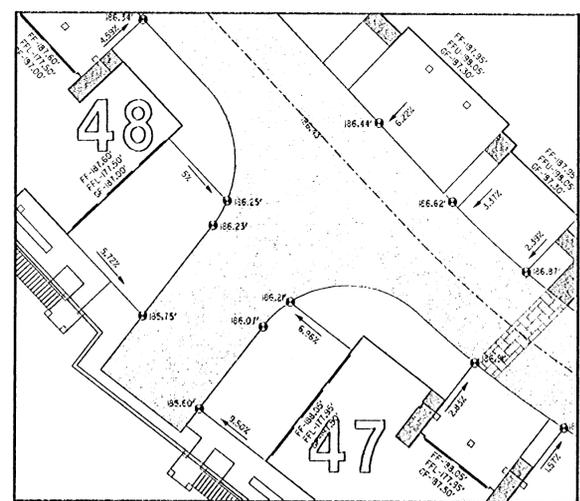
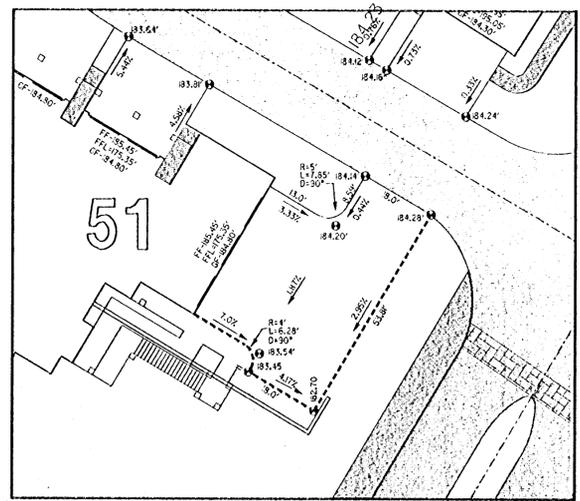
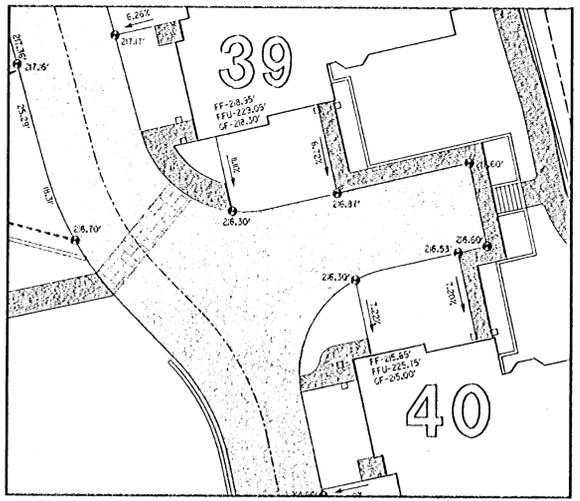
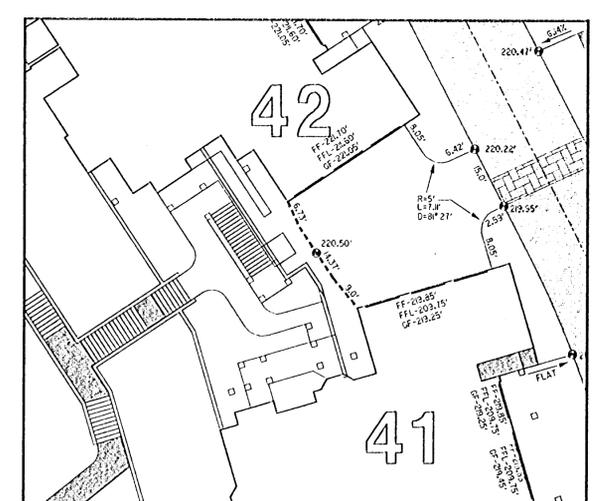
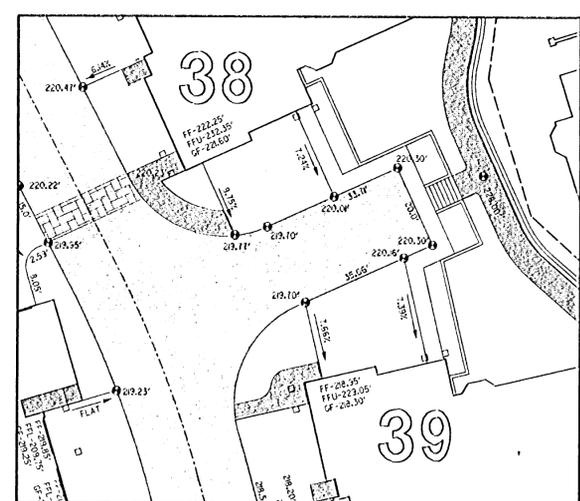
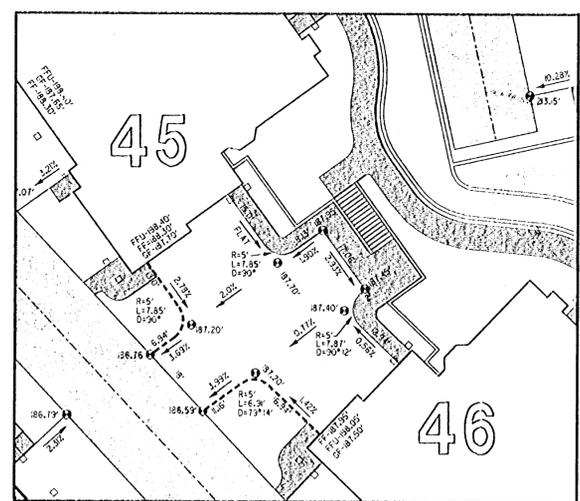
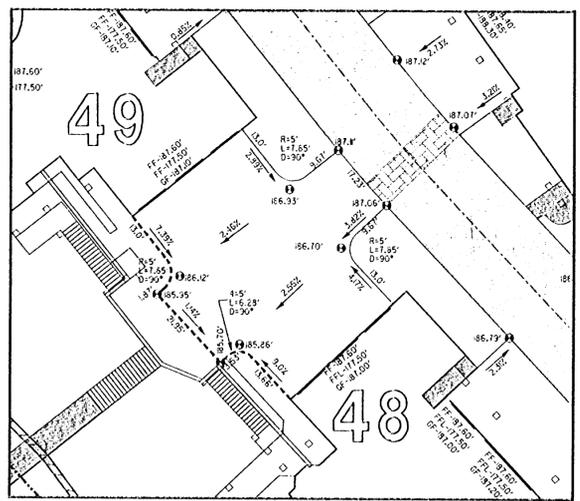
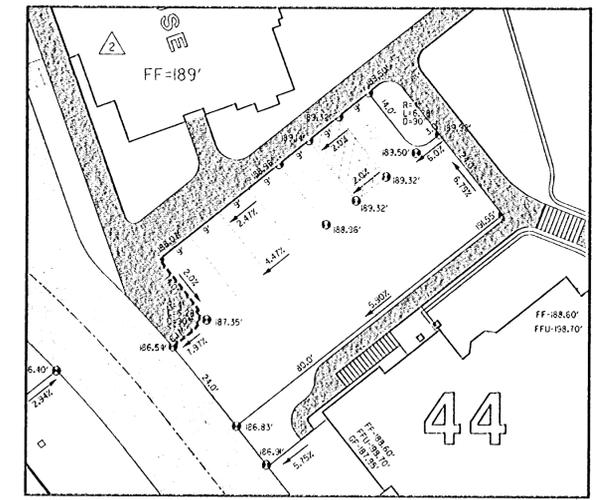


LEGEND

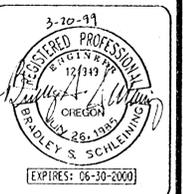
- EXISTING CONTOUR
- CATCH POINT
- DESIGN CONTOUR
- SPOT ELEVATION
- FF - FINISH FLOOR ELEVATION
- FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
- FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
- GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY
- EXTRUDED CURB



- ADD TO LEGEND
- FLIPPED CLUB HOUSE ADJUSTED GRADES IN PARKING AREA AND CLUB HOUSE FF.
- CHANGED SYMBOL
- ALL FINISH FLOOR, TOP OF CURB, AND GARAGE FLOOR HAVE BEEN ADJUSTED



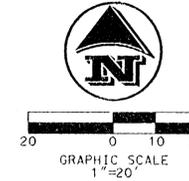
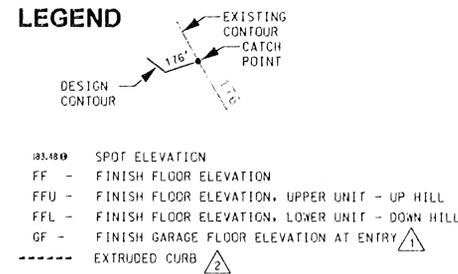
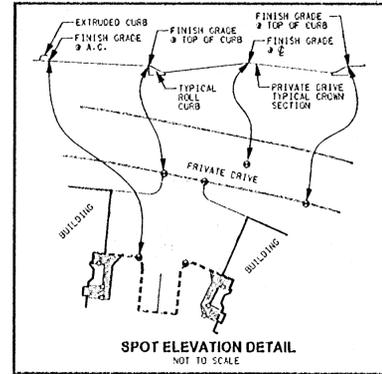
GRADING PLAN DETAILS - SHEET 1 OF 4
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



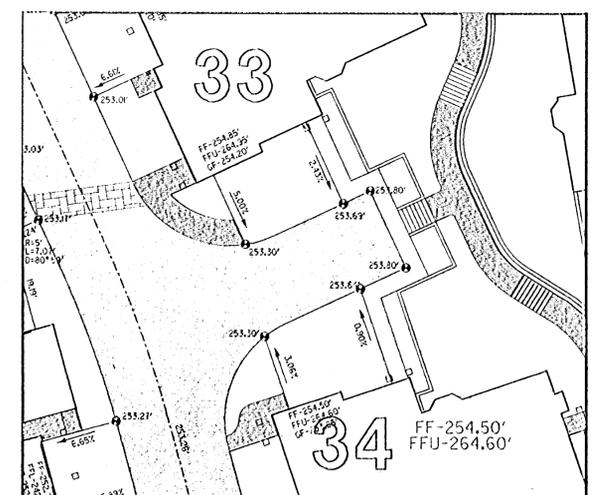
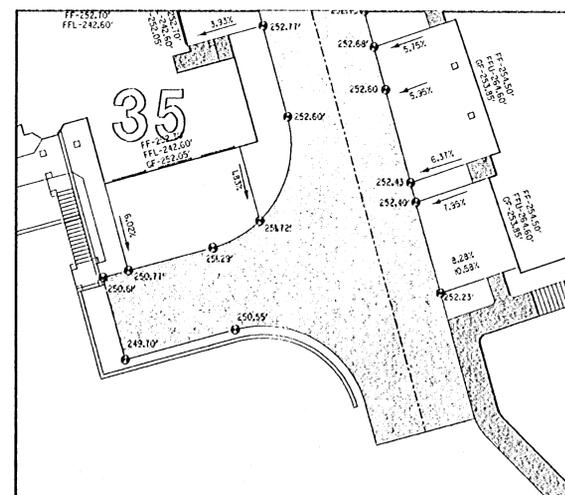
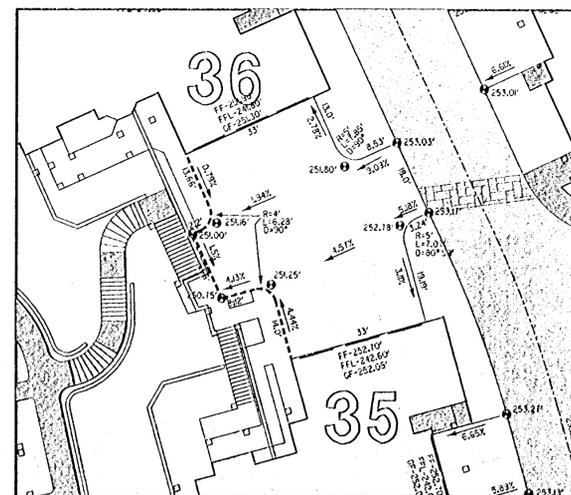
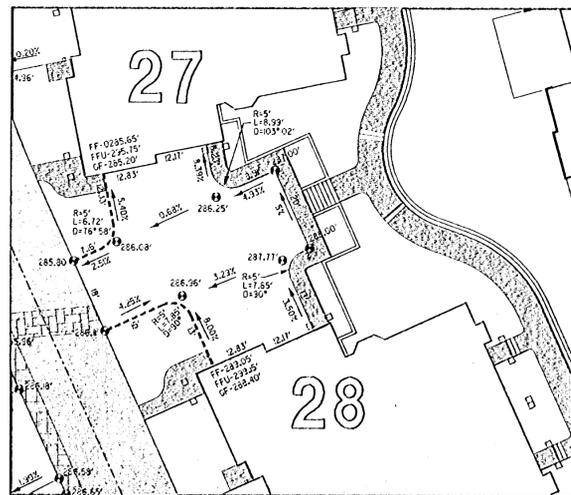
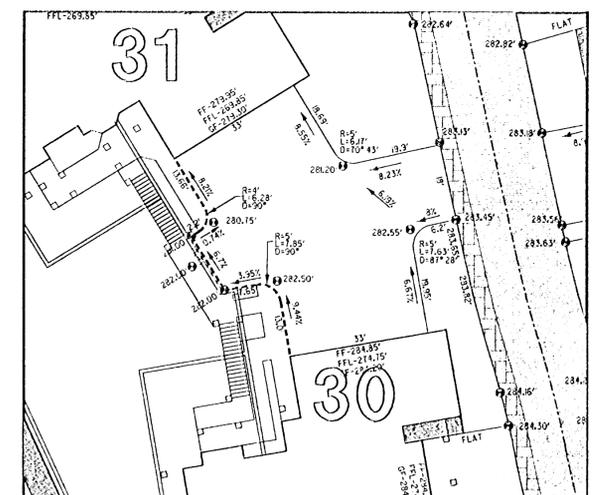
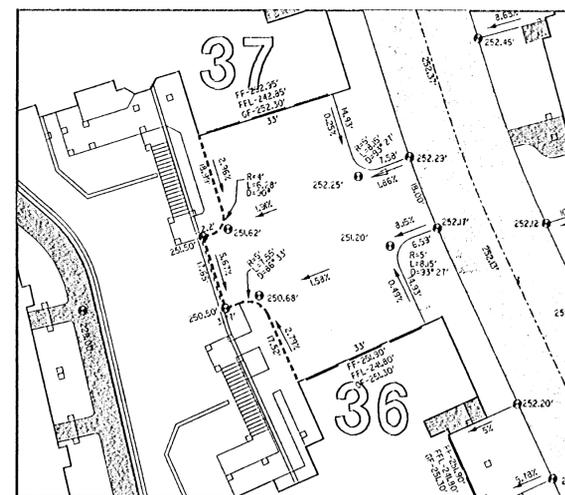
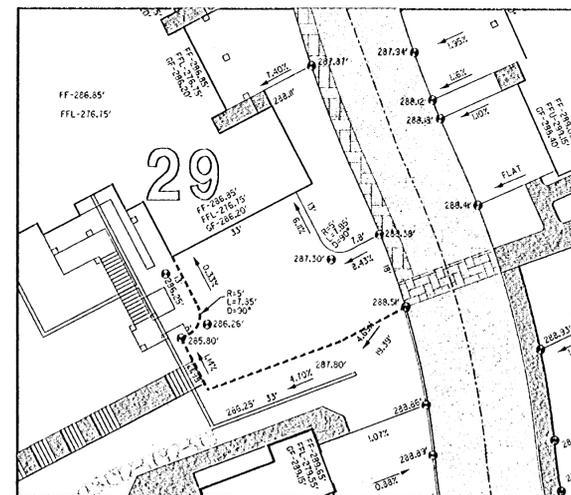
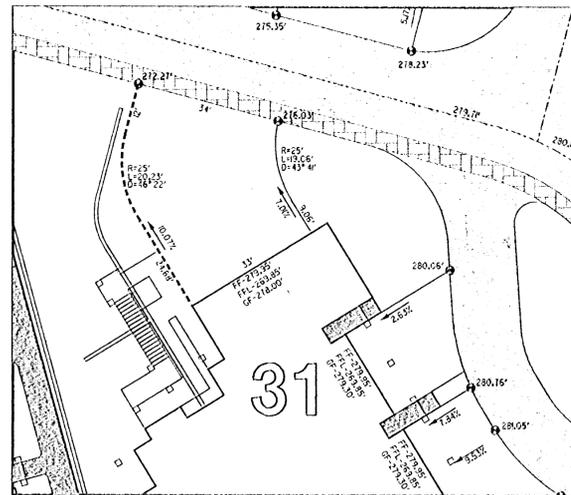
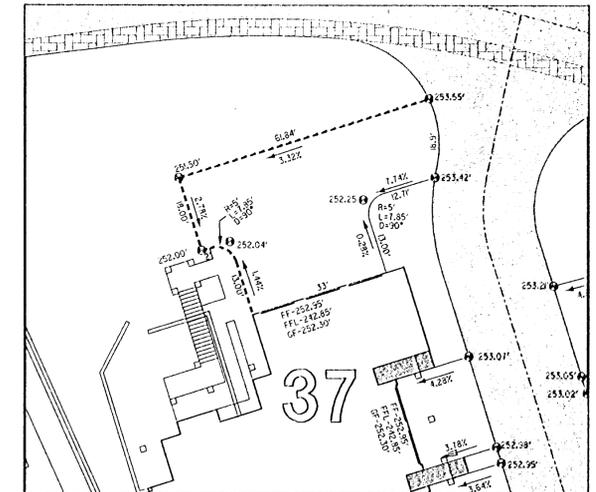
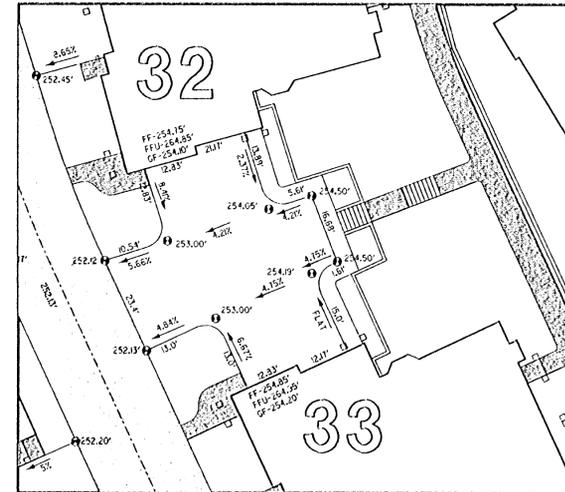
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- 1 ADDED TO LEGEND
- 2 CHANGED SYMBOL
- 3 ADJUSTED ALL FINISH FLOOR, TOP OF CURB, AND GARAGE FLOOR ELEVATIONS

