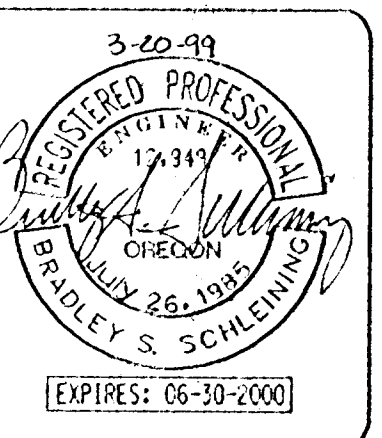


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| 3-20-99 | DESIGNED: BSS | CHECKED: BSS |
| | DRAWN: BSS/RT | DATE: 11-20-98 |
| | JOB NO.: 96-340 | SCALE: |

COVER SHEET
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



W.B. WELLS
and associates inc.
ENGINEERS • SURVEYORS • PLANNERS
4230 NE FREMONT STREET
PORTLAND, OREGON 97213
PHONE (503) 284-5896
FAX. (503) 284-8530



GENERAL NOTES:

- ALL CONSTRUCTION AND MATERIALS SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE CITY OF WEST LINN, THE OREGON ADMINISTRATIVE RULES AND OTHER APPLICABLE CODES.
- THE CONTRACTOR AND/OR SUB-CONTRACTOR SHALL HAVE A MINIMUM OF ONE (1) SET OF APPROVED CONSTRUCTION PLANS ON THE JOB SITE AT ALL TIMES DURING CONSTRUCTION.
- THE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS AND LICENSES BEFORE STARTING CONSTRUCTION.
- NO CONSTRUCTION SHALL COMMENCE UNTIL PLANS ARE APPROVED AND ISSUED BY THE CITY OF WEST LINN.
- EXISTING UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE. THE EXISTENCE OR NON-EXISTENCE OF THE UTILITY. THE CONTRACTOR SHALL VERIFY THE EXISTENCE, TYPE AND LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION. IN CASES WHERE ACTUAL LOCATION OR INFORMATION VARIES FROM THE PLANS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF THE VARIANCE AND STOP WORK IN THE AFFECTED AREA UNTIL DIRECTED.
- THE CONTRACTOR SHALL COORDINATE WITH ALL OF THE UTILITY COMPANIES THE MARKING OF ALL EXISTING UTILITIES. COMPLIANCE WITH OREGON REVISED STATUTES 757.541 TO 757.571 IS REQUIRED. THE CITY OF WEST LINN IS A MEMBER OF ONE CALL. FOR EXISTING SANITARY SEWER AND STORM LINE, CALL (503) 216-6693.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL UTILITY SERVICES TO THE SITE GAS, ELECTRICITY, PHONE, CABLE, ETC. ALL UTILITIES SHALL BE PLACED UNDERGROUND.
- THE CONTRACTOR SHALL DO ALL OF THE WORK SHOWN ON THE DRAWINGS AND ALL INCIDENTAL WORK CONSIDERED NECESSARY TO COMPLETE THE PROJECT IN AN ACCEPTABLE MANNER.
- STREET LIGHTING SHALL BE MADE COMPLETE AND OPERATIONAL ALONG THE PRIVATE ACCESS DRIVE, I.E. POLES INSTALLED AND WIRED. SEE STREET LIGHTING PLANS (BY OTHERS) FOR DETAILS.
- ALL MATERIAL USED SHALL BE PER THE APPROVED PLANS. ANY ALTERNATES SHALL BE APPROVED BY THE ENGINEER AND THE CITY OF WEST LINN PRIOR TO ITS USE. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO SUPPLY ALL NECESSARY INFORMATION FOR THE PROPER EVALUATION OF SUBMITTED ALTERNATIVES. ALLOW A MINIMUM OF TWO WEEKS FOR APPROVAL.
- ANY PLAN CHANGES SHALL BE SUBMITTED TO THE ENGINEER AND APPROVED BY THE CITY PRIOR TO ANY WORK. IT WILL BE THE CONTRACTORS' RESPONSIBILITY TO SUPPLY ALL NECESSARY INFORMATION FOR THE PROPER EVALUATION OF PLAN CHANGES. ALLOW A MINIMUM OF TWO WEEKS FOR APPROVAL.
- THE CONTRACTOR SHALL KEEP THE CITY, AND W. B. WELLS & ASSOC., INC. ADVISED OF HIS/HER PROGRESS SO THAT INSPECTION CAN BE PERFORMED IN AN EFFICIENT MANNER. A MINIMUM OF TWENTY-FOUR (24) HOUR NOTICE MUST BE GIVEN FOR TESTS OR INSPECTIONS.
- THE CONTRACTOR SHALL KEEP AN ACCURATE RECORD OF AS-BUILTS. SAID AS-BUILTS SHALL BE SUBMITTED TO W. B. WELLS & ASSOC., INC. UPON REQUEST.
- THE CONTRACTOR SHALL, UPON REQUEST, SUBMIT TO THE CITY AND/OR W. B. WELLS & ASSOC., INC. PROOF FROM THE MATERIAL SUPPLIERS THAT THE MATERIAL HAS BEEN TESTED AND FOUND IN COMPLIANCE WITH THE ABOVE SPECIFICATIONS. THEY SHALL CERTIFY THAT ALL MATERIALS TO BE DELIVERED TO THE JOB SITE MEET OR EXCEED SAID SPECIFICATIONS PRIOR TO DELIVERY. ANY MATERIAL NOT CERTIFIED BY THE SUPPLIER SHALL BE REMOVED FROM THE JOB SITE AT NO ADDITIONAL COST TO THE OWNER.
- THE OWNER, CITY AND W. B. WELLS & ASSOC., INC. SHALL RESERVE THE RIGHT TO CALL FOR COMPACTION TESTS. IF THE TESTS DO NOT SHOW SUITABLE RESULTS, THE CONTRACTOR SHALL CORRECT IT AT NO COST TO THE OWNER. THE COMPACTION TESTS IN TRENCHES MUST OBTAIN 95% OF AASHTO T-99 TEST METHOD.
- THE CONTRACTOR SHALL COMPLY WITH THE EROSION CONTROL (NPDES) PERMIT PER D.E.Q. REQUIREMENTS. EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY WORK. NON-COMPLIANCE IS THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR IS RESPONSIBLE FOR MANAGING CONSTRUCTION ACTIVITIES TO INSURE ADJACENT PUBLIC STREETS ARE KEPT CLEAR OF MUD, DEBRIS, AND DUST TO THE SATISFACTION OF THE CITY OF LAKE OSWEGO. THE CONTRACTOR SHALL LEAVE ALL AREAS OF THE PROJECT FREE OF DEBRIS AND ANY UNUSED CONSTRUCTION MATERIALS.
- DURING SUMMER TIME CONSTRUCTION, THE CONTRACTOR SHALL CONTROL DUST FROM THE CONSTRUCTION ACTIVITIES.
- TRAFFIC DURING CONSTRUCTION SHALL BE CONTROLLED IN ACCORDANCE WITH THE M.U.T.C.D. AND THE CITY OF WEST LINN REQUIREMENTS.
- CONTRACTOR SHALL REMOVE ALL EXCESS MATERIAL FROM THE SITE AFTER APPROVAL FROM THE OWNER OR THE OWNER'S REPRESENTATIVE. SUCH MATERIALS SHALL BE DISPOSED OF AT AN APPROVED SITE.
- CONTRACTOR IS RESPONSIBLE FOR REMOVAL OF ALL GRUBBINGS, BRUSH, TREES (AS SPECIFIED), STUMPS, DEBRIS AND TRASH FROM THE SITE. ON-SITE BURIAL IS NOT ALLOWED. CONTRACTOR SHALL DISPOSE OF ALL SUCH MATERIALS AT AN APPROVED DISPOSAL LOCATION.
- CONTRACTOR SHALL NOT REMOVE ANY TREES OVER 5 INCHES IN DIAMETER, AT 4.5 FEET ABOVE GROUND. OTHERWISE A TREE CUTTING PERMIT SHALL BE SECURED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL OBTAIN A GRADING PERMIT FROM THE CITY AND ADHERE TO THE PROVISIONS THEREIN. ALL STRIPPINGS AND WASTE MATERIALS SHALL BE REMOVED FROM THE PROJECT SITE UNLESS OTHERWISE AUTHORIZED BY THE OWNER.
- ALL EARTHWORK AND GRADING SHALL COMPLY WITH SECTION 204 OF THE A.P.W.A. SPECIFICATIONS AND CHAPTER 70 OF THE UNIFORM BUILDING CODE.
- ANY STRUCTURAL FILLS UNDER BUILDINGS, PARKING, STREETS, ETC. SHALL BE CONSTRUCTED OF MATERIAL FREE FROM ORGANICS, FROZEN MATERIAL, CONCRETE, ASPHALTIC CONCRETE AND OTHER DEBRIS. FILLS SHALL CONSIST OF AN APPROVED ALL-WEATHER MATERIAL (I.E. ROCK, CLEAN NATIVE SOIL).

- FILLS SHALL BE CONSTRUCTED IN SUCCESSIVE HORIZONTAL LAYERS NOT EXCEEDING EIGHT (8) INCHES IN LOOSE THICKNESS. THE USE OF LOADED HAULING EQUIPMENT IN ALL BUT THE LAST TWO (2) FEET OF FILL WILL BE ACCEPTABLE FOR COMPACTION. PROVIDED THE REQUIRED DENSITY IS OBTAINED THROUGHOUT THE WIDTH AND DEPTH OF EACH LAYER. THE TOP TWO (2) FEET SHALL BE COMPACTED USING SUITABLE MECHANICAL COMPACTORS.
- THE SUBGRADE FOR ALL PRIVATE PARKING AREAS, TRAVEL WAYS, AND BUILDING PADS SHALL BE PREPARED AND BROUGHT TO GRADE BY THE CONTRACTOR TO MEET 95% OF AASHTO T-180. SUBGRADE UNDER PUBLIC IMPROVEMENTS SHALL MEET THE ACCEPTABLE RATE OF 95% OF AASHTO T-180.
- ALL BUT THE TOP TWO (2) FEET OF FILL UNDER PRIVATE IMPROVEMENTS SHALL MEET 90% OF AASHTO T-180 FOR COMPACTION. THE TOP TWO (2) FEET SHALL MEET 95% OF AASHTO T-180. ALL EXCAVATED CUT AREAS AND FILLS UNDER PUBLIC IMPROVEMENTS SHALL MEET THE ACCEPTABLE RATE OF 95% T-180.
- THE MATERIAL IN EACH LAYER SHALL BE OF A MOISTURE CONTENT TO MEET THE ABOVE COMPACTION REQUIREMENT. WETTING OR DRYING OF THE MATERIAL AND MANIPULATION TO SECURE A UNIFORM MOISTURE CONTENT THROUGHOUT THE LAYER MAY BE REQUIRED. SHOULD THE MATERIALS BE TOO WET TO PERMIT PROPER COMPACTION BY ROLLING, ALL WORK ON ALL PORTIONS OF THE FILL THUS AFFECTED SHALL BE DELAYED UNTIL THE MATERIAL HAS DRIED TO AN ACCEPTABLE MOISTURE CONTENT. WHERE APPLICABLE THE SITE GEOTECHNICAL ENGINEER SHALL VERIFY.
- ALL PIPE THAT IS INSTALLED AT A SLOPE GREATER THAN 20%, WILL HAVE CONCRETE PIPE ANCHORS INSTALLED PER APWA DRAWING NO. 303 ON SHEET 11 OF 42.

STORM SEWER NOTES:

- STORM SEWER PIPES SHALL BE ADS N-12 POLYETHYLENE PIPES OR OTHER APPROVED EQUIVALENT BY THE CITY. ALL PIPES SHALL HAVE WATERTIGHT CONNECTORS, EXCEPT WHERE DESIGNATED ON THE PLANS.
- PRIVATE STORM SEWER CONSTRUCTION AND MATERIALS SHALL CONFORM TO THE UNIFORM PLUMBING CODE AND ALL OTHER APPLICABLE SPECIFICATIONS.
- CATCH BASINS, INLETS, PIPE BEDDING AND TRENCH BACKFILL SHALL BE AS SHOWN ON THE DETAIL SHEETS AND MEET U.P.C.
- MAINTAIN A MINIMUM OF 2.5 FEET OF COVER OR AS PER PLAN.
- PRIVATE SEWER REQUIRES A CLEANOUT EVERY 100 FEET, AND AS SHOWN ON THE PLANS.
- ALL BUILDINGS SHALL DRAIN TO THE PRIVATE STORM SYSTEM AS SHOWN. STUB OUT SUPPLIED TO WITHIN 5' OF FOUNDATION.
- ALL LINES SHALL BE TESTED AND ACCEPTED PER CITY REQUIREMENTS.
- ROCK PROTECTION AT OUTFALLS SHALL BE TYPE 1 REASONABLY WELL GRADED WITH ROCK GRADATION AS FOLLOWS: MAX. STONE SIZE=6", MIN. STONE SIZE=2".
- STORM SEWER MANHOLES AND DETENTION PIPES MAY BE CONCRETE OR C.M.P. MATERIAL TO BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- CONTRACTOR TO COORDINATE STORM SEWER INSTALLATION WITH BUILDING ARCHITECTURAL AND MECHANICAL DESING PLANS.

SANITARY SEWER NOTES:

- ALL SANITARY SEWER LATERAL PIPES WITHIN THE STREET RIGHT-OF-WAY SHALL BE 3034 SDR 35 P.V.C. PIPE OR AN APPROVED EQUIVALENT BY THE CITY TO MEET ASTM P-3034-80 FOR MATERIAL INSTALLATION.
- ALL SERVICE LATERALS ARE TO BE 6" OR AS SHOWN, 3034 P.V.C. AT A MIN. SLOPE OF 2% UNLESS OTHERWISE SHOWN ON THE PLANS. LATERALS SHALL BE EXTENDED TO WITHIN 5' OF THE BUILDING FOUNDATION.
- SANITARY SEWER LATERALS SHALL BE IDENTIFIED BY A 2"x4" MARKER EXTENDING TO A POINT AT LEAST ONE (1) FOOT ABOVE THE FINISHED GROUND SURFACE. THE 2"x4" MARKER SHALL BE PAINTED GREEN, AND THE DEPTH NOTED ON THE MARKER.
- 14 GAUGE TRACER WIRE AND LOCATION TAPE WITH PROPER COLOR MUST BE LAID ON ALL P.V.C. AND/OR A.D.S. PIPES WHEN INSTALLING LINE, AND TO BELOW FRAMES OF MANHOLES OR CLEANOUTS WHEN LAYING NEW LINES.
- PIPE BEDDING SHALL CONFORM TO THE MATERIAL REQUIREMENTS OF THE CITY OF WEST LINN STANDARD.
- BACKFILL FOR TRENCHES WITHIN THE RIGHT-OF-WAY OR PAVED AREAS SHALL CONFORM TO THE MATERIAL REQUIREMENTS OF THE CITY OF WEST LINN STANDARD. BACKFILL OF THE TRENCHES SHALL BE GRANULAR (CLASS 4) AND COMPACTED AS SPECIFIED FOR THE SUBGRADE AND BASE. BACKFILL SHALL BE CONSIDERED A PART OF THE TRENCH CONSTRUCTION.
- TRENCHES OUTSIDE OF THE RIGHT-OF-WAY OR PAVED AREAS MAY BE BACKFILLED WITH SELECT NATIVE MATERIAL EXCAVATED FROM THE TRENCH IN ACCORDANCE WITH THE CITY OF WEST LINN STANDARD. NO WATER SETTLING OF TRENCH BACKFILL MATERIAL SHALL BE ALLOWED.
- WEST LINN REQUIRES THAT LINE AND GRADE OF ALL SANITARY SEWER LINES MEET A.P.W.A. SPECIFICATION 303.3.01 "LINE AND GRADE".
- THE CONTRACTOR SHALL PRESSURE TEST ALL LINES PER CITY PROCEDURES OR AN APPROVED EQUIVALENT. ALL SEWER LINES SHALL AIR TESTED PER A.P.W.A. DIVISION III, SECTION 503.5.16. A MANDRAL WILL BE REQUIRED TO CHECK ALL NEW LINES. TESTS SHALL BE OBSERVED BY THE ENGINEER, INSPECTOR AND CITY INSPECTORS.
- PRIVATE SEWER REQUIRES A CLEANOUT EVERY 100 FEET, AND AS SHOWN ON THE PLANS.
- CONTRACTOR TO COORDINATE SAN. SEWER INSTALLATION WITH BUILDING ARCHITECTURAL AND MECHANICAL DESING PLANS.

WATERLINE NOTES:

- WATER LINE / SERVICE SPECIFICATIONS & INFORMATION FOR WATER SYSTEM CONSTRUCTION (CITY WATER)
- WATER MAINS AND SERVICE LATERALS SHALL BE C900-81 PIPE CONFORMING TO AWWA CLASS 150. JOINTS SHALL BE GASKETED. PIPE FITTINGS SHALL BE CAST DUCTILE IRON, CONFORMING TO AWWA C110 OR C153. PIPE SHALL BE LAID IN SUCH A WAY THAT IT IS SUPPORTED ALONG ITS FULL LENGTH, INCLUDING DIGGING OF BELL HOLES.
 - WATER MAINS AND SERVICE LATERALS SHALL HAVE A MINIMUM COVER OF 36".
 - THRUST BLOCKS SHALL BE PROVIDED AT ALL CHANGES IN DIRECTION AND BRANCHES. THRUST BLOCK CONCRETE STRENGTH SHALL BE 2000 PSI. POUR THRUST BLOCKS AGAINST UNDISTURBED EARTH.
 - TAPPING SLEEVES SHALL ALL BE STAINLESS STEEL WITH STAINLESS STEEL BOLTS OF CAST IRON (DUCTILE IRON) MECHANICAL JOINT SLEEVES.
 - GRANULAR MATERIAL USED FOR PIPE BASE, PIPE ZONE AND TRENCH BACKFILL SHALL BE 3/4"-0 CRUSHED ROCK. PIPE BASE SHALL BE A MINIMUM OF 4" DEEP AT THE PIPE BARREL, AND NOT LESS THAN 3" DEEP AT THE BELL. GRANULAR BACKFILL SHALL BE COMPACTED TO 95% MAXIMUM DENSITY PER AASHTO T99 TEST METHOD AND NATIVE MATERIAL SHALL BE COMPACTED TO 85% OF IN-PLACE DENSITY OF SURROUNDING SOIL. BACKFILL UNDER STREETS SHALL BE IN ACCORDANCE WITH CLASS "A" BACKFILL AS INDICATED ON THE PLANS. NO WATER SETTLING OF TRENCH BACKFILL SHALL BE ALLOWED.
 - SERVICE LATERALS 1" OR SMALLER SHALL HAVE HIGH MOLECULAR PVC COMPRESSION FITTINGS.
 - INSPECTION OF THE WATER IMPROVEMENTS WILL BE MADE BY THE CITY PERSONNEL OR OTHERS DESIGNATED BY THE CITY. CONTRACTOR IS RESPONSIBLE TO CONTACT THE PROPER INSPECTOR A MINIMUM OF 48 HOURS PRIOR TO INSPECTION.
 - ALL MATERIALS, INSTALLATION, TESTS AND CHLORINATION SHALL BE IN STRICT ACCORDANCE WITH THE STANDARDS AND CODES OF THE CITY OF WEST LINN AND THE OREGON STATE HEALTH DIVISION ADMINISTRATIVE RULES, CHAPTER 333.
 - THE CITY HAS NOT PROVIDED THE ENGINEER WITH ADEQUATE INFORMATION FOR PROPERLY DESIGNING THE WATER SYSTEM. IF THE SYSTEM DOES NOT PROVIDE ADEQUATE PRESSURE OR VOLUME BOOSTER PUMPS WILL BE REQUIRED. THESE PLANS WILL BE MODIFIED AS MORE INFORMATION IS PROVIDED BY THE CITY.
 - THE FIRE PROTECTION WATER SYSTEM AND HYDRANTS WILL BE COMPLETED, TESTED AND APPROVED PRIOR TO BRINGING ANY COMBUSTIBLE MATERIALS ON SITE. PER THE FIRE MARSHALL. LOCATION OF THE HYDRANTS SHALL BE REVIEWED AND APPROVED BY THE MARSHALL.
 - CONTRACTOR TO COORDINATE WATER INSTALLATION WITH BUILDING ARCHITECTURAL AND MECHANICAL DESING PLANS.

