

City of West Linn
PRE-APPLICATION CONFERENCE MEETING
SUMMARY NOTES
April 15, 2010

SUBJECT: Key Bank at 19080 Willamette Drive

ATTENDEES: Applicants: Vina Anderson, Jeff Olson, Geoff Sherwin, Patrick Flanagan, Bob Asahara, Cheryl Ebsworth
Staff: Khoi Le (Engineering), Peter Spir (Planning), David Davies (Building), Karen Mohling (Tualatin Valley Fire and Rescue)

The following is a summary of the meeting discussion provided to you from staff meeting notes. Additional information may be provided to address any “follow-up” items identified during the meeting. These comments are PRELIMINARY in nature. Please contact the Planning Department with any questions regarding approval criteria, submittal requirements, or any other planning-related items. Please note disclaimer statement below.

Project Details

The proposal would create a single 3,900 square foot commercial/office building to be used as a Key Bank branch at 19080 Willamette Drive. The parcel comprises 46,109 square feet. The building would be located at the northwest corner of the lot abutting the Willamette Drive right of way (ROW). The property is zoned “General Commercial” (GC). Banks are “permitted outright” in the GC zone so long as Class II Design Review is addressed.

The site extends back or east 260 feet from the Willamette Drive ROW. The front two-thirds of the lot is flat. The land then drops down to the rear property line with slopes of over 25% in the final 50 feet. There are no drainageways, riparian zones, wetlands or other natural features as defined by the CDC on the property. The trees are few and limited to the rear sloped area near the property line. They appear to be three cottonwoods about 30-40 feet tall. These trees and change in topography provide a buffer between the site and the apartments to the rear of the site. (A survey is needed to determine if the trees are in fact on the applicant’s property.)

The applicant’s initial site plan with parking in front of the bank and two driveways onto Willamette Drive was modified after discussions with staff. The revised site plan, discussed at the pre-application conference, showed the building fronting on or contiguous to Willamette Drive with the parking and drive-throughs pushed to the rear and side. The second driveway was eliminated and a driveway connection to the north shown. Staff sees this revision as a very positive design change in keeping with our CDC.

Architecture

The design submitted by the applicant is a single story building clad in brick with a cornice wrapping around the upper edge of the roof. There are pop-out/bumps on the front and side elevations. While the bump-outs are fine, Staff would like to see the associated false front/raised parapet features eliminated on all elevations. This false front/raised parapet design can be seen at numerous locations in the Portland Metro area and although these designs were acceptable at a particular time and place, they have become outdated and repetitive. The fact that this design is repeated so often in this region means that it would be difficult for West Linn to project itself, through this design, as a unique place.

Staff strongly recommends selecting a well crafted conservative design that is less prone to being tied to a specific moment in time. Staff visits to commercial properties in the Metro area introduced a number of features shown in photos below that could make this an attractive part of Willamette Drive. Designs included the use of contrasting colored brick patterns, "stone" drivet patterns, and horizontal wood insets on the upper portion of the building. Any of these elements could produce a distinctive appearance. The use of smaller cornices that barely project from the building (or no cornices at all) also came across as a good way to go rather than using the exaggerated cornices common on so many retail or office buildings. Flat /horizontal rooflines are preferred over the already dated false front/parapets and peaks. Breaking the windows up into multiple lights was also a good feature.

Staff has looked at other designs by Key Bank and similar businesses in the region. To its credit, Key Bank is capable of some very innovative and attractive designs. A recladding of a commercial building on 4131 Hawthorne Street in Portland produced a unique contemporary design for Key Bank. Retrofitting a Key Bank into a historic landmark structure in Tualatin shows that Key Bank can produce products that are sensitive to a community's past. Meanwhile, a Key Bank in Vancouver, Washington (1408 SE 164th) eliminated the standard drivet upper surfaces and the False front and went with a flat roof and all brick cladding.

Architectural concerns include the need to meet transparency requirements of CDC 55.100(B)(6)(f). Sixty percent of the lineal frontage of the Willamette Drive elevation must have windows. The north and south facing elevations also need 30 percent transparency each. Glass must be clear and cannot be mirrored or smoked etc.

It is noted that the building elevation that fronts or abuts the Willamette Drive ROW includes the main entrance which is a positive response to the Transportation Planning Rule and CDC Chapter 55.100(B)(7). There is also a door shown on the south elevation.

There should be an eight-foot awning/overhang extending along the Willamette Drive frontage as well as any other elevations that have sidewalks or pedestrian facilities adjacent to them. Overhangs need eight feet vertical clearance. There should no more than a 1-1.5 foot break between awning sections. Please verify with Gail Curtis of ODOT (gail.e.curtis@odot.or.us) the ability to extend awnings/overhangs into the state ROW. (See also the attached letter from ODOT.)

