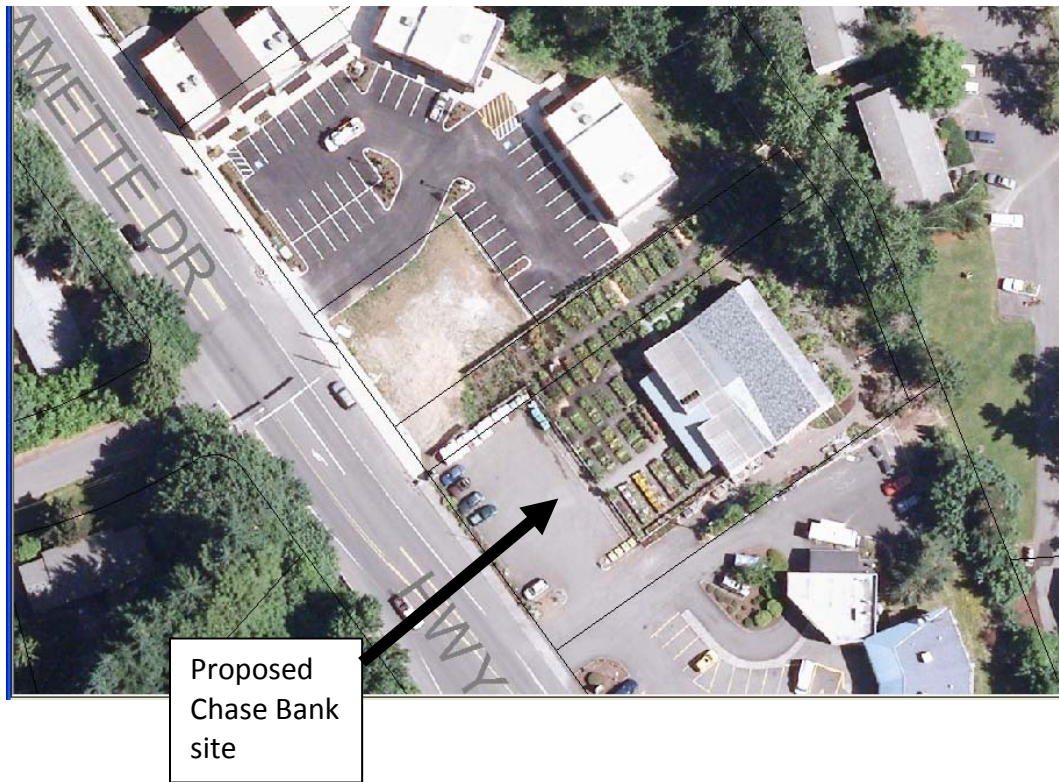


SUBJECT: Chase Bank at 19080 Willamette Drive

ATTENDEES: Applicants: Jon McAuley, Christopher Lind, Jeff Olson, Lisa Brevard, Chris Wonderly
Staff: Khoi Le (Engineering), Peter Spir (Planning) David Davies (Building)

Project Details



The bank building would extend along the edge of the Willamette Drive right of way (ROW). In addition to the bank, a smaller structure with two drive through lanes accommodating one 24-hour ATM and one voice automated teller (VAT) would be located at the rear or eastern portion of the site. Most of the site would be occupied by parking and driveways. One of the driveways will connect with Doug Seeley's Willamette Village commercial development to the north.

The property is zoned "General Commercial" (GC). Banks are "permitted outright" in the GC zone so long as Class II Design Review is addressed.

The site extends back or east 260 feet from the Willamette Drive ROW. The front two-thirds of the lot are flat. The land then drops down about 10 feet to a lower flat area at the rear of the existing Kasch's building.

There are no drainageways, riparian zones, wetlands or other natural features as defined by the CDC on the property. The trees are few and limited to the rear property line. They appear to be three cottonwoods about 30-40 feet tall. These trees provide some screening between the site and the apartments to the rear of the site. (A survey is needed to determine if the trees are in fact on the applicant's property.)

L-Series Prototype - Brick



Architecture

Design

By the notation on the plans (L-Series Prototype –Brick) it seems that the design is one of a number that could have been chosen. Be that as it may, this design is a single story structure. As seen from Willamette Drive the front elevation is broken into three parts with the central entry way section capped with a hipped/pyramidal roof. (The roof positively mirrors the design

of the Starbucks up the street (see photo below)) The entry way makes good use of glass with a flat protective awning and transom windows above the awning as well. Above the transom windows is an area for the main sign. The brick clad columns that are on each side of the entryway section breaks up the horizontal plane effectively. The vertical plane is broken into three elements by using different building materials including brick cladding at eye level. The Community Development Code (CDC) offers the following comments and standards:

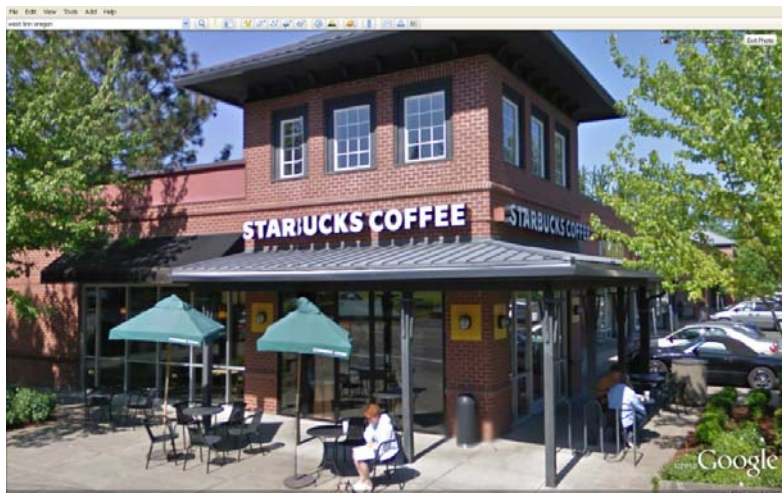
CDC 55.100(B)6. Architecture.

a. The predominant architecture of West Linn identified in the West Linn vision process was contemporary vernacular residential designs emphasizing natural materials: wood with brick and stone detail. Colors are subdued earth tones: grays, brown, off-whites, slate, and greens. Pitched roofs with overhanging eaves, decks, and details like generous multi-light windows with oversized trim are common. Also in evidence are the 1890s Queen Anne style homes of the Willamette neighborhood. Neo-traditional homes of the newer subdivisions feature large front porches with detailed porch supports, dormers, bracketed overhanging eaves, and rear parking for cars. Many of these design elements have already been incorporated in commercial and office architecture.

b. The proposed structure(s) scale shall be compatible with the existing structure(s) on site and on adjoining sites. Contextual design is required. Contextual design means respecting and incorporating prominent architectural styles, building lines, roof forms, rhythm of windows, building scale and massing, materials and colors of surrounding buildings in the proposed structure.

In addition to the CDC, the Robinwood Neighborhood Plan calls for developing “a common architectural and design theme for commercial development along Willamette Drive.” It also requires primary use of quality building materials such as stone, terra cotta and wood for frontage facades.

Regarding compatibility the following two photographs of nearby buildings are offered as examples that meet the CDC:





Entryway

CDC 55.100(B)(7) requires that all businesses have at least one main entry onto the street side. The front large well defined entryway on Willamette Drive meets the criterion. The transom looks good. Extending the awning to provide more rain protection (6-8 feet) is needed.

Transparency

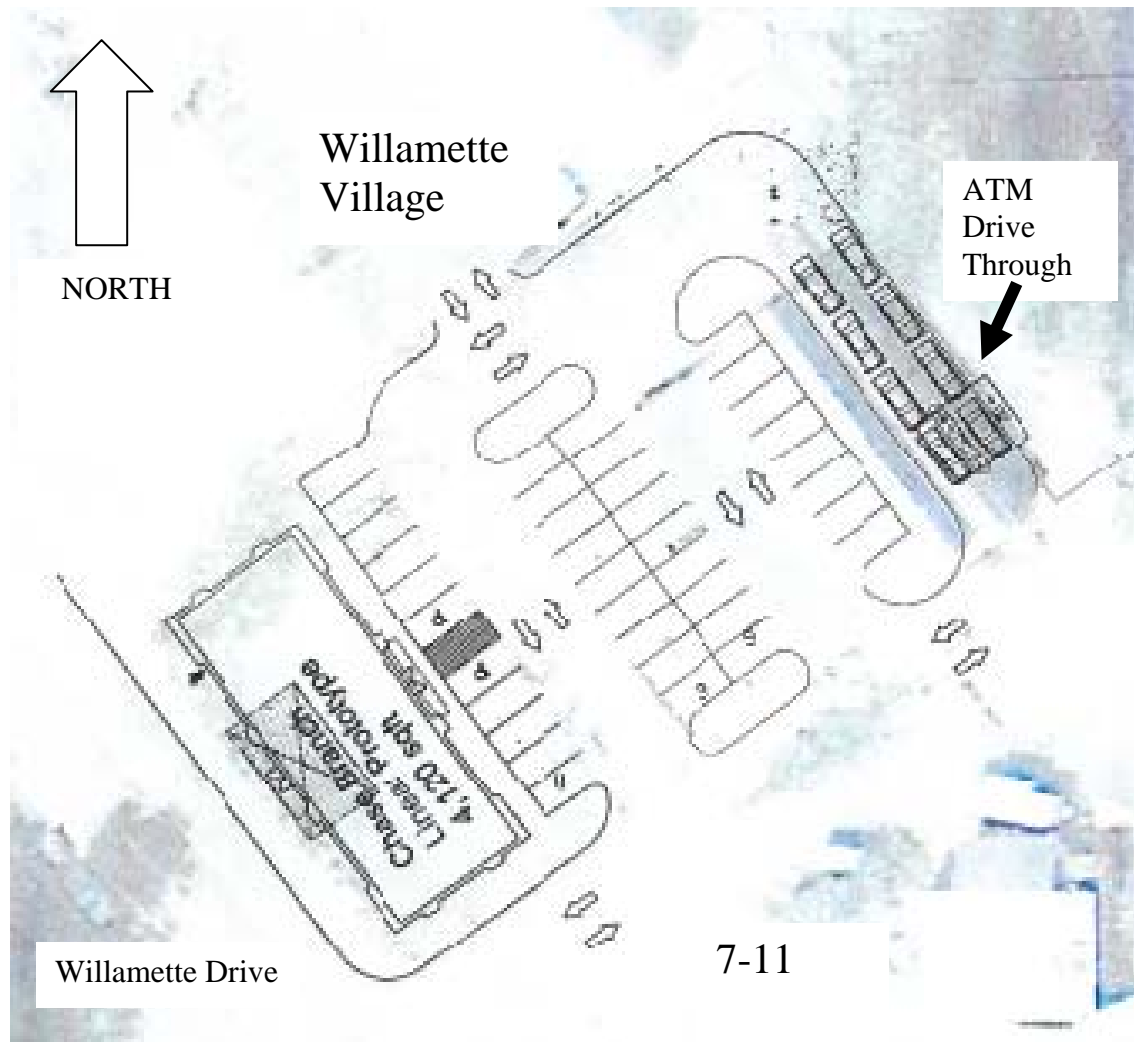
The transparency requirements of CDC 55.100(B)(6)(f) must be met. Sixty percent of the lineal frontage of the Willamette Drive elevation must have windows. At least one of the north or south facing elevations needs 30 percent transparency too. Glass must be clear and cannot be mirrored or smoked etc. The west facing or main elevation on Willamette Drive has about 53% transparency. That's good but it needs to be increased to meet the 60% standard. Both north and south facing elevation are blank. This is a problem.