BENCH MARK:

- THE EXISTING CONTOURS SHOWN ON THESE PLANS ARE DRAWN AT A TOLERANCE OF PLUS OR MINUS ONE FOOT. TOPOGRAPHIC SURVEY PROVIDED BY "W. B. WELLS & ASSOCIATES, INC."
- THE BASIS OF DATUM IS PER O.S.H.D. BENCH MARK RM2 "L631", BRASS DISK LOCATED OVER THE SOUTH BOUND LANES OF I-205, IN THE N'LY SIDE OF THE OVERPASS STRUCTURE FOR BLANKENSHIP ROAD, AT THE BASE OF THE HANDRAIL. THE ELEVATION IS 175.76'

ABBREVIATION:

| | | | |
|---------|---------------------|--------|-----------------------------|
| A.C. | ASPHALTIC CONCRETE | MIN. | MINIMUM |
| A.P. | ANGLE POINT | M.S.L. | MEAN SEA LEVEL |
| APPROX. | APPROXIMATE | N.T.S. | NOT TO SCALE |
| APT. | APARTMENT | PAV. | PAVEMENT |
| AV. | AVENUE | P.C. | POINT OF CURVATURE |
| C.B. | CATCH BASIN | PCC | POINT OF COMPOUND CURVATURE |
| CH | CHORD | PRC | POINT OF REVERSE CURVATURE |
| CHB | CHORD BEARING | P.T. | POINT OF TANGENCY |
| C/L | CENTERLINE | PT. | POINT |
| CONC. | CONCRETE | PVC | POLYVINYL CHLORIDE PIPE |
| DIA. | DIAMETER | R | RADIUS |
| DR. | DRIVE | RD. | ROAD |
| DWG. | DRAWING | RT. | RIGHT |
| EL | ELEVATION | S | SLOPE |
| ELEV. | ELEVATION | SAN. | SANITARY |
| E/P | EDGE OF PAVEMENT | ST. | STREET |
| ESMT | EASEMENT | STA. | STATION |
| EXTG. | EXISTING | STM. | STORM |
| F.G. | FINISH GRADE | SVC. | SERVICE |
| G.L. | GROUND LINE | S/W | SIDEWALK |
| GUT | GUTTER | T/C | TOP OF CURB |
| H.P. | HIGH POINT | TL | TAX LOT |
| HORIZ. | HORIZONTAL | TYP. | TYPICAL |
| I.E. | INVERT ELEVATION | U.G. | UNDERGROUND |
| L | LENGTH/ARC LENGTH | VERT. | VERTICAL |
| LF | LINEAR FEET | W/ | WITH |
| LOC. | LOCATION OR LOCATED | X/G | EXISTING GROUND |
| LT. | LEFT | Δ | INCLUDED ANGLE (DELTA) |

CONSULTANT NOTES:

THE CONTRACTOR IS RESPONSIBLE OF OBTAINING AND BEING FAMILIAR WITH ALL PLANS, REPORTS, SPECIFICATIONS AND RECOMMENDATIONS FROM THE FOLLOWING CONSULTANTS FOR THIS PROJECT. THEY SHALL INCORPORATE ALL CONSULTANT RECOMMENDATIONS FROM THE PREVIOUSLY MENTIONED SAID INFORMATION INTO THE CONSTRUCTION PRACTICES FOR THIS PROJECT. ALL INFORMATION IS AVAILABLE TO THE CONTRACTOR UPON REQUEST VIA:

FINAL SITE PLAN AND ARCHITECTURAL BUILDING PLANS BY:
MARK E. HUMPHREYS ARCHITECT - BUILDINGS 24 - 51
4370 N.E. HALSEY STREET, SUITE 235
PORTLAND, OR. 97213
TELE: (503) 249-6860

L R S ARCHITECTS - BUILDINGS 1 - 21
1121 S.W. SALMON STREET, SUITE 100
PORTLAND, OR. 97205
TELE: 503-221-1121 ATTN: MIKE GORMAN


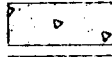
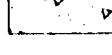



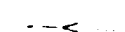





















GEOTECHNICAL SITE EVALUATION AND REPORT PREPARED BY:
GEOTECHNICAL RESOURCES INCORPORATED
9125 S.W. BEAVERTON-HILLSDALE HWY, SUITE 140
BEAVERTON, OR. 97005
TELE: 503-641-3478 ATTN: DAVE DRISCOLL, P.E.

THE LANDSCAPE PLANS PREPARED BY:
CHRISTOPHER FRESHLEY LANDSCAPE ARCHITECT - BUILDINGS 1 - 21
1020 S.W. TAYLOR STREET, SUITE 355
PORTLAND, OR. 97205
TELE: 503-222-9881 ATTN: CHRIS FRESHLEY

SITE STRUCTURAL ENGINEERING PLANS PREPARED BY:
CONLEE ENGINEERS, INC.
1309 SW BERTHA BLVD.
PORTLAND, OR. 97219
TELE: 503-244-0579 ATTN: DOUG GANNETT

THE WETLANDS DELINEATION AND MITIGATION PLANS PREPARED BY:
PACIFIC HABITAT SERVICES, INC.
9450 SW COMMERCE CIRCLE
WILSONVILLE, OR. 97070
TELE: 503-570-0800 ATTN: JOHN VAN STAVERN

LEGEND:

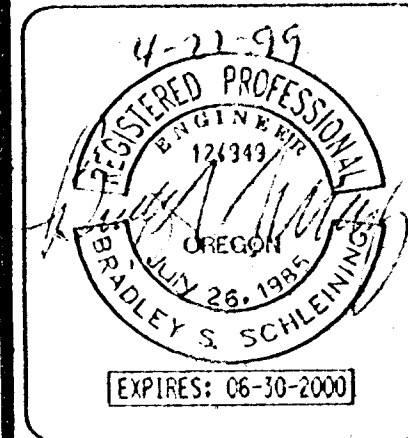
| EXISTING | | PROPOSED |
|---|----------------------|---|
|  | ASPHALT PAVEMENT | |
| | CONCRETE DRIVEWAY |  |
| | CONCRETE SIDEWALK |  |
|  | MAIL BOX | |
|  | ROAD SIGN | |
|  | ROAD CENTERLINE | |
|  | LOT LINE |  |
|  | GUY ANCHOR | |
|  | OVERHEAD POWER LINE | |
|  | POWER POLE |  |
| | SANITARY MANHOLE | |
| | SANITARY SEWER LINE | |
|  | DITCH INLET |  |
|  | STORM SEWER LINE |  |
|  | GAS VALVE | |
|  | UNDERGROUND GAS LINE | |
|  | FIRE HYDRANT | |
|  | WATER LINE |  |
|  | WATER METER |  |
|  | CONIFEROUS TREE | |
|  | DECIDUOUS TREE |  |
| | LANDSCAPING | |
|  | CLEANOUT |  |

CONTACT TELEPHONE NUMBERS:

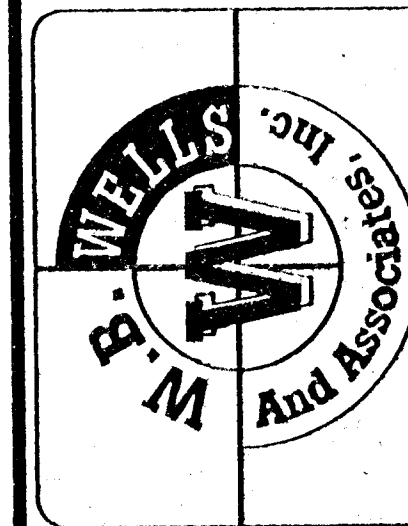
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|---------------------------|----------------|
| NORTHWEST NATURAL GAS | (503) 220-2415 |
| PORTLAND GENERAL ELECTRIC | (503) 671-1234 |
| T.C.I. CABLEVISION | (503) 605-4395 |
| U.S. WEST TELEPHONE | (503) 242-4325 |
| CITY OF WEST LINN | (503) 656-4211 |

| | |
|---------------------------------|----------------|
| REVISIONS | |
| 3-20-99 ADDED GENERAL NOTE # 30 | |
| | |
| | |
| DESIGNED: BSS | CHECKED: BSS |
| DRAWN: BSS/RT | DATE: 11-20-98 |
| JOB NO.: 96-340 | SCALE: |

CONSTRUCTION NOTES
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



W.B. WELLS
and associates inc.
ENGINEERS • SURVEYORS • PLANNERS
4230 NE FREMONT STREET
PORTLAND, OREGON 97213
PHONE (503) 284-5896
FAX (503) 284-8530



BEFORE THE LAND USE HEARINGS OFFICER
OF THE CITY OF WEST LINN, OREGON

FINAL ORDER DR-97-36/ZC-97-04/MISC-97-60 1
(SUMMERLINN APARTMENTS)

REGARDING APPLICATIONS BY SHOW TIMBER COMPANY FOR
DESIGN REVIEW AND PLANNED DEVELOPMENT APPROVAL,
TREE REMOVAL AND NATURAL AREA DEVELOPMENT FOR A
438-UNIT APARTMENT IN THE R-2.1 ZONE NORTHEAST OF
DEBOK ROAD AT 13TH AND GREENE STREETS IN WEST LINN, OREGON

IV. CONCLUSION AND ORDER

BASED ON THE ABOVE FINDINGS, THE HEARINGS OFFICER CONCLUDES THESE
APPLICATIONS SHOULD BE APPROVED SUBJECT TO THE CONDITIONS RECOMMENDED
BY CITY STAFF AS AMENDED TO BE CONSISTENT WITH THE ABOVE DISCUSSION.

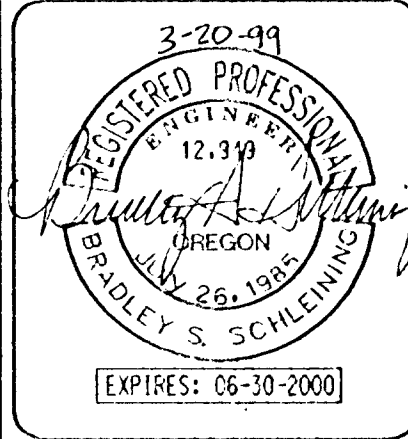
CASEFILE DR-97-36/ZC-97-04/MISC-97-60 IS HEREBY APPROVED,
SUBJECT TO THE CONDITIONS OF APPROVAL IN THE IN THE CITY OF WEST LINN
STAFF REPORT AND RECOMMENDATIONS TO THE HEARINGS OFFICER DATED
JANUARY 28, 1998 WITH THE FOLLOWING MODIFICATIONS:

- CONDITION OF APPROVAL 1 IS HEREBY DELETED.
- A. CONSTRUCT A MINIMUM FOUR-FOOT WIDE, HARD SURFACED PEDESTRIAN
PATH BETWEEN THE DEBAUCH ROAD ENTRANCE AND THE NORTH
END OF THE NORTH-SOUTH SECTION OF THE MAIN ACCESS
DRIVEWAY. THE PATH SHALL BE SUBSTANTIALLY PARALLEL TO THE
EAST-WEST SECTION OF THE MAIN ACCESS DRIVEWAY. IF THE
PATH ABUTS THE DRIVEWAY AND IS NOT GRADE-SEPARATED
(E.G., BY A FULL CURB), THE IT SHALL BE CONSTRUCTED WITH A
CONTRASTING PAVING MATERIAL AND DESIGNED AND BUILT TO
ACCOMMODATE WEIGHT LOADS OF EMERGENCY VEHICLES AND
DELIVERY TRUCKS.

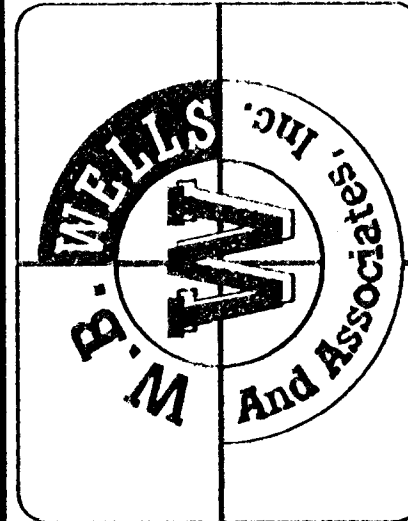
B. EXTEND AT LEAST ONE OF THE NORTH-SOUTH WALKWAYS TO THE
NORTH BOUNDARY OF THE SITE TO PROVIDE ACCESS TO THE
PUBLIC PARK.

C. THE APPLICANT SHALL GRANT TO THE CITY AN EASEMENT FOR
PEDESTRIAN AND BICYCLE ACCESS PURPOSES (1) FROM THE
GREENE STREET FRONTAGE OF THE SITE TO THE ACCESS POINT TO
THE PARK NORTH OF THE SITE OVER AT LEAST THE WESTERLY OFF-
ROAD PATHWAY PARALLEL TO THE MAIN NORTH-SOUTH DRIVEWAY,
AND (2) FROM THE DEBAUCH ROAD FRONTAGE TO THE NORTH
END OF THE NORTH-SOUTH SECTION OF THE MAIN ACCESS
DRIVEWAY OVER THE EAST-WEST PATHWAY REQUIRED IN
CONDITION 2.A. THE EASEMENTS SHALL ALLOW PUBLIC ACCESS
ON A DAILY BASIS BETWEEN 6 A.M. AND 9 P.M. THE
APPLICANT SHALL INSTALL SIGNS OR OTHER READILY APPARENT
MARKERS IDENTIFYING THE PUBLIC PATHWAYS AS SUCH, AND
MAY REQUIRE THAT PUBLIC USERS KEEP TO THE PATHWAYS.
- DEDICATE AND/OR DEED SUFFICIENT LAND TO THE CITY AT THE
NORTHEAST CORNER OF THE QUARRY AREA TO ACCOMMODATE A LINK
BETWEEN THE EXISTING TRAIL FROM TANNER CREST SUBDIVISION TO
THE EXISTING TRAIL FROM REMINGTON DRIVE. THE APPLICANT SHALL
CONSTRUCT AN ASPHALT PATH APPROVED BY THE CITY ENGINEER TO
LINK THESE TWO TRAILS SEGMENTS.
- PREPARE AND SUBMIT AN ANALYSIS TO THE PLANNING DIRECTOR OF THE
POTENTIAL ADVERSE ENVIRONMENTAL IMPACTS OF THE PROPOSED
BRIDGE OVER THE DRAINAGEWAY. THE ANALYSIS SHALL IDENTIFY
MEASURES WARRANTED TO PREVENT, REDUCE OR MITIGATE SUCH
POTENTIAL ADVERSE EFFECTS. THE APPLICANT SHALL CONSTRUCT A SMALL
BRIDGE ACROSS THE DRAINAGEWAY TO PROVIDE A PEDESTRIAN CROSSING
FOR THE PUBLIC ACCESS TRAIL CONNECTION TO THE ADJACENT CITY PARK
AS REQUIRED BY CONDITION 2. THE APPLICANT SHALL INCORPORATE
INTO THE BRIDGE AND ITS CONSTRUCTION PROCESS THE MEASURES
IDENTIFIED IN THE ANALYSIS TO AVOID, REDUCE OR MITIGATE THE
ADVERSE ENVIRONMENTAL IMPACTS OF THE BRIDGE, EXCEPT AS
OTHERWISE APPROVED BY THE PLANNING DIRECTOR. THE APPLICANT
SHALL PREPARE ALL REQUIRED STUDIES AND MITIGATION PLANS PURSUANT
TO CDC CHAPTERS 30 AND 32 FOR APPROVAL BY THE PLANNING
DIRECTOR AND SHALL IMPLEMENT THOSE STUDIES AND PLANS AS
DIRECTED.
- THE APPLICANT SHALL PETITION FOR VACATION OF THE PORTION OF THE
GREENE STREET RIGHT OF WAY ABUTTING THE SITE. THE CITY SHALL
NOT AUTHORIZE OCCUPANCY OF ANY DWELLING UNITS ON THE SITE UNTIL
THE VACATION IS APPROVED OR UNTIL THE PLANNING DIRECTOR FINDS
THE ISSUE OF GREENE STREET IS OTHERWISE RESOLVED. THE
APPLICANT SHALL CONSTRUCT A FOUR-FOOT WIDE GRAVEL PATH WITHIN
AN EASEMENT RETAINED BY THE CITY AS A CONDITION OF THE VACATION
OF THE RIGHT OF WAY. IF THE RIGHT OF WAY IS NOT VACATED, THE
APPLICANT SHALL INSTALL HALF-STREET IMPROVEMENTS CONSISTENT WITH
THE CDC OR APPLY FOR AND RECEIVE APPROVAL OF VARIANCE FROM
THE CITY.
- THE APPLICANT SHALL SUBMIT FRONT ELEVATION DRAWINGS OF THE FIVE
BUILDING TYPES SHOWING A MINIMUM THREE-FOOT PROJECTION
ACROSS AT LEAST TWO-THIRDS OF THE GARAGE DOOR ELEVATION OF ALL
STREET-FACING GARAGES. SUCH PROJECTIONS MAY INCLUDE A
PROJECTING SECOND STORY, PROJECTING BALCONIES, TRELLISES,
AWNINGS OR SIMILAR FEATURES, ATTACHED TO BUILDING OR FREE
STANDING. THE SUBMITTED PLANS SHALL BE APPROVED BY THE
PLANNING DIRECTOR BEFORE THE CITY APPROVES BUILDING PERMITS
FOR THE STRUCTURES IN QUESTION. EXCEPT FOR THESE MODIFICATIONS,
ALL BUILDING TYPES SHALL BE CONSTRUCTED AS SHOWN IN THE
APPLICANT'S SUBMITTAL, INCLUDING THE AMENDMENT PROPOSED AT
THE JANUARY 28 HEARING (EXHIBIT HO-13). COLOR AND MATERIAL
BOARDS SHALL BE OF EQUAL OR SUPERIOR BUILDING MATERIALS AND
SHALL BE APPROVED BY THE PLANNING DIRECTOR.
- THE TRANSITION AREA FOR NATURAL DRAINAGEWAY AND THE AREA NORTH
OF THE DRAINAGEWAY SHALL BE DEEDED OR DEDICATED TO THE CITY OF
WEST LINN.
- CONDITION OF APPROVAL 8 IS HEREBY DELETED.
- ALL SIGNIFICANT TREES, EXCEPT THOSE IDENTIFIED FOR REMOVAL BY THE
CITY ARBORIST, SHALL BE PROTECTED BY A SNOW FENCE 10 FEET
BEYOND THE DRIPLINE PRIOR TO SITE CLEARING AND GRUBBING.
- THE NATURAL DRAINAGEWAY SHALL BE PROTECTED FROM
ENCROACHMENT BY A SNOW FENCE IN ADDITION TO ALL NECESSARY
EROSION CONTROL MEASURES.
- THE APPLICANT SHALL PROVIDE A PLAN FOR FULL "INCH BY INCH"
MITIGATION FOR LOSS OF ANY SIGNIFICANT TREE(S) THE CITY ARBORIST
AUTHORIZES THE APPLICATION TO REMOVE. THIS PLAN SHALL BE
REVIEWED BY THE CITY ARBORIST.
- THE APPLICANT SHALL RE-DESIGN PARKING STALLS SO THAT A 50/50
SPLIT OF COMPACT AND FULL SIZED SPACES IS PROVIDED. ANY AREAS
GAINED SHALL BE LANDSCAPED OR USED FOR PEDESTRIAN FACILITIES.
THIS NEW DESIGN SHALL BE APPROVED BY THE PLANNING DIRECTOR.
- DESIGNATED VISITOR PARKING SHALL BE EVENLY DISTRIBUTED
THROUGHOUT THE PROJECT SITE, PURSUANT TO A PLAN APPROVED BY THE
PLANNING DIRECTOR. APPROVED GUEST PARKING SPACES MUST BE DESIGNATED
AS SUCH ON THE SITE CONSISTENT WITH CDC 46.150.A.16.
- MINOR ADJUSTMENTS TO THE ORIENTATION OF BUILDINGS OR THEIR
DESIGN SHALL BE PERMITTED ONLY AFTER APPROVAL BY THE PLANNING
DIRECTOR. ADDING UNITS IS NOT PERMITTED. ELIMINATING UNITS IS
PERMITTED, BUT THE TOTAL NUMBER OF UNITS MAY NOT GO UNDER 421
UNITS (80% RULE). MINOR SHIFTS IN FOOTPRINT MAY BE PERMITTED IF
IT IS DEMONSTRATED TO THE PLANNING DIRECTOR THAT EITHER
TOPOGRAPHY OR VEGETATION IS BETTER PROTECTED. GRADING IS
REDUCED, OR SUPERIOR DESIGN IS ACHIEVED.
- THE PROJECT SHALL BE CONSTRUCTED CONSISTENT WITH THE SUBMITTED
COMPOSITE SITE PLAN (SHEET 9 OF 30) OF THE APPLICATION WITH THE
AMENDMENT SHOWN IN EXHIBIT HO-13, UNLESS SPECIFICALLY
EXEMPTED UNDER CONDITIONS OF APPROVAL.
- THE APPLICANT SHALL CONFORM TO ALL FEDERAL, STATE AND LOCAL
POLICIES AND CODES UNLESS GRANTED A WRITTEN WAIVER,
MODIFICATION AND/OR VARIANCE BY THE APPROPRIATE DECIDING
BODY.
- STREETS- THE APPLICANT SHALL: 1) DEED OR DEDICATE, ALONG THE
DEVELOPMENT'S DEBOK ROAD FRONTAGE, HALF THE COLLECTOR STREET
60-FOOT RIGHT-OF-WAY; 2) CONSTRUCT HALF STREET IMPROVEMENTS
ALONG DEBOK AND BLANKENSHIP ROADS CONSISTENT WITH THE 10TH
STREET CORRIDOR STUDY BUILD-OUT PAVEMENT WIDTH REQUIREMENTS
AND CHAPTER 92 OF THE WEST LINN COMMUNITY DEVELOPMENT
CODE. (THE PLANTER STRIP AND SIDEWALK WIDTHS MAY BE ADJUSTED
IN ORDER TO PROTECT TREES, SAVE AN EXISTING STORM WATER
DRAINAGEWAY AND/OR AVOID NON-APPLICANT CONTROLLED STREET
RIGHT-OF-WAY CONSTRAINTS); 3) IMPROVE 13TH STREET FROM THE
DEVELOPMENT SITE TO BLANKENSHIP ROAD ACCORDING TO THE CITY
ENGINEER'S REQUIREMENTS; 4) SUBMIT AND PROCESS A VACATION
REQUEST FOR THE GREENE STREET RIGHT-OF-WAY; 5) DELINEATE AND
INSTALL A GRAVEL PEDESTRIAN TRAIL WITHIN THE CENTER 20 FEET OF THE
EXISTING GREENE STREET RIGHT-OF-WAY (THE APPLICANT SHALL SUBMIT
A PRIVATE-PUBLIC DELINEATION PLAN FOR THIS PATHWAY PER CDC
55.100.G TO THE CITY ENGINEER AND PLANNING DIRECTOR FOR
APPROVAL); 6) CONSTRUCT THE 10TH STREET CORRIDOR STREET
IMPROVEMENTS REQUIRED BY THE CITY TRAFFIC STUDY CURRENTLY
BEING DEVELOPED BY THE TRAFFIC ENGINEERING CONSULTANT,
KITTELSON & ASSOCIATES (MINIMUM IMPROVEMENTS FOR THE
SUMMERLINN APARTMENTS DEVELOPMENT SHALL BE THE
CONSTRUCTION OF THE TWO TRAFFIC SIGNAL LIGHTS AND ASSOCIATED
IMPROVEMENTS AT THE WEST BOUND I-205 FREEWAY OFF-RAMP AND
10TH STREET, AND THE 10TH STREET/SALAMO ROAD/ BLANKENSHIP
ROAD INTERSECTIONS AND SIDEWALK BETWEEN RIVER FALLS
SHOPPING CENTER SIDEWALK AND 8TH AVENUE; 7) GRANT TOWING
AND TICKETING ENFORCEMENT RIGHTS ON THE FIRE, LIFE AND SAFETY
ACCESS CORRIDORS WITHIN THE DEVELOPMENT; 8) CONSTRUCT THE
PRIVATE PARKING/DRIVEWAY ISLES AND FIRE TURNAROUNDS TO NOT
EXCEED 15% AND 8% GRADES RESPECTIVELY; AND 9) DESIGN AND
CONSTRUCT A SIGNAGE PLAN (FOR THE PLANNING DIRECTOR'S AND THE
CITY ENGINEER'S APPROVAL) TO ADEQUATELY DELINEATE THE CHANGES
FROM PUBLIC ACCESS TO PRIVATE ACCESS FOR DRIVEWAY ISLES AND
PEDESTRIAN PATHS FOR THE DEVELOPMENT.
- STORM. THE APPLICANT SHALL: 1) DESIGN THE DEBOK ROAD
IMPROVEMENTS AND PRIVATE BUILDING SITE GRADING TO RETAIN AS
MUCH AS POSSIBLE OF THE MAN-MADE DRAINAGE WAY DITCH ALONG
DEBOK ROAD (THIS MAY REQUIRE COMBINING THE DEVELOPMENT'S
PRIVATE WATER QUALITY SWALE AND THE EXISTING SWALE); 2) RECORD
WITH THE COUNTY AN AGREEMENT WITH THE CITY THAT REQUIRES THE
PROPERTY OWNER TO OPERATE AND MAINTAIN THE PRIVATE STORM
DETENTION AND WATER QUALITY FACILITIES AND PROVIDE THIRD PARTY
CERTIFICATION TO THE CITY THAT IT IS WORKING PROPERLY ON AN
ANNUAL BASIS; 3) DESIGN AND CONSTRUCT THE PRIVATE ON-SITE STORM
DRAINAGE FACILITIES TO MINIMIZE INTER-BASIN TRANSFERS OF STORM
WATER; 4) DETAIN THE DOWNSTREAM STORM DRAINAGE RUN-OFF WITH
PRIVATE STORM WATER DETENTION FACILITIES SO THAT THE 1-, 5-
AND 10-YEAR POST DEVELOPMENT RELEASE RATES ARE EQUAL TO THE 1-, 5-
AND 10-YEAR PRE-DEVELOPMENT RELEASE RATES; 5) EXTEND THE 18-
INCH STORM DRAINAGE MAIN STUB-OUT LOCATED AT BLANKENSHIP
ROAD AND 13TH STREET TO THE PROPOSED PRIVATE STORM SYSTEM
OUTFALL AT THE TOP OF 13TH STREET; 6) CONSTRUCT THE STORM
DRAINAGE MASTER PLAN PROJECT 32-01A (10TH STREET CULVERT
CROSSING) OR, PROVIDE ADDITIONAL ON-SITE DETENTION OR OTHER
MITIGATION SUFFICIENT TO DEMONSTRATE THAT ADDITIONAL STORMWATER
RUNOFF GENERATED BY DEVELOPMENT ON THIS SITE WILL NOT EXCEED
THE CAPACITY OF THE 10TH STREET CULVERT CROSSING DURING A 100-
YEAR STORM EVENT.
- WATER. THE APPLICANT SHALL: 1) FINANCE THE REVIEW OF THE
DEVELOPMENT'S FIRE AND DOMESTIC WATER SYSTEM DEMANDS WITH
THE CITY'S NEW WATER MASTER PLAN, MONTGOMERY WATSON, TO
ESTABLISH ALL NECESSARY OFF-SITE AND ON-SITE WATER
IMPROVEMENTS REQUIRED FOR THE DEVELOPMENT. THE MINIMUM
OFF-SITE MASTER PLAN WATER TRANSMISSION MAIN CONSTRUCTION
IMPROVEMENTS REQUIRED SHALL INCLUDE: A) PHASE II OF THE
WILLAMETTE FALLS DRIVE WATER TRANSMISSION MAIN, AND B) THE
WATERLINE ON BLANKENSHIP ROAD BETWEEN 13TH STREET AND
DEBOK; 2) PERFORM ACTUAL FIRE FLOW TESTS ON THE VARIOUS NEW
PRIVATE FIRE HYDRANTS, DURING AN INDUCED HIGH WATER DEMAND
DAY, THAT PROVIDE PROOF THAT THE FIRE FLOW IS ADEQUATE TO MEET
EACH OF THE BUILDINGS WITHIN ITS SERVICE AREA; 3) OBTAIN WRITTEN
APPROVAL FROM THE CITY ENGINEER AND THE CITY FIRE MARSHALL
THAT THE NECESSARY HYDRANT FIRE FLOW IS AVAILABLE PRIOR TO ANY
BUILDING-RELATED CONSTRUCTION WITH COMBUSTIBLE MATERIALS; 4)
RECORD WITH THE COUNTY AN AGREEMENT WITH THE CITY THAT
REQUIRES THE PROPERTY OWNER TO PROVIDE ANNUAL CERTIFICATION TO
THE CITY'S FIRE DEPARTMENT THAT THE PRIVATE FIRE SYSTEM IS
OPERATING PROPERLY; AND 5) DESIGN AND CONSTRUCT THE PRIVATE
DOMESTIC WATER SYSTEMS TO SERVICE THE BUILDINGS LOCATED IN
VARIOUS MASTER PLAN PRESSURE ZONES FROM THE SAME ZONE.
WATER FOR FIRE FLOW REQUIREMENTS MAY BE ALLOWED TO CROSS
PRESSURE ZONES. THE FIRE FLOW REQUIREMENTS OF THE BUILDINGS AT
THE TOP OF THE WILLAMETTE ZONE MAY REQUIRE THAT THESE
BUILDINGS USE FIREWALLS AND/OR SPRINKLERS TO MAKE UP ANY
PRIVATE HYDRANT FIRE FLOW DEFICIENCIES.
- SANITARY SEWER. THE APPLICANT SHALL: A.) DESIGN AND CONSTRUCT
THE SEWER SYSTEM TO AVOID ANY INTER-BASIN TRANSFERS; OR B.) 1)
FINANCE A REVIEW (LESS THAN \$500) BY THE CITY'S SANITARY SEWER
MASTER PLAN CONSULTANT, MURRAY SMITH, OF THE IMPACT OF
PROPOSED INTER-BASIN TRANSFERS OF THE SEWAGE SYSTEM ON THE
NEED FOR ANY OFF-SITE SANITARY SEWER MAIN SYSTEM
IMPROVEMENTS; AND 2) CONSTRUCT ANY OFF-SITE IMPROVEMENTS
THAT ARE DETERMINED NECESSARY FROM THE REVIEW TO ALLOW THE
PROPOSED INTERBASIN TRANSFER.
- THE APPLICANT SHALL CONSTRUCT ALL OF THE DEVELOPMENT'S 1)
PUBLIC STREET AND
NECESSARY PRIVATE PARKING/DRIVEWAY ISLE IMPROVEMENTS; 2)
PUBLIC AND NECESSARY PRIVATE STORM DRAINAGE WATER QUALITY,
DETENTION AND OUT-FALL FACILITIES; 3) PUBLIC AND PRIVATE
(DOMESTIC AND FIRE FLOW) WATER TRANSMISSION IMPROVEMENTS;
AND 4) PUBLIC AND NECESSARY PRIVATE SANITARY SEWER MAIN
TRANSMISSION IMPROVEMENTS PRIOR TO THE BEING GRANTED ANY
BUILDING FINAL INSPECTION AND/OR OCCUPANCY OF ANY PHASE OF THE
DEVELOPMENT.
- THE APPLICANT SHALL INSTALL A SIX-FOOT TALL SOLID WOOD FENCE OR
OTHER DESIGN THAT PROPERLY SCREENS GLARE ALONG THE MAIN
DRIVEWAY AISLE #1 WHEN IT IS ADJACENT TO THE NORTH PROPERTY
LINE.