At least a 12-foot wide sidewalk is required along the entire Willamette Drive frontage. (The wider the better.) Cutouts for street trees are needed adjacent to the curb. Trees shall be 30 feet on center. Staff appreciated the very broad sidewalk (eight to nine feet) on the south elevation too.

The revised site plan shows the building moved up to the front property line per the Transportation Planning Rule and CDC 55.100(B)(7). The parking is on the side or south elevation and at the rear of the site. The three drive-through windows at the rear of the bank building. Cars exiting the drive-through and the bank property can make use of the driveway that connects through the property to the north.

Access

Access from Willamette Drive would utilize an existing driveway that is shared with the 7-11 development next door. The applicant stated that they would like to reduce the driveway width to 24 feet. An ODOT access permit would have to be obtained. The applicant asked about removing a landscaped median strip adjacent to the entry driveway so as to allow easier access to the parking area on the south side of the bank. Khoi Le explained that the landscaped median must be retained so as to allow queuing or stacking room for vehicles trying to exit the site and minimize conflicts with traffic entering the site and cars maneuvering in the parking area.

The applicant asked whether the landscaped median strip could be less than 5 feet. CDC 54.020(E)(3)(b)(d) requires landscaped strips to be at least five feet wide. This criterion also requires that parking areas adjacent to the property perimeter be at least five feet wide. To allow a reduced width or to eliminate some, or all, of the landscaped median would require a Class II Variance. Please refer to CDC Chapter 75 for the approval criterion.

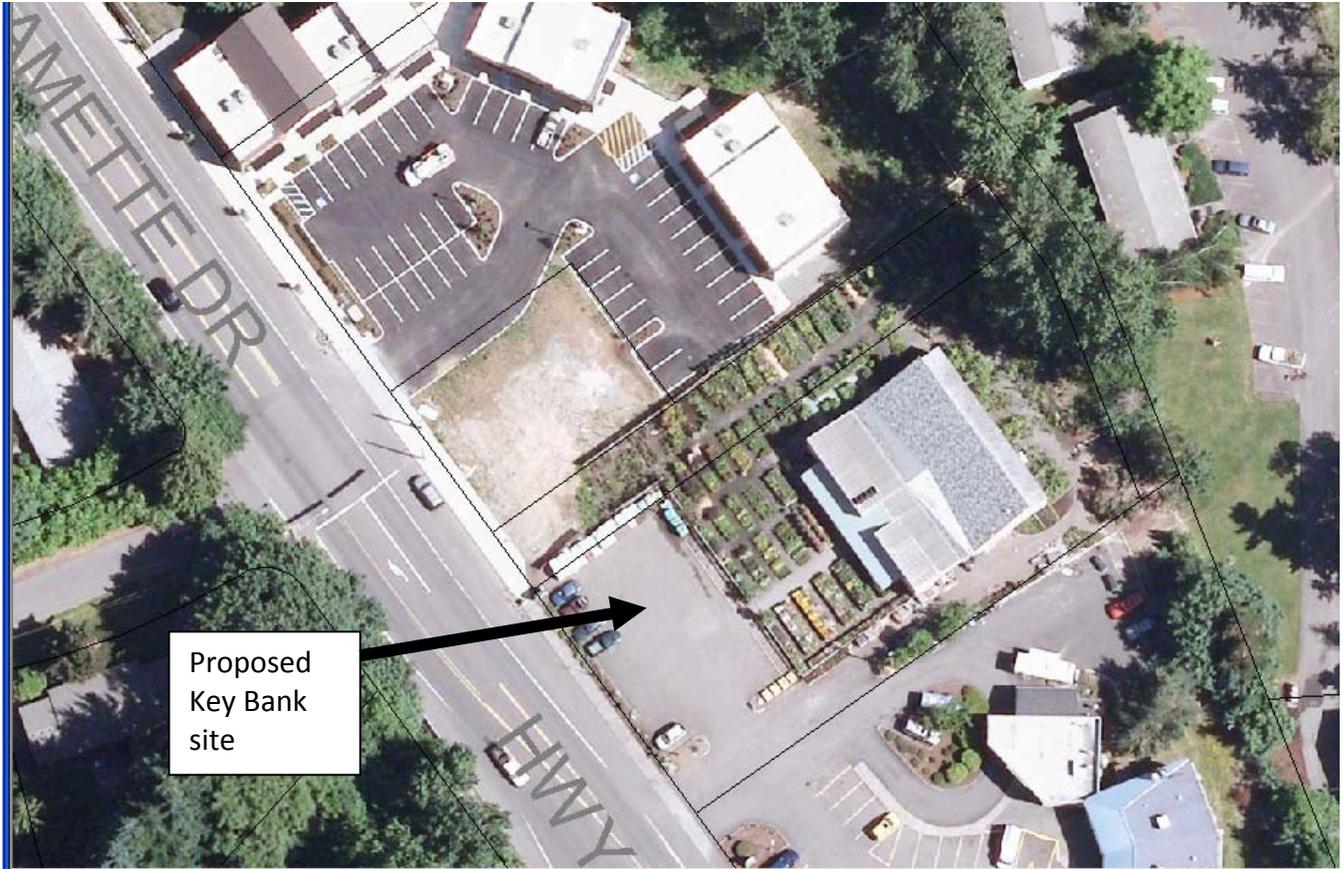
The applicant's site plan shows the connection to the adjacent property to the north. The developer of that property (to the north) is required to provide a 24-foot wide driveway and access easement for the benefit of this property (Kasch's). The condition of approval for that application (DR-06-43) is as follows:

“At such time that the commercially zoned property to the south (Assessor’s Map 21E 23AA taxlot 703) currently known as Kasch’s, applies for a design review application, this applicant shall provide and construct a 24 foot wide driveway along the south property line to connect the two parcels. Further, the applicant shall prepare and sign a waiver of remonstrance to the connection, construction and use of a driveway from the taxlot 703 (Kasch’s property) onto the applicant’s property. Further, the applicant shall sign and record a reciprocal and mutual access easement that would allow traffic from the taxlot 703 (Kasch’s property) to exit via the applicant’s parking lot and driveway and vice versa. These documents shall be reviewed and approved by the City Attorney.”

The intent also is that the traffic from the property to the north could traverse the Kasch's site and use their point of ingress/egress.

Pedestrian facilities in the parking lot will be required per CDC 55.100(B)(7) and 46.150(A)(20). Parking is calculated at one space per 350 square feet of gross floor area. Half of the spaces must be compact (8X16) and half standard (9X18). ADA spaces are required per 46.150(B).

Landscaping is explained in 54.020(E)(2)(3) and 54.070. Although there is ample room at the rear of the site to meet the total 20% landscaping requirement, there is the requirement for landscaping within the parking lot that needs to be attended to (CDC Chapter 54).



Drive through lanes: Design and Sustainability (not related to approval criterion)

Drive-through lanes are shown at the rear of the building. Discussion of standards is in CDC 46.110. Staff would offer the observation that three drive-through lanes seem excessive. Staff acknowledges that the most frequent number of lanes at banks, in a local survey, is three, but the dynamics of banking are changing. As more people manage their bank accounts online or access their money by debit cards and ATMs, the rationale for three lanes may be diminishing. Also, the reduction in staff means that there will probably only be one staffer to handle three lanes; not one per lane, so the speed of processing is no faster for three lanes than it is for two lanes.

In addition, the City has concerns with drive throughs in terms of sustainability and pollution. Review of industry literature produced the following positive solution:

“Cars idling for more than 10 seconds use more gas and create more global warming pollution than simply restarting the engine. Surprised? It's true - the 10-second rule has been proven empirically. The 10-second rule was originally published on the Canadian Office of Energy Efficiency's Idle-Free Zone

webpage. Their results were replicated by the American Society of Mechanical Engineers, which found that restarting uses the same amount of fuel as idling with the air conditioner on for 6 seconds.

Zions Bank in Utah is asking its customers to turn the key on air pollution.

In a voluntary effort to help clean up Utah's smog, all Zions locations are asking their customers to cut their engines when waiting in line at the drive-through.

Bank patrons who use the pneumatic tubes are greeted by large stickers reading, "Turn your key, be idle free. By turning your engine off when waiting, you will breathe easier and save gas."

The statewide campaign originated in Salt Lake City, where the city was partnering with businesses "to identify ways to educate the public on ways to improve our air quality," said Rob Brough, executive vice president for Zions Bank. "The drive-through seemed like the logical way."

Only time will tell if the campaign has a measurable effect, he said. In the meantime, the campaign has garnered positive comments from customers. "We are a community bank and we live in this community with our families as well, and we all benefit by having cleaner air," he said.

Joe Thomas of the Utah Division of Air Quality said the program is such a good idea that he himself cuts his engine anytime he's at any bank drive-through. If a car is going to be running on idle for several minutes, cutting the engine is a simple and easy way to improve gas mileage, he said. There is a nationwide program to encourage school bus drivers to reduce idle time as well.