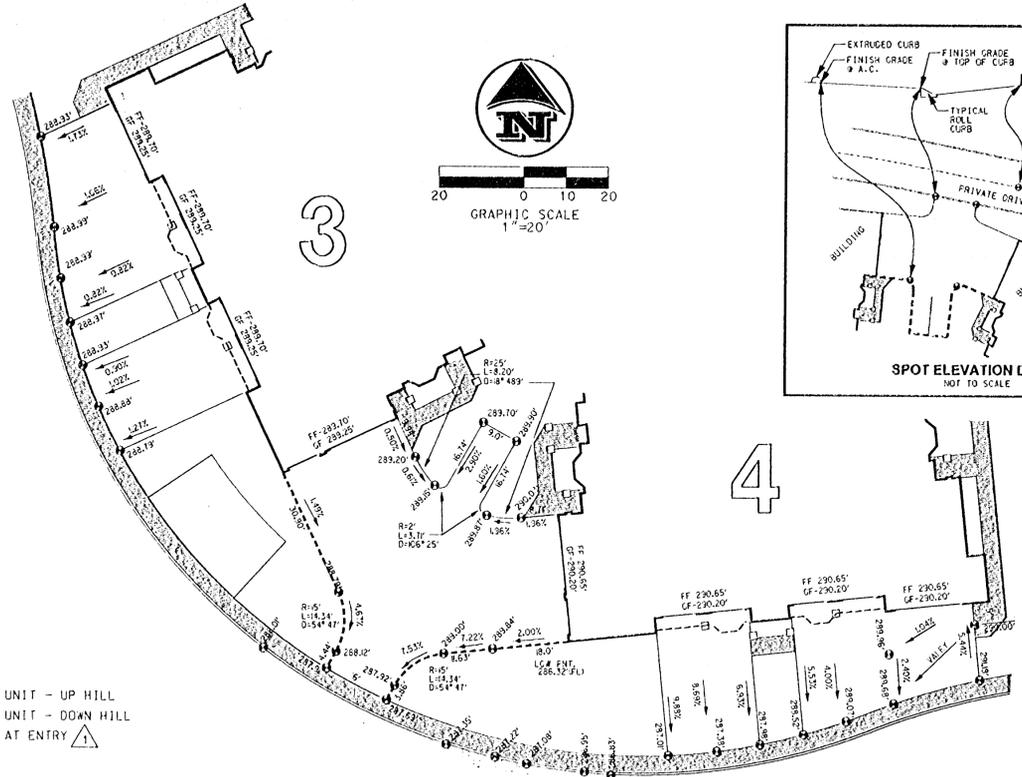
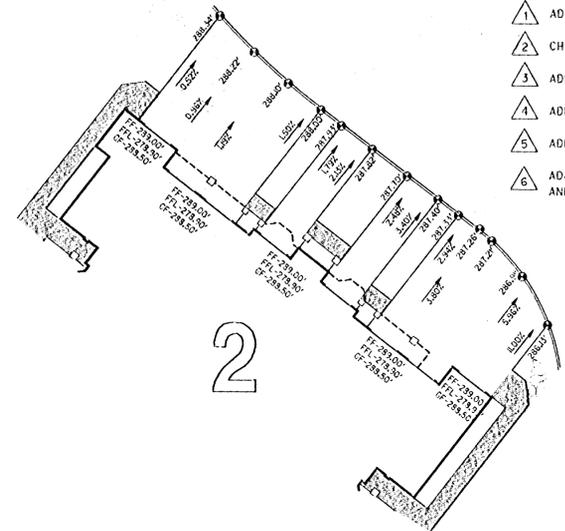
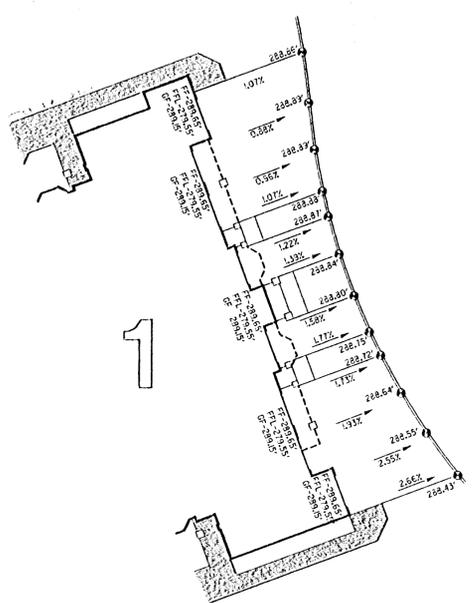


GRADING PLAN DETAILS - SHEET 2 OF 4
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

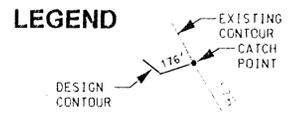


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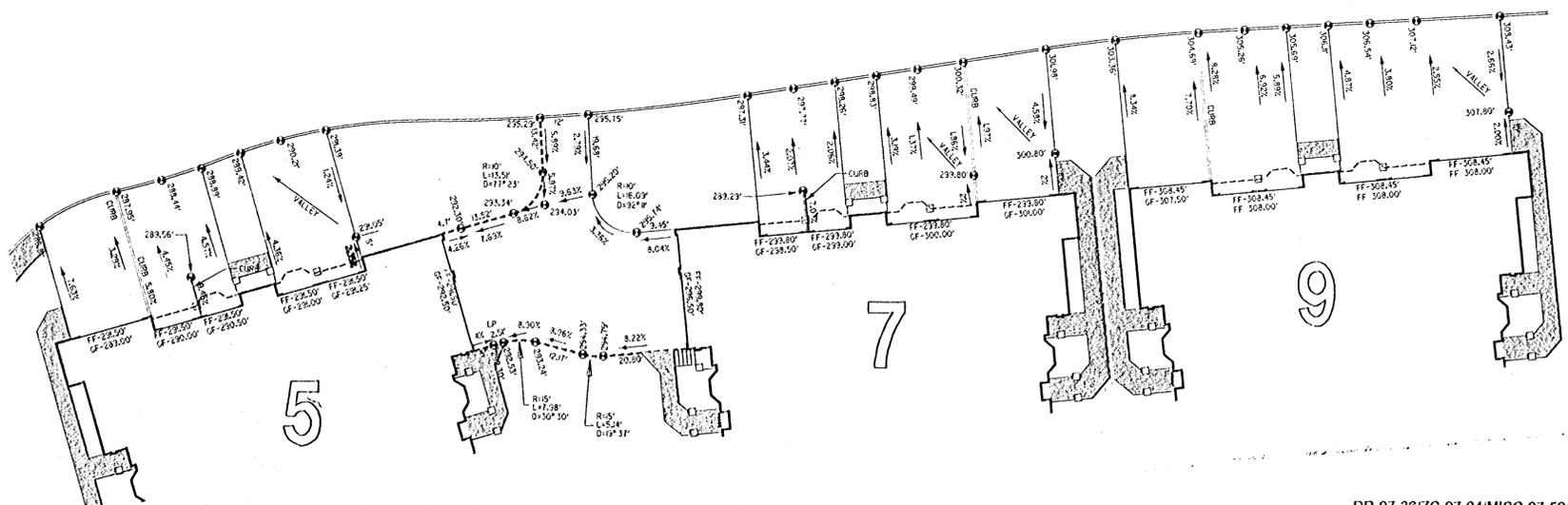
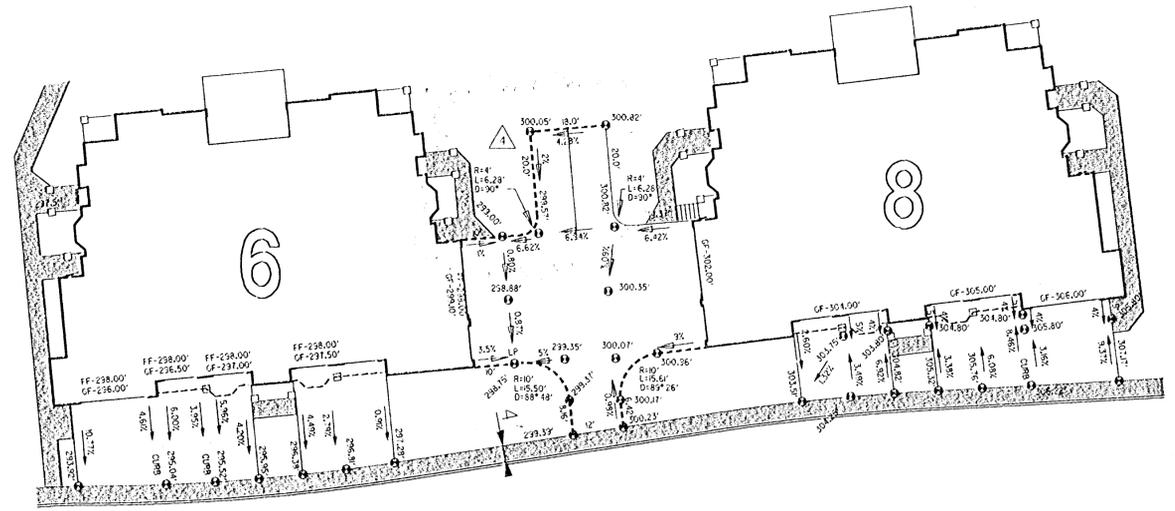
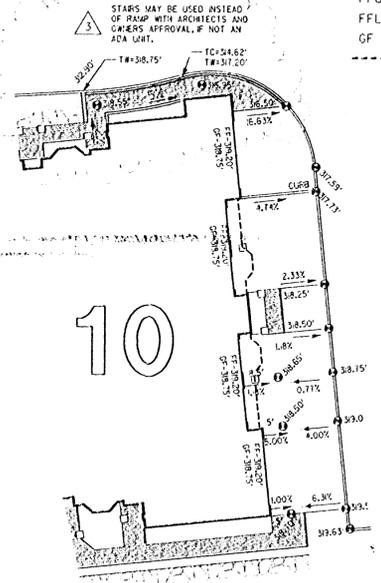
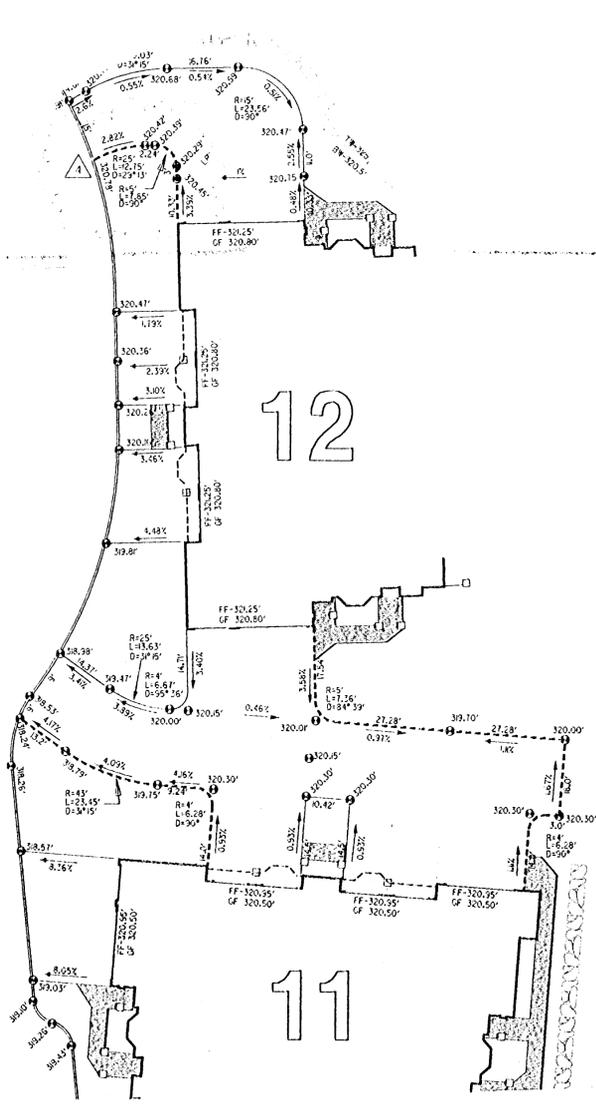
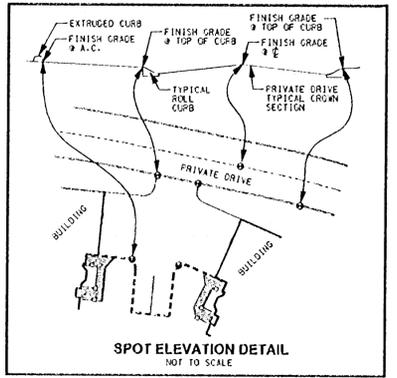
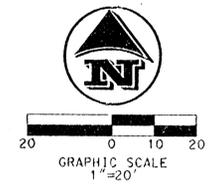




- 1 ADDED NOTE TO LEGEND
- 2 CHANGED SYMBOL
- 3 ADDED DETAILS
- 4 ADDED EXTRUDED CURB
- 5 ADDED MISCELLANEDUS DETAILS TO GRADING
- 6 ADJUSTED ALL FINISHED FLOOR, TOP OF CURB AND GARAGE FLOOR ELEVATIONS



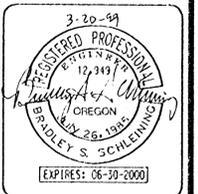
- 88.19 0 SPOT ELEVATION
- FF - FINISH FLOOR ELEVATION
- FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
- FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
- GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY
- EXTRUDED CURB



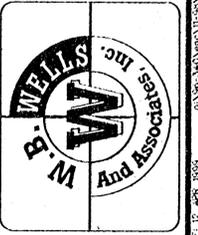
APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

REVISIONS	DESIGNED: BSS	CHECKED: BSS
3-20-99	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.: 96-340	SCALE: 1"=20'

GRADING PLAN DETAILS - SHEET 3 OF 4
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

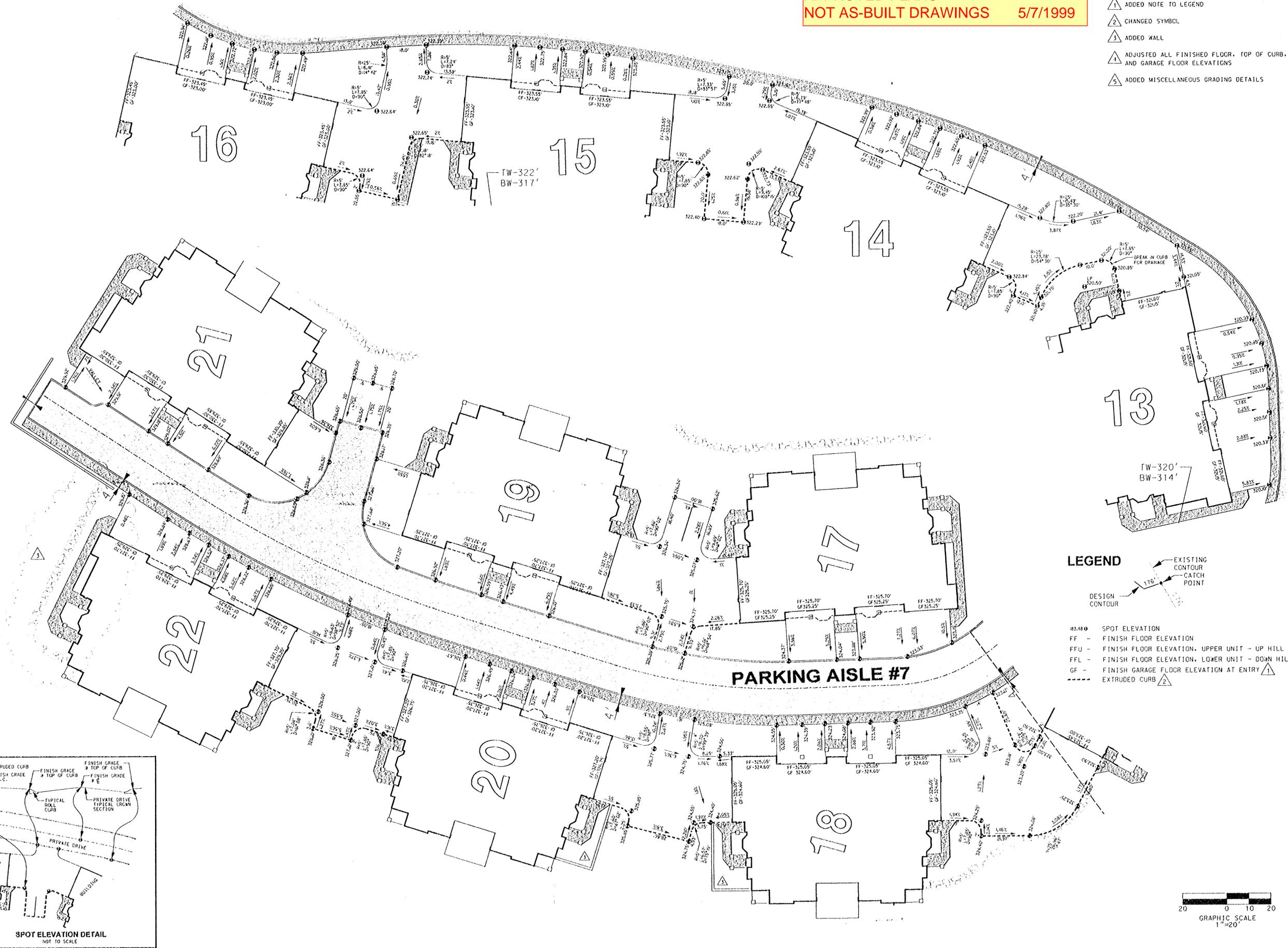


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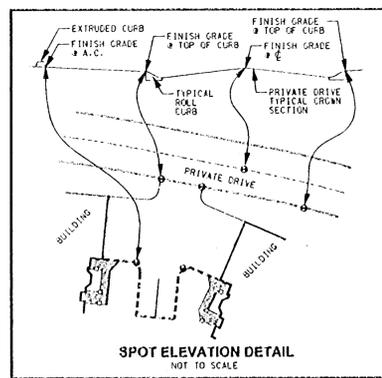
APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

- △ ADDED NOTE TO LEGEND
- △ CHANGED SYMBOL
- △ ADDED WALL
- △ ADJUSTED ALL FINISHED FLOOR, TOP OF CURB, AND GARAGE FLOOR ELEVATIONS
- △ ADDED MISCELLANEOUS GRADING DETAILS



LEGEND

- EXISTING CONTOUR
- DESIGN CONTOUR
- SPOT ELEVATION
- FF - FINISH FLOOR ELEVATION
- FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
- FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
- GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY
- EXTRUDED CURB



REVISIONS

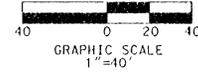
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	DRAWN: BSS/RT		SCALE: 1"=20'

GRADING PLAN DETAILS - SHEET 4 OF 4
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- LEGEND**
- SEDIMENT FENCE
 - INLET PROTECTION
 - GRAVEL CONSTRUCTION ENTRANCE

EROSION CONTROL MEASURES SHOWN ARE THE MINIMUM ACCEPTABLE FOR DRY WEATHER CONSTRUCTION. SEE EROSION CONTROL NOTES SHEET 24.

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

REVISIONS
3-20-99
DESIGNED: BSS
CHECKED: BSS
DRAWN: BSS/RT
DATE: 11-20-98
JOB NO.: 96-340
SCALE:

EROSION CONTROL PLAN - SHEET 1 OF 3
 for
SUMMERLINN APARTMENTS
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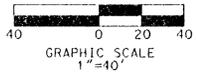
EROSION CONTROL MEASURES SHOWN ARE THE MINIMUM ACCEPTABLE FOR DRY WEATHER CONSTRUCTION. SEE EROSION CONTROL NOTES SHEET 24.