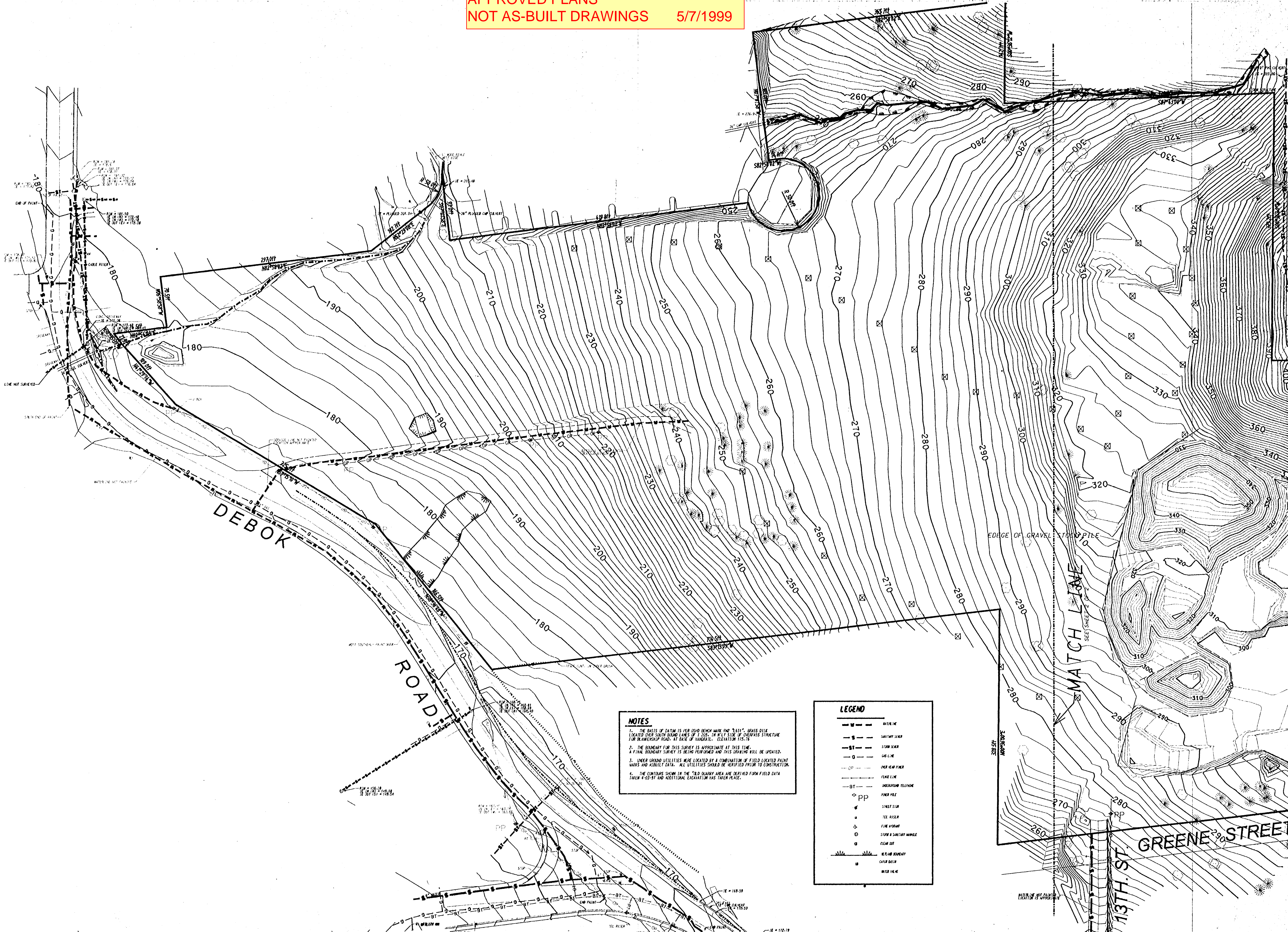
CONDITIONS OF APPROVAL
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



W.B. WELLS
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APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



REVISIONS

TOPOGRAPHIC SURVEY

For

SUMMERLINN APARTMENTS

CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

REGISTERED
PROFESSIONAL
LAND SURVEYOR

David W. Mills

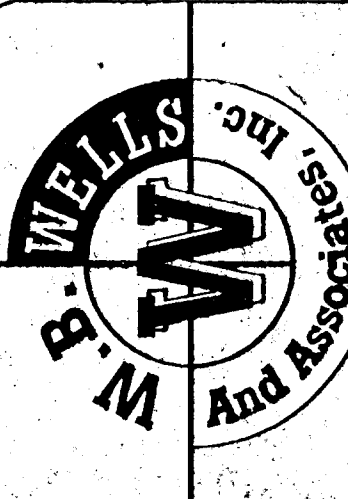
OREGON
JULY 18, 1980
DAVID W. MILLS
1915

EXPIRES: 12-31-00

d.w.mills

W.B. WELLS
and associates inc.

ENGINEERS • SURVEYORS • PLANNERS
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PHONE (503) 284-5896
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SHEET 4 of 42

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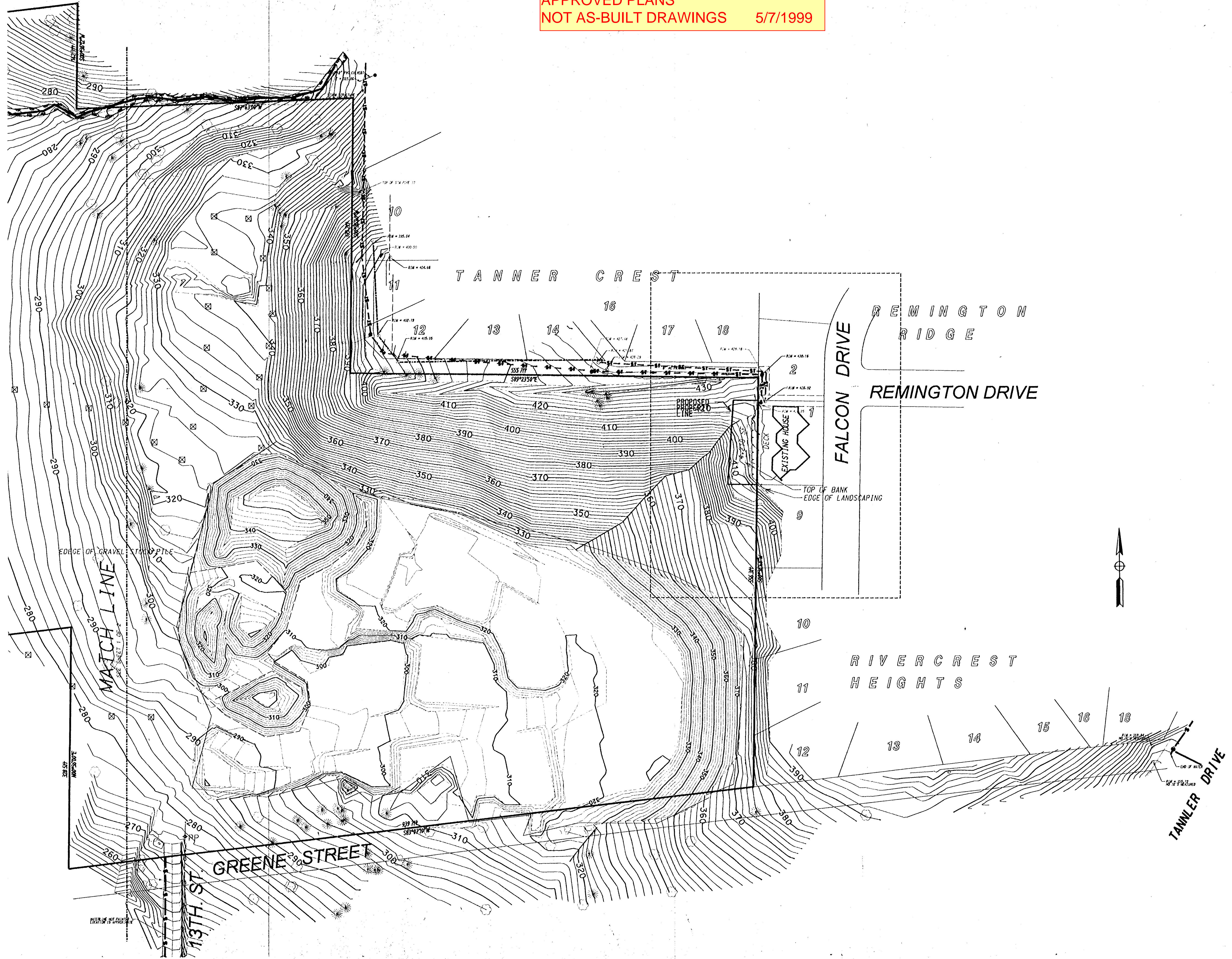
42

and Associates, Inc.

POST

01 DATE: 10 25R 1993

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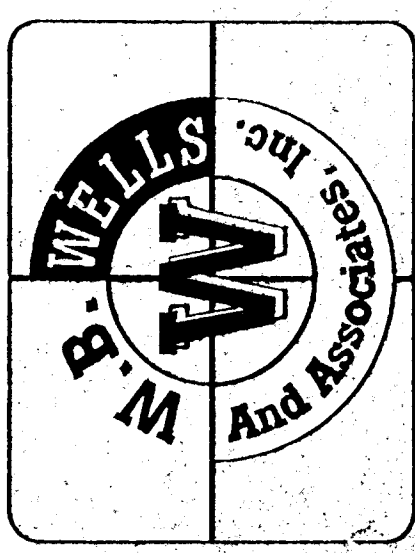


| REVISIONS | | | |
|-----------|--------------------|--------------|-----------------|
| 3-20-99 | | | |
| 10-01-97 | RETOPO QUARRY AREA | CHECKED: DWM | DATE: 6-24-97 |
| | DESIGNED: BSS | DRAWN: DWM | JOB NO.: 96-340 |
| | | | SCALE: 1"=60' |

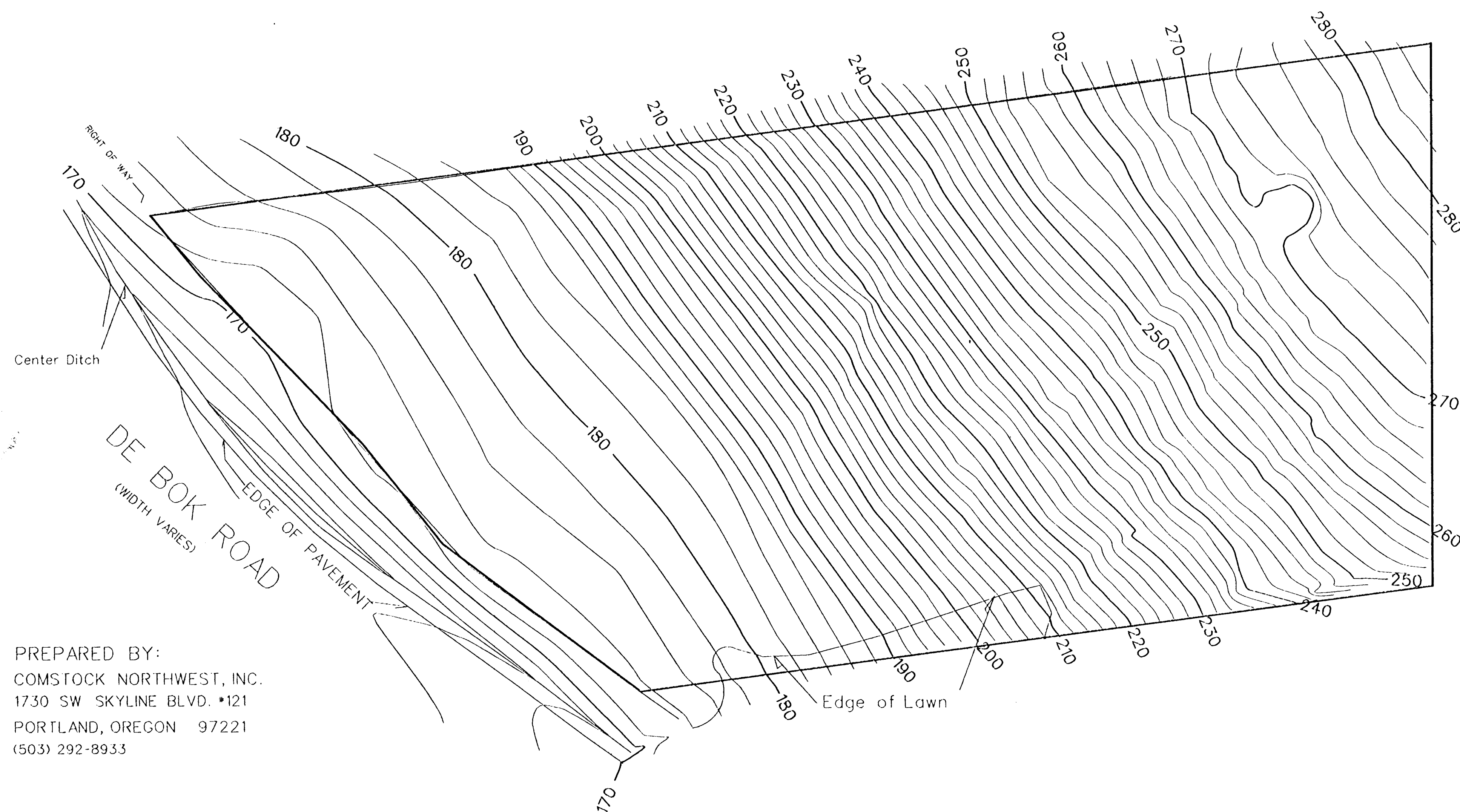
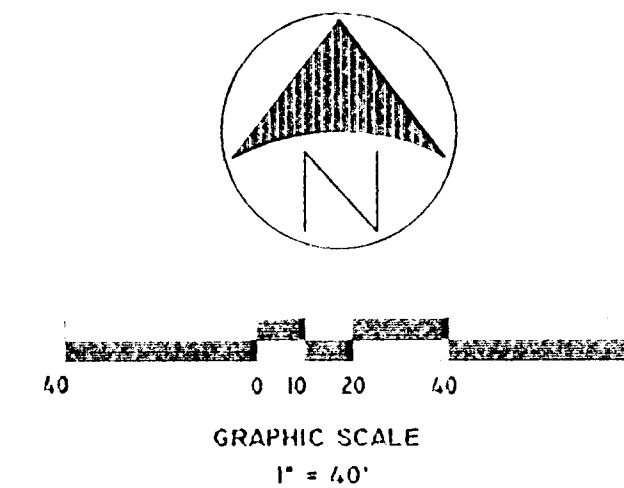
TOPOGRAPHIC SURVEY
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

REGISTERED
PROFESSIONAL
LAND SURVEYOR
David W. Wells
OREGON
JULY 19, 1980
DAVID W. WELLS
1915
EXPIRES: 12-31-99
4/16/99

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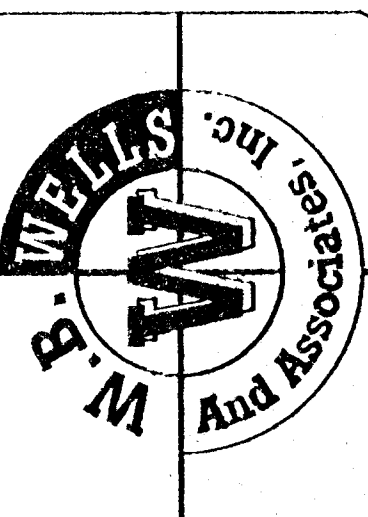
TOPOGRAPHIC SURVEY
OF TAX LOT 100 2S 1E 35CB
LOCATED IN THE SW 1/4 SEC.35, T2S, R1E, W.M.
CITY OF WEST LINN CLACKAMAS COUNTY OREGON
DECEMBER 4, 1995 BENCH MARK "L631" EL.=175.76'