"Definitely when the car is idling, you just wasted energy," Thomas said. "You aren't doing anything." Cutting the engine in a bank drive-through is an especially good idea because in between filling out paperwork and having a conversation with the teller, the transaction could take several minutes, he said."

Future development

The applicant asked if there are things that could be done now or in the layout of the site that would facilitate the development of the rear portion of the site in the future. Staff stated that since the availability of parking is the main limitation at commercial sites, the applicant should first determine how much room is available for parking in excess of the bank parking requirements. If there is room for additional parking, the number of spaces will dictate how much additional retail space could be built. For example, if there is room for four extra parking spaces, you would multiply that by the retail space calculations in Chapter 46.090 (one space per 240 square feet yields an allowable 960 square feet of retail space.) There is also the need to provide 20 percent landscaping for the total site plus landscaping of parking areas.

Noise

Whereas speakers were a concern during the design review process for McDonalds near this property, the limited hours of operation for the bank mean that the noise generated by the drive-through speakers will probably not conflict with city noise standards. A noise study will be required. Because Kasch's garden store operated at this site for decades the noise study would not be per the "previously unused commercial property" standards. See CDC 55.100(D).

Buffering Uses

Typically when commercial uses are proposed adjacent to residential uses there is a need for buffering through the construction of sound walls, landscaping, etc. But because this site plan pushes all the activities forward towards Willamette Drive, there will not be the same levels of concern. The distance from the rear parking area to apartments to the east of the site will be 125 feet. The cottonwoods will also offer screening. Also the grade differential between the commercial and residential areas should diminish impacts. At most, a wooden fence at the rear of the parking lot would be required to block glare from headlights.

Bike Parking

CDC 46.150(D) requires two bike parking spaces (based on stated building size), one of which shall be covered per this code section.

Internal Pedestrian Facilities

The use of different colors, textures or materials to encourage use of pedestrian routes through the parking and driveway areas is required per 55.100(B)(7)(d)(e).

Engineering Comments

Khoi Le

Kle@westlinnoregon.gov

TRAFFIC

The City's Transportation System Plan (TSP) has developed a design concept for Willamette Drive but that design concept only relates to the right of way (ROW). No additional ROW is expected to be required.

At the current conditions, the City TSP identifies that the intersection between Willamette Drive and Pimlico Drive has been operating at a deficient level of service.

By 2030, there will be several intersections along Hwy 43 that will be operating at deficient level of service. The two nearest intersections to the project site are Willamette Drive/Cedar oak Drive and Willamette Drive/Hidden Springs Road. Therefore, a traffic impact analysis shall be required.

TRAFFIC IMPACT ANALYSIS SCOPE OF WORKS

Traffic Impact Study shall be required on either option. Traffic Impact Study shall provide following information and analysis.

- Executive Summary
- Introduction
- Location Description

- Trip Generation
- Trip Distribution
- Operational Analysis
- Safety Analysis

Analysis shall also be done for the following intersections.

- Willamette Drive and Cedaroak Drive
- Willamette Drive and Hidden Springs Road

Traffic Impact Study shall also include any scopes and analysis that are required by ODOT.

Per City of West Linn Transportation Master Plan, following is classification for those streets.

- Willamette Drive – Major Arterial
- Cedaroak Drive – Neighborhood Route
- Hidden Springs Road – Minor Arterial

Engineering must scope the applicant's traffic report in addition to ODOT.

ODOT contact Gail Curtis, Senior Planner at Gail.E.Curtis@odot.or.us

DRIVEWAY

Many existing driveways along Willamette Drive do not meet the current TSP spacing requirement of 300 feet. The current shared driveway between the proposed development and 7-Eleven and the adjacent development to the North is approximately 200 feet apart. It does not meet the spacing requirement therefore any additional driveway between these two driveways shall not be allowed.

There is an access agreement in place when the adjacent property to the North came in for developing. The applicant should plan on using this driveway.

If access agreement with 7-Eleven has not been established, an established agreement will be required.

STREET IMPROVEMENT

Street improvements shall be required. Street improvements shall consist of street pavement replacement with new curb as well as new sidewalk. Street improvements shall meet both City of West Linn and ODOT requirements.

Development shall require obtaining all permits necessary from ODOT for the development. Development shall require providing proof of approval from ODOT before City construction permit is issued.

A new sidewalk along Willamette Drive shall, at minimum, be as wide as the existing sidewalk along the adjacent property to the north. Dedication or public easement may require keeping public pedestrian walk way inside public right of way or public easement.

STORM DRAINAGE IMPROVEMENT

Since the previous development (Kasch's) never had storm treatment water treatment and detention, the new development is required to provide storm water treatment and detention to meet the City of West Linn Standards.