LEGEND

-  SF SEDIMENT FENCE
-  INLET PROTECTION
-  GRAVEL CONSTRUCTION ENTRANCE

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

-  ADDED SEDIMENT FENCE
-  ADDED NOTE

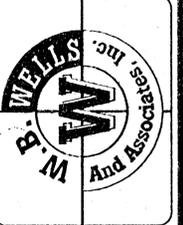


REVISIONS	DESIGNED: BSS	CHECKED: BSS
3-20-99	DRAWN: BSS/RT	DATE: 11-20-98
		JOB NO.: 56-340
		SCALE:

EROSION CONTROL PLAN - SHEET 2 OF 3
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SEE SHEET 22

SEE SHEET 22

- △ ADDED NOTE
- △ CHANGED SYMBOL
- △ ADDED SEDIMENT FENCE

GREENE STREET

13th STREET

EROSION CONTROL MEASURES SHOWN ARE THE MINIMUM ACCEPTABLE FOR DRY WEATHER CONSTRUCTION. SEE EROSION CONTROL NOTES SHEET 24.

IF THIS SITE IS UNDER CONSTRUCTION DURING WET WEATHER THE PONDS MUST BE CONSTRUCTED FIRST AND DEVELOPED INTO SEDIMENTATION PONDS. SITE SPECIFIC ADJUSTMENTS WILL BE NECESSARY

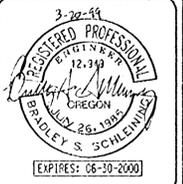
LEGEND

- SF — SEDIMENT FENCE
- ▣ INLET PROTECTION
- ▣ GRAVEL CONSTRUCTION ENTRANCE



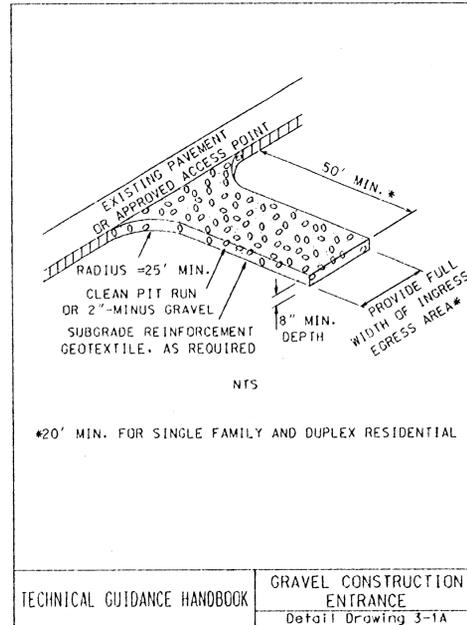
**APPROVED PLANS
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EROSION CONTROL PLAN - SHEET 3 OF 3
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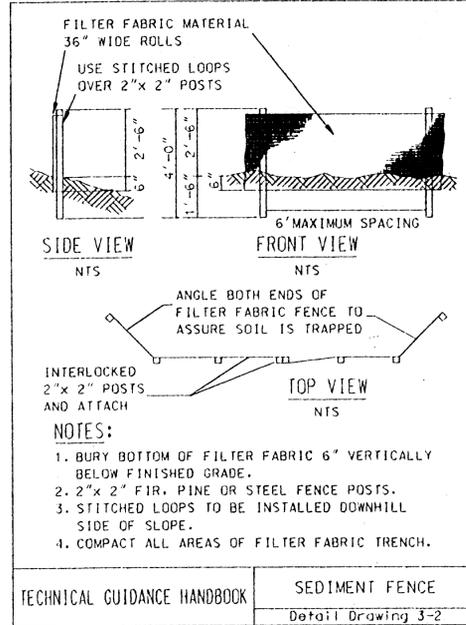


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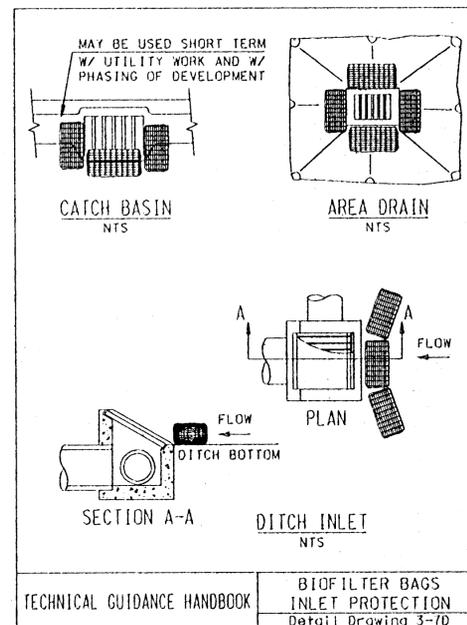




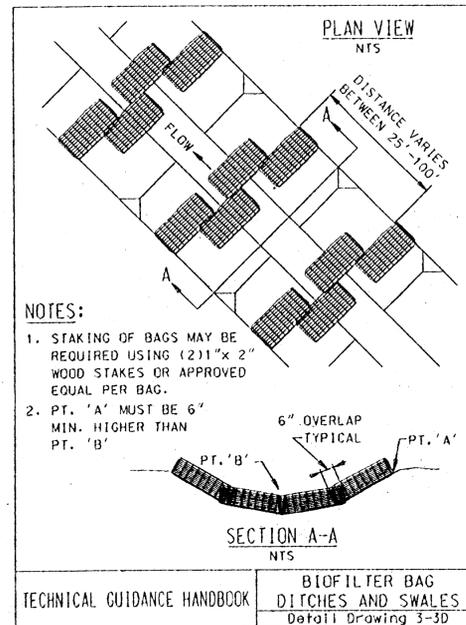
TECHNICAL GUIDANCE HANDBOOK GRAVEL CONSTRUCTION ENTRANCE
Detail Drawing 3-1A



TECHNICAL GUIDANCE HANDBOOK SEDIMENT FENCE
Detail Drawing 3-2



TECHNICAL GUIDANCE HANDBOOK BIOFILTER BAG INLET PROTECTION
Detail Drawing 3-7D



TECHNICAL GUIDANCE HANDBOOK BIOFILTER BAG DITCHES AND SWALES
Detail Drawing 3-3D

EROSION CONTROL NOTES

ALL EROSION CONTROL WORK WILL FOLLOW THE GUIDELINES ESTABLISHED IN OREGON SURFACE WATER QUALITY FACILITIES HANDBOOK, CLACKAMAS COUNTY AUGUST 1994.

APPROVAL OF THIS ESC PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G. SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).

THE IMPLEMENTATION OF THESE ESC PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED AND VEGETATION/LANDSCAPING IS ESTABLISHED.

THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED IN THE FIELD PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE FLAGGED CLEARLY LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE APPLICANT/CONTRACTOR FOR THE DURATION OF CONSTRUCTION.

THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED DRY WEATHER SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NECESSARY FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT-LADEN WATER DOES NOT LEAVE THE SITE.

THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.

THE ESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH, OR WITHIN 48 HOURS OF A STORM EVENT.

AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A TRAPPED CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNSTREAM SYSTEM.

STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.

UNDISTURBED BUFFERS

DOWN HILL UNDISTURBED BUFFER ON PROPERTY OF EQUAL OR GREATER AREA TO DISTURBED AREA ON 10% OR LESS SLOPES MAY BE USED AS AN ALTERNATIVE TO SEDIMENT BARRIERS.

SEDIMENT FENCE

THE FILTER FABRIC SHALL BE PURCHASED ON A CONTINUOUS ROLL CUT TO LENGTH OF THE BARRIER TO AVOID USE OF JOINTS. WHEN JOINTS ARE NECESSARY FILTER CLOTH SHALL BE SPICED ONLY AT A SUPPORT POST WITH A MINIMUM 6" OVERLAP, AND BOTH ENDS SECURELY FASTENED TO THE POST.

THE FILTER FABRIC FENCE SHALL BE INSTALLED TO FOLLOW THE CONTOURS, WHERE FEASIBLE. THE FENCE POSTS SHALL BE SPACED A MAXIMUM OF SIX FEET APART AND DRIVEN SECURELY INTO THE GROUND A MINIMUM OF 18".

A TRENCH SHALL BE EXCAVATED, ROUGHLY 8" WIDE BY 12" DEEP, UPSLOPE AND ADJACENT TO THE WOOD POST TO ALLOW THE FILTER FABRIC TO BE BURIED.

WHEN STANDARD STRENGTH FILTER FABRIC IS USED, A WIRE SUPPORT FENCE SHALL BE FASTENED SECURELY TO THE UPSLOPE SIDE OF THE POSTS USING HEAVY DUTY WIRE STAPLES AT LEAST 1" LONG, TIE WIRE OR HOG RINGS. THE WIRE SHALL EXTEND INTO THE TRENCH A MINIMUM OF 4" AND SHALL NOT EXTEND MORE THAN 30" ABOVE THE ORIGINAL GROUND SURFACE.

THE STANDARD STRENGTH FILTER FABRIC SHALL BE STAPLED OR WIRED TO THE FENCE, AND 6" OF THE FABRIC SHALL BE EXTENDED INTO THE TRENCH. THE FABRIC SHALL NOT EXTEND MORE THAN 30" ABOVE THE ORIGINAL GROUND SURFACE. FILTER FABRIC SHALL NOT BE STAPLED TO EXISTING TREES.

WHEN EXTRA-STRENGTH FILTER FABRIC AND CLOSER POST SPACING ARE USED, THE WIRE MESH SUPPORT FENCE MAY BE ELIMINATED. IN SUCH A CASE, THE FILTER FABRIC IS STAPLED OR WIRED DIRECTLY TO THE POSTS WITH ALL OTHER PROVISIONS OF THE ABOVE STANDARD NOTE FOR STANDARD STRENGTH FILTER FABRIC APPLYING.

SEDIMENT FENCES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFUL PURPOSE, AND AFTER THE UPSLOPE AREA HAS BEEN PERMANENTLY STABILIZED.

SEDIMENT FENCES SHALL BE INSPECTED BY APPLICANT/CONTRACTOR IMMEDIATELY AFTER EACH RAINFALL, AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.

AT NO TIME SHALL MORE THAN ONE FOOT DEPTH OF SEDIMENT BE ALLOWED TO ACCUMULATE BEHIND STRAW BALE SEDIMENT BARRIERS AND/OR BIOFILTER BAGS. SEDIMENT SHOULD BE REMOVED OR REGRADED INTO THE SLOPE, OR NEW LINES OF BARRIERS INSTALLED UPHILL OF SEDIMENT-LADEN BARRIERS.

BIOFILTER BAGS

BIOFILTER BAGS SHALL BE CLEAN 100% RECYCLED WOOD PRODUCT WASTE. SIZE OF BAG SHALL BE 18" X 8" X 30" AND WEIGH APPROXIMATELY 45 POUNDS, AND MADE OF 1/2 INCH PLASTIC MESH.

SEEDING / MULCHING

RECOMMENDED SEED MIXTURE:

TYPE	PERCENT BY WEIGHT
ELKA DWARF PERENNIAL RYEGRASS	80
CREeping RED FESCUE	20
APPLICATION RATE:	100 POUNDS MINIMUM PER ACRE

FERTILIZER SHALL BE 12-16-8 WITH 50% OF THE NITROGEN DERIVED FROM UREA-FORMALDEHYDE, AND APPLIED AT A RATE OF 400 POUNDS PER ACRE.

SEED AND MULCH AT A RATE OF 2000 POUNDS PER ACRE WITH HEAVY BONDING AGENT OR NETTING AND ANCHORS. MULCH SHALL BE A WOOD CELLULOSE FIBER OR OTHER MATERIAL SUITABLE FOR HYDROMULCHING.

TEMPORARY/PERMANENT HYDROSEEDING OR ACCEPTABLE SEEDING AND MULCHING MUST BE PROVIDED WHENEVER PERENNIAL COVER CANNOT BE ESTABLISHED ON SITES WHICH WILL BE EXPOSED FOR 60 DAYS OR MORE.

DESIGN CRITERIA/SPECIFICATIONS: TEMPORARY EROSION CONTROL GRASSES

A. TEMPORARY GRASS COVER MEASURES MUST BE FULLY ESTABLISHED BY NOVEMBER 1ST OR OTHER COVER MEASURES WILL HAVE TO BE IMPLEMENTED UNTIL ADEQUATE GRASS COVERAGE IS ACHIEVED. TO ESTABLISH AN ADEQUATE GRASS STAND FOR CONTROLLING EROSION BY NOVEMBER 1, IT IS RECOMMENDED THAT SEEDING AND MULCHING OCCUR BY SEPTEMBER 1. (ANY SEEDING PLANNED AFTER SEPTEMBER 1, MUST HAVE DISTRICT APPROVAL AND REQUIRES THE USE OF PRE-GERMINATED GRASS SEEDS.)

B. HYDROMULCH SHALL BE APPLIED WITH GRASS SEEDS AT A RATE OF 2,000 LB /ACRE ON SLOPES STEEPER THAN 10%. HYDROSEED AND MULCH SHALL BE APPLIED WITH A BONDING AGENT (TACKIFIER). APPLICATION RATE AND METHODOLOGY TO BE IN ACCORDANCE WITH SEED SUPPLIER'S RECOMMENDATIONS.

C. DRY, LOOSE, NEED-FREE STRAW USED AS MULCH SHALL BE APPLIED AT DOUBLE THE HYDROMULCH APPLICATION REQUIREMENT (4,000 LB/ACRE). ANCHOR STRAW BY WORKING IN BY HAND OR WITH EQUIPMENT (ROLLERS, CLEAT TRACKS, ETC.).

D. MULCH SHALL BE SPREAD UNIFORMLY IMMEDIATELY FOLLOWING SEEDING.

E. SOIL PREPARATION: TOP SOIL SHOULD BE PREPARED ACCORDING TO LANDSCAPE PLANS, IF AVAILABLE, OR RECOMMENDATIONS OF GRASS SEED SUPPLIER. IT IS RECOMMENDED THAT SLOPES BE ROUGHENED BEFORE SEEDING BY "TRACK-WALKING" (DRIVING A CRAWLING TRACTOR UP AND DOWN SLOPES TO LEAVE A PATTERN OF CLEAT IMPRINTS PARALLEL TO SLOPE CONTOUR) OR OTHER METHOD TO PROVIDE MORE STABLE SITES FOR SEEDS TO REST.

F. FERTILIZER FOR GRASS SEEDS: IN ACCORDANCE WITH SUPPLIER'S RECOMMENDATIONS. DEVELOPMENT AREAS WITHIN 50 FEET OF WATER BODIES AND WETLANDS MUST USE A NON-PHOSPHORUS FERTILIZER.

G. NETTING AND ANCHORS, AS NEEDED. FOR DISTURBED AREAS ON SLOPES AND IN DITCHES/SWALES, BIODEGRADABLE NETTING, OR JUTE IS DESIRABLE AND MAY BE USED INSTEAD OF BONDING AGENTS TO PROVIDE A STABLE AREA FOR SEEDING. NETTING SHOULD BE ANCHORED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

H. WATERING: SEEDING SHALL BE SUPPLIED WITH ADEQUATE MOISTURE TO ESTABLISH GRASS. SUPPLY WATER AS NEEDED, ESPECIALLY IN ABNORMALLY HOT OR DRY WEATHER OR ON ADVERSE SITES. WATER APPLICATION RATES SHOULD BE CONTROLLED TO PROVIDE ADEQUATE MOISTURE WITHOUT CAUSING RUNOFF.

I. RE-SEEDING: AREAS WHICH FAIL TO ESTABLISH GRASS COVER ADEQUATE TO PREVENT EROSION SHALL BE RE-SEEDED AS SOON AS SUCH AREAS ARE IDENTIFIED, AND ALL APPROPRIATE MEASURES TAKEN TO ESTABLISH ADEQUATE COVER.

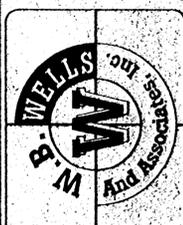
REVISIONS	ADDED NOTE
3-20-99	

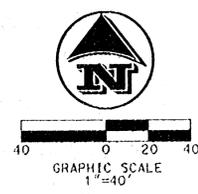
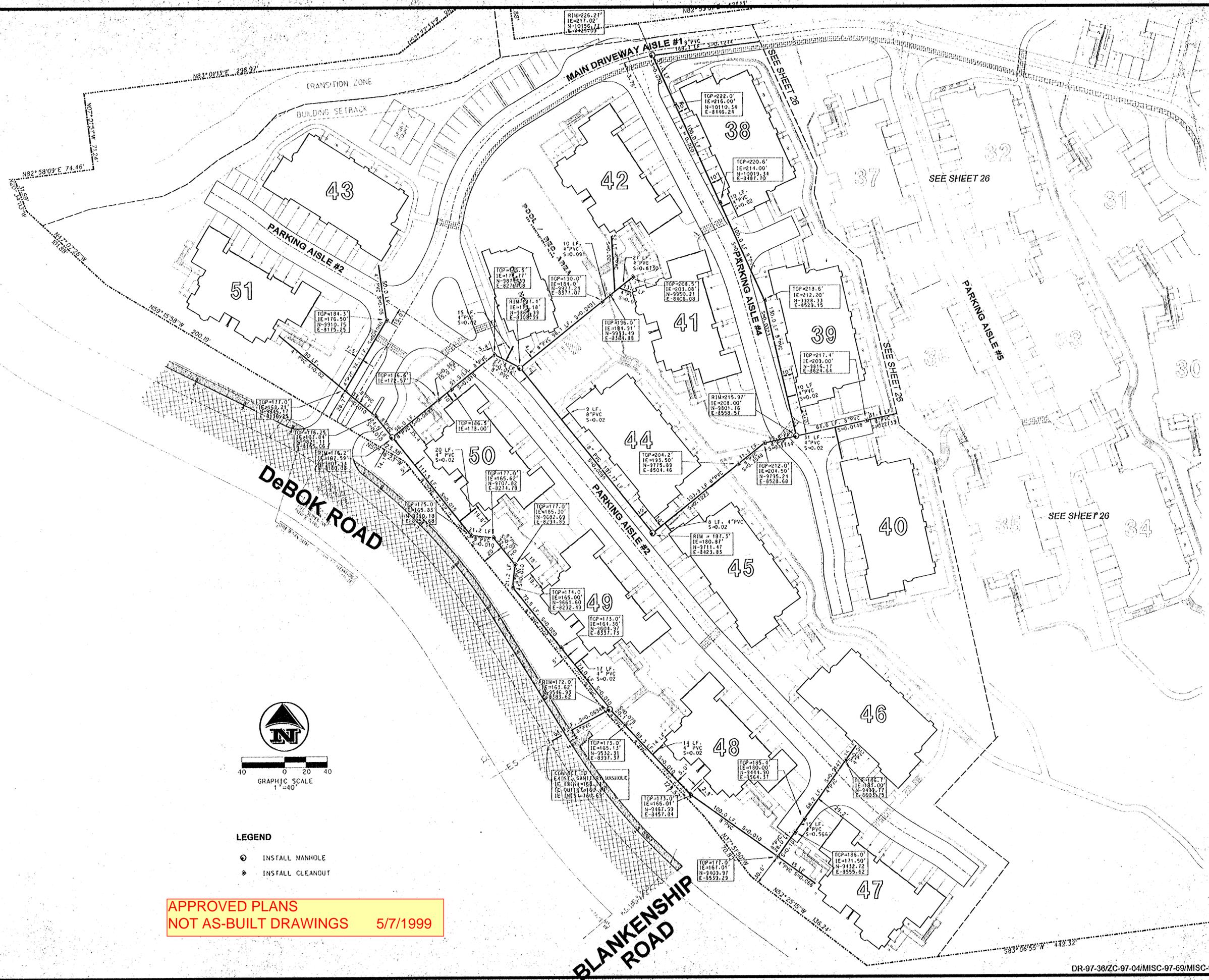
DESIGNED: BSS
CHECKED: BSS
DRAWN: BSS/RT
DATE: 11-20-98
JOB NO.: 96-340
SCALE:

EROSION CONTROL NOTES AND DETAILS
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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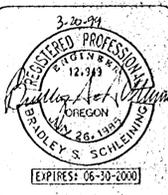


- LEGEND**
- INSTALL MANHOLE
 - INSTALL CLEANOUT

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

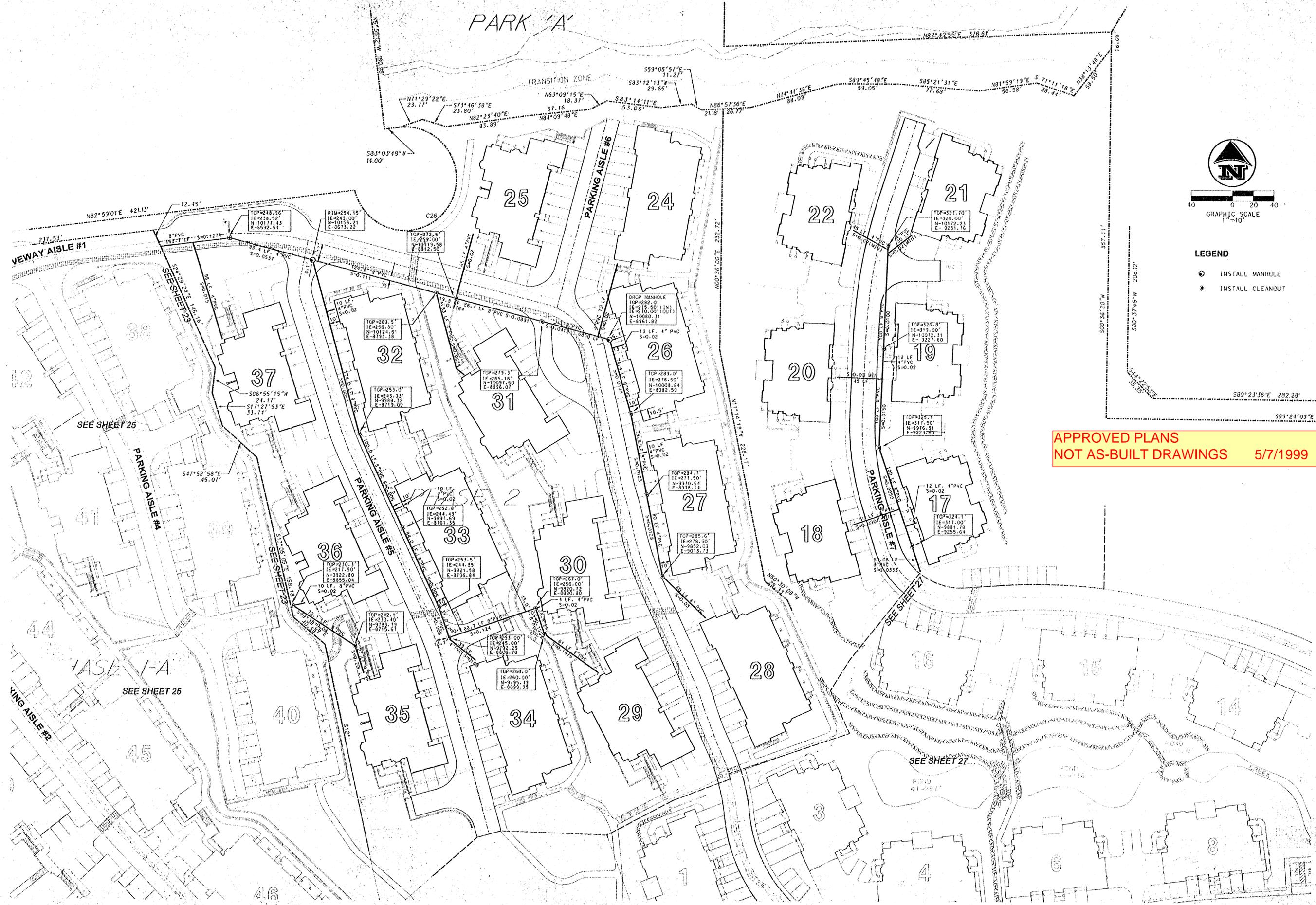
REV. NO.	DATE	SCALE
3-20-99	DESIGNED: BSS CHECKED: BSS	DATE: 11-20-98
	DRAWN: BSS/RT	JOB NO.: 96-340

SANITARY SEWER PLAN SHEET 1 OF 3
for
SUMMERLINN APARTMENTS
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- LEGEND**
- INSTALL MANHOLE
 - ◐ INSTALL CLEANOUT

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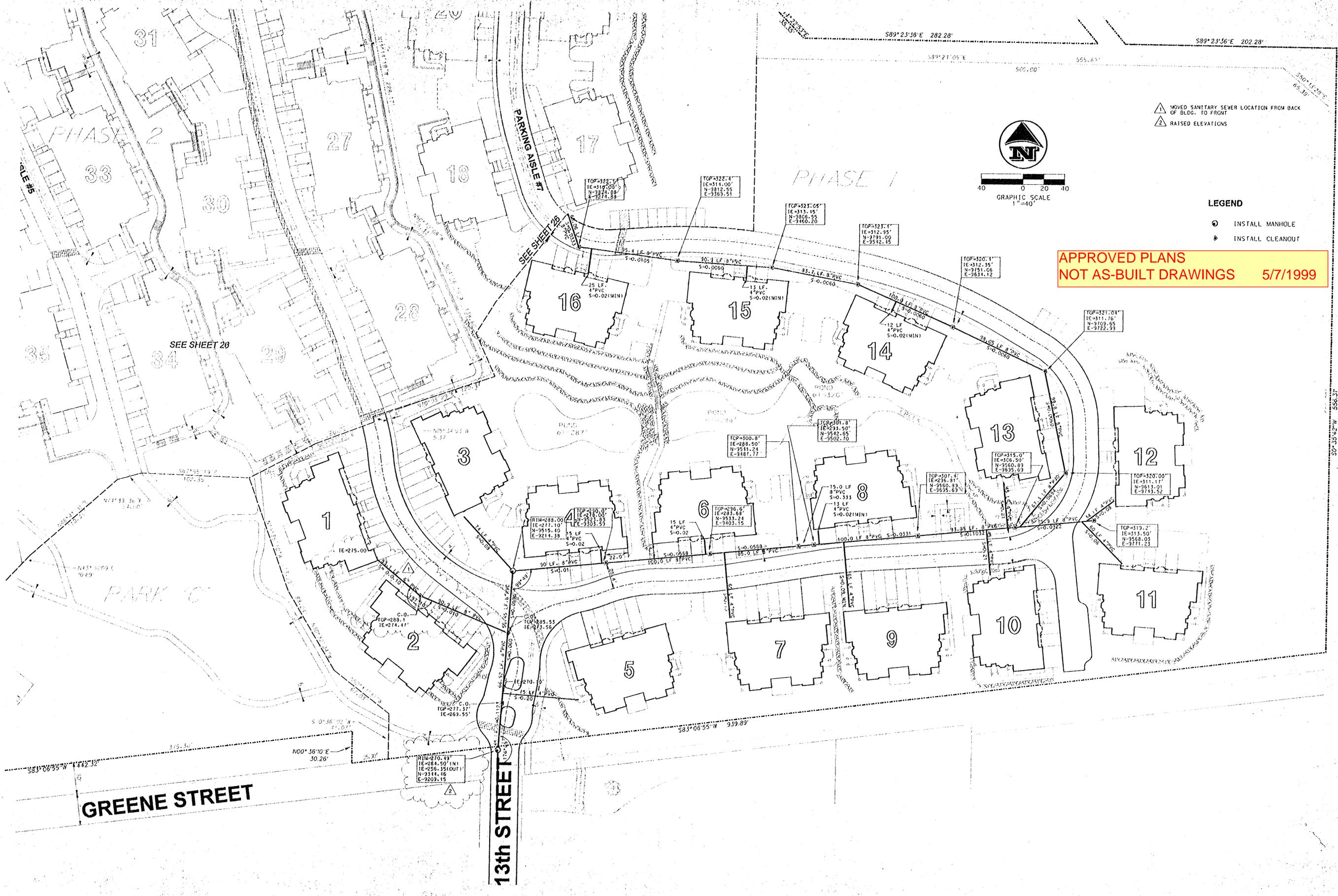
REVISIONS	3-20-99
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DRAWN: BSS/RT	DATE: 11-20-98
JOB NO.: 36-340	SCALE:

SANITARY SEWER PLAN SHEET 2 OF 3
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CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



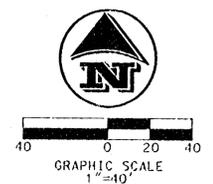
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**APPROVED PLANS
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- ▲ MOVED SANITARY SEWER LOCATION FROM BACK OF BLDG. TO FRONT
- ▲ RAISED ELEVATIONS



- LEGEND**
- INSTALL MANHOLE
 - INSTALL CLEANOUT

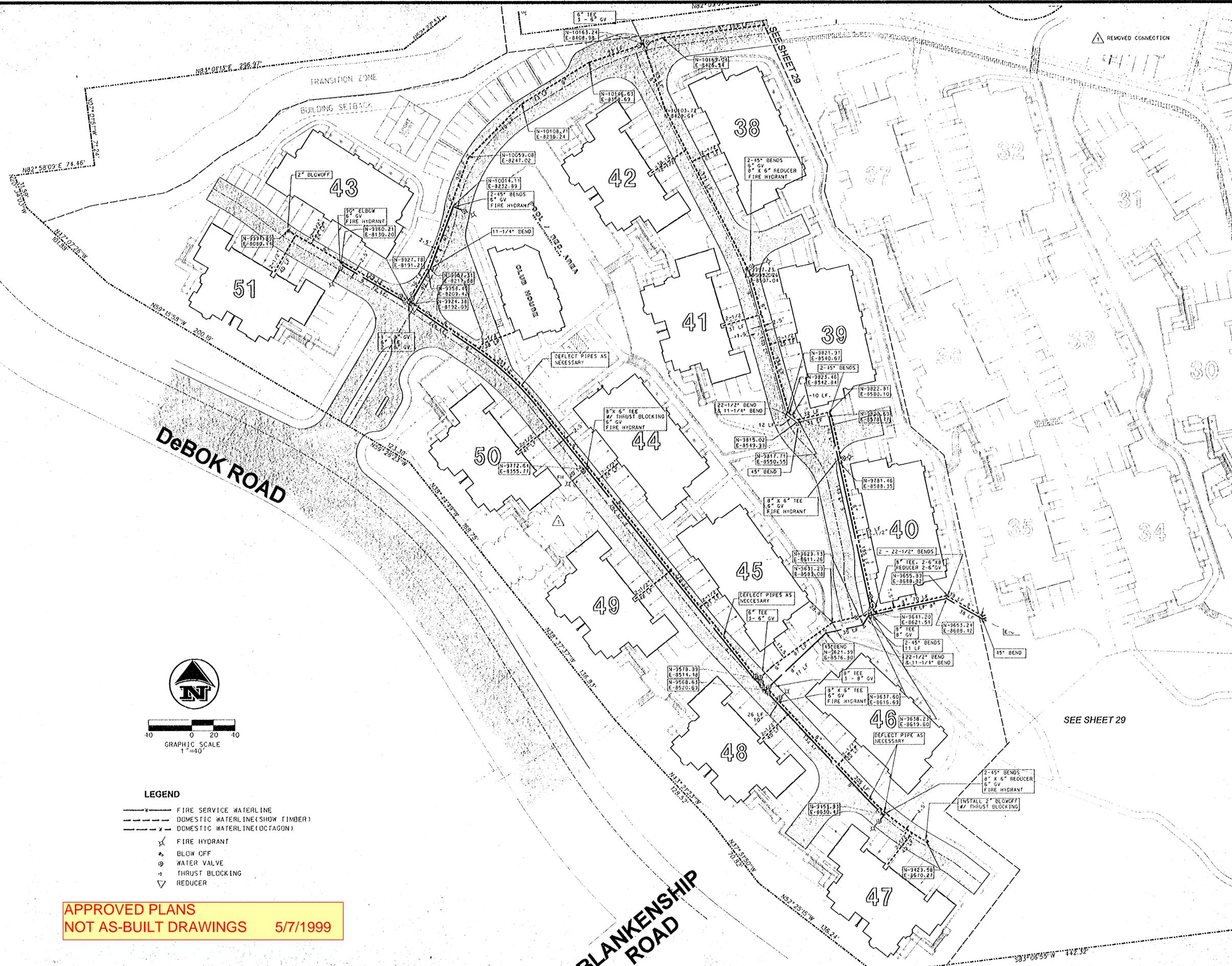
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3-20-99	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.: 96-340	SCALE:

SANITARY SEWER PLAN SHEET 3 OF 3
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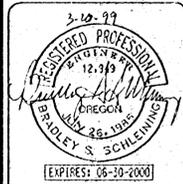


APPROVED PLANS
 NOT AS-BUILT DRAWINGS 5/7/1999

- LEGEND**
- x— FIRE SERVICE WATERLINE
 - - - DOMESTIC WATERLINE (SHOW FIMBER)
 - - - DOMESTIC WATERLINE (OCTAGON)
 - FIRE HYDRANT
 - ⊗ BLOW OFF
 - ⊕ WATER VALVE
 - ⊖ THRUST BLOCKING
 - ▽ REDUCER

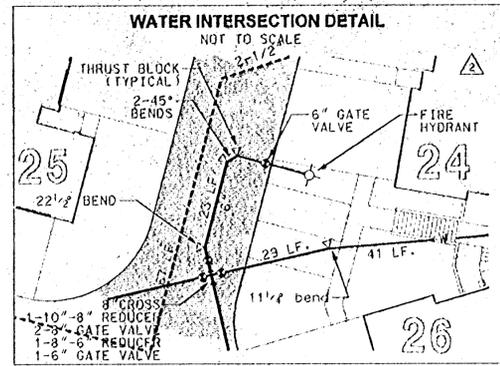
REVISIONS	DESIGNED: BSS	CHECKED: BSS
3-20-95	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.: 56-340	SCALE:

WATER PLAN SHEET 1 OF 3
 for
SUMMERLINN APARTMENTS
 CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

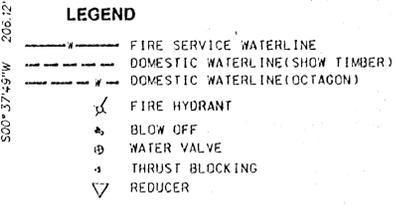
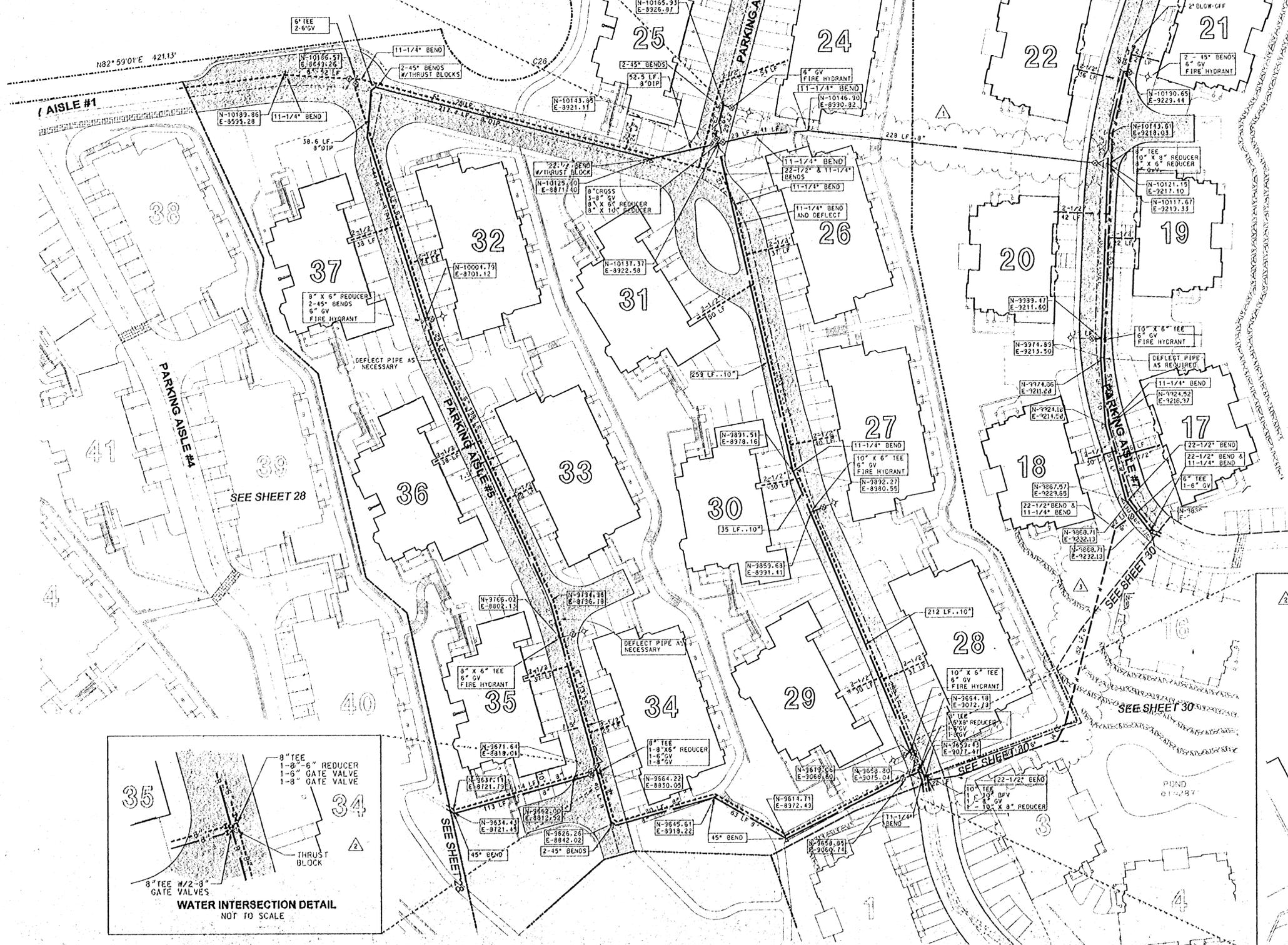


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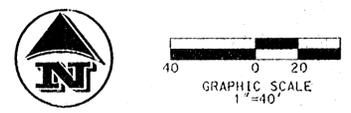




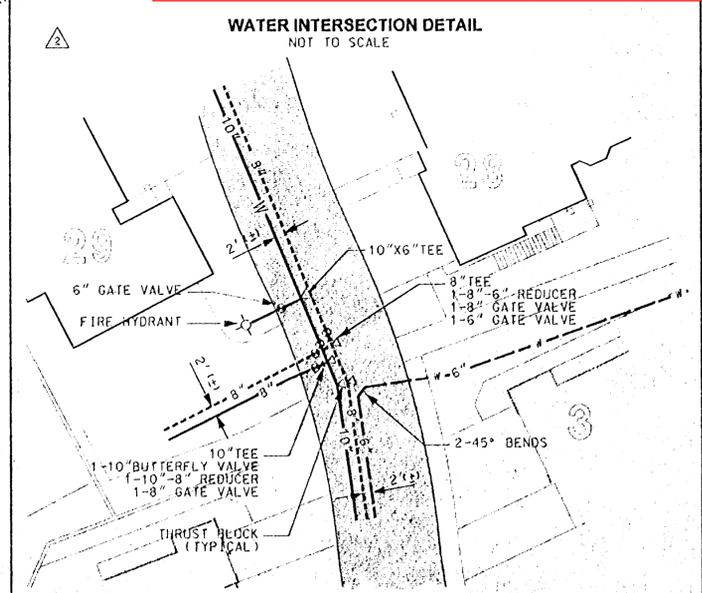
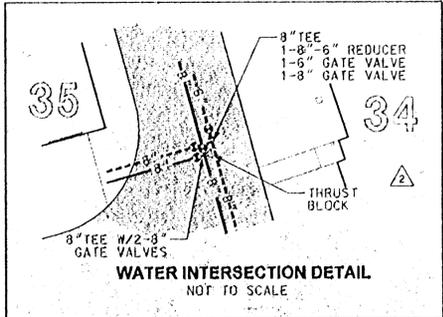
- △ REMOVED DOMESTIC WATERLINE AND METERS
- △ ADDED DETAILS
- △ ADDED DOMESTIC WATERLINE LOOP CONNECTION



NOTE: BUILDINGS 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, & 37 MAY REQUIRE WATER PRESSURE BOOSTERS TO MEET POTABLE WATER PRESSURE REQUIREMENTS AS SET FORTH BY SYSTEM DESIGN CONSULTANTS, INC. PRESSURE REQUIREMENTS ARE 50 PSI (MINIMUM,) SERVICING THE BUILDING.