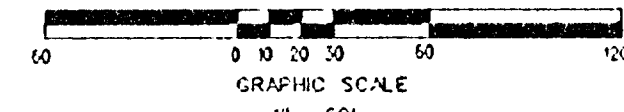
PREPARED BY:
COMSTOCK NORTHWEST, INC.
1730 SW SKYLINE BLVD. #121
PORTLAND, OREGON 97221
(503) 292-8933

TOPOGRAPHIC SURVEY
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

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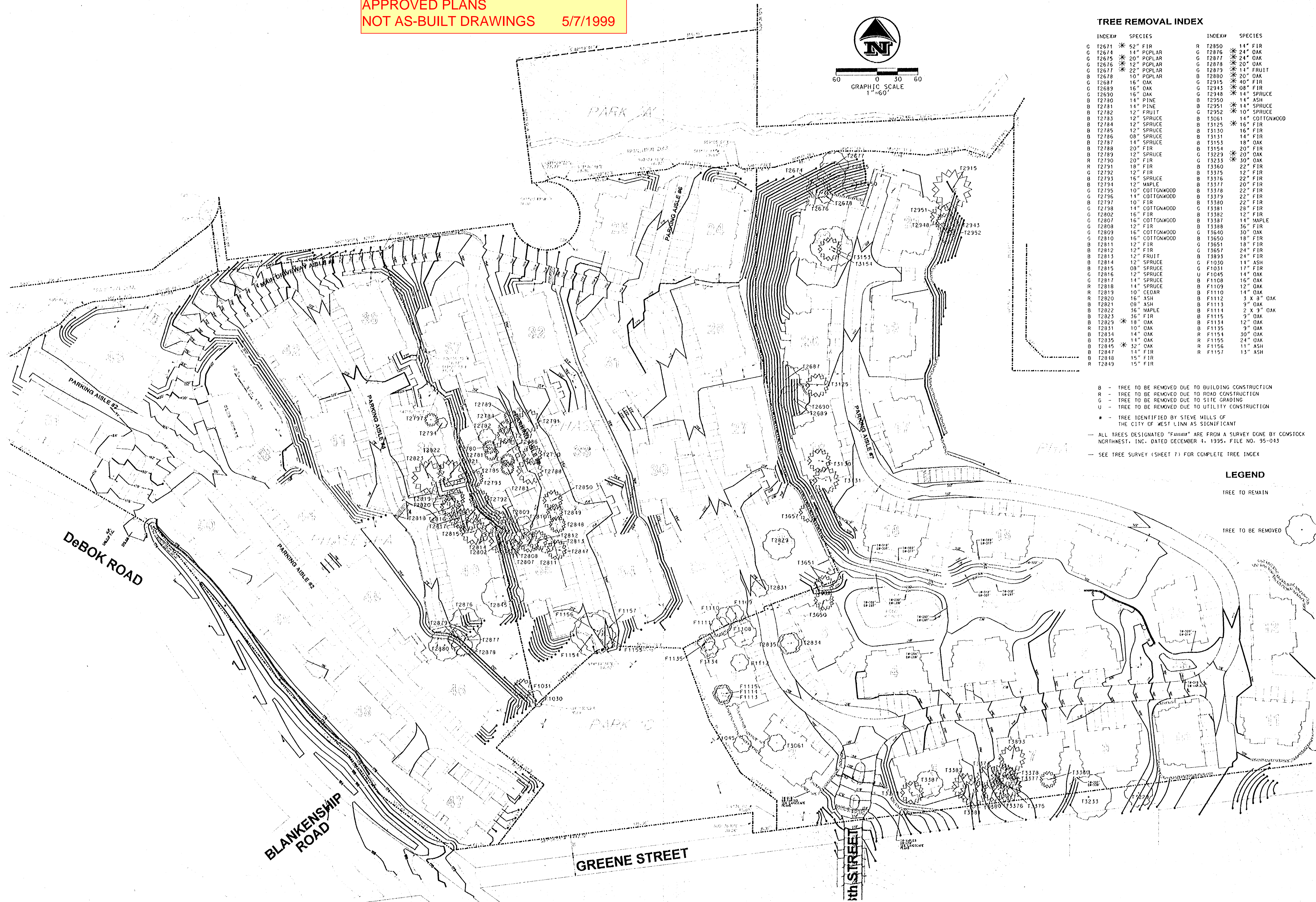


APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



TREE INDEX

| INDEX# | SPECIES | INDEX# | SPECIES | INDEX# | SPECIES | INDEX# | SPECIES |
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| T1664 | 18" BIRCH | R T2780 | 14" PINE | SIP T2876 | 24" OAK | R T3131 | 14" FIR |
| T1665 | 12" FIR | R T2781 | 14" PINE | SIP T2877 | 24" OAK | R T3133 | 18" OAK |
| T1666 | 12" BIRCH | R T2782 | 12" FRUIT | R T2878 | 20" OAK | R T3134 | 20" FIR |
| T2471 | 20" COTTONWOOD | R T2783 | 12" SPRUCE | R T2879 | 14" FRUIT | R T3201 | 42" FIR |
| T2474 | 12" COTTONWOOD | R T2784 | 12" SPRUCE | R T2880 | 20" OAK | T3208 | 36" OAK |
| T2529 | 14" ASH | R T2785 | 12" SPRUCE | T2889 | 48" OAK | T3211 | 10" OAK |
| T2635 | 32" OAK | R T2786 | 08" SPRUCE | T2893 | 36" OAK | T3215 | 24" FIR |
| T2637 | 36" ASH | R T2787 | 14" SPRUCE | T2894 | 14" FIR | T3216 | 10" OAK |
| T2638 | 12" FIR | R T2788 | 20" FIR | T2898 | 18" FIR | T3217 | 10" OAK |
| T2639 | 14" MADRONE | R T2789 | 12" SPRUCE | T2899 | 16" FIR | T3221 | 08" OAK |
| T2641 | 26" FIR | R T2790 | 20" FIR | T2903 | 18" OAK | T3225 | 08" FRUIT |
| T2643 | 30" FIR | R T2791 | 18" FIR | SIP T2915 | 10" FIR | SIP T3229 | 20" OAK |
| T2644 | 36" FIR | R T2792 | 12" FIR | SIP T2919 | 48" MAPLE | SIP T3233 | 30" OAK |
| T2646 | 14" MADRONE | R T2793 | 16" SPRUCE | T2921 | 14" MAPLE | T3235 | 22" FIR |
| T2650 | 48" FIR | R T2794 | 12" MAPLE | T2923 | 24" MAPLE | T3236 | 30" OAK |
| T2660 | 16" ASH | R T2795 | 10" COTTONWOOD | T2925 | 12" FRUIT | T3237 | 10" FIR |
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| T2667 | 24" ASH | R T2798 | 14" COTTONWOOD | T2931 | 14" OAK | T3240 | 30" OAK |
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| T2670 | 14" FIR | R T2801 | 16" COTTONWOOD | R T2935 | 08" FIR | T3242 | 32" FIR |
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| T2672 | 14" FIR | R T2803 | 16" COTTONWOOD | R T2937 | 14" ASH | T3244 | 12" FIR |
| T2673 | 14" POPLAR | R T2804 | 16" COTTONWOOD | R T2938 | 14" SPRUCE | T3245 | 12" FIR |
| T2674 | 14" POPLAR | R T2805 | 12" FIR | R T2939 | 10" SPRUCE | T3246 | 22" FIR |
| T2675 | 20" POPLAR | R T2806 | 12" FIR | R T2940 | 10" SPRUCE | T3247 | 22" FIR |
| T2676 | 12" POPLAR | R T2807 | 12" FRUIT | R T2941 | 12" FIR | T3248 | 22" FIR |
| T2677 | 22" POPLAR | R T2808 | 12" SPRUCE | R T2942 | 12" SPRUCE | T3249 | 22" FIR |
| T2678 | 10" POPLAR | R T2809 | 08" SPRUCE | R T2943 | 20" FIR | T3250 | 22" FIR |
| T2679 | 10" MADRONE | R T2810 | 08" SPRUCE | R T2944 | 20" FIR | T3251 | 22" FIR |
| T2680 | 16" OAK | R T2811 | 12" SPRUCE | R T2945 | 20" FIR | T3252 | 22" FIR |
| T2681 | 16" OAK | R T2812 | 12" SPRUCE | R T2946 | 20" FIR | T3253 | 22" FIR |
| T2682 | 16" OAK | R T2813 | 12" SPRUCE | R T2947 | 20" FIR | T3254 | 22" FIR |
| T2683 | 16" OAK | R T2814 | 12" SPRUCE | R T2948 | 20" FIR | T3255 | 22" FIR |
| T2684 | 16" OAK | R T2815 | 12" SPRUCE | R T2949 | 20" FIR | T3256 | 22" FIR |
| T2685 | 16" OAK | R T2816 | 12" SPRUCE | R T2950 | 20" FIR | T3257 | 22" FIR |
| T2686 | 16" OAK | R T2817 | 12" SPRUCE | R T2951 | 20" FIR | T3258 | 22" FIR |
| T2687 | 16" OAK | R T2818 | 12" SPRUCE | R T2952 | 20" FIR | T3259 | 22" FIR |
| T2688 | 16" OAK | R T2819 | 12" SPRUCE | R T2953 | 20" FIR | T3260 | 22" FIR |
| T2689 | 16" OAK | R T2820 | 12" SPRUCE | R T2954 | 20" FIR | T3261 | 22" FIR |
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| T2694 | 16" OAK | R T2825 | 12" SPRUCE | R T2959 | 20" FIR | T3266 | 22" FIR |
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| T2697 | 16" OAK | R T2828 | 12" SPRUCE | R T2962 | 20" FIR | T3269 | 22" FIR |
| T2698 | 16" OAK | R T2829 | 12" SPRUCE | R T2963 | 20" FIR | T3270 | 22" FIR |
| T2699 | 16" OAK | R T2830 | 12" SPRUCE | R T2964 | 20" FIR | T3271 | 22" FIR |
| T2700 | 16" OAK | R T2831 | 12" SPRUCE | R T2965 | 20" FIR | T3272 | 22" FIR |
| T2701 | 16" OAK | R T2832 | 12" SPRUCE | R T2966 | 20" FIR | T3273 | 22" FIR |
| T2702 | 16" OAK | R T2833 | 12" SPRUCE | R T2967 | 20" FIR | T3274 | 22" FIR |
| T2703 | 16" OAK | R T2834 | 12" SPRUCE | R T2968 | 20" FIR | T3275 | 22" FIR |
| T2704 | 16" OAK | R T2835 | 12" SPRUCE | R T2969 | 20" FIR | T3276 | 22" FIR |
| T2705 | 16" OAK | R T2836 | 12" SPRUCE | R T2970 | 20" FIR | T3277 | 22" FIR |
| T2706 | 16" OAK | R T2837 | 12" SPRUCE | R T2971 | 20" FIR | T3278 | 22" FIR |
| T2707 | 16" OAK | R T2838 | 12" SPRUCE | R T2972 | 20" FIR | T3279 | 22" FIR |
| T2708 | 16" OAK | R T2839 | 12" SPRUCE | R T2973 | 20" FIR | T3280 | 22" FIR |
| T2709 | 16" OAK | R T2840 | 12" SPRUCE | R T2974 | 20" FIR | T3281 | 22" FIR |
| T2710 | 16" OAK | R T2841 | 12" SPRUCE | R T2975 | 20" FIR | T3282 | 22" FIR |
| T2711 | 16" OAK | R T2842 | 12" SPRUCE | R T2976 | 20" FIR | T3283 | 22" FIR |
| T2712 | 16" OAK | R T2843 | 12" SPRUCE | R T2977 | 20" FIR | T3284 | 22" FIR |
| T2713 | 16" OAK | R T2844 | 12" SPRUCE | R T2978 | 20" FIR | T3285 | 22" FIR |
| T2714 | 16" OAK | R T2845 | 12" SPRUCE | R T2979 | 20" FIR | T3286 | 22" FIR |
| T2715 | 16" OAK | R T2846 | 12" SPRUCE | R T2980 | 20" FIR | T3287 | 22" FIR |
| T2716 | 16" OAK | R T2847 | 12" SPRUCE | R T2981 | 20" FIR | T3288 | 22" FIR |
| T2717 | 16" OAK | R T2848 | 12" SPRUCE | R T2982 | 20" FIR | T3289 | 22" FIR |
| T2718 | 16" OAK | R T2849 | 12" SPRUCE | R T2983 | 20" FIR | T3290 | 22" FIR |
| T2719 | 16" OAK | R T2850 | 12" SPRUCE | R T2984 | 20" FIR | T3291 | 22" FIR |
| T2720 | 16" OAK | R T2851 | 12" SPRUCE | R T2985 | 20" FIR | T3292 | 22" FIR |
| T2721 | 16" OAK | R T2852 | 12" SPRUCE | R T2986 | 20" FIR | T3293 | 22" FIR |
| T2722 | 16" OAK | R T2853 | 12" SPRUCE | R T2987 | 20" FIR | T3294 | 22" FIR |
| T2723 | 16" OAK | R T2854 | 12" SPRUCE | R T2988 | 20" FIR | T3295 | 22" FIR |
| T2724 | 16" OAK | R T2855 | 12" SPRUCE | R T2989 | 20" FIR | T3296 | 22" FIR |
| T2725 | 16" OAK | R T2856 | 12" SPRUCE | R T2990 | 20" FIR | T3297 | 22" FIR |
| T2726 | 16" OAK | R T2857 | 12" SPRUCE | R T2991 | 20" FIR | T3298 | 22" FIR |
| T2727 | 16" OAK | R T2858 | 12" SPRUCE | R T2992 | 20" FIR | T3299 | 22" FIR |
| T2728 | 16" OAK | R T2859 | 12" SPRUCE | R T2993 | 20" FIR | T3300 | 22" FIR |
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| T2735 | 16" OAK | R T2866 | 12" SPRUCE | R T3000 | 20" FIR | T3307 | 22" FIR |
| T2736 | 16" OAK | R T2867 | 12" SPRUCE | R T3001 | 20" FIR | T3308 | 22" FIR |
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| T2738 | 16" OAK | R T2869 | 12" SPRUCE | R T3003 | 20" FIR | T3310 | 22" FIR |
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| T2743 | 16" OAK | R T2874 | 12" SPRUCE | R T3008 | 20" FIR | T3315 | 22" FIR |
| T2744 | 16" OAK | R T2875 | 12" SPRUCE | R T3009 | 20" FIR | T3316 | 22" FIR |
| T2745 | 16" OAK | R T2876 | 12" SPRUCE | R T3010 | 20" FIR | T3317 | 22" FIR |
| T2746 | 16" OAK | R T2877 | 12" SPRUCE | R T3011 | 20" FIR | T3318 | 22" FIR |
| T2747 | 16" OAK | R T2878 | 12" SPRUCE | R T3012 | 20" FIR | T3319 | 22" FIR |
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| T2749 | 16" OAK | R T2880 | 12" SPRUCE | R T3014 | 20" FIR | T3321 | 22" FIR |
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| T2751 | 16" OAK | R T2882 | 12" SPRUCE | R T3016 | 20" FIR | T3323 | 22" FIR |
| T2752 | 16" OAK | R T2883 | 12" SPRUCE | R T3017 | 20" FIR | T3324 | 22" FIR |
| T2753 | 16" OAK | R T2884 | 12" SPRUCE | R T3018 | 20" FIR | T3325 | 22" FIR |
| T2754 | 16" OAK | R T2885 | 12" SPRUCE | R T3019 | 20" FIR | T3326 | 22" FIR |
| T2755 | 16" OAK | R T2886 | 12" SPRUCE | R T3020 | 20" FIR | T3327 | 22" FIR |
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| T2760 | 16" OAK | R T2891 | 12" SPRUCE | R T3025 | 20" FIR | T3332 | 22" FIR |
| T2761 | 16" OAK | R T2892 | 12" SPRUCE | R T3026 | 20" FIR | T3333 | 22" FIR |
| T2762 | 16" OAK | R T2893 | 12" SPRUCE | R T3027 | 20" FIR | T3334 | 22" FIR |
| T2763 | 16" OAK | R T2894 | 12" SPRUCE | R T3028 | 20" FIR | T3335 | 22" FIR |
| T2764 | 16" OAK | R T2895 | 12" SPRUCE | R T3029 | 20" FIR | T3336 | 22" FIR |
| T2765 | 16" OAK | R T2896 | 12" SPRUCE | R T3030 | 20" FIR | T3337 | 22" FIR |
| T2766 | 16" OAK | R T2897 | 12" SPRUCE | R T3031 | 20" FIR | T3338 | 22" FIR |
| T2767 | 16" OAK | R T2898 | 12" SPRUCE | R T3032 | 20" FIR | T3339 | 22" FIR |
| T2768 | 16" OAK | R T2899 | 12" SPRUCE | R T3033 | 20" FIR | T3340 | 22" FIR |
| T2769 | 16" OAK | R T2900 | 12" SPRUCE | R T3034 | 20" FIR | T3341 | 22" FIR |
| T2770 | 16" OAK | R T2901 | 12" SPRUCE | R T3035 | 20" FIR | T3342 | 22" FIR |
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| T2772 | 16" OAK | R T2903 | 12" SPRUCE | R T3037 | 20" FIR | T3344 | 22" FIR |
| T2773 | 16" OAK | R T2904 | 12" SPRUCE | R T3038 | 20" FIR | T3345 | 22" FIR |
| T2774 | 16" OAK | R T2905 | 12" SPRUCE | R T3039 | 20" FIR | T3346 | 22" FIR |
| T2775 | 16" OAK | R T2906 | 12" SPRUCE | R T3040 | 20" FIR | T3347 | 22" FIR |
| T2776 | 16" OAK | R T2907 | 12" SPRUCE | R T3041 | 20" FIR | T3348 | 22" FIR |
| T2777 | 16" OAK | R T2908 | 12" SPRUCE | R T3042 | 20" FIR | T3349 | 22" FIR |
| T2778 | 16" OAK | R T2909 | 12" SPRUCE | R T3043 | 20" FIR | T3350 | 22" FIR |
| T2779 | 16" OAK | R T2910 | 12" SPRUCE | R T3044 | 20" FIR | T3351 | 22" FIR |
| T2780 | 16" OAK | R T2911 | 12" SPRUCE | R T3045 | 20" FIR | T3352 | 22" FIR |
| T2781 | 16" OAK | R T2912 | 12" SPRUCE | R T3046 | 20" FIR | T3353 | 22" FIR |
| T2782 | 16" OAK | R T2913 | 12" SPRUCE | R T3047 | 20" FIR | T3354 | 22" FIR |
| T2783 | 16" OAK | R T2914 | 12" SPRUCE | R T3048 | 20" FIR | T3355 | 22" FIR |
| T2784 | 16" OAK | R T2915 | 12" SPRUCE | R T3049 | 20" FIR | T3356 | 22" FIR |
| T2785 | 16" OAK | R T2916 | 12" SPRUCE | R T3050 | 20" FIR | T3357 | 22" FIR |
| T2786 | 16" OAK | R T2917 | 12" SPRUCE | R T3051 | 20" FIR | T3358 | 22" FIR |
| T2787 | 16" OAK | R T2918 | 12" SPRUCE | R T3052 | 20" FIR | T3359 | 22" FIR |
| T2788 | 16" OAK | R T2919 | 12" SPRUCE | R T3053 | 20" FIR | T3360 | 22" FIR |
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| T2790 | 16" OAK | R T2921 | 12" SPRUCE | R T3055 | 20" FIR | T3362 | 22" FIR |
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| T2798 | 16" OAK | R T2929 | 12" SPRUCE | R T3063 | 20" FIR | T3370 | 22" FIR |
| T2799 | 16" OAK | R T2930 | 12" SPRUCE | R T3064 | 20" FIR | T3371 | 22" FIR |
| T2800 | 16" OAK | R T2931 | 12" SPRUCE | | | | |



TREE REMOVAL INDEX

| INDEX# | SPECIES | INDEX# | SPECIES |
|---------|----------------|---------|----------------|
| G T2671 | 52" FIR | R T2850 | 14" FIR |
| G T2674 | 14" POPLAR | G T2876 | 24" OAK |
| G T2675 | 20" POPLAR | G T2877 | 24" OAK |
| G T2676 | 12" POPLAR | G T2878 | 20" OAK |
| G T2677 | 22" POPLAR | G T2879 | 20" OAK |
| B T2678 | 10" POPLAR | B T2880 | 20" OAK |
| G T2687 | 16" OAK | G T2915 | 40" FIR |
| G T2689 | 16" OAK | G T2943 | 08" FIR |
| G T2690 | 16" OAK | G T2948 | 14" SPRUCE |
| G T2700 | 14" PINE | B T2950 | 14" ASH |
| B T2781 | 14" PINE | B T2951 | 14" SPRUCE |
| B T2782 | 12" FRUIT | G T2952 | 10" SPRUCE |
| B T2783 | 12" SPRUCE | B T3061 | 14" COTTONWOOD |
| B T2784 | 12" SPRUCE | B T3125 | 16" FIR |
| B T2785 | 12" SPRUCE | B T3130 | 16" FIR |
| B T2786 | 08" SPRUCE | B T3131 | 14" FIR |
| B T2787 | 14" SPRUCE | B T3153 | 18" OAK |
| B T2788 | 20" FIR | B T3154 | 20" FIR |
| B T2789 | 12" SPRUCE | G T3229 | 20" OAK |
| R T2790 | 20" FIR | G T3233 | 30" OAK |
| R T2791 | 18" FIR | B T3360 | 22" FIR |
| G T2792 | 12" FIR | B T3375 | 12" FIR |
| B T2793 | 16" SPRUCE | B T3376 | 22" FIR |
| B T2794 | 12" MAPLE | B T3377 | 20" FIR |
| G T2795 | 10" COTTONWOOD | B T3378 | 22" FIR |
| G T2796 | 14" COTTONWOOD | B T3379 | 22" FIR |
| B T2797 | 10" FIR | B T3380 | 22" FIR |
| G T2798 | 14" COTTONWOOD | G T3381 | 28" FIR |
| G T2802 | 16" FIR | B T3382 | 12" MAPLE |
| G T2807 | 16" COTTONWOOD | B T3387 | 14" MAPLE |
| G T2808 | 12" FIR | B T3388 | 36" FIR |
| G T2809 | 16" COTTONWOOD | G T3640 | 30" OAK |
| G T2810 | 16" COTTONWOOD | B T3650 | 18" FIR |
| B T2811 | 12" FIR | G T3651 | 18" FIR |
| B T2812 | 12" FIR | G T3657 | 24" FIR |
| B T2813 | 12" FRUIT | B T3893 | 24" FIR |
| B T2814 | 12" SPRUCE | G T1030 | 14" ASH |
| B T2815 | 08" SPRUCE | G T1031 | 17" FIR |
| G T2816 | 12" SPRUCE | U T1045 | 14" OAK |
| G T2817 | 14" SPRUCE | B T1108 | 16" OAK |
| R T2818 | 14" SPRUCE | B T1109 | 12" OAK |
| R T2819 | 10" CEDAR | B T1110 | 14" OAK |
| R T2820 | 16" ASH | B T1112 | 3 X 8" OAK |
| B T2821 | 08" ASH | B T1113 | 9" OAK |
| B T2822 | 36" MAPLE | B T1114 | 2 X 3" OAK |
| B T2823 | 36" FIR | B T1115 | 9" OAK |
| B T2829 | 18" OAK | B T1134 | 12" OAK |
| R T2831 | 10" OAK | B T1135 | 9" OAK |
| B T2834 | 14" OAK | R T1154 | 30" OAK |
| B T2835 | 14" OAK | R T1155 | 24" OAK |
| B T2845 | 32" OAK | R T1156 | 11" ASH |
| B T2847 | 14" FIR | R T1157 | 13" ASH |
| B T2848 | 15" FIR | | |
| R T2849 | 15" FIR | | |

B - TREE TO BE REMOVED DUE TO BUILDING CONSTRUCTION
R - TREE TO BE REMOVED DUE TO ROAD CONSTRUCTION
G - TREE TO BE REMOVED DUE TO SITE GRADING
U - TREE TO BE REMOVED DUE TO UTILITY CONSTRUCTION
* - TREE IDENTIFIED BY STEVE WILLS OF THE CITY OF WEST LINN AS SIGNIFICANT
— ALL TREES DESIGNATED "Furnish" ARE FROM A SURVEY DONE BY CONSTOCK NORTHWEST, INC. DATED DECEMBER 4, 1995, FILE NO. 95-043
— SEE TREE SURVEY (SHEET 7) FOR COMPLETE TREE INDEX

LEGEND

TREE TO REMAIN

TREE TO BE REMOVED

REVISIONS

3-20-99

DESIGNED: BSS

CHECKED: BSS

DRAWN: BSS/RT

DATE: 11-20-98

SCALE:

JOB NO.: 98-340

TREE REMOVAL PLAN

for

SUMMERLIN APARTMENTS

CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

3-20-99

REGISTERED PROFESSIONAL

ENGINEER

WILLIAM S. SCHLEIFER

OREGON

BRADLEY S. SCHLEIFER

EXPIRES: 06-30-2000

W.B. WELLS

and associates inc.