If the development triggers the need for DEQ approval, proof of DEQ approval shall be required before City construction permit will be issued.

SANITARY SEWER

There is an existing public sanitary sewer main line in the back of the property available for connection. (Easement may be required to access the line if it is on adjacent property.)

WATER IMPROVEMENT

This property is currently located in the Robinwood Pressure Zone. The Robinwood Water Pressure Zone is currently deficient under emergency conditions however there are no improvements along or nearby the proposed development listed in the City Water Master Plan. Therefore the proposed development shall be required to pay water SDC for the additional water demand.

When water demand occurs, a hydraulic analysis shall be required. The developer will be required to pay Murray & Smith Associates for the analysis making sure the increased demand will not worsen the current water system.

FIRE

Since the Robinwood Pressure Zone is deficient, a fire flow test shall be required. Development and building plans shall require proof of approval from TVFR before the City releases construction and building permits. (Deputy Fire Marshall Karen Mohling's comments on behalf of TVFR were generally positive and identified few requirements for the bank. Hydrant locations may require further review. She may be contacted at 503-612-7012 or Karen.Mohling@TVFR.com)

OTHER UTILIY IMPROVEMENTS

No overhead utilities and utility poles, with the exception of street lights, shall be allowed along the project frontage. All existing overhead utilities along the project frontage shall be placed underground. All existing utility poles along the project frontage shall be removed. The applicant shall be responsible for removing existing poles and placing existing overhead utilities underground.

Building Department Comments

Building Official David Davies explained the system development charges (SDCs) that would be imposed on this development. The most significant point was that the Road SDC's will be in excess of \$1,000,000. The applicant would get credit for PM peak trip generation produced by Kasch's, but that would only reduce the SDC to the area around \$750,000 dollars. This SDC charge does not include other utility related SDC's, building permits, etc. (Contact David Davies at ddavies@westlinnoregon.gov or Jim Clark who prepares the SDC calculations at jclark@westlinnoregon.gov)

Process

Schedule and conduct a neighborhood meeting pursuant to CDC Section 99.038. Follow the requirements exactly. The Robinwood Neighborhood Association meets on the second Tuesday of the month. Contact Tom Boes, President, at 699-6112 or e-mail at TCBOES@gmail.com.

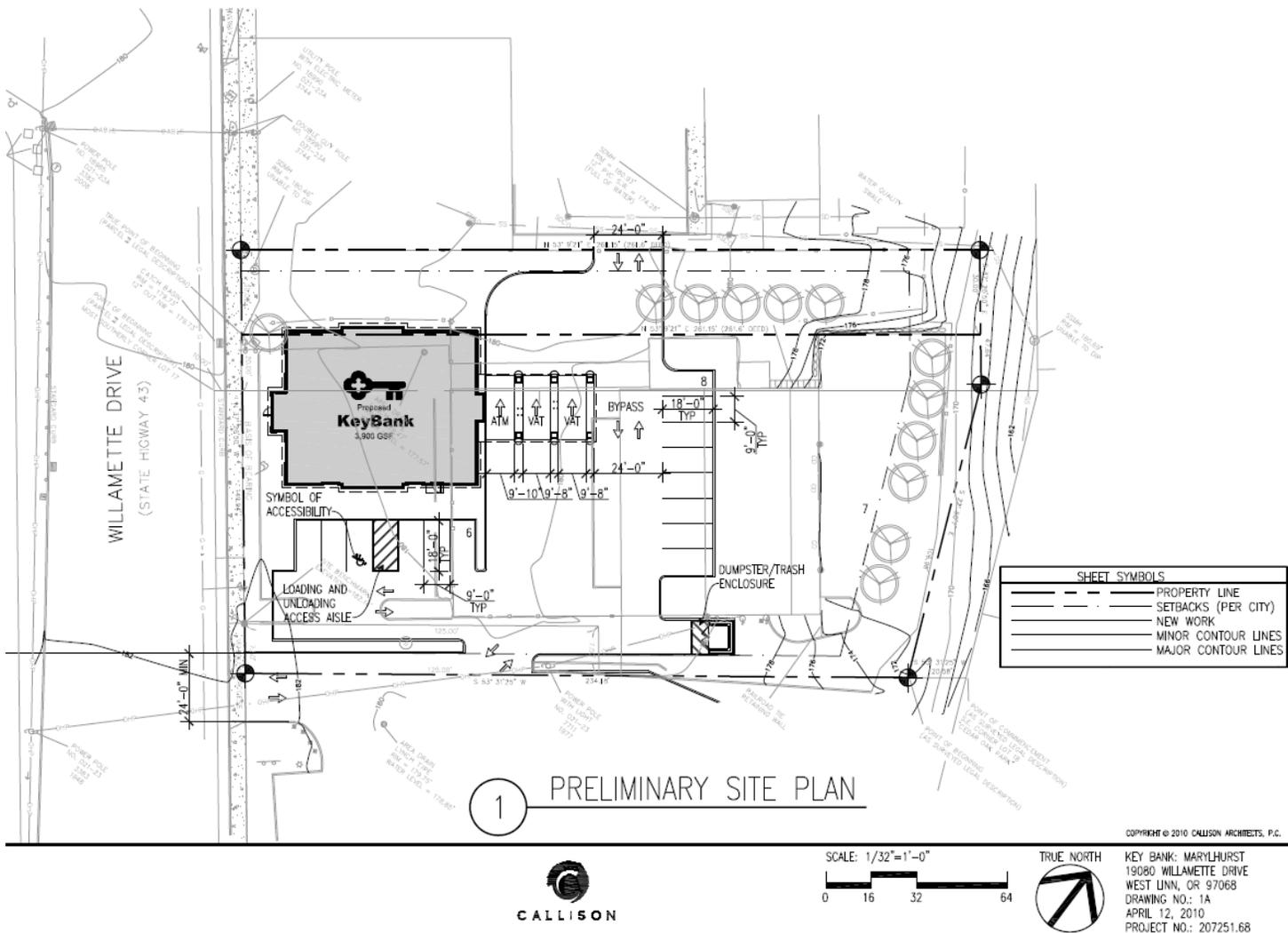
Then complete the Class II Design Review application form and submit it to the Planning Department with deposit/fees based upon the fee schedule. (The deposit/fee is 4,000 dollars plus four percent of the construction value as determined by the Building Department. Cost overruns could result in payment of additional deposit/fees.) Accompanying this will be a full and complete submittal and full response to the approval criteria per the requirements of CDC Chapters 55. Other applicable chapters include 46, 48, 52 and 54. If you submit signs for review at this time you can avoid additional delays of up to two months to obtain the sign permit later. Sign permit fee is 250 dollars. If a Class II Variance is required the deposit fee is 1,800 dollars per each variance request. The submittal requirements and approval criterion are in CDC Chapter 75.