The CDC allows for "transferring" window square footage. Thus it would be possible for the applicant to get credit for any surplus transparency on a re-designed front or side elevation. The applicant could apply for a variance: requesting that transparency on the rear elevation be "transferred" or credited to the front or side elevations. The applicant could strengthen the variance request by adding some acceptable kind of wall decoration, bas relief, art work etc. on those blank elevations. The applicant mentioned using a faux window with a poster inside. Staff is agreeable to looking at a drawing or photograph of how that would look but it sounds like that would still have to be attached to a variance request.

Building Materials

The single story building looks to be clad in drivet from grade to two feet above grade (bulkhead), then brick from about two feet to nine feet followed by more drivet which is capped by a contrasting colored cornice. The use of smaller cornices that barely project from the building (or no cornices at all) is preferred rather than using the exaggerated cornices

common on so many retail or office buildings. Flat /horizontal rooflines are preferred over the dated false front/parapets and peaks.



Sidewalk

A 12-foot wide sidewalk with cut outs for trees along the curb edge was called for in the CDC. It was noted that the West Linn OR 43 Conceptual Design Plan calls for a 15-foot sidewalk combining 10 feet for pedestrians and five feet for tree cut outs, utilities and street furniture; but as staff noted, although that document has been adopted by City Council it has not been incorporated in the CDC so the 12-foot dimension would prevail. Still, if the applicant wants to improve the product, they could go with the 15-foot configuration.



Awnings

Awnings should extend perpendicular from the face of the building rather than slope down at a 45 degree angle. The awnings should extend out 6-8 feet with 8 feet of vertical clearance. The awnings should also extend continuously across the entire front building elevation to provide meaningful protection from rain and sun. The awnings should be permanent material (metal) and not fabric. The applicant proposed that the valence area of the awning be blue. Staff is agreeable to looking at a drawing or photograph of how that will look. But the preference is that it be left black, metal or natural earth tones. Gail Curtis of ODOT is supportive of the awning extending into the ROW but a permit is needed from them.



Awnings

In an earlier pre-application meeting for this site, both staff and Tom Boes (RNA) wanted to see the awnings extend perpendicular from the face of the building rather than slope down at a 45 degree angle. The awnings should extend out 6-8 feet with 8 feet of vertical clearance. The awnings should also extend continuously across the entire front building elevation to provide meaningful protection from rain and sun.

Shared Driveway

The applicant's site plan shows the connection to the adjacent property to the north. The developer of that property (to the north) is required to provide a 24-foot wide driveway and

access easement for the benefit of this property (Kasch's). The condition of approval for that application (DR-06-43) is as follows:

“At such time that the commercially zoned property to the south (Assessor’s Map 21E 23AA tax lot 703) currently known as Kasch’s, applies for a design review application, this applicant shall provide and construct a 24 foot wide driveway along the south property line to connect the two parcels. Further, the applicant shall prepare and sign a waiver of remonstrance to the connection, construction and use of a driveway from the tax lot 703 (Kasch’s property) onto the applicant’s property. Further, the applicant shall sign and record a reciprocal and mutual access easement that would allow traffic from the tax lot 703 (Kasch’s property) to exit via the applicant’s parking lot and driveway and vice versa. These documents shall be reviewed and approved by the City Attorney.”



The intent also is that the traffic from each property could, by mutual access easement, traverse each other's site to their points of ingress/egress. The City and ODOT endorse this.

The use of different colors, textures or materials to encourage use of pedestrian routes through the parking and driveway areas is required per 55.100(B)(7)(d)(e) and 46.150(A)(20).

A pedestrian link needs to be provided on the north or south side of the common driveway.

Color

The neutral colors and brick cladding around the middle of the building are good. The blue fabric awnings should be replaced with flat metal awnings. If the applicant has any local examples of how these colors and the blue lighting look, they are encouraged to let staff know so we can visit those sites.