ENGINEERS • SURVEYORS • PLANNERS

4230 NE FREMONT STREET

PORTLAND, OREGON 97213

PHONE (503) 284-5896

FAX (503) 284-8530

W.B. WELLS

And Associates Inc.

SHEET

8 of 42

DR-97-38/ZC-97-04/MISC-97-59/MISC-97-60

04-11-99

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

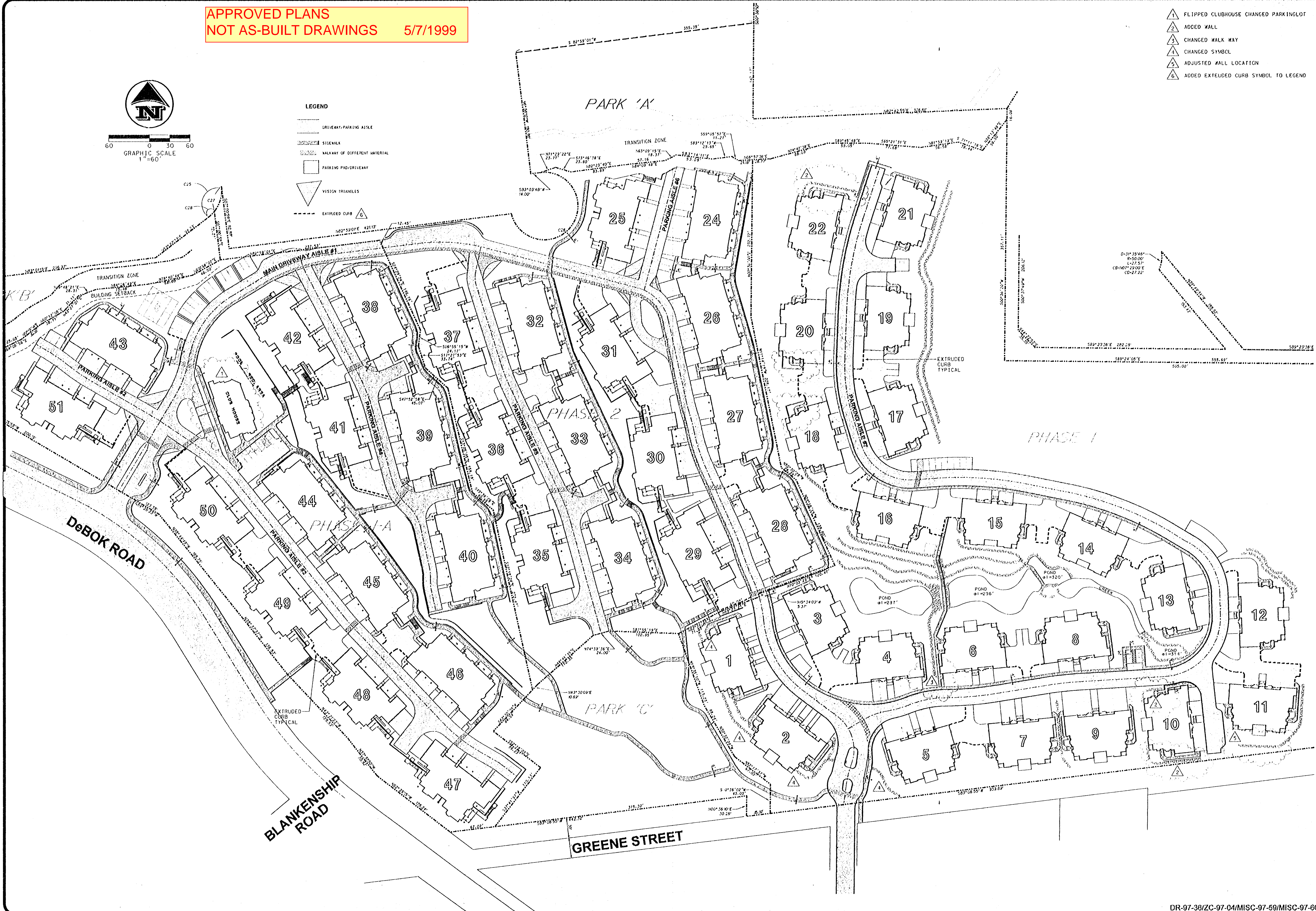


GRAPHIC SCALE
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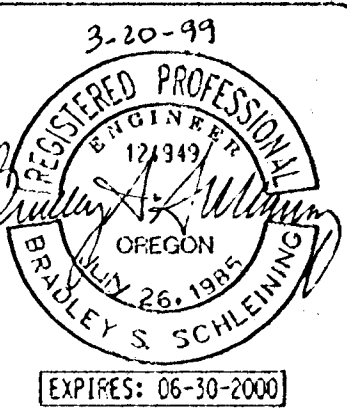
LEGEND

- DRIVEWAY/PARKING AISLE
- SIDEWALK
- WALKWAY OF DIFFERENT MATERIAL
- PARKING PAD/DRIVEWAY
- VISION TRIANGLES
- EXTRUDED CURB

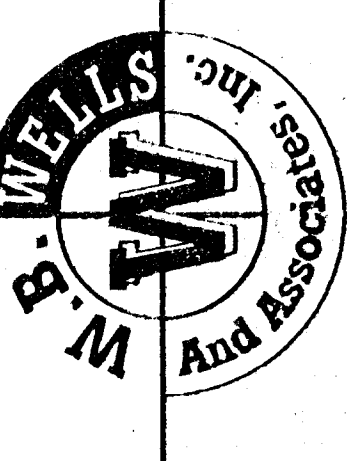
- FLIPPED CLUBHOUSE CHANGED PARKINGLOT
- ADDED WALL
- CHANGED WALK WAY
- CHANGED SYMBOL
- ADJUSTED WALL LOCATION
- ADDED EXTRUDED CURB SYMBOL TO LEGEND



COMPOSITE SITE PLAN
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



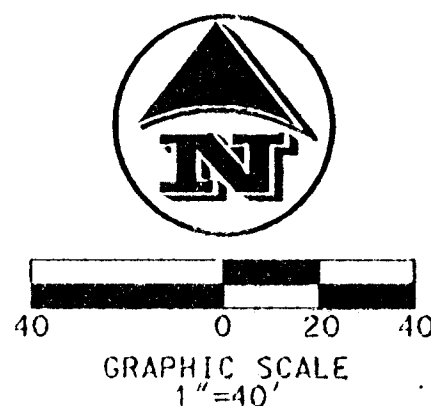
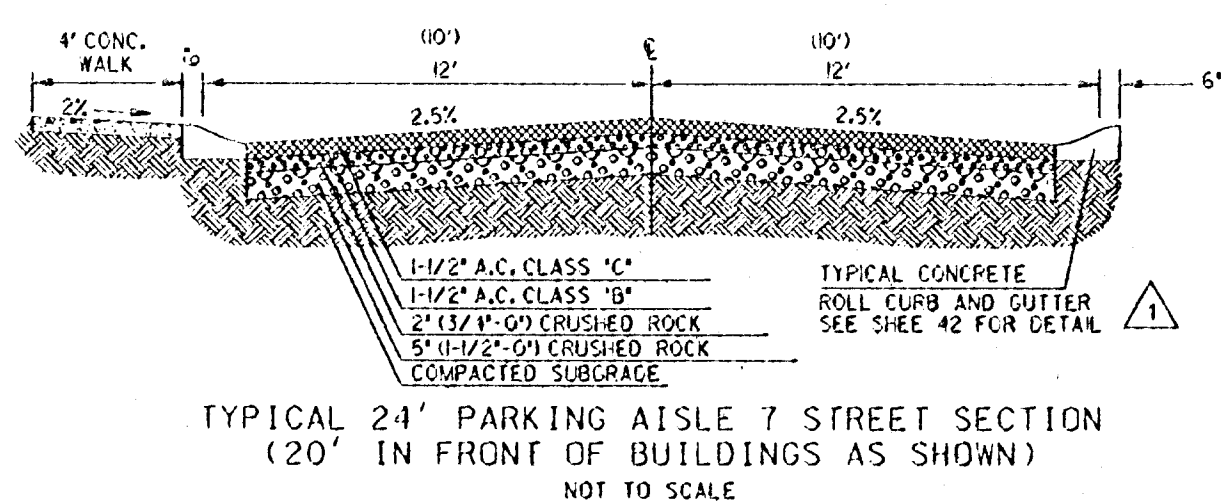
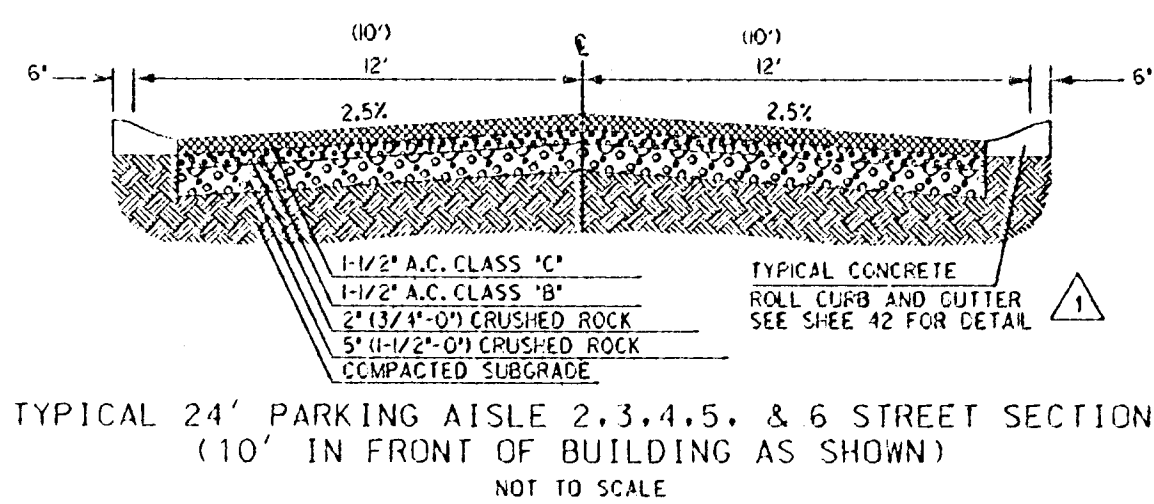
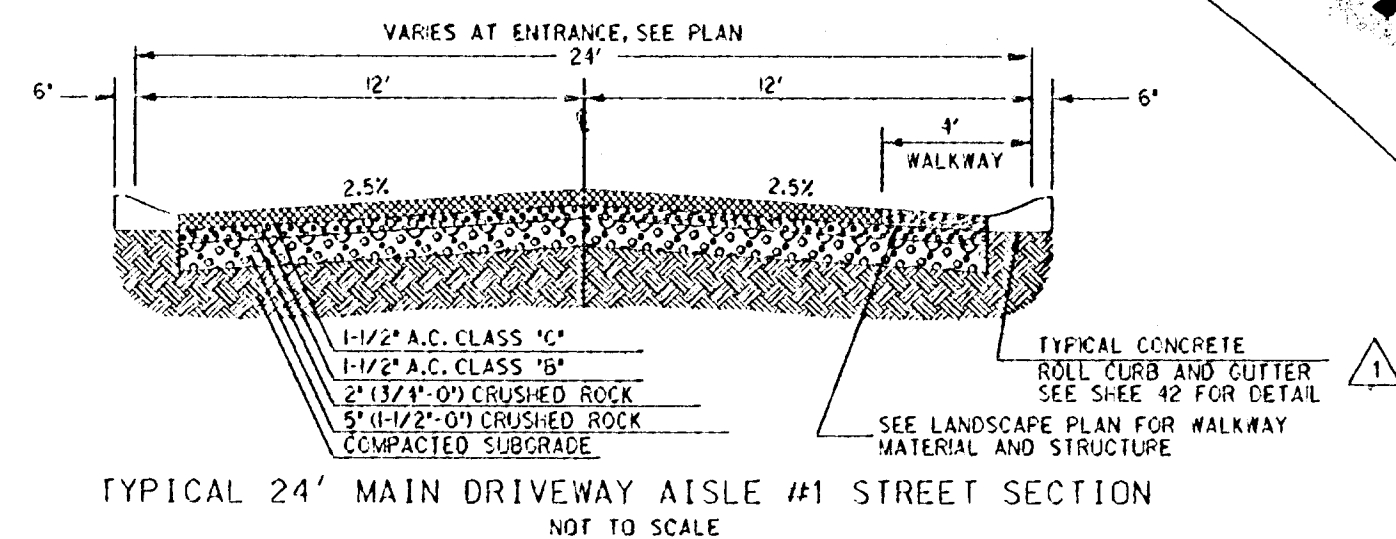
W.B. WELLS
and associates inc.
ENGINEERS • SURVEYORS • PLANNERS
4230 NE FREMONT STREET
PORTLAND, OREGON 97213
PHONE (503) 284-5896
FAX (503) 284-5530



ADDED NOTE
CHANGED SYMBOL
FLIPPED CLUBHOUSE AND CHANGED PARKING

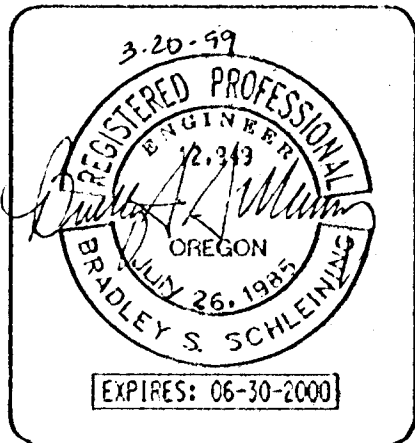
LEGEND

- EXTRUDED CURB
- DRIVEWAY/PARKING AISLE
- SIDEWALK
- WALKWAY OF DIFFERENT MATERIAL
- PARKING PAD/DRIVEWAY
- VISION TRIANGLES

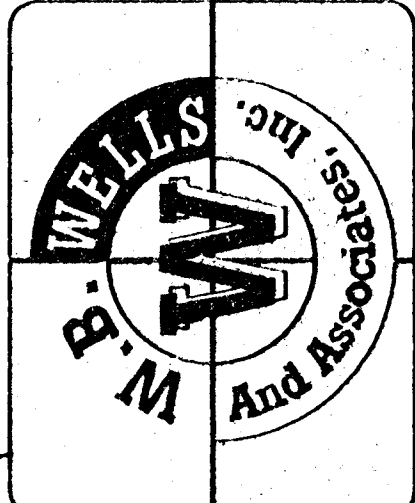


| REVISIONS | | | |
|-----------|--|-----------------|----------------|
| 3-20-99 | | DESIGNED: BSS | CHECKED: BSS |
| | | DRAWN: BSS/RT | DATE: 11-20-98 |
| | | JOB NO.: 96-340 | SCALE: |

SITE PLAN SHEET 1 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



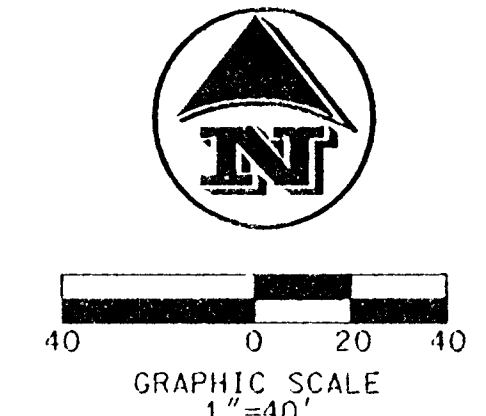
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APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



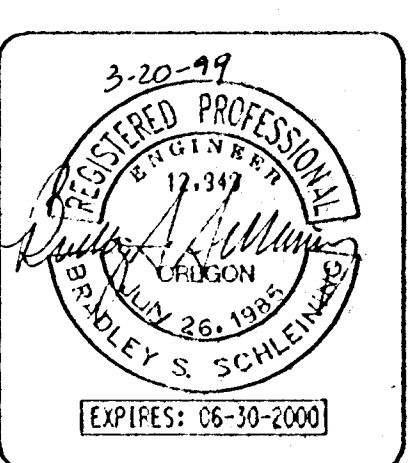
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- △ CHANGED WALL SYMBOL
- △ CHANGED SYMBOL



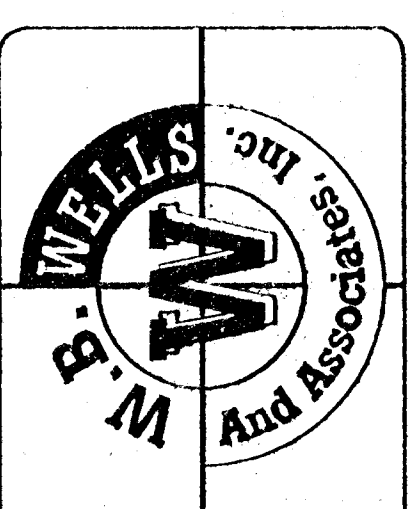
- LEGEND**
- EXTRUDED CURB
 - DRIVEWAY/PARKING AISLE
 - SIDEWALK
 - WALKWAY OF DIFFERENT MATERIAL
 - PARKING PAD/DRIVEWAY
 - △ VISION TRIANGLES

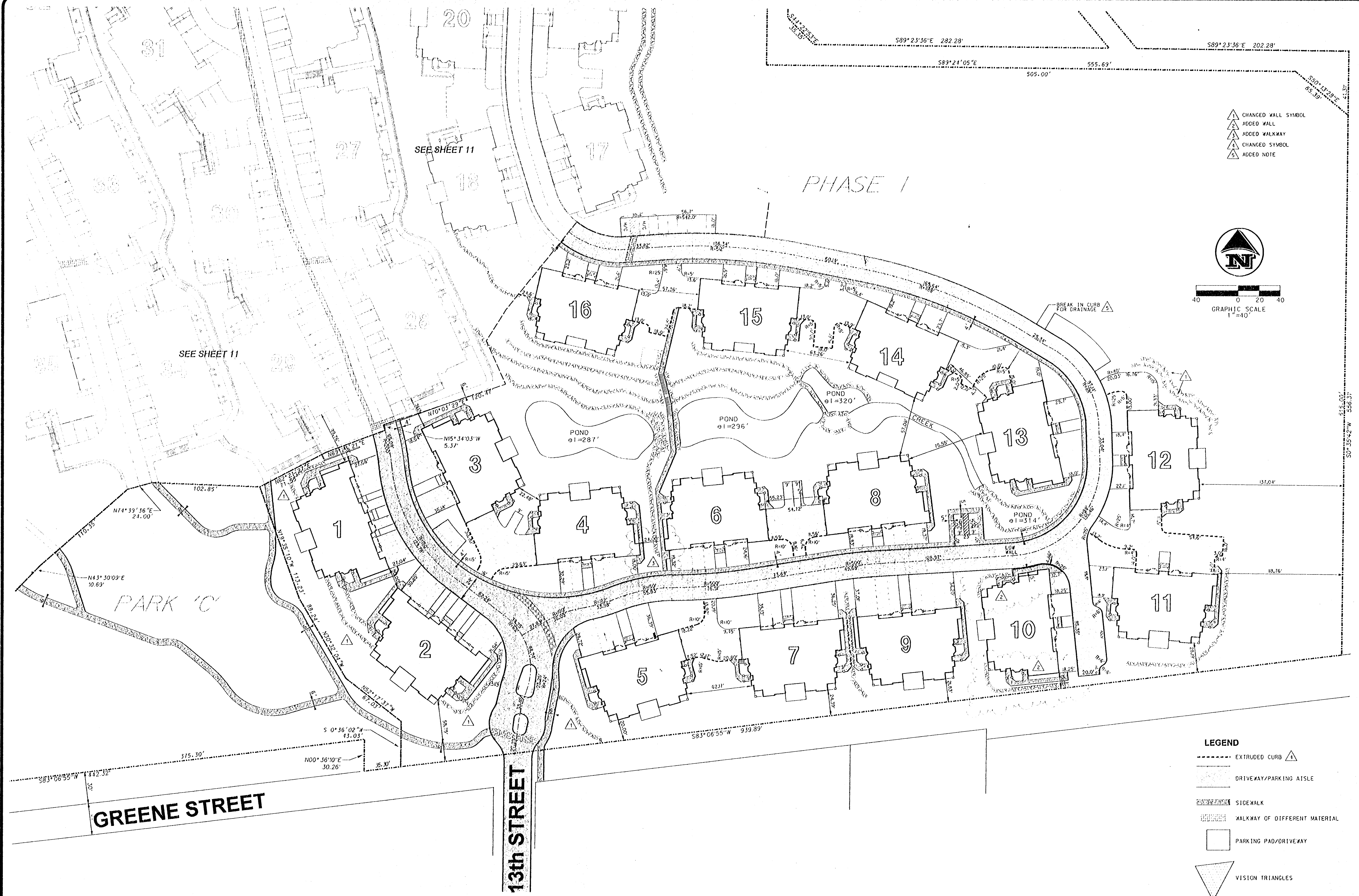
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|-----------|--|-----------------|----------------|
| 3-20-99 | | DESIGNED: BSS | CHECKED: BSS |
| | | DRAWN: BSS/RT | DATE: 11-20-98 |
| | | JOB NO.: 96-340 | SCALE: |