The City has 30 days to determine if the application is complete or not. Most applications are incomplete, usually due to inadequate responses to approval criteria or stating "NA" in response to submittal requirements or approval criteria. The applicant has 180 days to make it complete, although usually it is complete within three months of the original submittal. Once complete, the City has 120 days to exhaust all local review and appeals.

Once complete, the application will be noticed to property owners within 500 feet of the site perimeter. There will be Planning Commission hearing about four weeks after the determination of completeness. Appeals are heard by City Council.

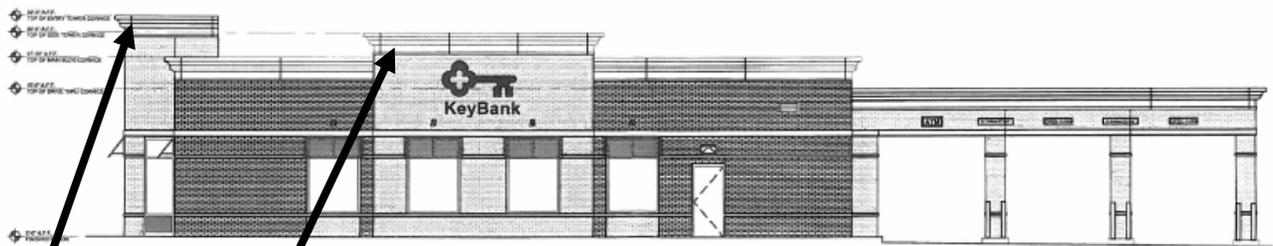
Typical land use applications can take 6-10 months from beginning to end.

DISCLAIMER: This summary discussion covers issues identified to date. It does not imply that these are the only issues. The burden of proof is on the applicant to demonstrate that all approval criteria have been met. These notes do not constitute an endorsement of the proposed application. Staff responses are based on limited material presented at this pre-application meeting. New issues, requirements, etc. could emerge as the application is developed. These pre-application notes shall be invalid 18 months from the date of the pre-app. After that date, another pre-app would be required. Also, new state laws and development code amendments can impact the feasibility of a project.