Parking

Placing the parking at the rear of the building is correct per code. It is noted that the parking is based on the land use category in CDC Chapter 46 of "Professional offices, banks, savings and loans...." Parking is calculated at one space per 350 square feet of gross floor area. Half of the spaces must be compact (8X16) and half standard (9X18). One ADA space (van accessible) is

required per 46.150(B). Three stacking spaces (20 feet each) are required for each drive through aisle.

The 4,120 square foot building requires 11.77 spaces which then are rounded up to 12 spaces.

Drive through lanes: Design and Sustainability (not related to approval criterion)

Drive-through lanes are shown at the rear of the building.

The City has concerns with drive throughs in terms of sustainability and pollution. Review of industry literature produced the following positive solution:

"Cars idling for more than 10 seconds use more gas and create more global warming pollution than simply restarting the engine. Surprised? It's true - the 10-second rule has been proven empirically. The 10-second rule was originally published on the Canadian Office of Energy Efficiency's Idle-Free Zone webpage. Their results were replicated by the American Society of Mechanical Engineers, which found that restarting uses the same amount of fuel as idling with the air conditioner on for 6 seconds.

Zions Bank in Utah is asking its customers to turn the key on air pollution.

In a voluntary effort to help clean up Utah's smog, all Zions locations are asking their customers to cut their engines when waiting in line at the drive-through.

Bank patrons who use the pneumatic tubes are greeted by large stickers reading, "Turn your key, be idle free. By turning your engine off when waiting, you will breathe easier and save gas."

The statewide campaign originated in Salt Lake City, where the city was partnering with businesses "to identify ways to educate the public on ways to improve our air quality," said Rob Brough, executive vice president for Zions Bank. "The drive-through seemed like the logical way."

Only time will tell if the campaign has a measurable effect, he said. In the meantime, the campaign has garnered positive comments from customers. "We are a community bank and we live in this community with our families as well, and we all benefit by having cleaner air," he said.

Joe Thomas of the Utah Division of Air Quality said the program is such a good idea that he himself cuts his engine anytime he's at any bank drive-through. If a car is going to be running on idle for several minutes, cutting the engine is a simple and easy way to improve gas mileage, he said. There is a nationwide program to encourage school bus drivers to reduce idle time as well.

"Definitely when the car is idling, you just wasted energy," Thomas said. "You aren't doing anything." Cutting the engine in a bank drive-through is an especially good idea because in between filling out paperwork and having a conversation with the teller, the transaction could take several minutes, he said."

Staff counted 37 spaces including two ADA spaces. This far exceeds the amount allowed by the CDC. The CDC only allows the minimum 12 spaces to be exceeded by 10% or two spaces for a maximum of 14 spaces. Thirteen spaces must be eliminated. There is no basis to support a Variance.

Access/ODOT

Access from Willamette Drive would utilize an existing driveway that is shared with the 7-11 development next door. The applicant proposes a 30-foot wide driveway width. The maximum curb cut width on Highway 43 is 40 feet but it requires ODOT approval. An ODOT approach permit would have to be obtained. According to a telephone message from Gail Curtis no traffic study is needed for ODOT but the City still requires one. Gail Curtis was also supportive of the joint driveway access to the adjacent property to the north. (Please also refer to Gail Curtis' letter from ODOT attached relating to a previous development proposal.)

Landscaping

Landscaping is explained in 54.020(E)(2)(3) and 54.070. There is ample room at the rear of the site to meet the total overall 20% landscaping requirement. There is also the requirement that five percent of the rear parking lot comprise landscaping. (This assumes that the applicant will reduce the parking spaces to the maximum of 14 allowed by the CDC.) Landscaping is explained in 54.020(E)(2)(3) and 54.070. Street trees along Willamette Drive at 35 feet on center are required in cutouts adjacent to the curb.

A landscaped strip at least five feet wide is required between this site and the properties on all sides and rear per CDC 54.020(E)(3)(b)(d).

Noise

The sound of cars using the drive through, the noise of the loudspeaker at the drive through plus general traffic noise in the parking lot are anticipated. Because Kasch's garden store operated at this site for decades the noise study would not be per the "previously unused commercial property" standards. See CDC 55.100(D). The recommendations of the noise study would be an important part of the noise mitigation plan.

Screening the rear of the site and parking lot, from adjacent residential uses, with a six foot tall solid masonry wall would mitigate glare but more importantly, the noise from the 24-hour drive through area.

Bike Parking

CDC 46.150(D) requires two bike parking spaces (based on stated building size). At least one shall be covered. Both shall be located near the front entrance where they can be easily observed.

Signs

One freestanding sign at 32 sq ft. is allowed. The maximum height is eight feet and it must be mounted per CDC 52.210(G). A maximum of three wall signs are allowed. Signs shall not exceed 10% of the square footage of the wall they are mounted on either individually or collectively. (See CDC Chapter 52.300.) The applicant proposes a sign on the ATM. Only three wall mounted signs per business are permitted. Given the desire to have three on the bank there would not be additional signage available for the ATM. A variance would be the only

option. Given that drive through windows at fast food restaurants get extra signage, a case could be made.

Refuse and Recycling Containment

Refuse and recycling facilities are required and are detailed in CDC section 55.100(O). Landscaped screening and buffering is required.