APPROVED PLANS
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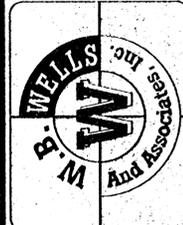


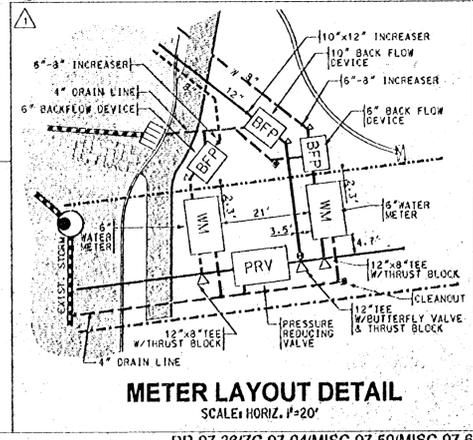
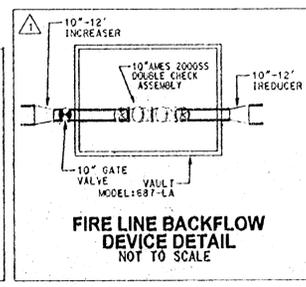
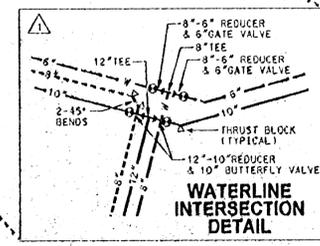
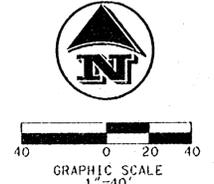
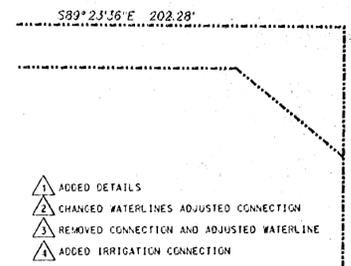
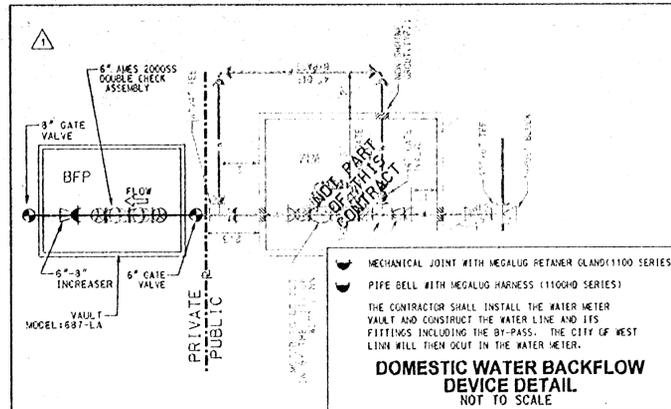
3-20-99	DESIGNED: BSS	CHECKED: BSS	DATE: 11-20-98
	DRAWN: BSS/RT		SCALE:
	JOB NO.: 56-540		

WATER PLAN SHEET 2 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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GREENE STREET

13th STREET

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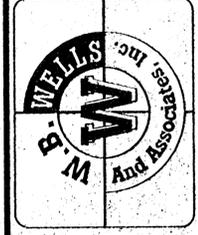
REVISIONS

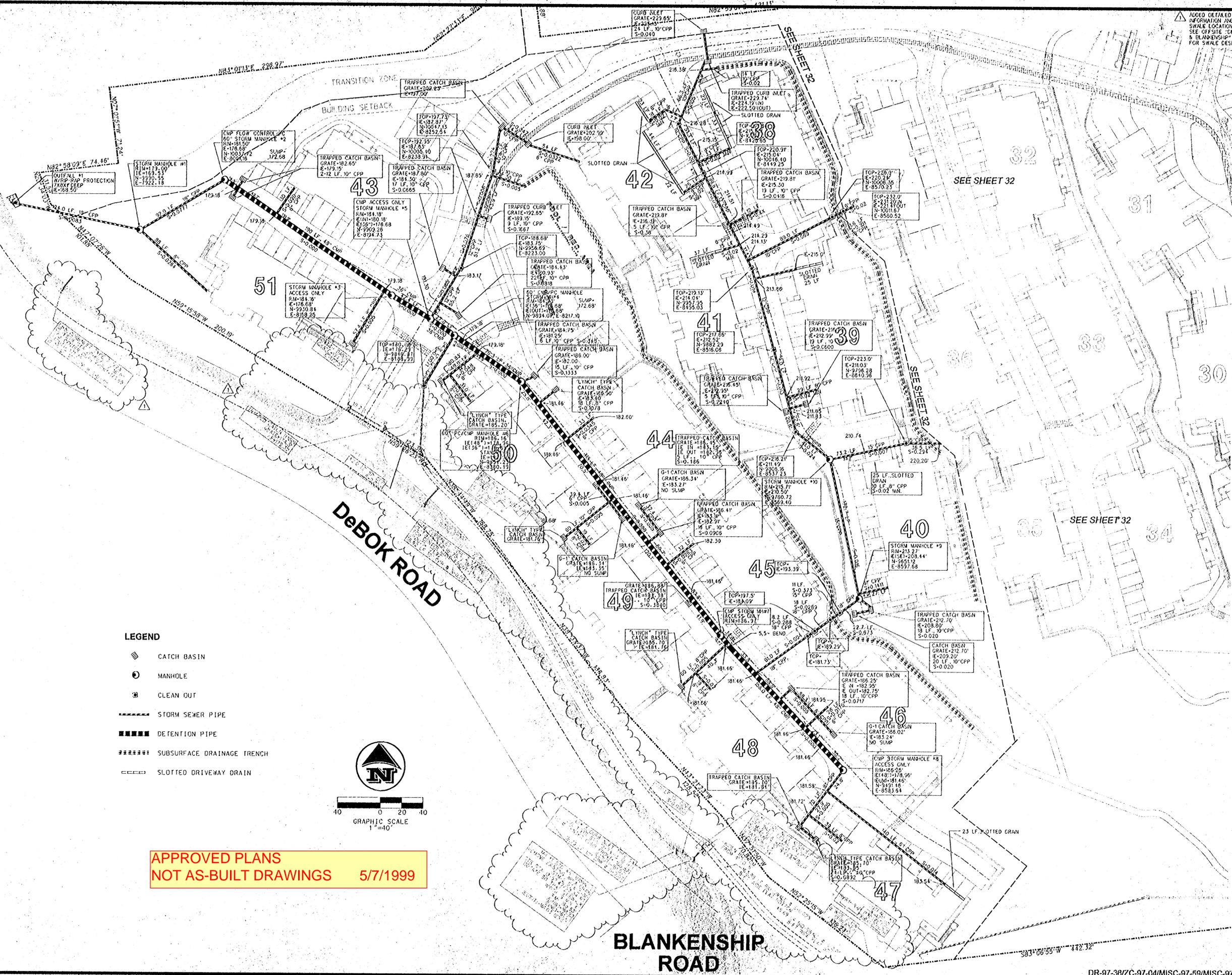
3-20-95	
DESIGNED: BSS	CHECKED: BSS
DRAWN: BSS/RT	DATE: 11-20-95
JOB NO.: 96-340	SCALE:

WATERLINE PLAN SHEET 3 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

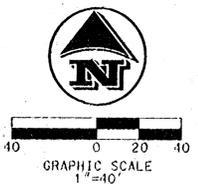


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- LEGEND**
- CATCH BASIN
 - MANHOLE
 - CLEAN OUT
 - STORM SEWER PIPE
 - DETENTION PIPE
 - SUBSURFACE DRAINAGE TRENCH
 - SLOTTED DRIVEWAY DRAIN

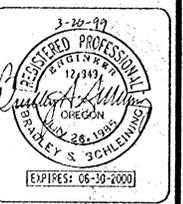


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ADDED DETAILED INFORMATION AND SWALE LOCATION SEE OFFSITE DEBOK & BLANKENSHIP PLANS FOR SWALE DESIGN

REVISIONS	DESIGNED: BSS	CHECKED: BSS
3-20-99	DRAWN: BSS/ART	DATE: 1-11-20-98
		JOB NO.: 1-96-340
		SCALE:

STORM SEWER PLAN SHEET 1 OF 3
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△ ADJUST STORM LINE, ADDED DETAILS
 △ ADDED STORM LINE

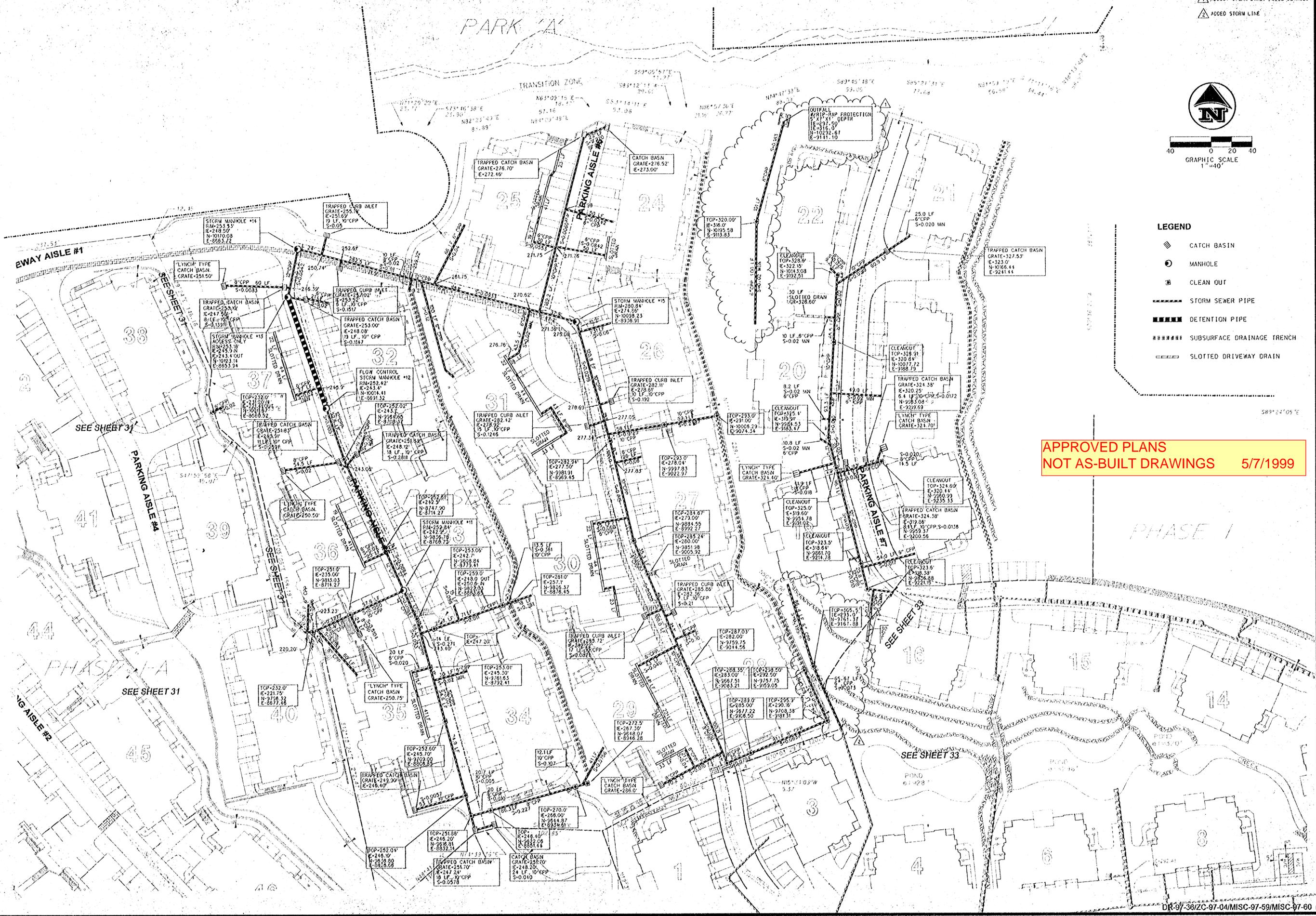


0 20 40
 GRAPHIC SCALE
 1" = 40'

LEGEND

- CATCH BASIN
- MANHOLE
- CLEANOUT
- STORM SEWER PIPE
- DETENTION PIPE
- SUBSURFACE DRAINAGE TRENCH
- SLOTTED DRIVEWAY DRAIN

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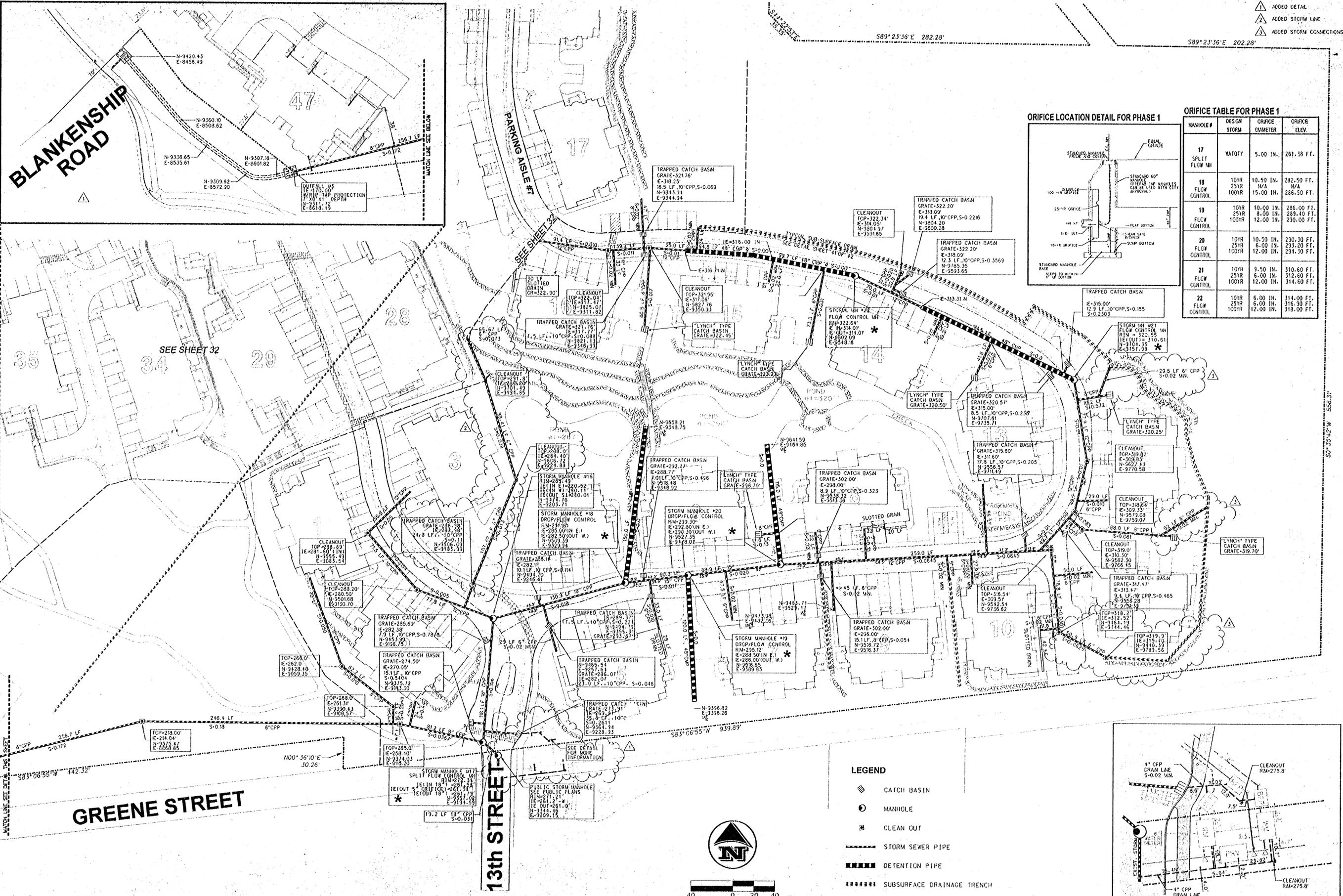
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	JOB NO.: 56-340	SCALE:

STORM SEWER PLAN SHEET 2 OF 3
 for
SUMMERLINN APARTMENTS
 CITY OF WEST Linn, CLACKAMAS COUNTY, OREGON



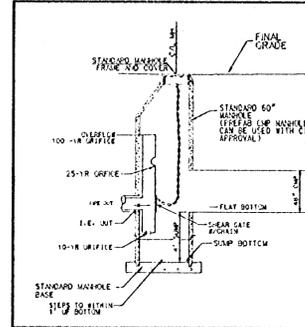
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- ▲ ADDED DETAIL
- ▲ ADDED STORM LINE
- ▲ ADDED STORM CONNECTIONS

ORIFICE LOCATION DETAIL FOR PHASE 1



ORIFICE TABLE FOR PHASE 1

MANHOLE#	DESIGN STORM	ORIFICE DIAMETER	ORIFICE ELEV.
17	WATQTY	5.00 IN.	261.38 FT.
18	10YR FLOW CONTROL	10.50 IN. / 15.00 IN.	282.50 FT. / N/A
19	10YR FLOW CONTROL	10.00 IN. / 12.00 IN.	286.00 FT. / 230.00 FT.
20	10YR FLOW CONTROL	10.50 IN. / 12.00 IN.	230.30 FT. / 231.20 FT.
21	10YR FLOW CONTROL	9.50 IN. / 12.00 IN.	310.60 FT. / 312.60 FT.
22	10YR FLOW CONTROL	6.00 IN. / 12.00 IN.	314.00 FT. / 318.00 FT.