1 PROTOTYPICAL FRONT ELEVATION
SCALE: 1/8"=1'-0"



2 PROTOTYPICAL SIDE ELEVATION
SCALE: 1/8"=1'-0"

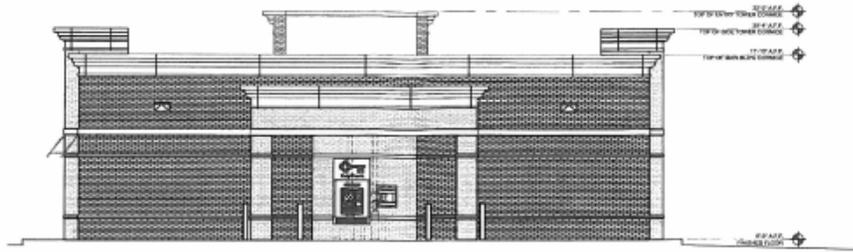


SCALE: 1/8"=1'-0"

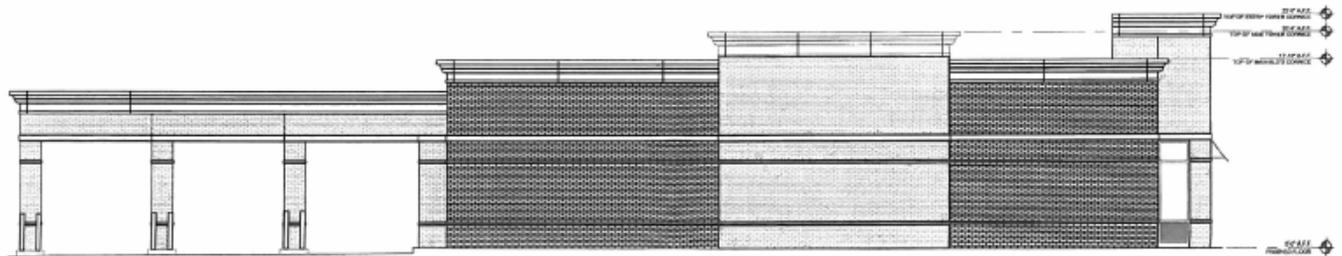


©2010 K&B DESIGN ARCHITECTS, P.C.
KEY BANK: MARTLBURST
1500 WILLAMETTE DRIVE
WEST Linn, OR 97068
DRAWING NO: 3
APRIL 1, 2010
PROJECT NO: 207251.05

Staff would like to see the elimination of these parapets and drop it down to the level of the dominant roofline.