SITE PLAN SHEET 2 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

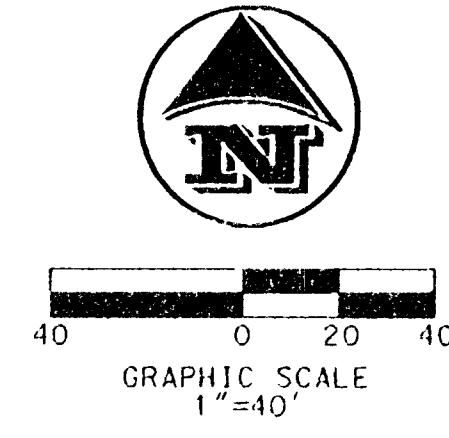


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- CHANGED WALL SYMBOL
- ADDED WALL
- ADDED WALKWAY
- CHANGED SYMBOL
- ADDED NOTE



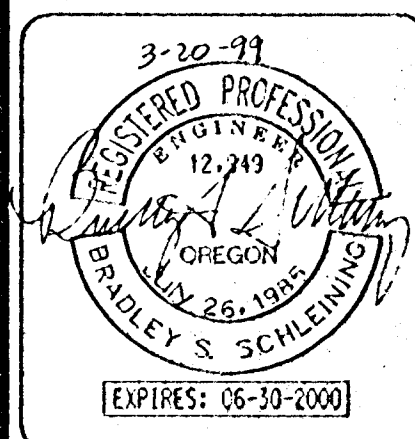
- LEGEND**
- EXTRUDED CURB
 - DRIVEWAY/PARKING AISLE
 - SIDEWALK
 - WALKWAY OF DIFFERENT MATERIAL
 - PARKING PAD/DRIVEWAY
 - VISION TRIANGLES

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

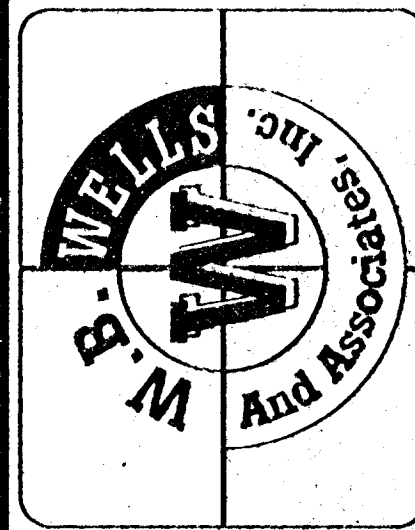
DR-97-30/ZC-97-04/MISC-97-59/MISC-97-60

| REVISIONS | | | |
|-----------------|----------------|--|--|
| 3-20-99 | | | |
| DESIGNED: BSS | CHECKED: BSS | | |
| DRAWN: BSS/RT | DATE: 11-20-98 | | |
| JOB NO.: 98-340 | SCALE: | | |

SITE PLAN SHEET 3 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



W.B. WELLS
and associates inc.
ENGINEERS • SURVEYORS • PLANNERS
4230 NE FREMONT STREET
PORTLAND, OREGON 97213
PHONE (503) 284-5896
FAX (503) 284-8530



| NO. | NORTH | EAST | NO. | NORTH | EAST | NO. | NORTH | EAST | NO. | NORTH | EAST | NO. | NORTH | EAST |
|-----|-----------|----------|-----|-----------|----------|-----|-----------|----------|-----|----------|----------|-----|----------|----------|
| 85 | 10287.403 | 8988.453 | 114 | 10091.248 | 8860.238 | 143 | 10042.713 | 8559.187 | 172 | 9739.305 | 8497.505 | 201 | 9539.142 | 8324.273 |
| 86 | 10157.932 | 8956.324 | 115 | 10025.103 | 8815.229 | 144 | 10142.741 | 8514.623 | 173 | 9553.142 | 8457.400 | 202 | 9536.154 | 8297.088 |
| 87 | 10157.423 | 9031.553 | 116 | 9965.248 | 8848.792 | 145 | 9956.238 | 8532.330 | 174 | 9472.877 | 8452.722 | 203 | 9536.273 | 8235.806 |
| 88 | 10265.713 | 9050.401 | 117 | 10117.434 | 8694.179 | 146 | 9824.234 | 8562.682 | 175 | 9527.243 | 8489.498 | | | |
| 89 | 10267.698 | 8920.986 | 118 | 9990.653 | 8728.815 | 147 | 9851.164 | 8623.317 | 176 | 9502.308 | 8453.171 | | | |
| 90 | 10149.873 | 8889.953 | 119 | 10018.039 | 8788.443 | 148 | 9860.426 | 8598.432 | 177 | 9371.123 | 8497.216 | | | |
| 91 | 10138.349 | 8827.305 | 120 | 10124.123 | 8759.473 | 149 | 9777.061 | 8599.543 | 178 | 9445.626 | 8600.819 | | | |
| 92 | 10256.325 | 8841.763 | 121 | 9938.424 | 8753.389 | 150 | 9843.564 | 8627.553 | 179 | 9368.539 | 8581.051 | | | |
| 93 | 10127.768 | 8959.456 | 122 | 9819.511 | 8808.925 | 151 | 9872.368 | 8684.824 | 180 | 9323.452 | 8633.238 | | | |
| 94 | 10001.572 | 8935.103 | 123 | 9956.425 | 8863.066 | 152 | 9780.359 | 8685.096 | 181 | 9434.885 | 8558.002 | | | |
| 95 | 10023.364 | 9054.287 | 124 | 9956.044 | 8816.616 | 153 | 9860.393 | 8507.472 | 182 | 9381.827 | 8476.261 | | | |
| 96 | 10135.514 | 9023.614 | 125 | 9764.599 | 8826.114 | 154 | 9915.400 | 8413.675 | 183 | 9516.069 | 8453.203 | | | |
| 97 | 9381.707 | 8938.111 | 126 | 9640.128 | 8868.901 | 155 | 9956.134 | 8405.604 | 184 | 9467.775 | 8483.181 | | | |
| 98 | 9352.811 | 9023.783 | 127 | 9671.095 | 8926.773 | 156 | 9870.504 | 8428.461 | 185 | 9622.522 | 8440.711 | | | |
| 99 | 9875.374 | 9085.197 | 128 | 9775.134 | 8931.488 | 157 | 10019.467 | 8452.426 | 186 | 9715.757 | 8366.228 | | | |
| 100 | 9383.783 | 9063.774 | 129 | 9670.754 | 8906.627 | 158 | 10112.006 | 8485.097 | 187 | 9653.673 | 8318.020 | | | |
| 101 | 9806.703 | 9050.138 | 130 | 9785.816 | 8775.058 | 159 | 10052.344 | 8332.370 | 188 | 9601.518 | 8363.352 | | | |
| 102 | 9686.303 | 9104.159 | 131 | 9776.125 | 8712.313 | 160 | 9936.333 | 8374.110 | 189 | 9757.347 | 8332.524 | | | |
| 103 | 9123.319 | 9159.767 | 132 | 9658.299 | 8728.062 | 161 | 9955.378 | 8170.719 | 190 | 9851.192 | 8258.041 | | | |
| 104 | 9823.520 | 9113.584 | 133 | 9348.045 | 8639.890 | 162 | 10036.150 | 8059.668 | 191 | 9789.038 | 8209.833 | | | |
| 105 | 9685.030 | 9045.938 | 134 | 9839.333 | 8748.105 | 163 | 10085.117 | 8103.376 | 192 | 9736.343 | 8255.764 | | | |
| 106 | 9732.357 | 9389.413 | 135 | 9824.535 | 8670.336 | 164 | 10026.342 | 8136.258 | 193 | 9937.035 | 8114.253 | | | |
| 107 | 9708.375 | 9311.424 | 136 | 9839.208 | 8645.104 | 165 | 9828.204 | 8343.380 | 194 | 9939.322 | 8009.546 | | | |
| 108 | 9673.028 | 9366.583 | 137 | 10001.693 | 8677.120 | 166 | 9725.528 | 8425.405 | 195 | 9922.355 | 7938.455 | | | |
| 109 | 9303.801 | 9376.636 | 138 | 10115.531 | 8641.306 | 167 | 9774.342 | 8463.284 | 196 | 9890.330 | 8049.818 | | | |
| 110 | 9729.351 | 9355.864 | 139 | 10063.550 | 8581.047 | 168 | 9800.219 | 8400.678 | 197 | 9913.723 | 8022.665 | | | |
| 111 | 9386.352 | 8839.223 | 140 | 9936.333 | 8539.701 | 169 | 9710.623 | 8438.760 | 198 | 9884.706 | 8236.143 | | | |
| 112 | 9317.384 | 8837.771 | 141 | 10125.633 | 8450.242 | 170 | 9604.138 | 8515.850 | 199 | 9884.742 | 8306.614 | | | |
| 113 | 9389.638 | 8923.515 | 142 | 10006.090 | 8504.719 | 171 | 9650.387 | 8561.383 | 200 | 9528.334 | 8225.432 | | | |

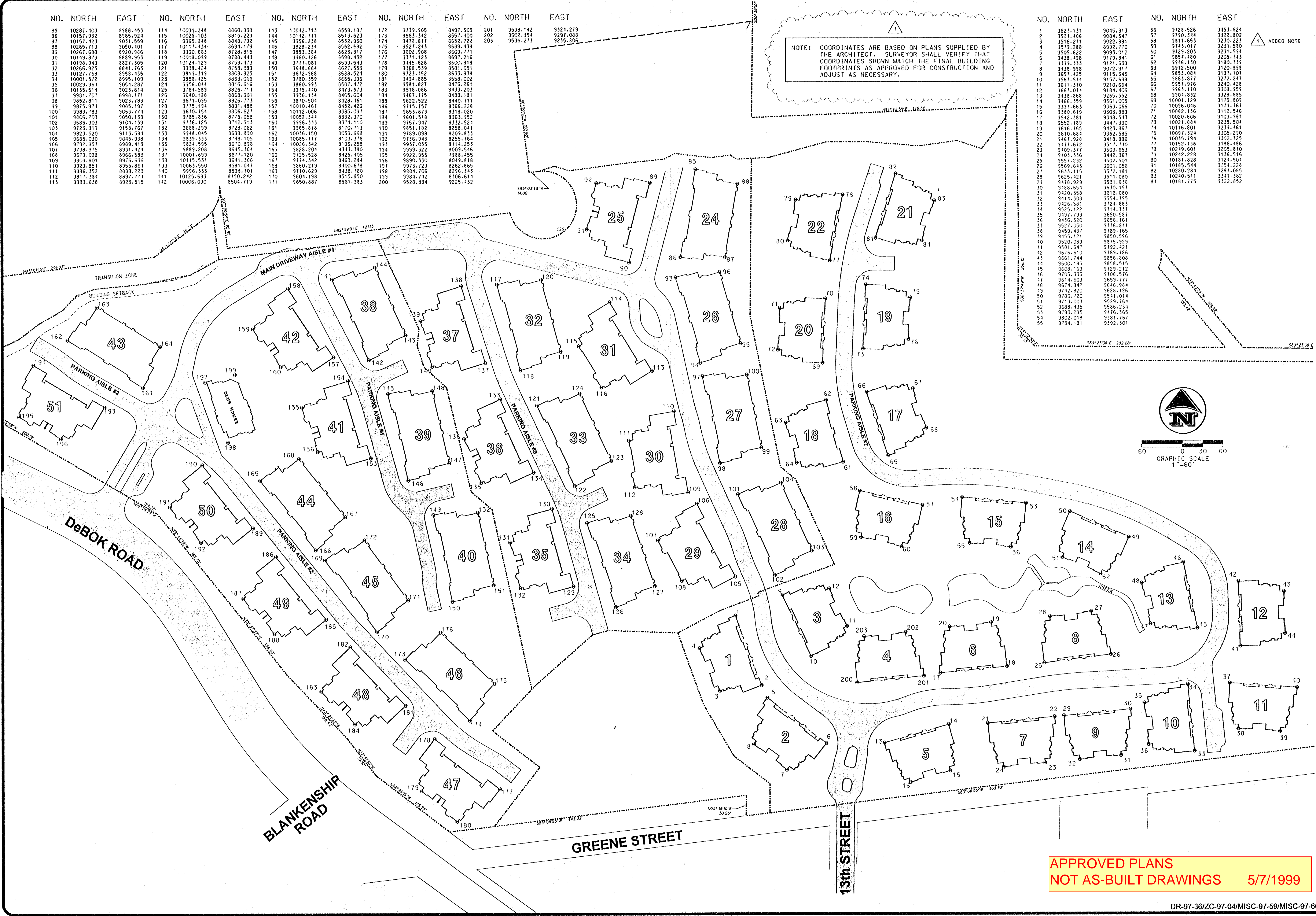
NOTE: COORDINATES ARE BASED ON PLANS SUPPLIED BY THE ARCHITECT. SURVEYOR SHALL VERIFY THAT COORDINATES SHOWN MATCH THE FINAL BUILDING FOOTPRINTS AS APPROVED FOR CONSTRUCTION AND ADJUST AS NECESSARY.

| NO. | NORTH | EAST | NO. | NORTH | EAST | NO. | NORTH | EAST |
|-----|----------|----------|-----|-----------|----------|-----|-------|------|
| 1 | 9627.131 | 9045.313 | 56 | 9739.526 | 9453.624 | | | |
| 2 | 9524.406 | 9044.647 | 57 | 9730.334 | 9302.802 | | | |
| 3 | 9516.271 | 9022.881 | 58 | 9811.653 | 9230.223 | | | |
| 4 | 9579.288 | 8932.770 | 59 | 9743.017 | 9231.580 | | | |
| 5 | 9505.521 | 9033.012 | 60 | 9729.203 | 9231.594 | | | |
| 6 | 9438.458 | 9173.341 | 61 | 9854.480 | 9205.743 | | | |
| 7 | 9339.333 | 9121.639 | 62 | 9346.130 | 9180.739 | | | |
| 8 | 9416.998 | 9072.917 | 63 | 9312.500 | 9120.899 | | | |
| 9 | 9607.425 | 9115.145 | 64 | 9853.684 | 9127.107 | | | |
| 10 | 9567.574 | 9157.693 | 65 | 9883.877 | 9272.247 | | | |
| 11 | 9611.370 | 9210.664 | 66 | 9957.376 | 9240.428 | | | |
| 12 | 9667.074 | 9184.405 | 67 | 9963.170 | 9308.359 | | | |
| 13 | 9438.868 | 9265.552 | 68 | 9904.832 | 9328.685 | | | |
| 14 | 9466.353 | 9361.005 | 69 | 10001.123 | 9175.803 | | | |
| 15 | 9337.863 | 9363.656 | 70 | 10036.046 | 9179.167 | | | |
| 16 | 9320.619 | 9303.883 | 71 | 10032.136 | 9112.546 | | | |
| 17 | 9542.381 | 9348.543 | 72 | 10020.606 | 9103.381 | | | |
| 18 | 9552.189 | 9447.390 | 73 | 10021.881 | 9235.504 | | | |
| 19 | 9616.765 | 9423.867 | 74 | 10116.801 | 9239.461 | | | |
| 20 | 9610.684 | 9362.585 | 75 | 10037.124 | 9305.230 | | | |
| 21 | 9467.928 | 9418.886 | 76 | 10035.794 | 9302.725 | | | |
| 22 | 9477.672 | 9517.710 | 77 | 10152.136 | 9186.486 | | | |
| 23 | 9409.377 | 9503.653 | 78 | 10219.601 | 9205.870 | | | |
| 24 | 9403.336 | 9442.367 | 79 | 10242.228 | 9136.516 | | | |
| 25 | 9557.232 | 9502.501 | 80 | 10181.828 | 9124.504 | | | |
| 26 | 9569.643 | 9601.056 | 81 | 10185.544 | 9254.228 | | | |
| 27 | 9633.115 | 9572.181 | 82 | 10280.284 | 9284.085 | | | |
| 28 | 9625.421 | 9511.080 | 83 | 10240.511 | 9341.362 | | | |
| 29 | 9478.929 | 9531.636 | 84 | 10181.775 | 9322.552 | | | |
| 30 | 9488.654 | 9630.157 | | | | | | |
| 31 | 9420.358 | 9616.080 | | | | | | |
| 32 | 9414.308 | 9554.795 | | | | | | |
| 33 | 9426.531 | 9724.683 | | | | | | |
| 34 | 9525.122 | 9714.737 | | | | | | |
| 35 | 9497.793 | 9650.587 | | | | | | |
| 36 | 9436.520 | 9656.761 | | | | | | |
| 37 | 9527.050 | 9716.341 | | | | | | |
| 38 | 9459.437 | 9789.165 | | | | | | |
| 39 | 9455.121 | 9850.536 | | | | | | |
| 40 | 9520.089 | 9875.329 | | | | | | |
| 41 | 9581.647 | 9732.421 | | | | | | |
| 42 | 9676.610 | 9789.786 | | | | | | |
| 43 | 9651.744 | 9856.808 | | | | | | |
| 44 | 9600.185 | 9858.515 | | | | | | |
| 45 | 9608.169 | 9729.212 | | | | | | |
| 46 | 9705.335 | 9708.576 | | | | | | |
| 47 | 9614.603 | 9659.777 | | | | | | |
| 48 | 9674.842 | 9646.384 | | | | | | |
| 49 | 9742.820 | 9629.126 | | | | | | |
| 50 | 9730.720 | 9541.014 | | | | | | |
| 51 | 9713.003 | 9529.764 | | | | | | |
| 52 | 9688.435 | 9586.234 | | | | | | |
| 53 | 9733.235 | 9476.365 | | | | | | |
| 54 | 9802.018 | 9181.767 | | | | | | |
| 55 | 9734.181 | 9392.301 | | | | | | |

ADDED NOTE



GRAPHIC SCALE
1"=60'



APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

REVISIONS

3-20-99

DESIGNED: BSS

CHECKED: BSS

DRAWN: BSS/RT

DATE: 11-20-98

JOB NO.: 96-340

SCALE:

BUILDING COORDINATES

for

SUMMERLINN APARTMENTS

CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

3-20-99

REGISTERED PROFESSIONAL ENGINEER

W.B. WELLS

EXPIRES: 06-30-2000

W.B. WELLS and associates inc.

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W.B. WELLS and Associates Inc.

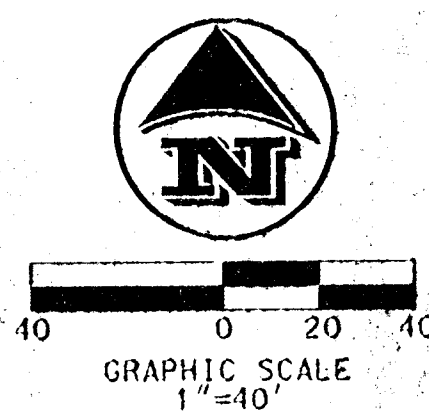
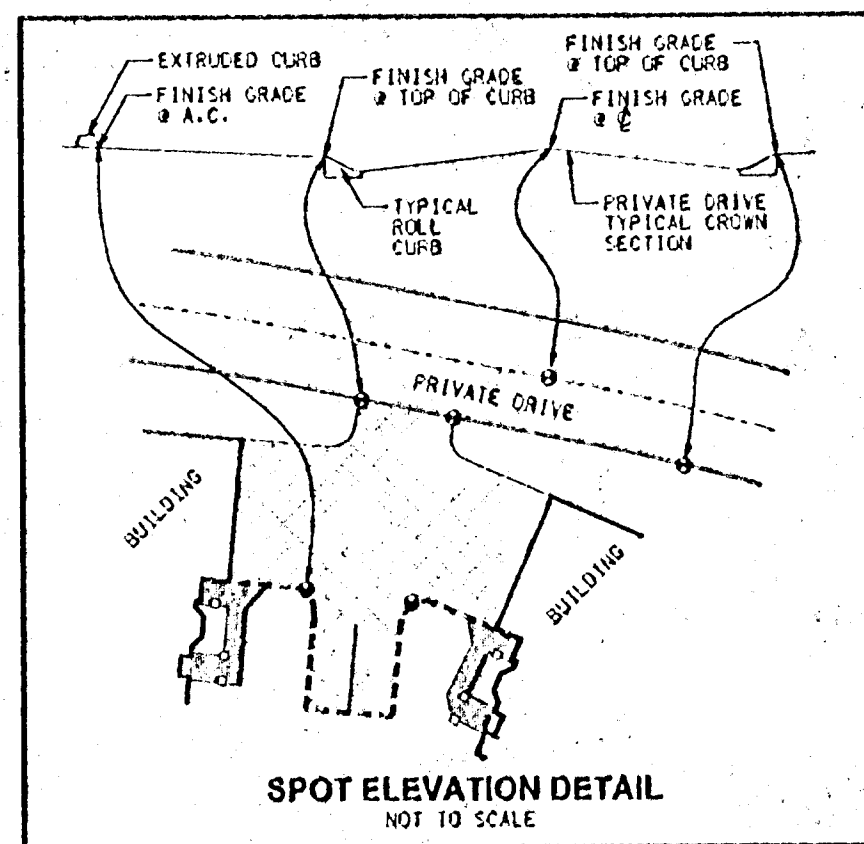
13 of 42

DR-97-36/ZC-97-04/MISC-97-59/MISC-97-60

13 of 42

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

NOTE: WALL DESIGN, STRUCTURAL AND DESIGN LAYOUT, BY OTHERS.
THE GENERAL CONCEPT IS SHOWN ON THESE PLANS.