REVISIONS

3-20-95	DESIGNED: BSS	CHECKED: BSS
	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.: 98-340	SCALE:

STORM SEWER PLAN SHEET 3 OF 3
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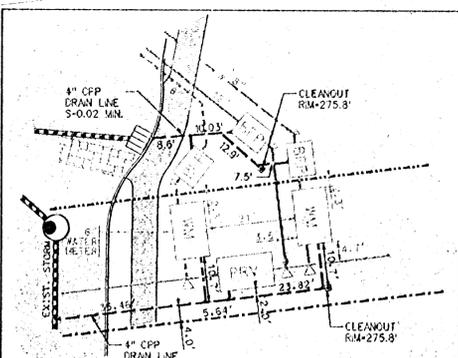
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APPROVED PLANS
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LEGEND

- CATCH BASIN
- MANHOLE
- ⊙ CLEAN OUT
- STORM SEWER PIPE
- ▬ DETENTION PIPE
- ▬▬▬ SUBSURFACE DRAINAGE TRENCH
- ▬▬▬ SLOTTED DRIVEWAY DRAIN
- * SEE ORIFICE TABLE

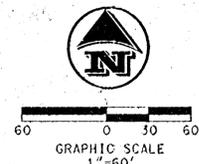


VAULT DRAIN LAYOUT DETAIL
SCALE: HORIZ. 1"=20'

DR-97-38/ZC-97-04/MISC-97-59/MISC-97-60

CURVE TABLE

#	RADIUS	DELTA	LENGTH	CH. BEARING	DISTANCE
C1	162.00'	20°33'21"	58.12'	N20°28'03"E	57.81'
C2	138.00'	48°32'34"	116.92'	N34°27'48"E	113.45'
C3	250.00'	14°35'26"	63.66'	N66°01'48"E	63.49'
C4	250.00'	09°38'31"	42.07'	N18°08'46"E	42.02'
C5	262.00'	21°46'07"	39.54'	S86°08'55"E	39.35'
C6	83.00'	09°25'48"	10.37'	S10°32'51"E	10.36'
C7	83.00'	53°23'05"	58.81'	S39°05'30"E	56.70'
C8	262.00'	04°44'58"	21.69'	S14°43'16"E	21.69'
C9	218.00'	05°51'00"	24.30'	S14°10'05"E	24.29'
C10	262.00'	13°01'44"	59.58'	S17°45'27"E	59.45'
C11	238.00'	07°32'56"	31.36'	S20°29'51"E	31.33'
C12	200.00'	18°38'43"	65.08'	S14°56'58"E	64.80'
C13	150.00'	47°16'47"	123.78'	S29°16'00"E	120.30'
C14	62.00'	22°36'43"	24.47'	S41°35'59"E	24.31'
C15	62.00'	50°22'53"	54.52'	S05°06'08"E	52.78'
C16	150.00'	12°55'52"	33.85'	S13°37'23"W	33.78'
C17	100.00'	20°39'27"	36.05'	N10°02'09"E	35.86'
C18	162.00'	12°01'05"	33.38'	N14°21'20"E	33.32'
C19	150.00'	21°11'55"	55.63'	N18°58'15"E	55.31'
C20	500.00'	08°16'09"	76.51'	N85°12'40"E	76.44'
C21	500.00'	05°41'34"	49.68'	N83°40'21"E	49.66'
C22	88.00'	83°11'51"	135.46'	N42°25'16"E	122.48'
C23	88.00'	51°33'49"	88.41'	N30°27'34"W	84.74'
C24	512.00'	15°15'28"	136.34'	N86°19'03"W	135.94'
C25	75.00'	71°11'21"	101.04'	N55°21'05"W	93.57'
C26	250.00'	19°08'40"	83.53'	N07°11'05"W	83.15'
C27	500.00'	15°48'37"	137.37'	N10°17'34"E	137.53'
C28	262.00'	17°12'14"	78.67'	S47°13'21"E	78.37'
C29	250.00'	04°36'49"	20.13'	S40°55'38"E	20.13'
C30	238.00'	16°36'12"	68.97'	S51°32'09"E	68.13'
C31	62.00'	07°32'12"	8.16'	S56°04'09"E	8.15'
C32	262.00'	08°56'30"	40.89'	N55°27'50"W	40.85'
C33	262.00'	08°15'36"	37.77'	N55°07'23"W	37.74'
C34	262.00'	11°49'43"	54.09'	S21°27'04"E	53.99'
C35	62.00'	28°53'33"	31.26'	S29°58'53"E	30.93'
C36	62.00'	32°07'22"	34.76'	S28°22'05"E	34.31'

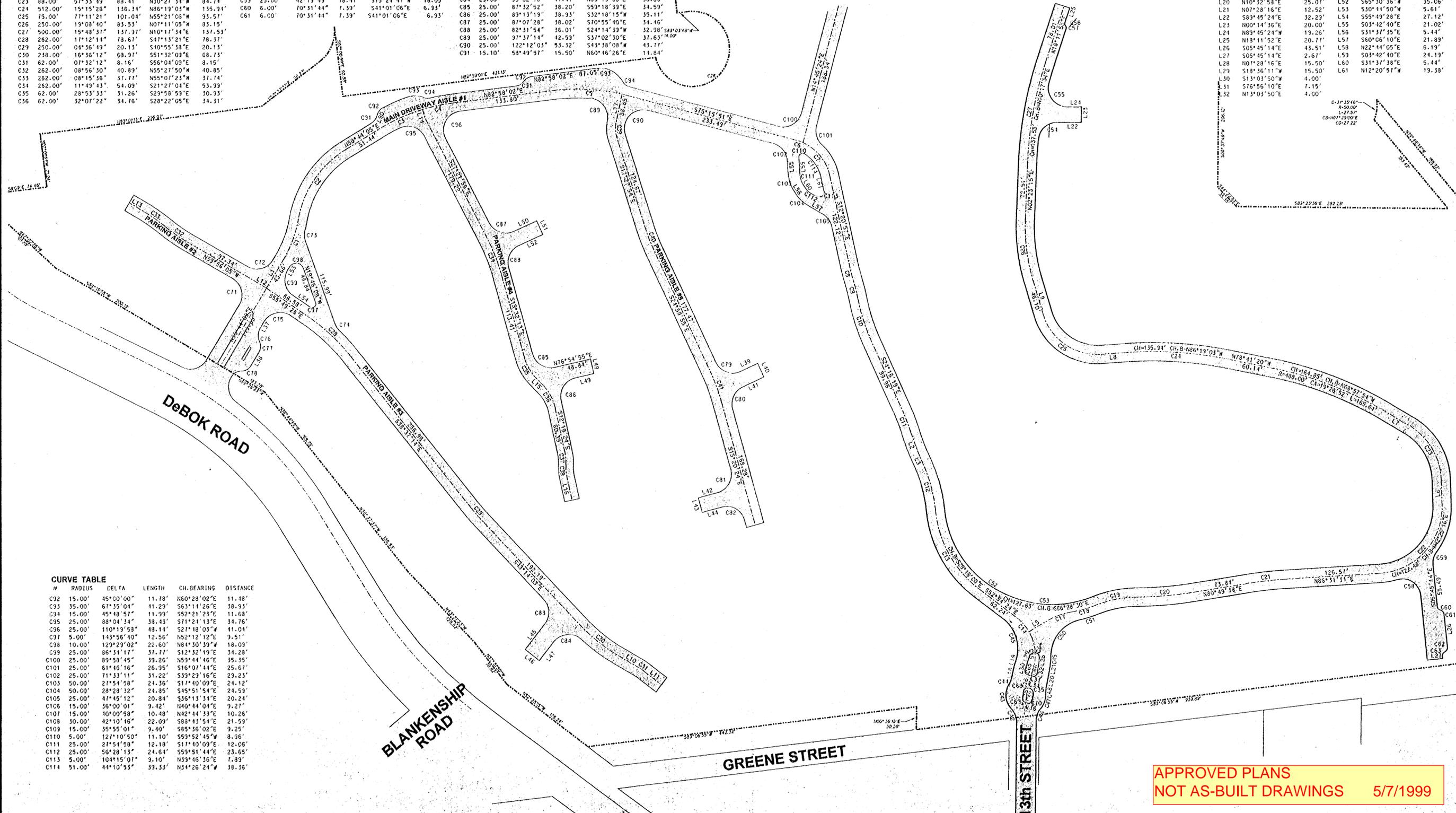


LINE TABLE

#	BEARING	DISTANCE
L1	N30°46'43"E	43.66'
L2	S20°29'51"E	34.49'
L3	S24°16'19"W	9.37'
L4	S07°09'27"W	25.15'
L5	N59°42'26"E	27.63'
L6	N01°40'40"W	33.04'
L7	N59°14'28"W	34.05'
L8	S86°03'14"W	33.82'
L9	N16°45'25"W	46.10'
L10	S59°50'15"E	30.02'
L11	S52°18'03"E	29.07'
L12	N58°47'30"W	19.98'
L13	N59°15'11"W	19.62'
L14	S16°40'29"E	12.00'
L15	S44°25'46"E	13.33'
L16	S12°18'24"E	31.47'
L17	S11°46'07"W	38.89'
L18	N13°03'50"E	19.16'
L19	N18°36'11"E	5.08'
L20	N10°32'58"E	25.07'
L21	N07°28'16"E	12.52'
L22	S89°45'24"E	32.29'
L23	N00°14'36"E	20.00'
L24	N89°45'24"W	19.26'
L25	N18°11'52"E	20.77'
L26	S05°45'14"E	43.51'
L27	S05°45'14"E	2.67'
L28	N01°28'16"E	15.50'
L29	S18°36'11"W	15.50'
L30	S13°03'50"W	4.00'
L31	S16°56'10"E	7.15'
L32	N13°03'50"E	4.00'
L33	N13°03'50"E	7.50'
L34	N19°42'15"W	2.71'
L35	S07°09'27"W	3.99'
L36	S16°56'10"E	3.37'
L37	S10°44'37"W	9.50'
L38	S10°44'37"W	20.46'
L39	N65°00'05"E	25.00'
L40	S24°59'55"E	20.00'
L41	S65°00'05"W	31.13'
L42	S14°39'36"E	26.86'
L43	S15°20'24"E	20.00'
L44	N14°39'36"E	26.86'
L45	S37°41'57"W	43.07'
L46	S52°18'03"E	24.00'
L47	N37°41'57"E	31.51'
L48	S12°18'24"E	20.00'
L49	S16°54'55"W	25.38'
L50	N65°30'36"E	33.71'
L51	S24°29'24"E	20.00'
L52	S65°30'36"W	35.06'
L53	S10°44'50"W	5.61'
L54	S05°49'28"E	27.12'
L55	S03°42'40"E	21.02'
L56	S31°37'35"E	5.44'
L57	S60°06'10"E	21.89'
L58	N22°44'05"E	6.19'
L59	S03°42'40"E	24.19'
L60	S31°37'39"E	5.44'
L61	N12°20'51"W	19.38'

REVISIONS

3-20-99	DESIGNED: BSS	CHECKED: BSS
4-22-99	DRAWN: BSS/RT	DATE: 11-20-98
4-22-99 Change Lines to Current Lines		JOB NO.: 96-340
		SCALE:



CURVE TABLE

#	RADIUS	DELTA	LENGTH	CH. BEARING	DISTANCE
C32	15.00'	45°00'00"	11.78'	N60°28'02"E	11.48'
C33	35.00'	61°35'04"	41.29'	S63°14'26"E	38.93'
C34	15.00'	45°48'51"	11.99'	S52°21'23"E	11.68'
C35	25.00'	88°04'34"	38.43'	S11°24'13"E	34.76'
C36	25.00'	110°13'58"	48.11'	S27°18'03"W	41.04'
C37	5.00'	143°56'40"	12.66'	N52°12'12"E	9.51'
C38	10.00'	129°29'02"	22.60'	N81°30'39"W	18.09'
C39	25.00'	86°34'17"	37.77'	S12°32'19"E	34.28'
C100	25.00'	89°58'45"	39.26'	N59°44'46"E	35.35'
C101	25.00'	61°46'16"	26.95'	S16°07'41"E	25.67'
C102	25.00'	71°33'11"	31.22'	S39°29'16"E	29.23'
C103	50.00'	27°54'58"	24.36'	S17°40'09"E	24.12'
C104	50.00'	28°28'32"	24.85'	S45°51'54"E	24.59'
C105	25.00'	47°45'12"	20.84'	S38°13'34"E	20.24'
C106	15.00'	36°00'01"	9.42'	N40°44'04"E	9.27'
C107	15.00'	10°00'58"	10.48'	N42°44'33"E	10.26'
C108	30.00'	42°10'46"	22.09'	S88°43'54"E	21.59'
C109	15.00'	35°55'01"	9.40'	S85°36'02"E	9.25'
C110	5.00'	127°10'50"	11.10'	S93°52'45"W	8.36'
C111	25.00'	27°54'58"	12.18'	S17°40'09"E	12.06'
C112	25.00'	56°28'13"	24.64'	S59°51'44"E	23.65'
C113	5.00'	104°15'07"	3.10'	N39°46'36"E	7.89'
C114	51.00'	44°10'53"	33.33'	N34°26'24"W	38.36'

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

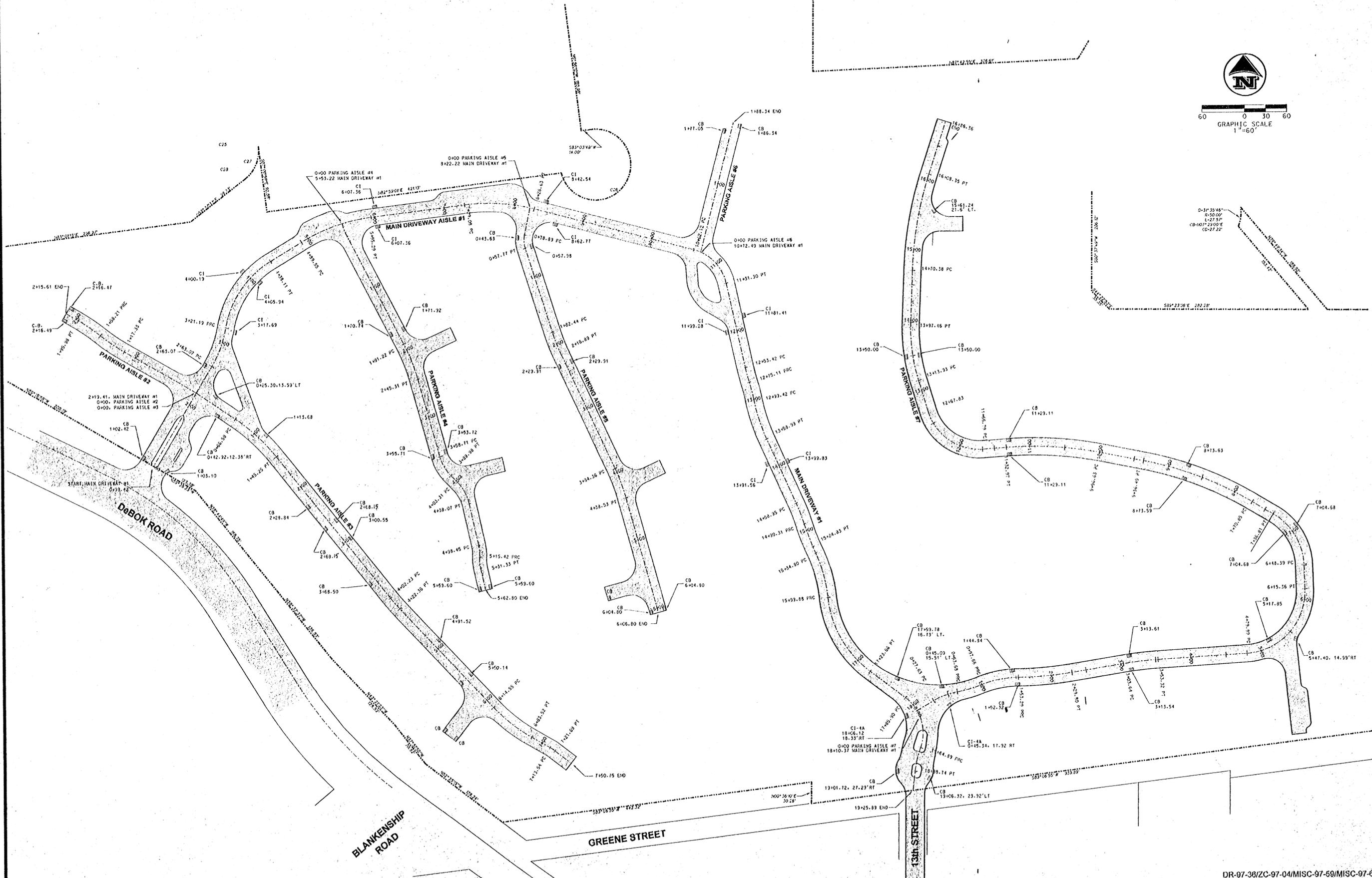
DRIVEWAY GEOMETRY PLAN
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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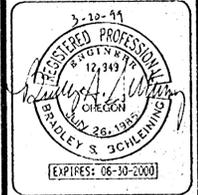


APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



3-20-99	DESIGNED: BSS	CHECKED: BSS
	DRAWN: BSS/AT	DATE: 11-20-98
		JOB NO.: 56-340
REVISIONS		

STATIONING PLAN
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

VERTICAL CURVE DATA			
LENGTH	150		
PVC STA	7+88.46	ELEV	252
PVI STA	8+63.16	ELEV	255.43
PVT STA	9+38.46	ELEV	266.68

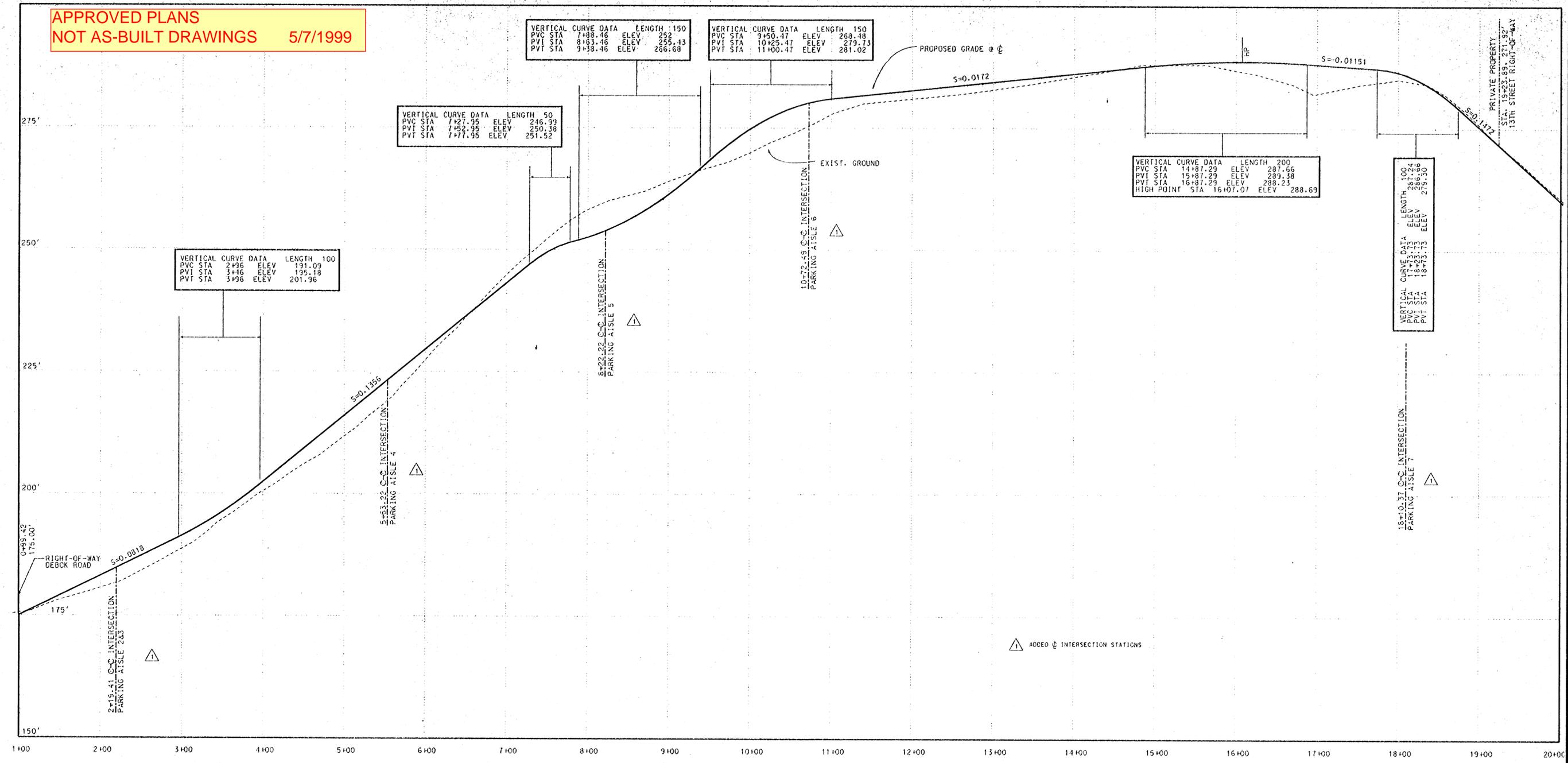
VERTICAL CURVE DATA			
LENGTH	150		
PVC STA	9+50.47	ELEV	268.48
PVI STA	10+25.47	ELEV	279.73
PVT STA	11+00.47	ELEV	291.02