Setbacks

CDC 19.070(A)(7) states: *"For lot lines that abut an arterial, there shall be no minimum yard dimensions or minimum building setback area, and the maximum building setback shall be 20 feet. The front setback area between the street and the building line shall consist of landscaping or a combination of non-vehicular hardscape areas (covered with impervious surfaces) and landscaped areas, with at least 25 percent of the front setback area consisting of landscaped areas. If there are not street trees within the public right-of-way, the front setback area shall include such trees per the requirements of the City Arborist."*

That code section runs counter to design concepts for that street which require that all buildings have a zero foot setback for 100% of their (building) frontage. Staff and the City do not want to back away from the zero foot setback and the desire to create a more dynamic social space along Willamette Drive. Therefore the applicant needs to apply for, with full staff support, a Class II Variance from the 25% landscape standard.

Setbacks on the side lot lines is zero feet. At the rear the setback is 20 feet for buildings. Additional buffering may be required at the rear to mitigate impacts.

Lighting

Site lighting is allowed but no off site glare is permitted.

Lighting should be designed to enhance defensible space.

Blue uplighting may be permitted so long as it focuses on the building and signs.

ENGINEERING COMMENTS

Khoi Le

Kle@westlinnoregon.gov

TRAFFIC

The property is currently located in the corridor where the City Transportation System Plan has developed a specific conceptual plan for this particular corridor.

At the current conditions, the City TSP identifies that the intersection between Hwy 43 and Pimlico Drive has been operating at a deficient level of service.

TSP recommends a traffic signal to be installed when warranted. Thus traffic impact shall be required for this intersection to determine whether or not the signal is required.

By 2030, there will be several intersections along Hwy 43 that will be operating at deficient level of service. The two nearest intersections to the project site are Hwy 43/Cedaroak Drive and Hwy 43/Hidden Springs Road. Therefore a traffic impact analysis shall be required.

TRAFFIC IMPACT ANALYSIS SCOPE OF WORKS

Traffic Impact Study shall be required on either option. Traffic Impact Study shall provide following information and analysis:

- Executive Summary
- Introduction
- Location Description
- Trip Generation
- Trip Distribution
- Operational Analysis
- Safety Analysis

Analysis shall also be done for the following intersections.

- Hwy 43 and Cedaroak Drive
- Hwy 43 and Hidden Springs Road

Traffic Impact Study shall also include any scopes and analysis that are required by ODOT.

Per City of West Linn Transportation Master Plan, following is classification for those streets.

- Hwy 43 – Major Arterial
- Cedaroak Drive – Neighborhood Route
- Hidden Springs Road – Minor Arterial

Engineering must scope the applicant's traffic report in addition to ODOT.

DRIVEWAY

Many existing driveway along Hwy 43 do not meet the current spacing requirement.

TSP – 300 feet spacing is required.

The current shared driveway between the proposed development and 7Eleven and the adjacent development to the North is approximately 200 feet apart. It does not meet the

spacing requirement therefore any additional driveway between these two driveways shall not be allowed.

There is an access agreement in place when the adjacent property to the North came in for developing. Please examine the option of sharing driveway access with this property.

If access agreement with 7-11 has not been established, an established agreement will be required.

STREET IMPROVEMENT

Street improvement shall be required. Street improvement shall consist of street pavement replacement with new curb as well as new sidewalk.

New sidewalk shall be as wide as the existing sidewalk along the adjacent property located on the North. Existing sidewalk may need improvement so sidewalk width will be consistent along all properties.

Dedication or public easement may require keeping public pedestrian walkway inside public right of way or public easement.

Street improvement shall meet both City of West Linn and ODOT requirements.

Development shall require obtaining all permits necessary from ODOT for the development.

Development shall require providing proof of approval from ODOT before City construction permit being released for construction.

Provide a photometric analysis for existing street lights along the project frontage along Highway 43 to ensure whether or not additional street light shall be required.

Check with ODOT on the height clearance for street light mast arm if additional street light is needed.

STORM DRAINAGE IMPROVEMENT

Since the previous development has never provided storm treatment water treatment and detention, new development is required to provide storm water treatment and detention meeting the City of West Linn Standards.

If the development is required obtaining DEQ approval, development shall require providing proof of approval from DEQ before City construction permit being released for construction.

Storm Drainage Report shall be required.

SANITARY SEWER

There is existing public sanitary sewer main line in the back of the property available for connection.

WATER IMPROVEMENT

This property is currently located in the Robin Wood Pressure Zone. The Robin Wood Water Pressure Zone is currently deficient under emergency conditions however there is not any improvements along or nearby the proposed development listed in the City Water Master Plan. Therefore the proposed development shall require paying water SDC for the additional water demand.

When water demand occurs, a hydraulic analysis shall be required. The developer shall require paying Murray & Smith Associates for the analysis making sure the increased demand will not make the current water system worsen.

Development shall require providing proof of approval from the Fire Department before City construction permit being released for construction.

The applicant should contact Joel Komarek (jkomarek@ci.oswego.or.us) of Lake Oswego regarding the Lake Oswego water line running along the project frontage on Willamette Drive.