NOTE
WALLS ARE SHOWN TO DENOTE A CHANGE OF GRADE.
OWNER/CONTRACTOR TO DETERMINE TYPE OF WALL.

LEGEND

- EXISTING CONTOUR
- CATCH POINT
- DESIGN CONTOUR
- SPOT ELEVATION
- FF - FINISH FLOOR ELEVATION
- FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
- FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
- GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY

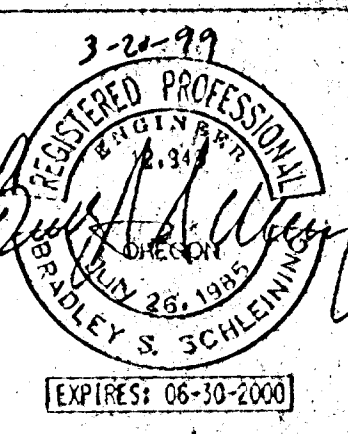
- ADDED TO LEGEND
- ADDED FINISH GARAGE FLOOR ELEVATIONS AND ADJUSTED ALL FINISHED FLOORS
- FLIPPED CLUB HOUSE AND CHANGED PARKING GRADES RAISED FF OF CLUB HOUSE 1.0'
- ALL SPOT ELEVATIONS ON TOP OF CURB HAVE BEEN ADJUSTED DOWN BY 2 1/2' TO REFLECT NEW CURB

SEE SHEET 15

NOTE: SEE LANDSCAPE PLAN FOR
DETAIL BUILDING ENTRANCE GRADING

BLANKENSHIP ROAD

GRADING PLAN - SHEET 1 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



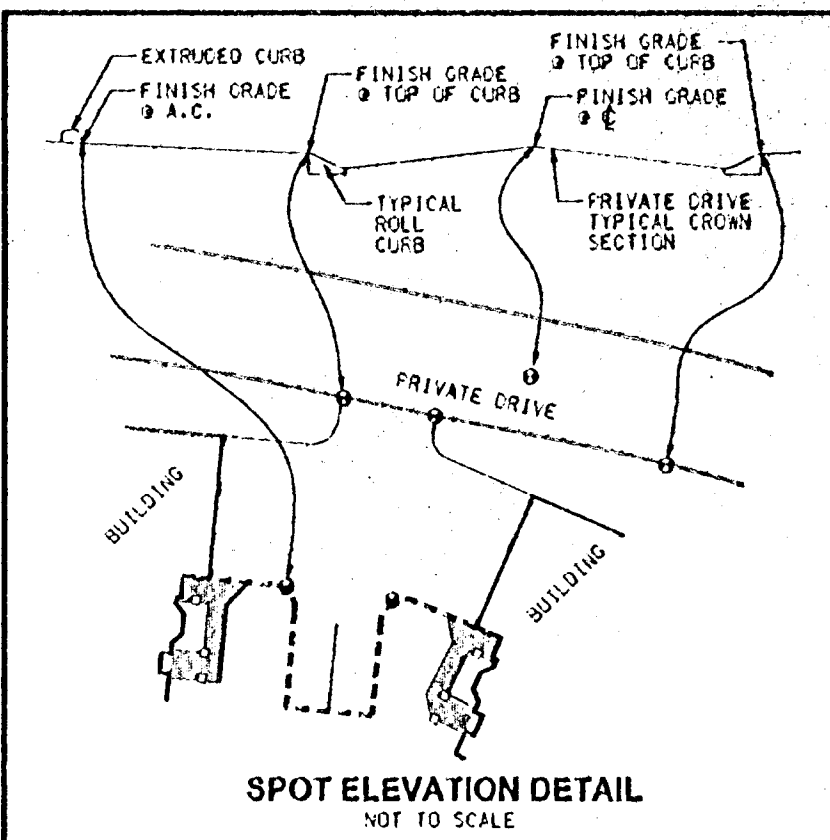
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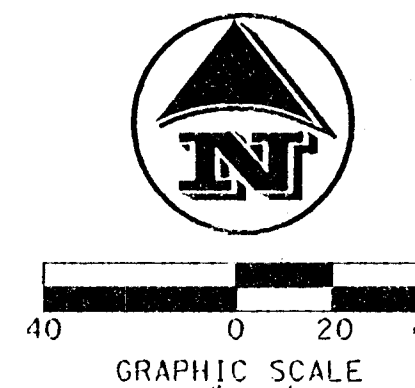
LEGEND

- EXISTING CONTOUR
DESIGN CONTOUR
CATCH POINT
SPOT ELEVATION
FF - FINISH FLOOR ELEVATION
FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY

NOTE: WALL DESIGN, STRUCTURAL AND DESIGN LAYOUT, BY OTHERS.
THE GENERAL CONCEPT IS SHOWN ON THESE PLANS.

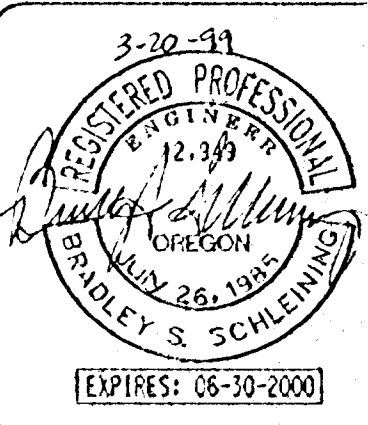


- ADD NOTE TO LEGEND
ADJUST FINISH FLOOR ELEVATIONS, GARAGE FLOORS STAYED THE SAME
ADDED WALLS
CHANGED WALL SYMBOL
ADDED GARAGE FLOOR ELEVATIONS TO ALL BLDGS.
ADJUST GRADES OF FINISH FLOORS BLDGS. 24-37

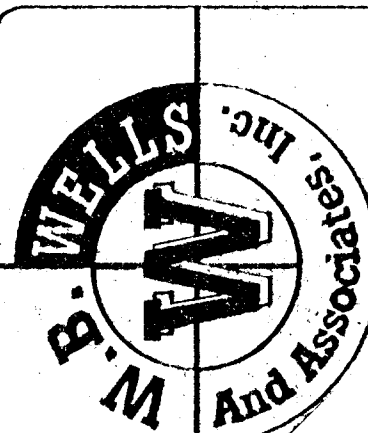


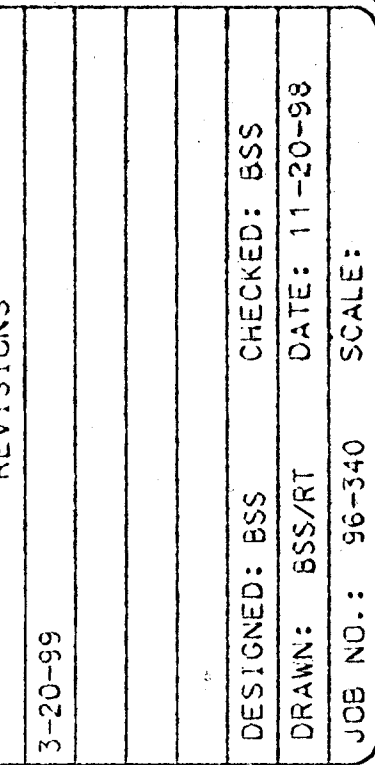
NOTE: SEE LANDSCAPE PLAN FOR
DETAIL BUILDING ENTRANCE GRADING

GRADING PLAN - SHEET 2 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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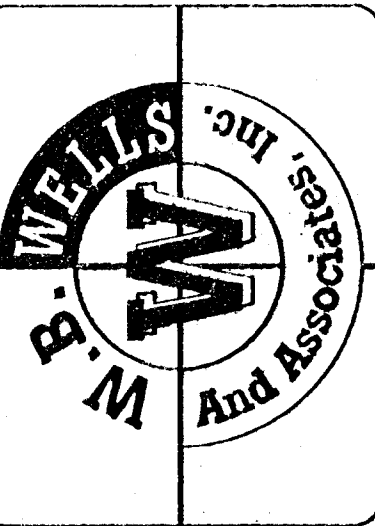