1 PROTOTYPICAL REAR ELEVATION
SCALE: 1/8"=1'-0"



2 PROTOTYPICAL SIDE ELEVATION
SCALE: 1/8"=1'-0"



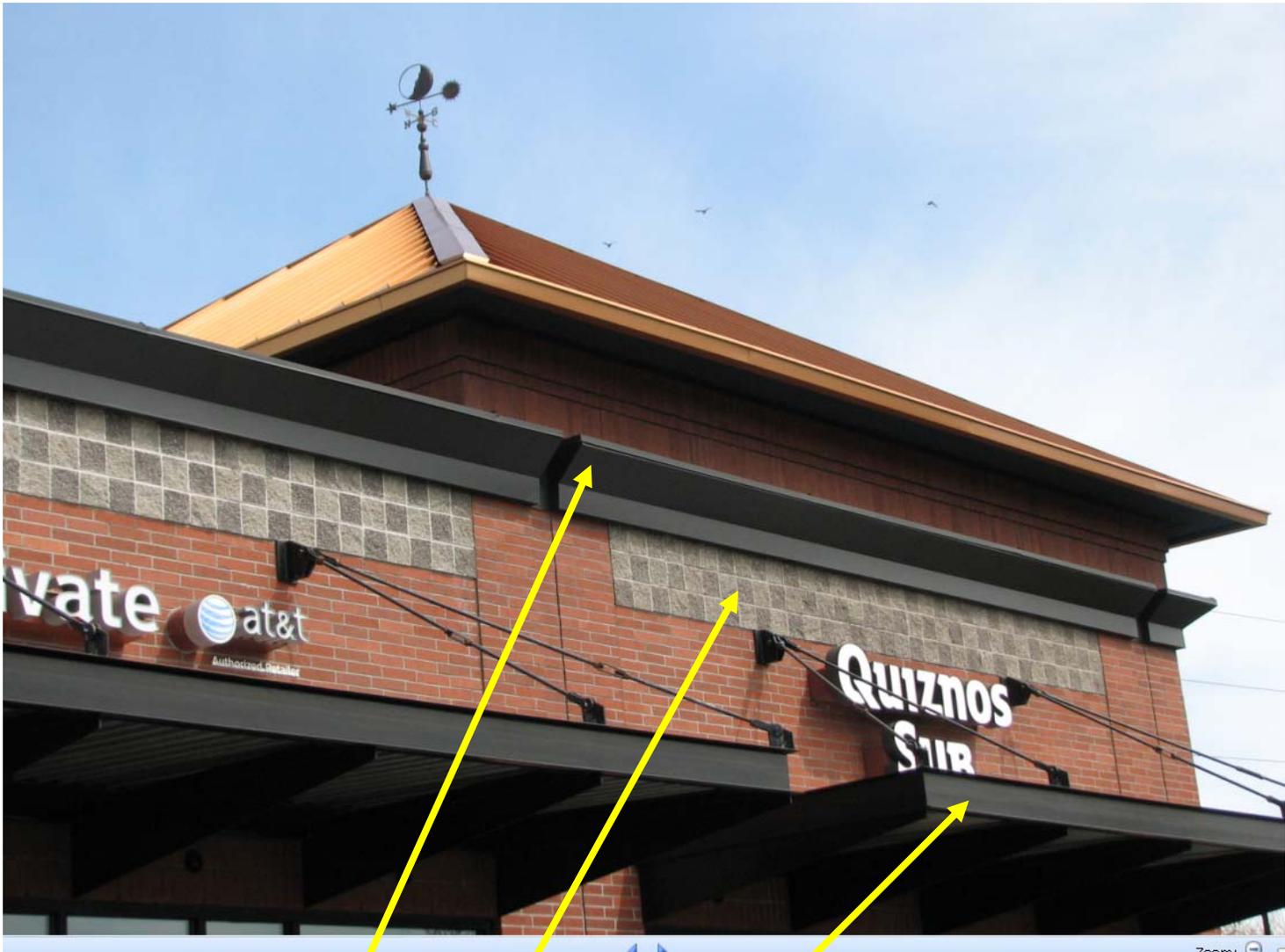
SCALE: 1/8"=1'-0"
0 4 8 16

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KEY BANK, MARYLENE
15000 WILLAMETTE DRIVE
WEST Linn, OR 97068
DRAWING NO.: 4
DATE: 1, 2010
PROJECT NO.: 307251.68



Staff found the use of contrasting bricks to be a very effective and dramatic means of creating visual interest in brick work. Herringbone patterns (below) were another option, although the eye seems to notice the contrast in color more than the contrast in pattern.





Understated black cornice looks very good. Note also the faux stone faced drivet which provides visual interest. Deep awnings are critical.





Horizontal wood insets make a unique design contribution. It also conveys a northwest look. Huge transparency broken up into multiple lights is great but the mirrored glass diminishes its value. Awnings cover the sidewalk. Simple flat roofline looks good.



LETTER FROM ODOT:

April 15, 2010

City of West Linn

Attention: Peter Spir

22500 Salamo Road, #1000

West Linn, OR 97068

SUBJECT: Key Bank Highway 43 Pre-Application

Dear Mr. Spir,

As you know, ODOT owns and operates the Highway 43 and has an interest in ensuring that area land use is compatible with its safe and efficient operation. We have not received a preliminary site plan so are at a disadvantage to respond but wish to address a couple of potential issues.

The development of the scope for the traffic impact analysis should include our participation to ensure the correct standards are applied. Please have Key Bank's traffic engineer contact Avi Tayar, PE with ODOT at 503-731-8221 or Abraham.tayar@odot.state.or.us to coordinate.

We also wish to raise concern about locating the building along the Highway 43 right of way, as we understand is being considered. While we developing an inviting pedestrian environment with no parking between the building and the street, we want to make sure the driveway sight distance is not limited. Are there applicable city standards to address this?

Regarding placing awnings in the Highway 43 right of way, we need more information to determine if awnings would be allowed and a permit would be required. We consider such requests on a case by case basis. Our primary concern is safety and liability. We want to make sure the awnings are placed to allow full sight distance at the driveway(s). If it would be helpful, I can provide an example of the permit conditions that applied to a similar situation which include provisions to eliminate ODOT liability. Steve Schalk with District 2A is the ODOT Permit Specialist. He can be reached at 503-229-5002 or Steven.B.Schalk@odot.state.or.us

Please provide me with a copy of the applicant's formal submittal. If you or the applicants have any questions regarding the above comments, I can be reached at 503.731.8206.

Sincerely,

Gail Curtis, AICP

Senior Transportation Planner

C: Avi Tayar, PE, ODOT Region 1 Traffic

Steve Schalk, ODOT District 2A

Oregon Department of Transportation

ODOT Region 1

123 NW Flanders St

Portland, OR 97209

Telephone (503)731-8200

FAX (503)731-8259









View from property to north looking towards the point where the connective driveway would be installed between the two properties.