OTHER UTILITY IMPROVEMENTS

No overhead utilities and utility poles with the exception of street lights shall be allowed along the project frontage. All existing overhead utilities along the project frontage shall be placed underground. All existing utility poles along the project frontage shall be removed. Development shall be responsible for expenses removing existing poles and placing existing overhead utilities underground.

STREET SDC

Applicant must pay Street SDC. Street SDC calculations are based on the Total Trip Generation from the Development during PM Peak Hour.

Applicant can use Total Trip Generation for a Drive-Through Bank from the ITE 8th Edition or use Total Trip Generation from the Traffic Impact Study prepared by an Oregon License Professional Traffic Engineer.

Applicant shall receive credit from Trip Generation from the former Kasch's Nursery.

BUILDING DEPARTMENT COMMENTS

The system development charges (SDCs) including the Road SDC's which could be a significant amount. The applicant would get credit for trip generation produced by Kasch's. The applicant's traffic study will be used as the basis for projecting the Chase bank trip generation which in turn will be used to compute the SDC's. This SDC charge does not include other utility related SDC's, building permits, etc. (Contact David Davies at ddavies@westlinnoregon.gov or Jim Clark who prepares the SDC calculations at jclark@westlinnoregon.gov).

CONCLUSION

Staff could support this application contingent upon, but not exclusively:

1. Extending a permanent flat or modest sloped metal canopy/awning across front elevation.
2. Meeting permitted noise standards.
3. Adequate transparency requirements on three building elevations (north, south and west) per code.
4. Reduce parking spaces to a maximum of 14 including ADA space.
5. Construct driveway to property to the north with mutual access easement.
6. Use minimal cornices, limit roof variations to hipped/pyramidal roof over main entry.

PROCESS

Schedule and conduct a neighborhood meeting pursuant to CDC Section 99.038. Follow the requirements exactly. The Robinwood Neighborhood Association (RNA) meets on the second Tuesday of the month. Contact Tom Boes, President, at 699-6112 or e-mail at TCBOES@gmail.com. For this meeting the applicant shall provide all available plans, architectural drawings and explanatory narrative to the RNA at least ten days prior to the meeting.

Required permit: Class II Design Review with Class II Variances (front setback/landscaping, transparency, signs, etc.)

Complete the Class II Design Review application form and submit it to the Planning Department with deposit/fees based upon the fee schedule. (The deposit/fee for design review is 4,000 dollars plus four percent of the construction value as determined by the Building Department (20,000 dollar minimum). Staff will bill hours against these deposits. Surplus deposit money will be returned on completion of the land use permit process. Cost overruns could result in payment of additional deposit/fees.

Accompanying the application and deposit/fees will be a full and complete submittal and full response to the approval criteria per the requirements of CDC Chapters 55. Other applicable chapters include 46, 48, 52 and 54. If you submit signs for review at this time you can avoid additional delays of up to a month to obtain the sign permit later. Sign permit fee is 250 dollars. In the event that a Class II Variance(s) is needed, the deposit fee is 1,800 dollars and

the submittal and approval criteria of CDC Chapter 75 must be addressed. If multiple variances are needed, each additional application deposit fee is 900 dollars.

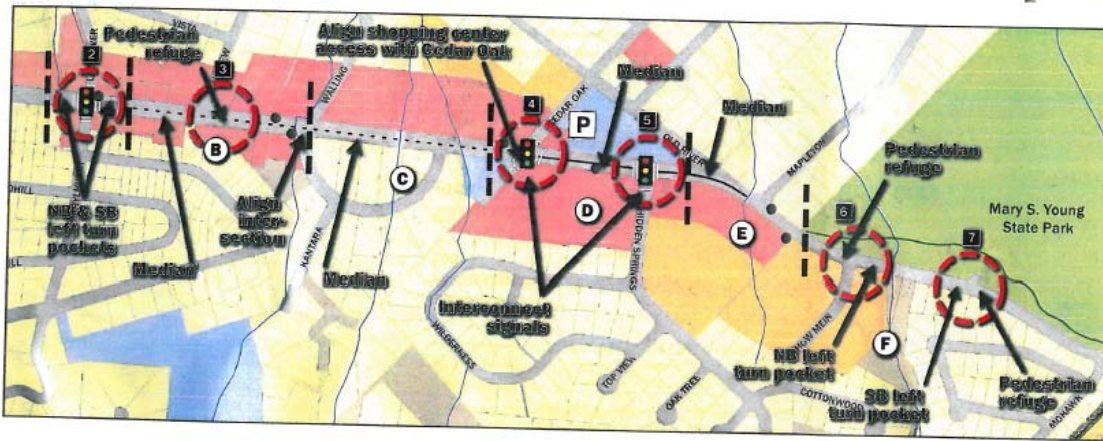
The City has 30 days to determine if the application is complete or not. Most applications are incomplete, usually due to inadequate responses to approval criteria or stating “NA” in response to submittal requirements or approval criteria. Statutorily, the applicant has 180 days to make it complete, although usually it is complete within three to four months of the original submittal date. Once complete, the burden shifts to the City and the City has 120 days to exhaust all local review (hearings) and appeals.

Once complete, the application will be noticed to property owners within 500 feet of the site perimeter. There will be Planning Commission hearing about four weeks after the determination of completeness. Appeals are heard by City Council.

Typical land use applications can take 6-10 months from beginning to end.