VERTICAL CURVE DATA			
LENGTH	50		
PVC STA	7+27.95	ELEV	246.93
PVI STA	7+52.95	ELEV	250.38
PVT STA	7+77.95	ELEV	251.52

VERTICAL CURVE DATA			
LENGTH	100		
PVC STA	2+96	ELEV	191.09
PVI STA	3+46	ELEV	195.18
PVT STA	3+96	ELEV	201.96

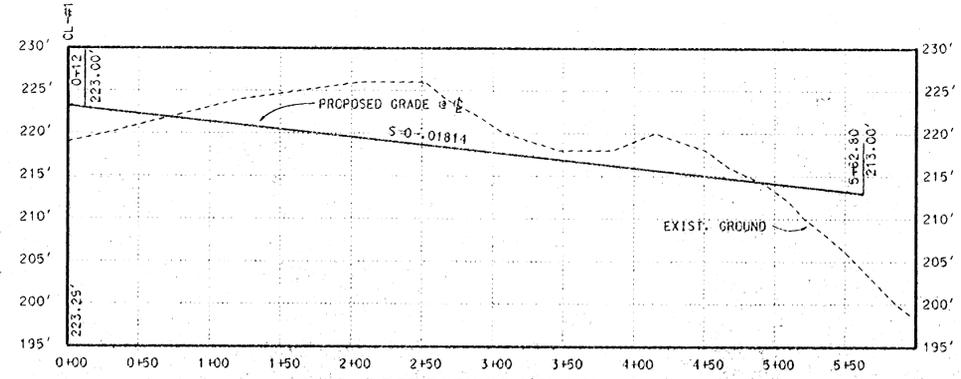
VERTICAL CURVE DATA			
LENGTH	200		
PVC STA	14+87.29	ELEV	287.66
PVI STA	15+87.29	ELEV	289.38
PVT STA	16+87.29	ELEV	288.23
HIGH POINT STA	16+07.07	ELEV	288.69

VERTICAL CURVE DATA			
LENGTH	100		
PVC STA	17+35.73	ELEV	281.24
PVI STA	17+95.73	ELEV	286.06
PVT STA	18+55.73	ELEV	275.30



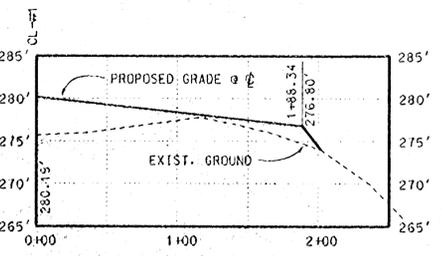
PROFILE FOR MAIN DRIVEWAY AISLE #1

SCALE: HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'



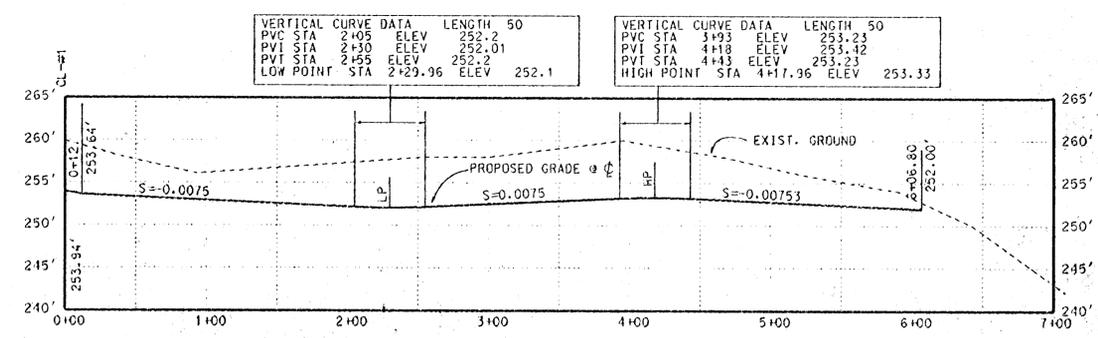
PROFILE FOR PARKING AISLE #4

SCALE: HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'



PROFILE FOR PARKING AISLE #6

SCALE: HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'

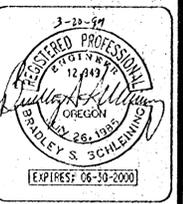


PROFILE FOR PARKING AISLE #5

SCALE: HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'

REVISIONS			
3-20-99			
DESIGNED:	BSS	CHECKED:	BSS
DRAWN:	BSS/RT	DATE:	11-20-98
JOB NO.:	96-340	SCALE:	

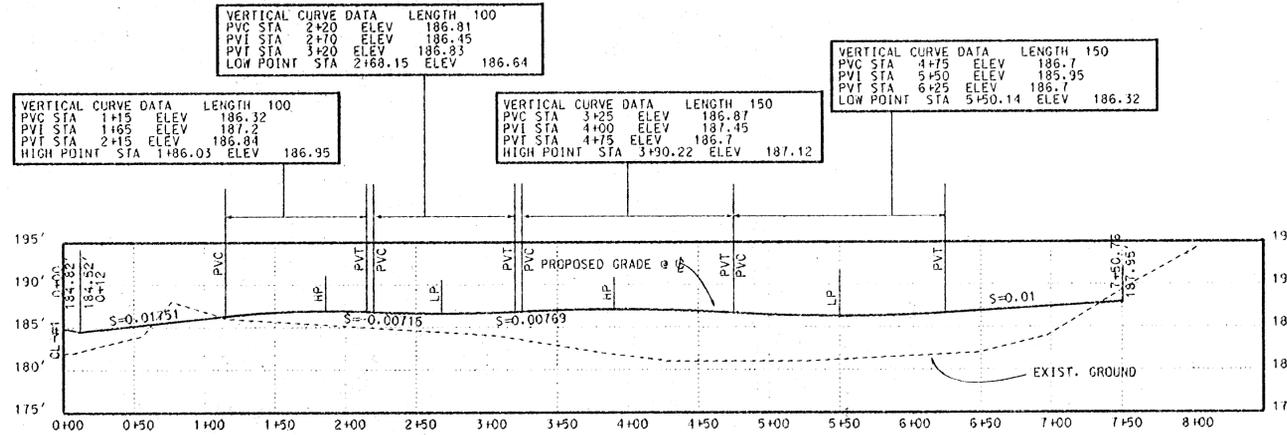
PARKING AISLE PROFILES SHEET 1 OF 2
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



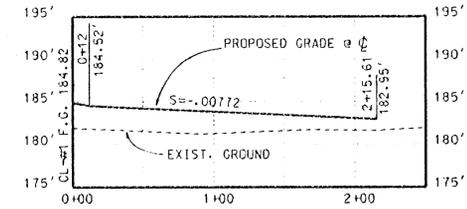
W.B. WELLS
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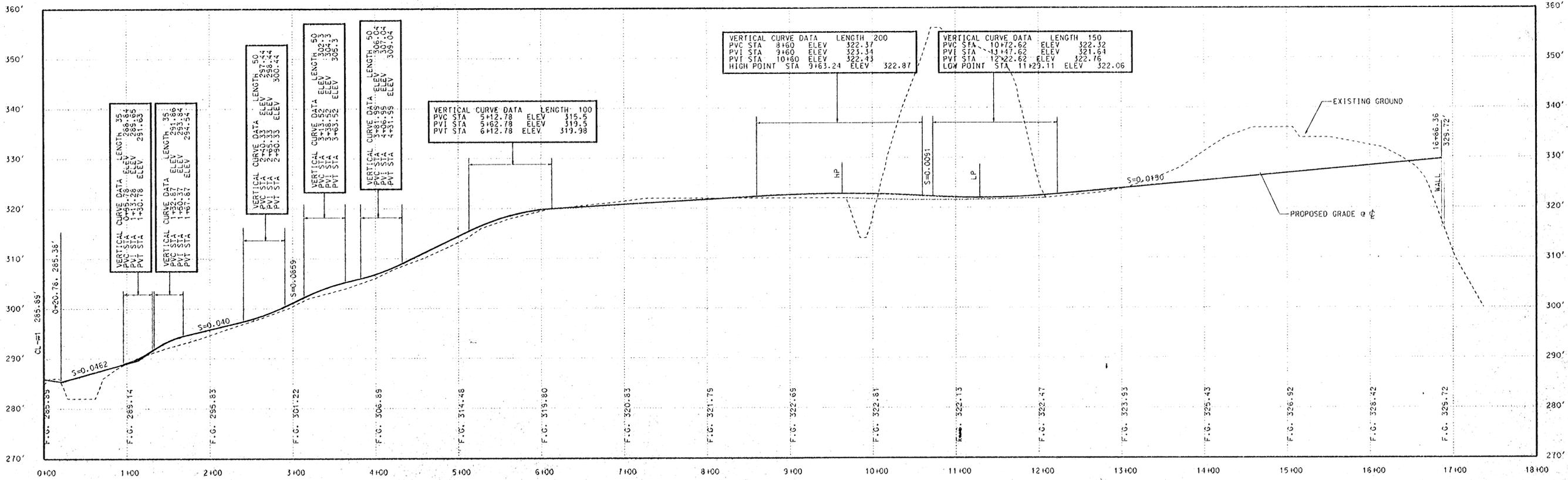
APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



PROFILE FOR PARKING AISLE #3
SCALE: HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'



PROFILE FOR PARKING AISLE #2
SCALE: HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'



PROFILE FOR PARKING AISLE #7
SCALE: HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'

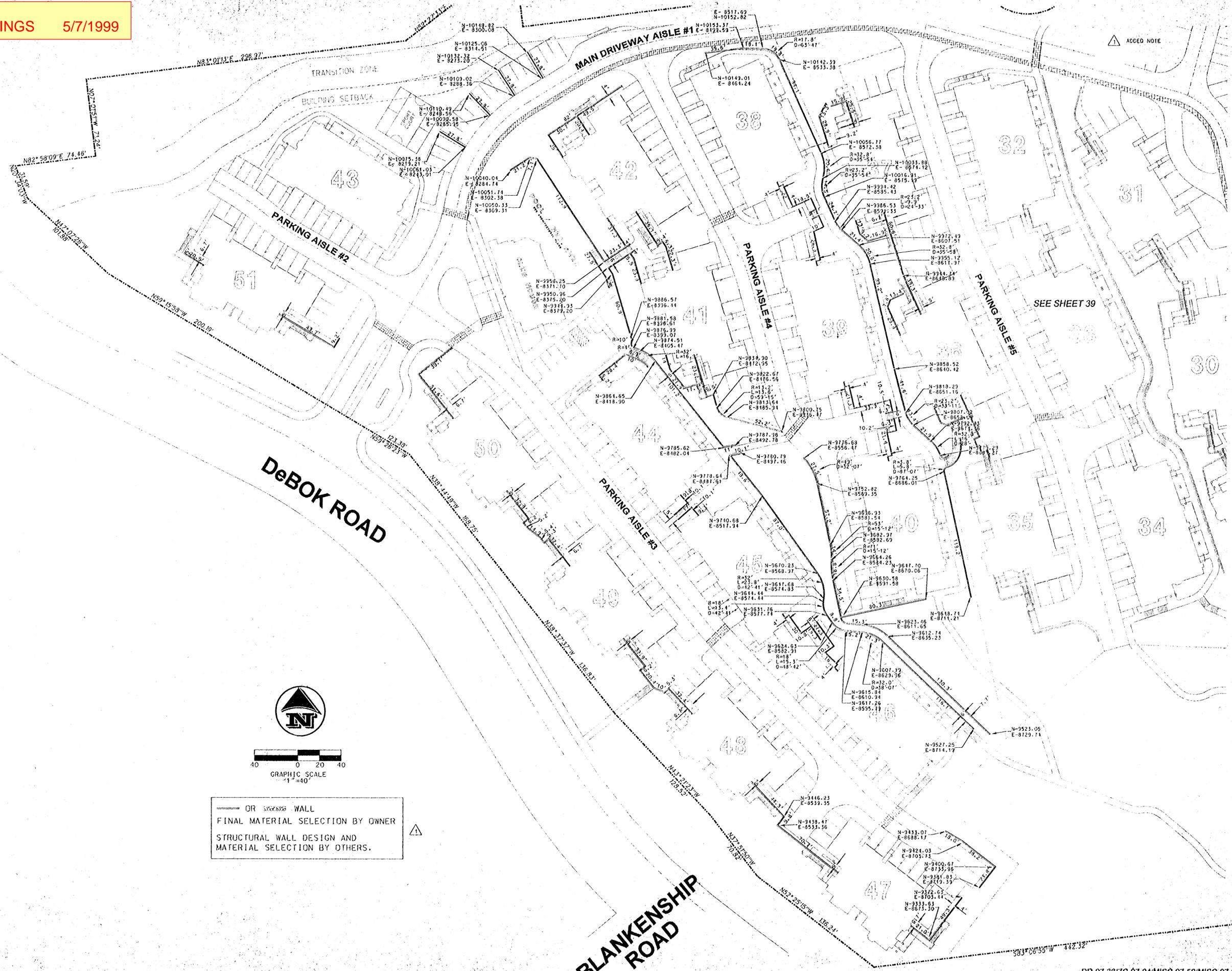
REVISIONS
3-20-99
DESIGNED: BSS
DRAWN: BSS/RT
CHECKED: BSS
DATE: 11-20-98
JOB NO.: 96-340
SCALE:

PARKING AISLE PROFILES SHEET 2 OF 2
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



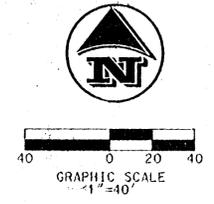
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△ ADDED NOTE

— OR STRUCTURE WALL
 FINAL MATERIAL SELECTION BY OWNER
 STRUCTURAL WALL DESIGN AND
 MATERIAL SELECTION BY OTHERS. △

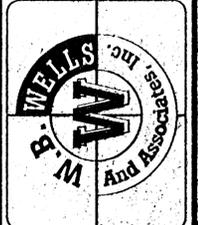


REVISIONS
3-20-99
DESIGNED: BSS
DRAWN: BSS/RT
CHECKED: BSS
DATE: 11-20-98
JOB NO.: 96-340
SCALE:

WALL GEOMETRY PLAN SHEET 1 OF 3
 for
SUMMERLINN APARTMENTS
 CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

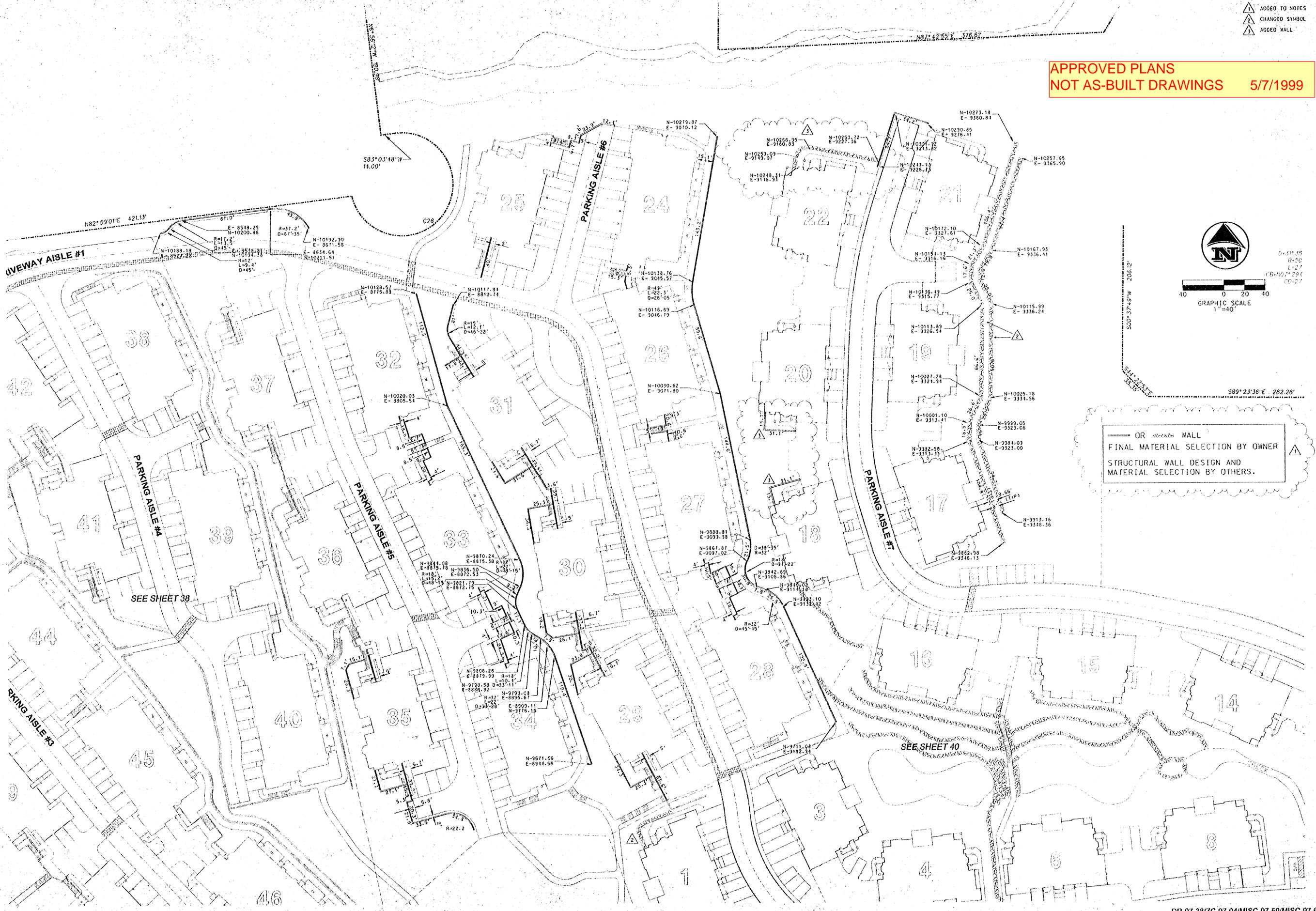


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- ▲ ADDED TO NOTES
- ▲ CHANGED SYMBOL
- ▲ ADDED WALL