DISCLAIMER: This summary discussion covers issues identified to date. It does not imply that these are the only issues. The burden of proof is on the applicant to demonstrate that all approval criteria have been met. These notes do not constitute an endorsement of the proposed application. Staff responses are based on limited material presented at this pre-application meeting. New issues, requirements, etc. could emerge as the application is developed. These pre-application notes shall be invalid 18 months from the date of the pre-app. After that date, another pre-app would be required. Also, new state laws and development code amendments can impact the feasibility of a project.

West Linn OR 43 Conceptual Design Plan



Final Report

January 4, 2008



APPROVED BY THE WEST LINN CITY COUNCIL

RESOLUTION 07-26

Segment D / Intersections 4 & 5

Segment D is generally an 80' right-of-way, with the Robinwood Shopping Center on one side, and the TriMet shared use park & ride on the other. The cross section responds to the greater amount of pedestrian traffic associated with these land uses, and provides 10' sidewalks on both sides of the street, separated from the bike path and roadway by a planting strip with tree wells. A median is planned for the entire length of Segment D. This median is not expected to negatively impact access along the segment, as the Park & Ride and the commercial use at the northwest corner of OR 43 and Cedar Oak both maintain access drives to side streets.

Furthermore, the plan recommends shifting the existing access drive to the Robinwood Shopping Center from its current midblock location to more closely align with Cedar Oak. This alignment will create a true, 4-leg intersection, and is expected to dramatically improve functionality and safety. It should be noted that the shopping center's parking lot configuration would have to change in order to accommodate this new access drive at the northern boundary of the property. Final determinations regarding specific designs for the new driveway, and the effect of driveway queuing on the existing parking lot's functionality are to be studied and determined during preliminary engineering.

As stated in the previous section, some issues associated with this realignment will need to be addressed during preliminary engineering, however. These issues include examining the effect of

the "skew" angle of Cedar Oak on the proposed improvement, as well as examining any right-of-way takes which may be associated with the improvement. Signage issues associated with proposed new right turn and right through lanes on OR 43 at Cedar Oak will also require ODOT examination. If these issues cannot be satisfactorily resolved, status quo conditions will prevail.

The current property owner has expressed a wish to maintain the existing entrance along HWY 43 as a "right-in / right-out" access drive. However, the recommended driveway realignment is recommended as a safety improvement. Therefore, any decision to keep the current driveway open will depend upon future studies analyzing the safety of maintaining this driveway. It should be noted that the current driveway location and its width negatively impact the pedestrian environment. Furthermore, TriMet has voiced a preference for closing the driveway, based on rider input, as it creates conflicts between automobiles and pedestrians, and endangers transit riders walking to and from nearby bus stops and the park and ride. These moves may also allow TriMet to adjust the current bus stops to better meet the needs of transit riders.

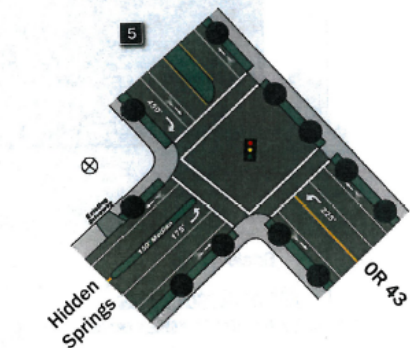
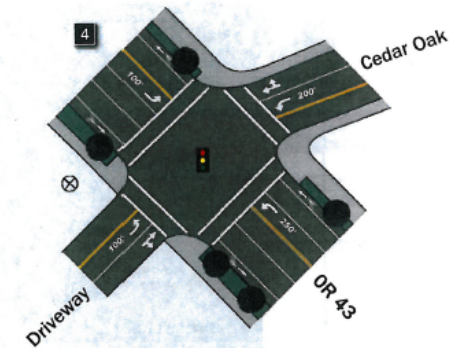
In addition to this realignment, the plan recommends installing a central median along Hidden Springs at the approach to OR 43. This median is designed to prevent left turn maneuvers from the shopping center onto Hidden Springs, and from Hidden Springs into the shopping center. The driveway's proximity to the signal

and the volume of traffic flowing through the Hidden Springs intersection makes this maneuver unsafe. The driveway will remain open as a "right-in / right-out" access point. Drivers wishing to make left turns into and out of the shopping center would be encouraged to do so at the access drive located at the rear side of the site (at the property's southern edge).

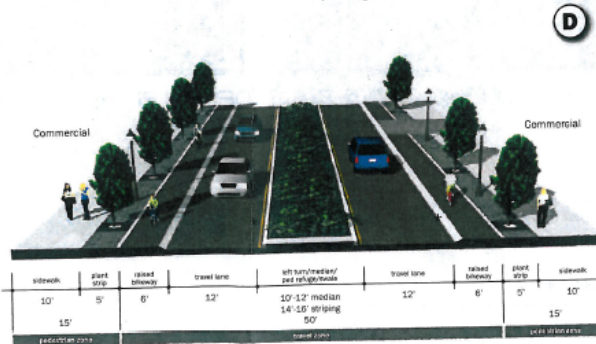
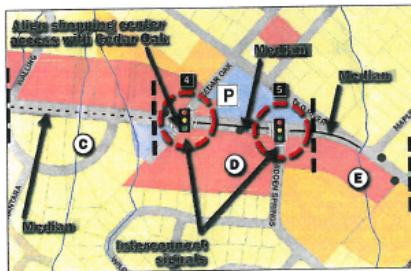
To further maximize vehicular mobility in this area, the plan recommends interconnecting the traffic signals at Cedar Oak and Hidden Springs.