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

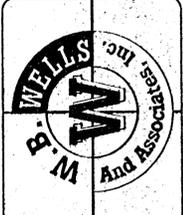


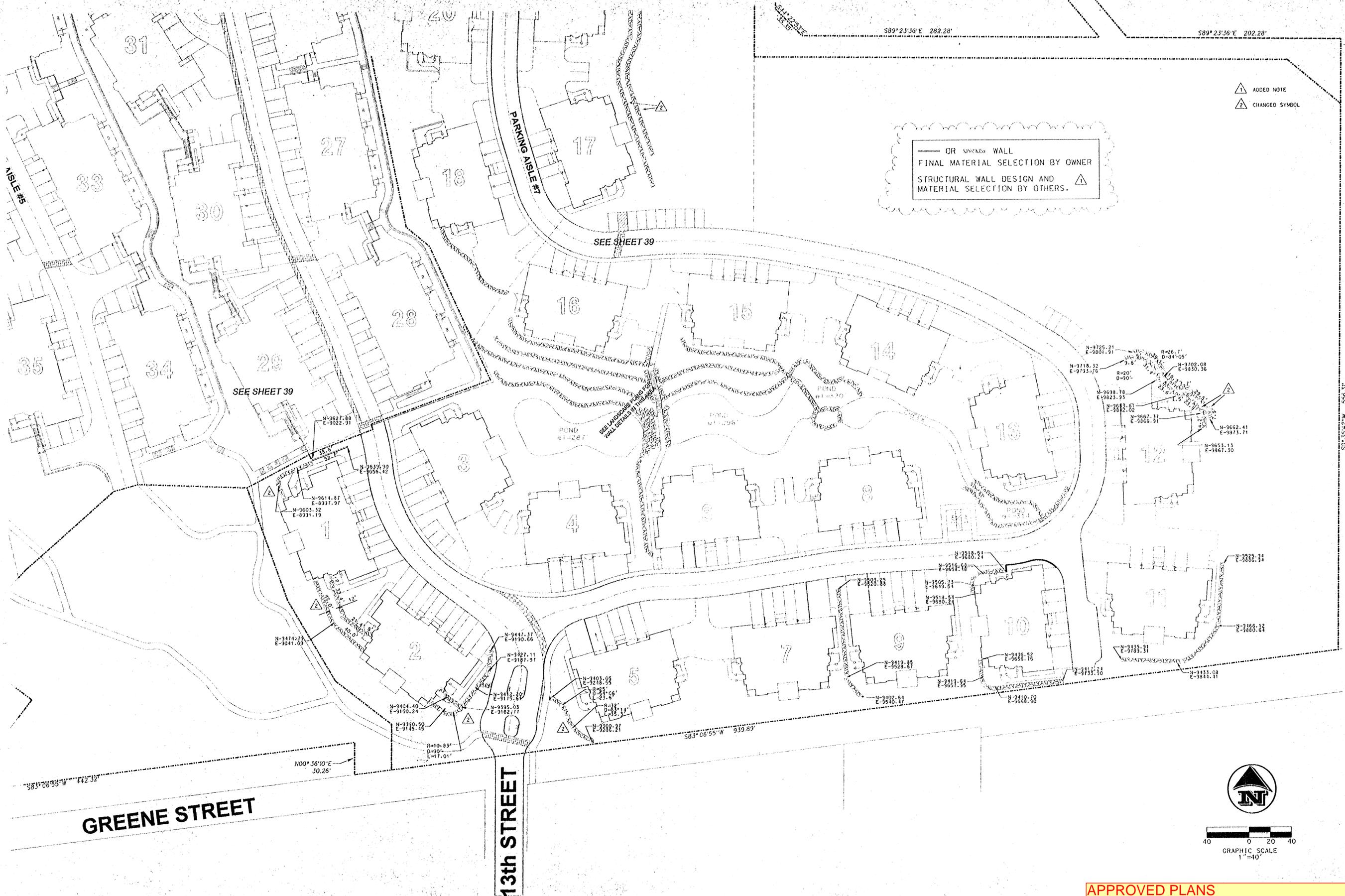
WALL GEOMETRY PLAN SHEET 2 OF 3

for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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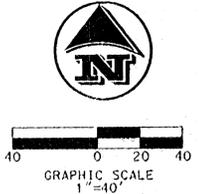


OR SINKING WALL
 FINAL MATERIAL SELECTION BY OWNER
 STRUCTURAL WALL DESIGN AND MATERIAL SELECTION BY OTHERS.

- △ ADDED NOTE
- △ CHANGED SYMBOL

GREENE STREET

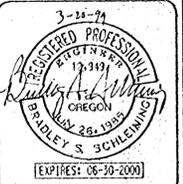
13th STREET



APPROVED PLANS
 NOT AS-BUILT DRAWINGS 5/7/1999

3-20-99	DESIGNED: BSS	CHECKED: BSS
	DRAWN: BSS/RT	DATE: 11-20-98
	JOB NO.: 96-340	SCALE:

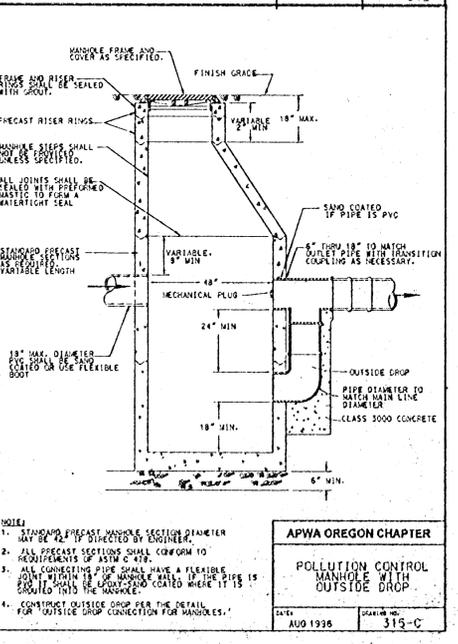
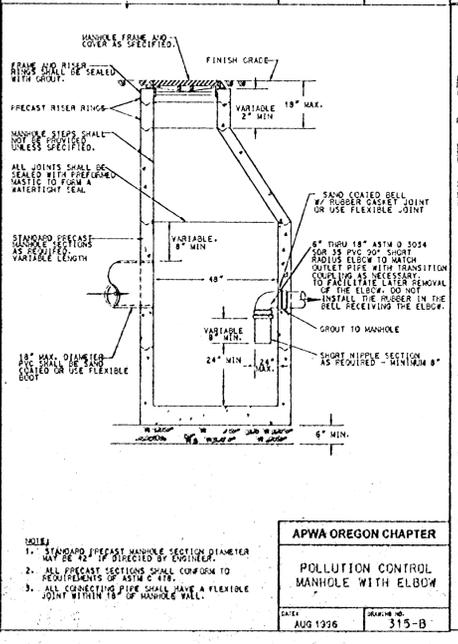
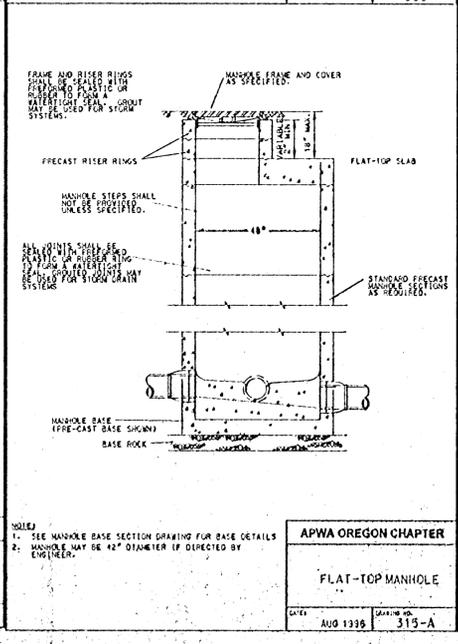
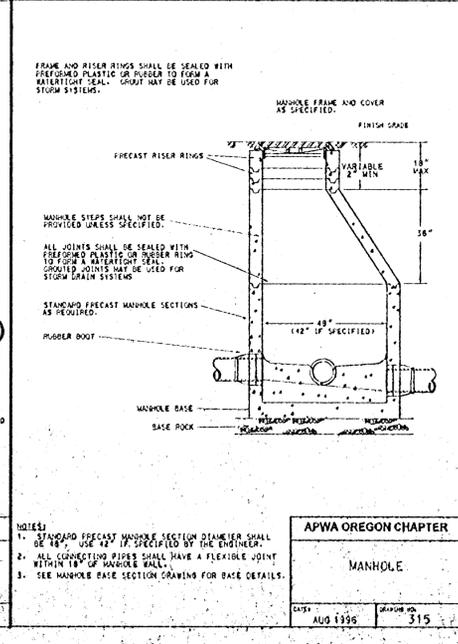
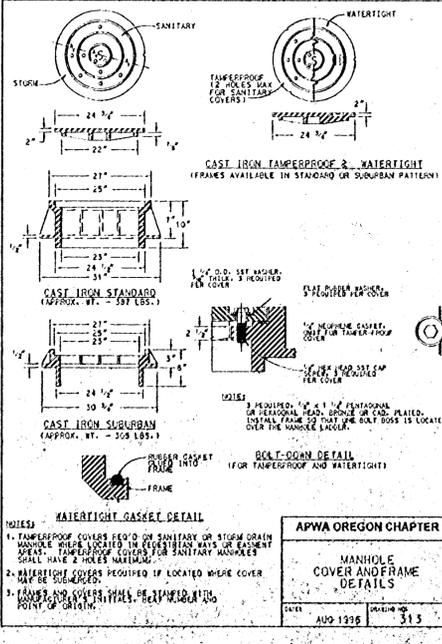
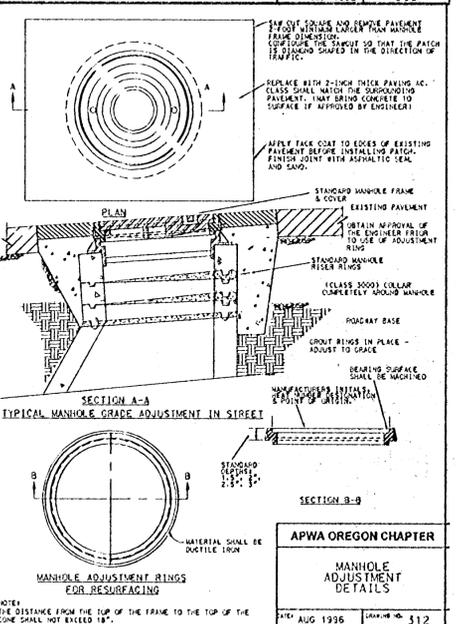
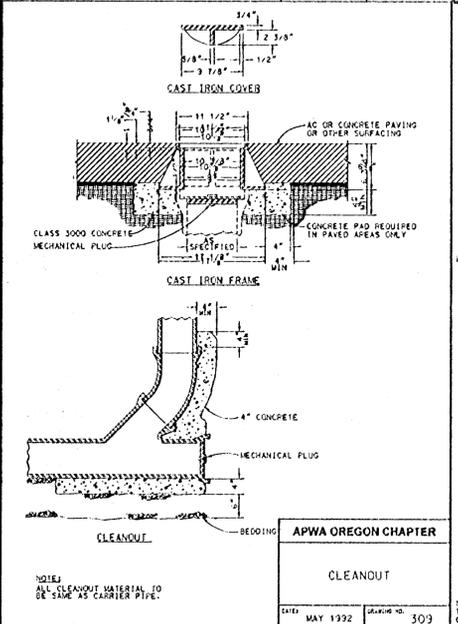
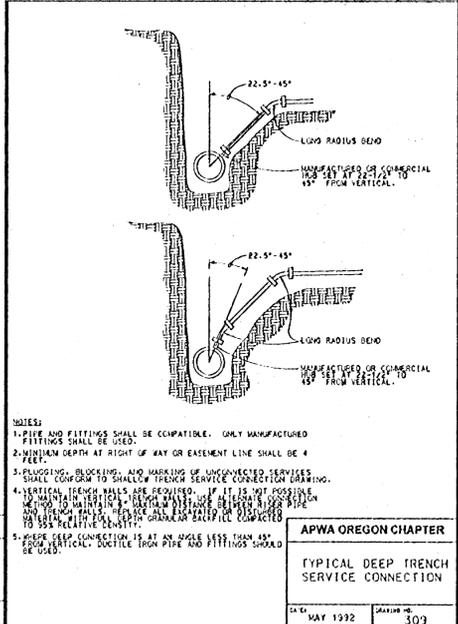
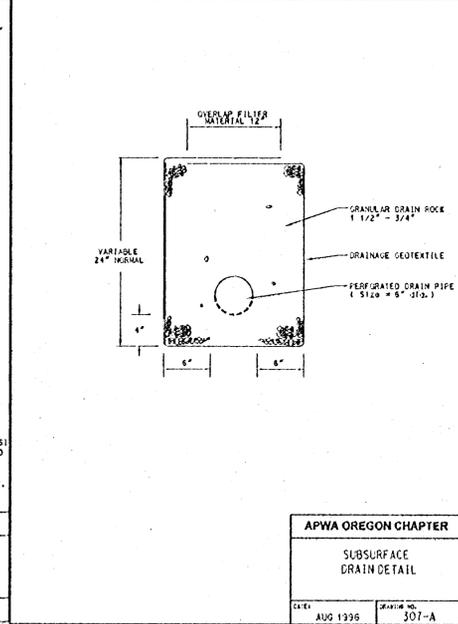
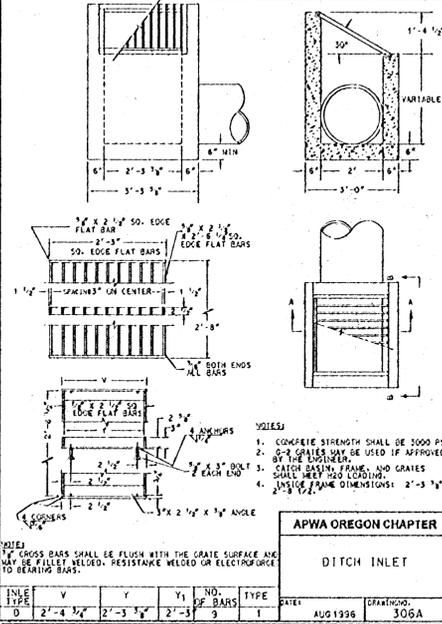
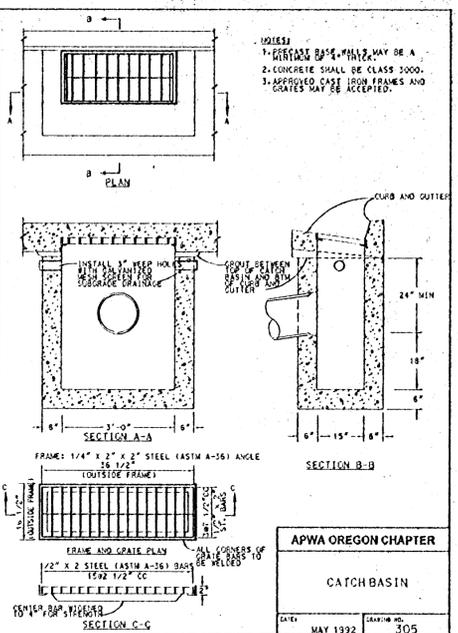
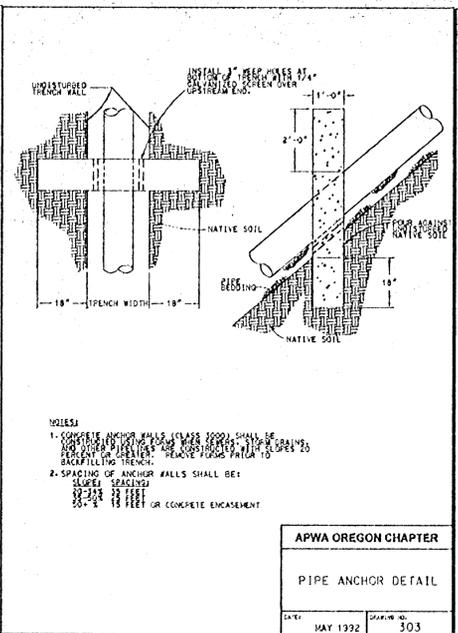
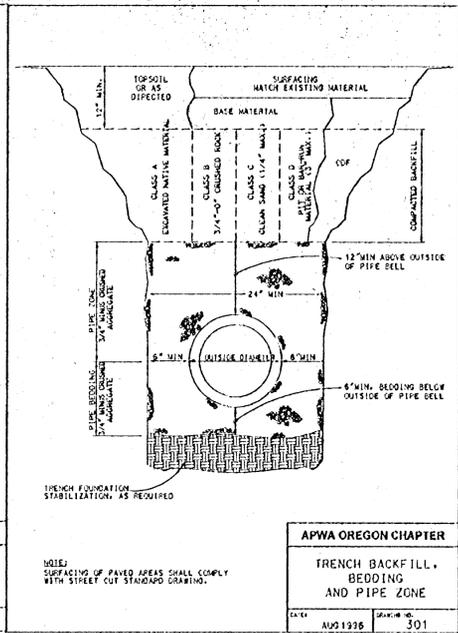
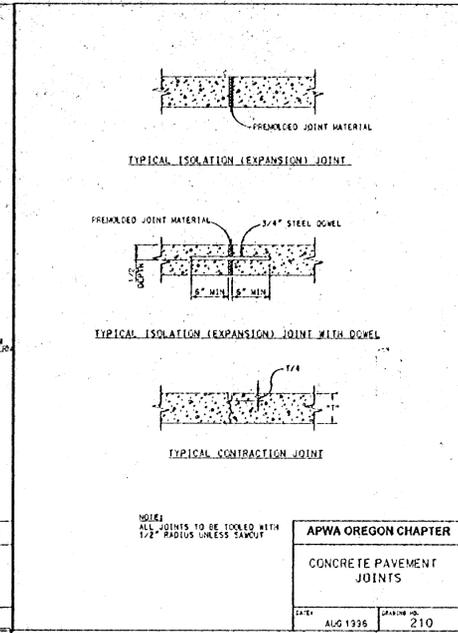
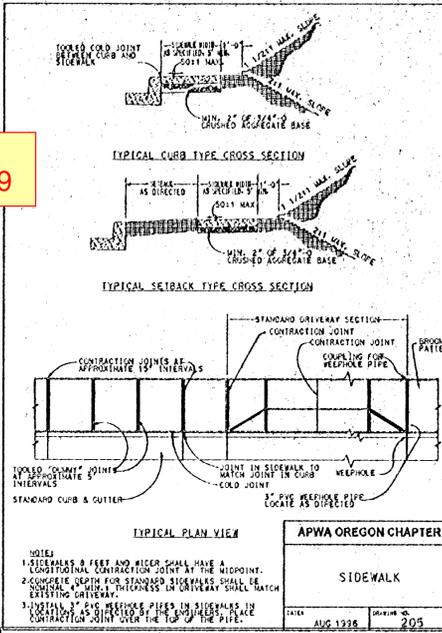
WALL GEOMETRY PLAN SHEET 3 OF 3
 for
 SUMMERLINN APARTMENTS
 CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



REVISIONS	DESIGNED: PC	CHECKED:
1. 3-20-99 ADDED SIZE TO SUBSURFACE DRAIN	DRAWN: PC/RT	DATE: 6-25-98
	JOB NO.: 95-164	SCALE: N/A

CONSTRUCTION DETAILS
for
SUMMERLIN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

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