The summary of impacts related to this intersection improvement at Cedar Oak Drive and Hidden Springs Road are as follows:

- Existing ROW on Highway 43 is approximately 75 to 78 feet. To accommodate turn lanes and minimize ROW takes, the proposed cross section with 15-foot planter/sidewalk on each side would be narrowed by approximately 9 feet on the west side, requiring additional ROW of 5 to 8 feet total width.
- Relocation of the existing commercial driveway between Cedar Oak and Hidden Springs will eliminate one driveway conflict. One driveway north of Cedar Oak Drive will conflict with the proposed left turn lane on Highway 43, but it is already a shared driveway and other access alternatives do not exist. One driveway south of Hidden Springs Road conflicts with the proposed turn lanes, but may be closed because it is one of three driveways that access a single parking lot. It is currently channelized as right-in, right-out.



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* Raised bikeway subject to ODOT review and approval
* Landscaping/swales subject to ODOT approval and maintenance agreement

LETTER FROM ODOT relating to an earlier pre-application conference for the same site:

April 15, 2010
City of West Linn
Attention: Peter Spir
22500 Salamo Road, #1000
West Linn, OR 97068

SUBJECT: Key Bank Highway 43 Pre-Application

Dear Mr. Spir,

As you know, ODOT owns and operates the Highway 43 and has an interest in ensuring that area land use is compatible with its safe and efficient operation. We have not received a preliminary site plan so are at a disadvantage to respond but wish to address a couple of potential issues.

The development of the scope for the traffic impact analysis should include our participation to ensure the correct standards are applied. Please have Key Bank's traffic engineer contact Avi Tayar, PE with ODOT at 503-731-8221 or Abraham.tayar@odot.state.or.us to coordinate.

We also wish to raise concern about locating the building along the Highway 43 right of way, as we understand is being considered. While we developing an inviting pedestrian environment with no parking between the building and the street, we want to make sure the driveway sight distance is not limited. Are there applicable city standards to address this?

Regarding placing awnings in the Highway 43 right of way, we need more information to determine if awnings would be allowed and a permit would be required. We consider such requests on a case by case basis. Our primary concern is safety and liability. We want to make sure the awnings are placed to allow full sight distance at the driveway(s). If it would be helpful, I can provide an example of the permit conditions that applied to a similar situation which include provisions to eliminate ODOT liability. Steve Schalk with District 2A is the ODOT Permit Specialist. He can be reached at 503-229-5002 or Steven.B.Schalk@odot.state.or.us

Please provide me with a copy of the applicant's formal submittal. If you or the applicants have any questions regarding the above comments, I can be reached at 503.731.8206.

Sincerely,

Gail Curtis, AICP

Senior Transportation Planner

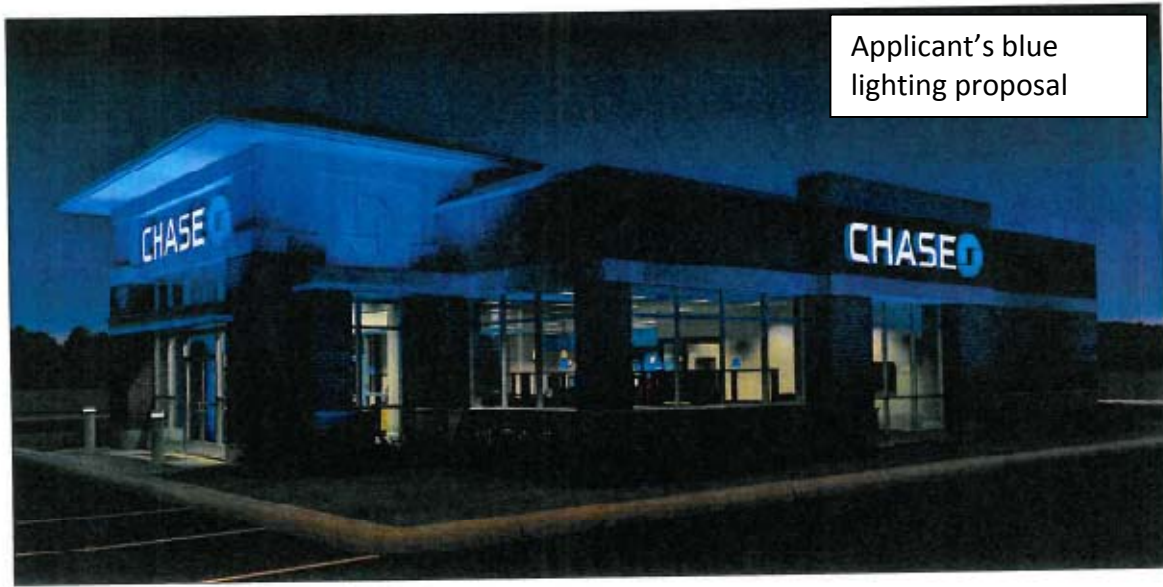
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