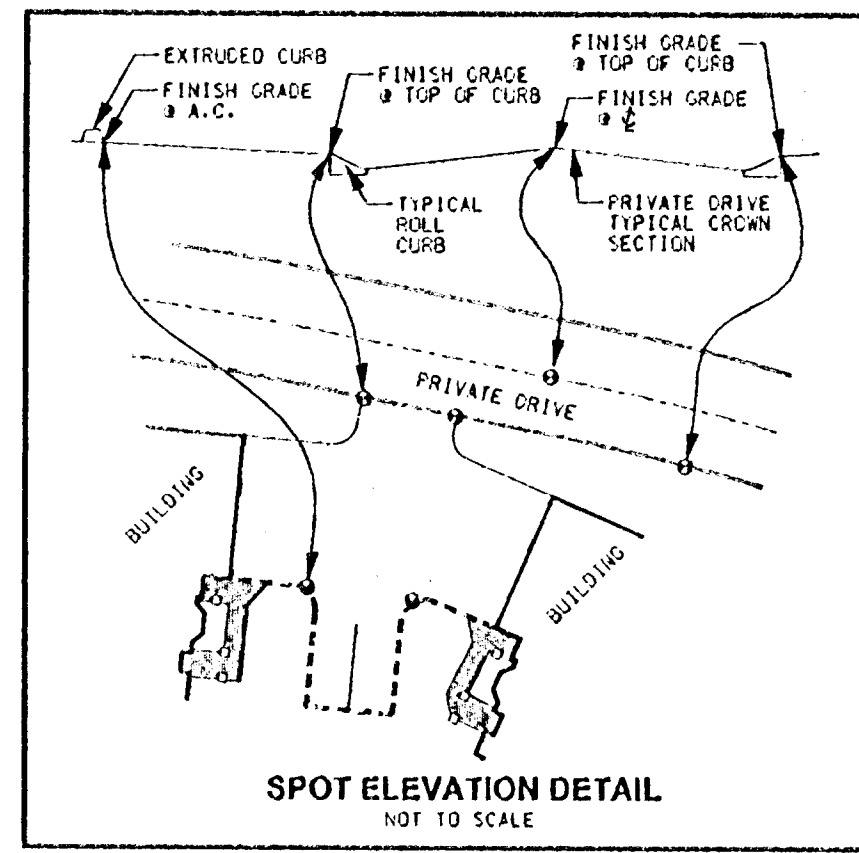
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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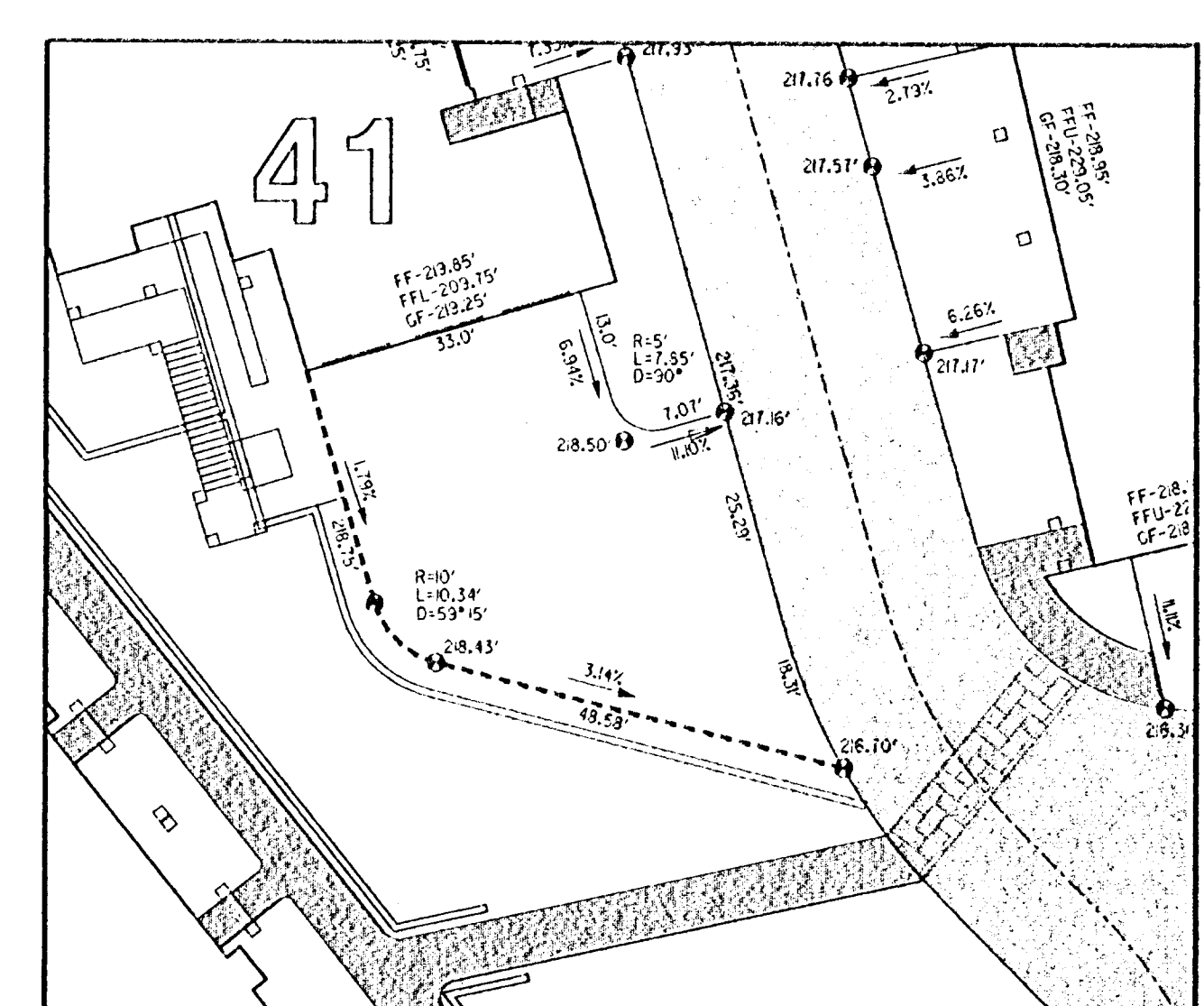
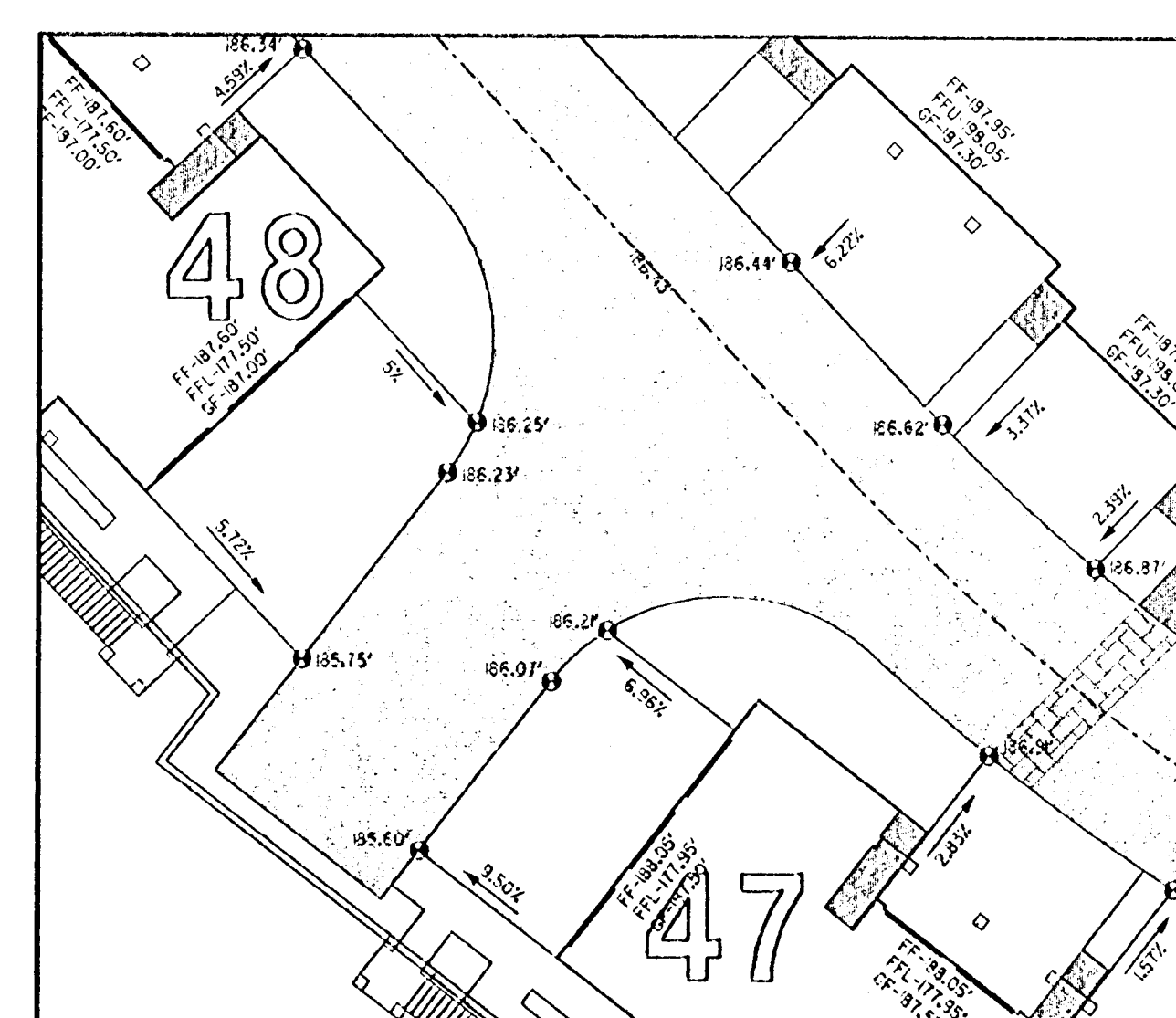
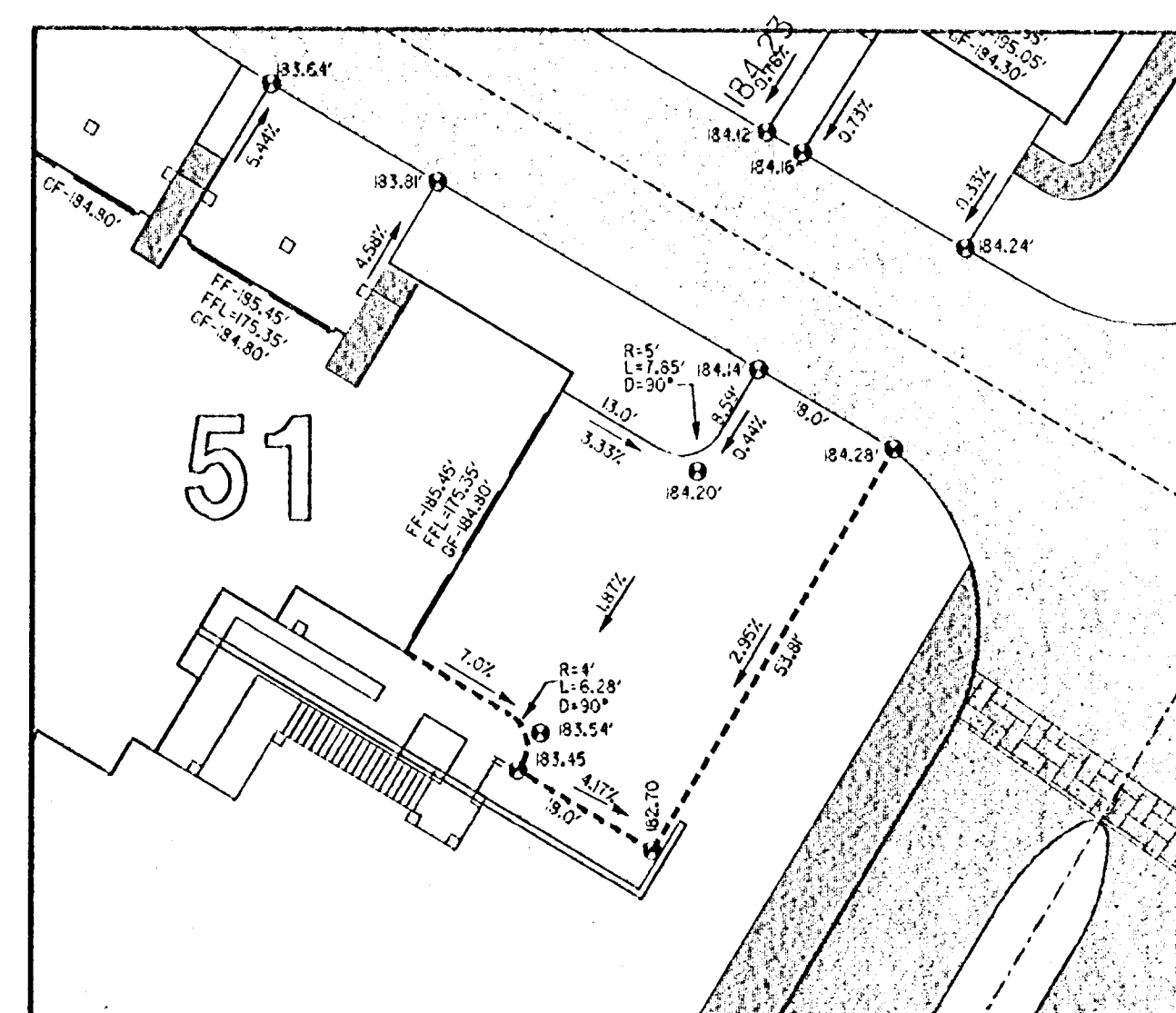
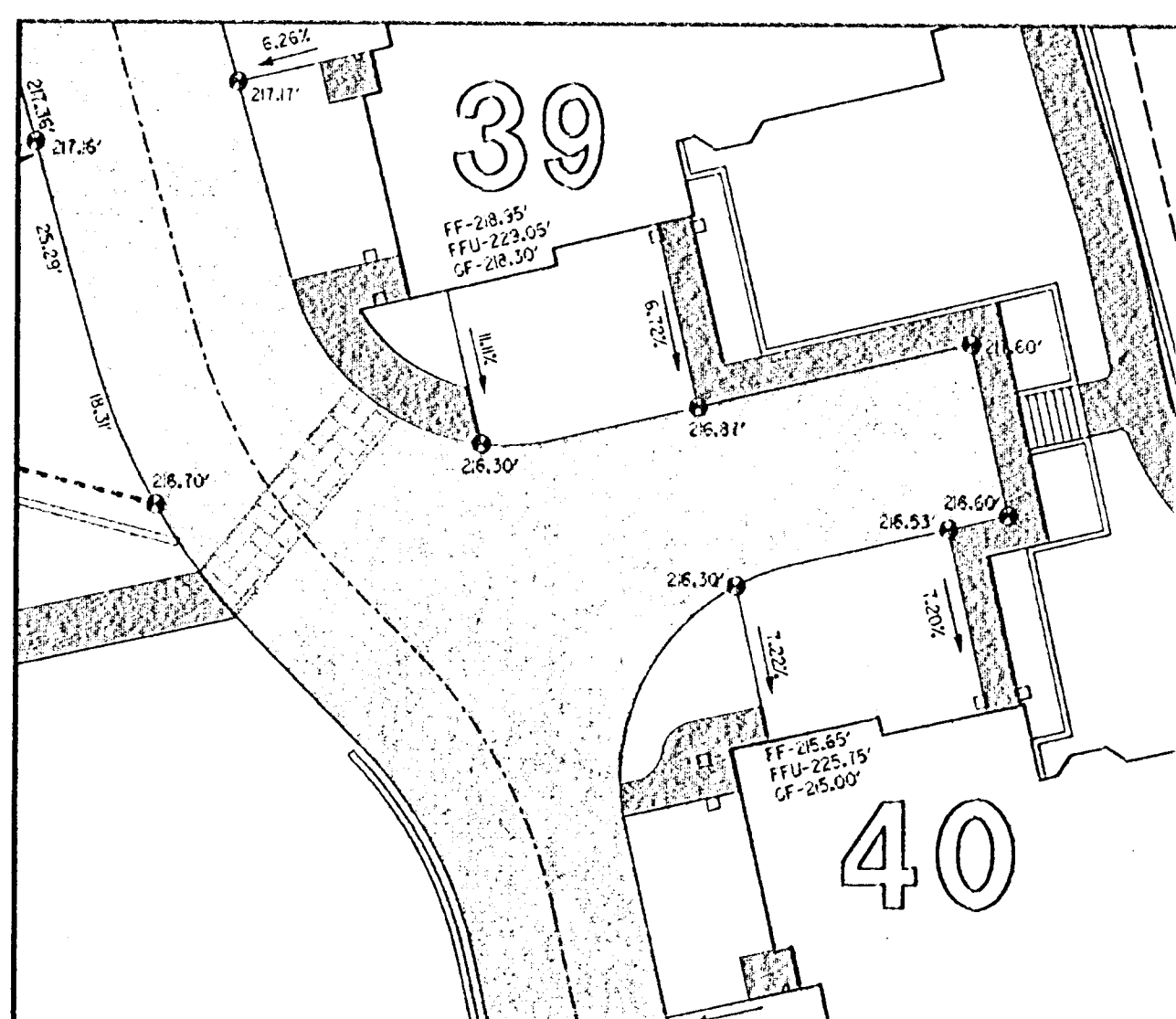
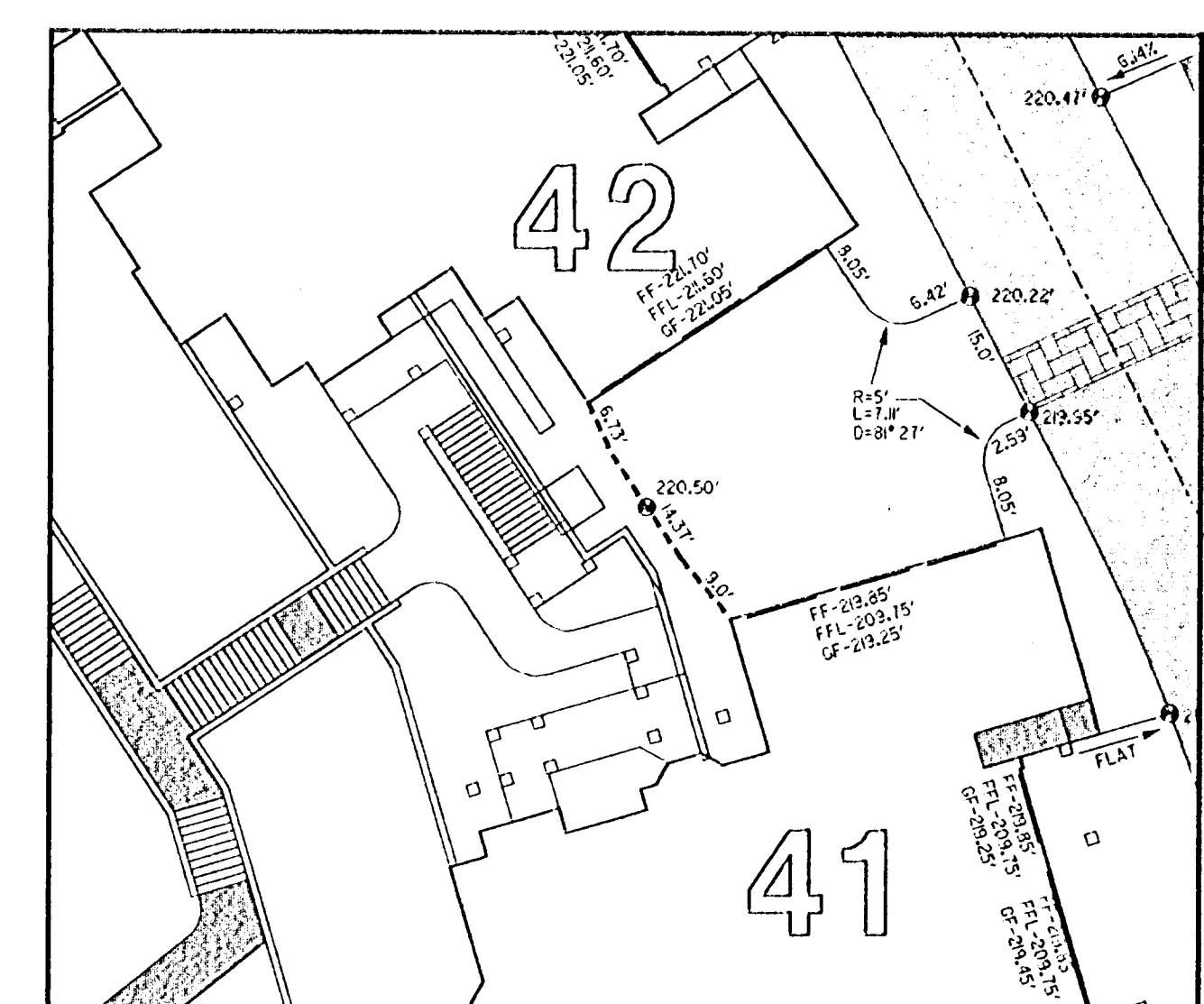
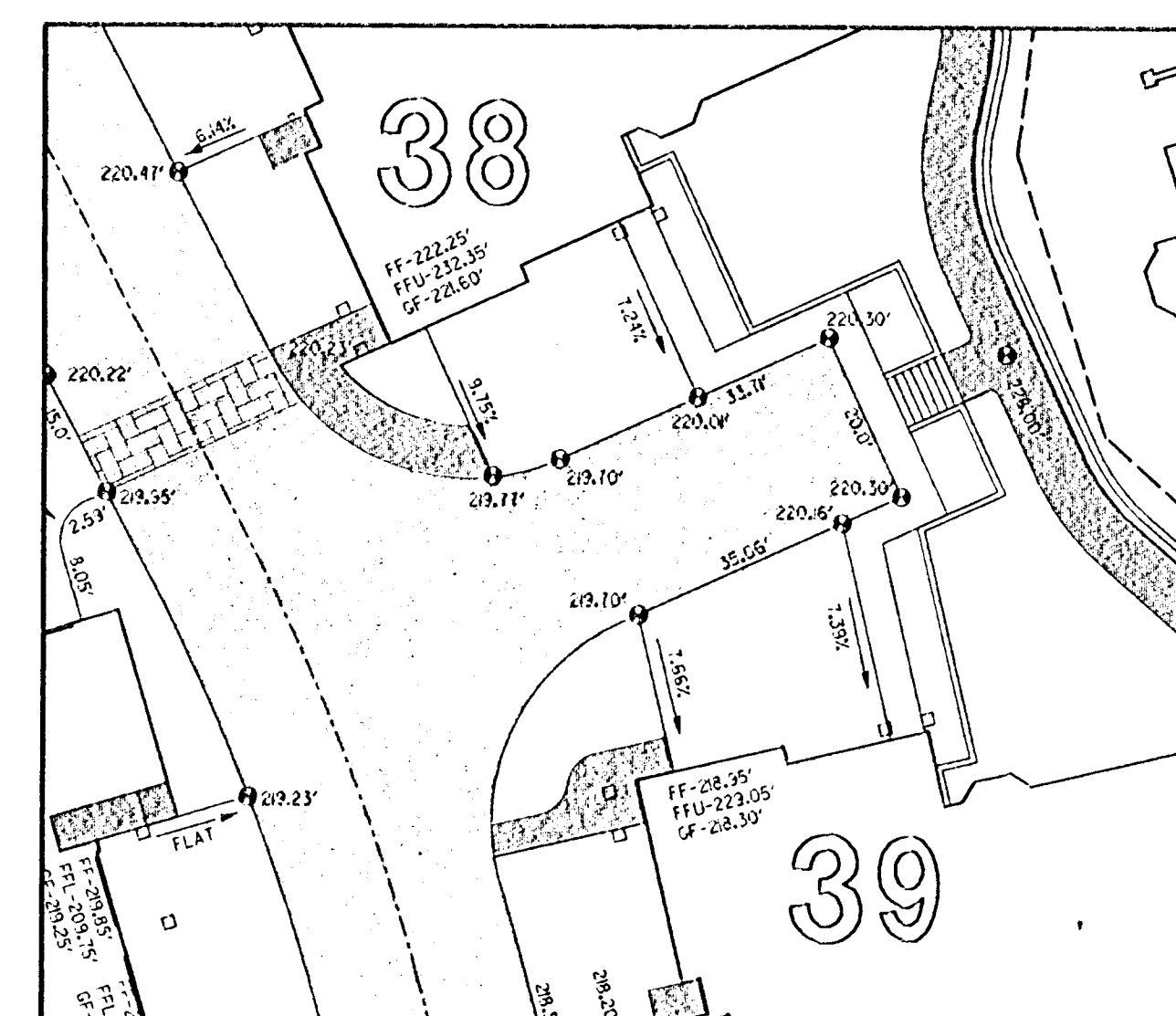
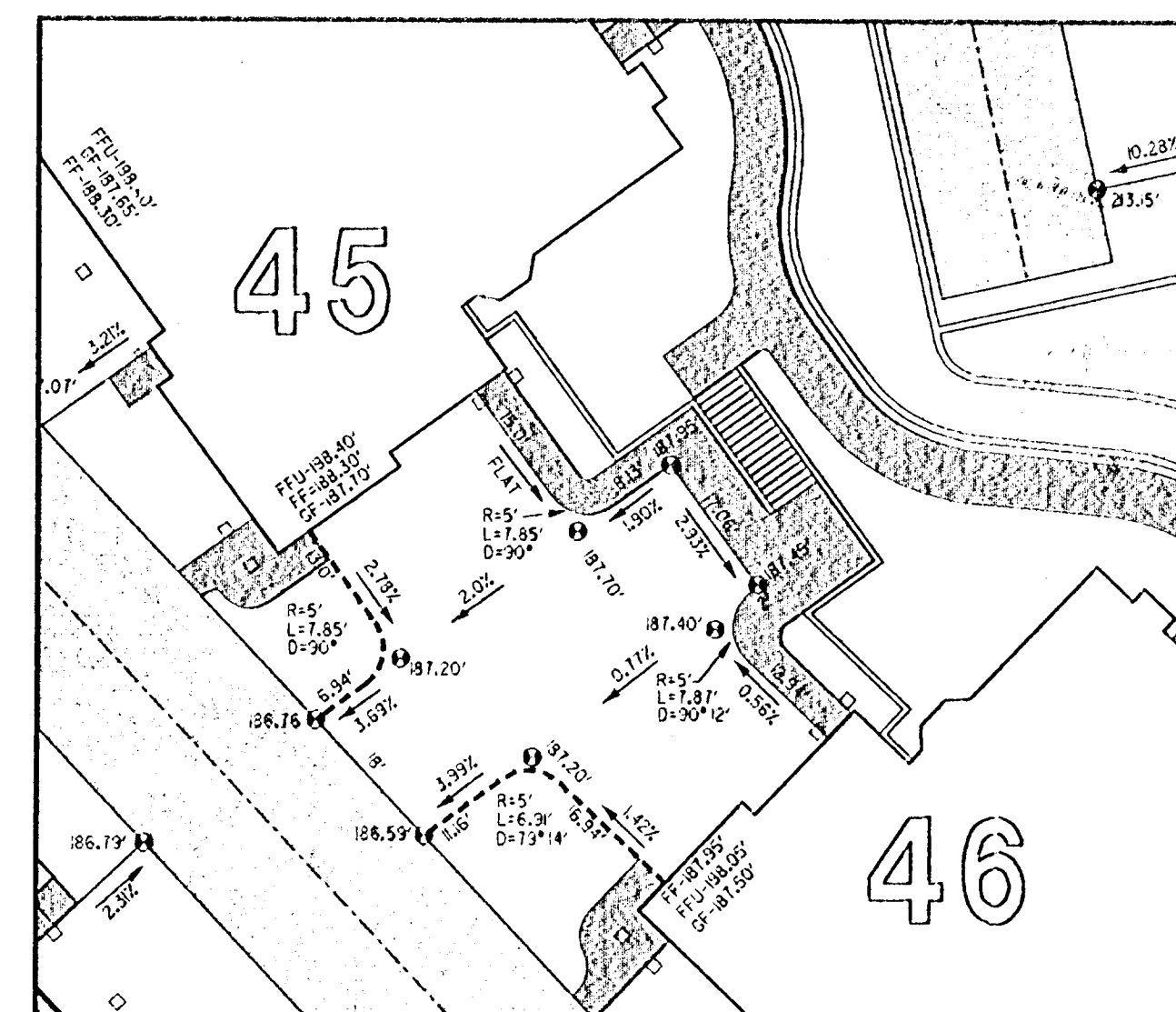
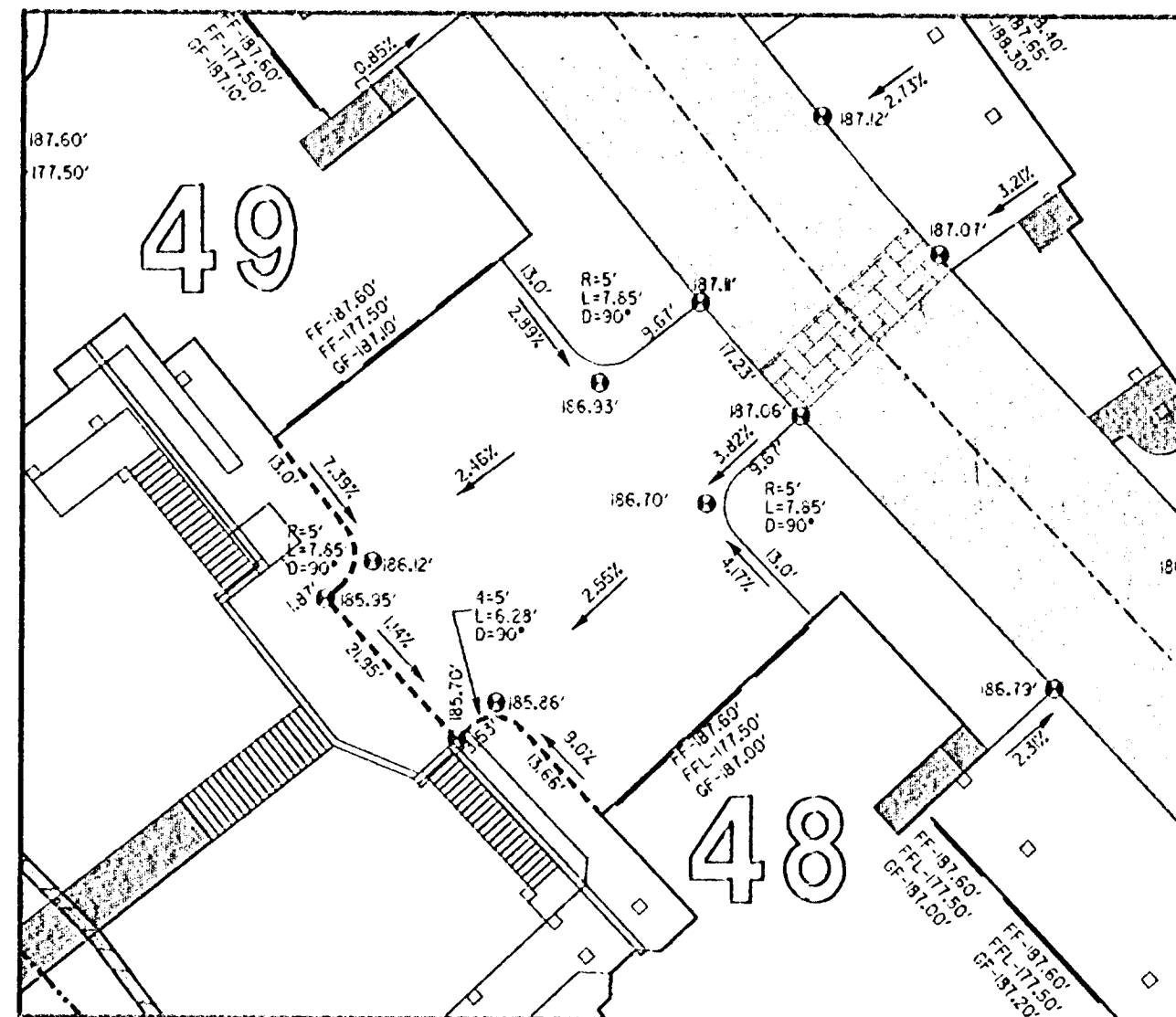
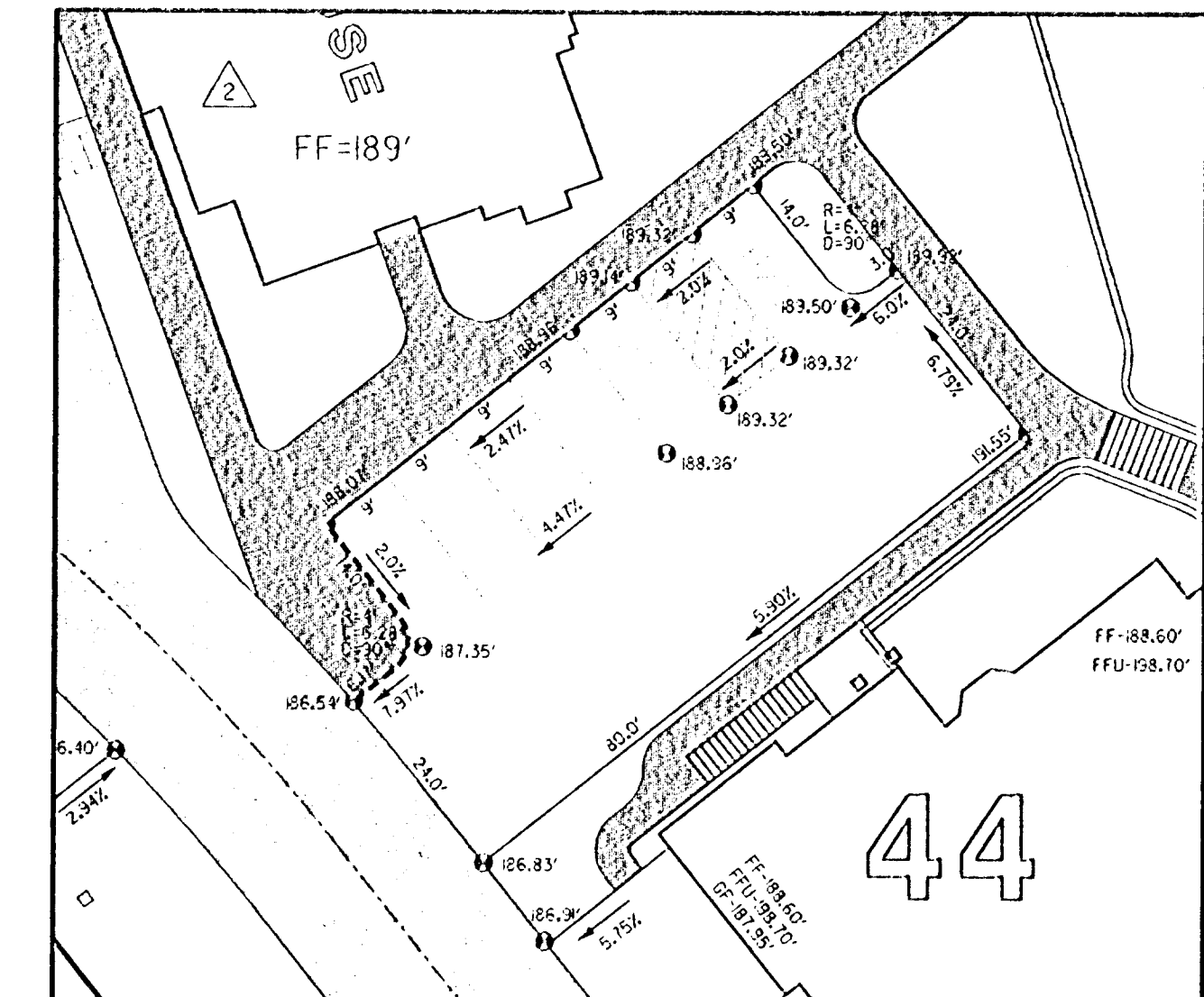
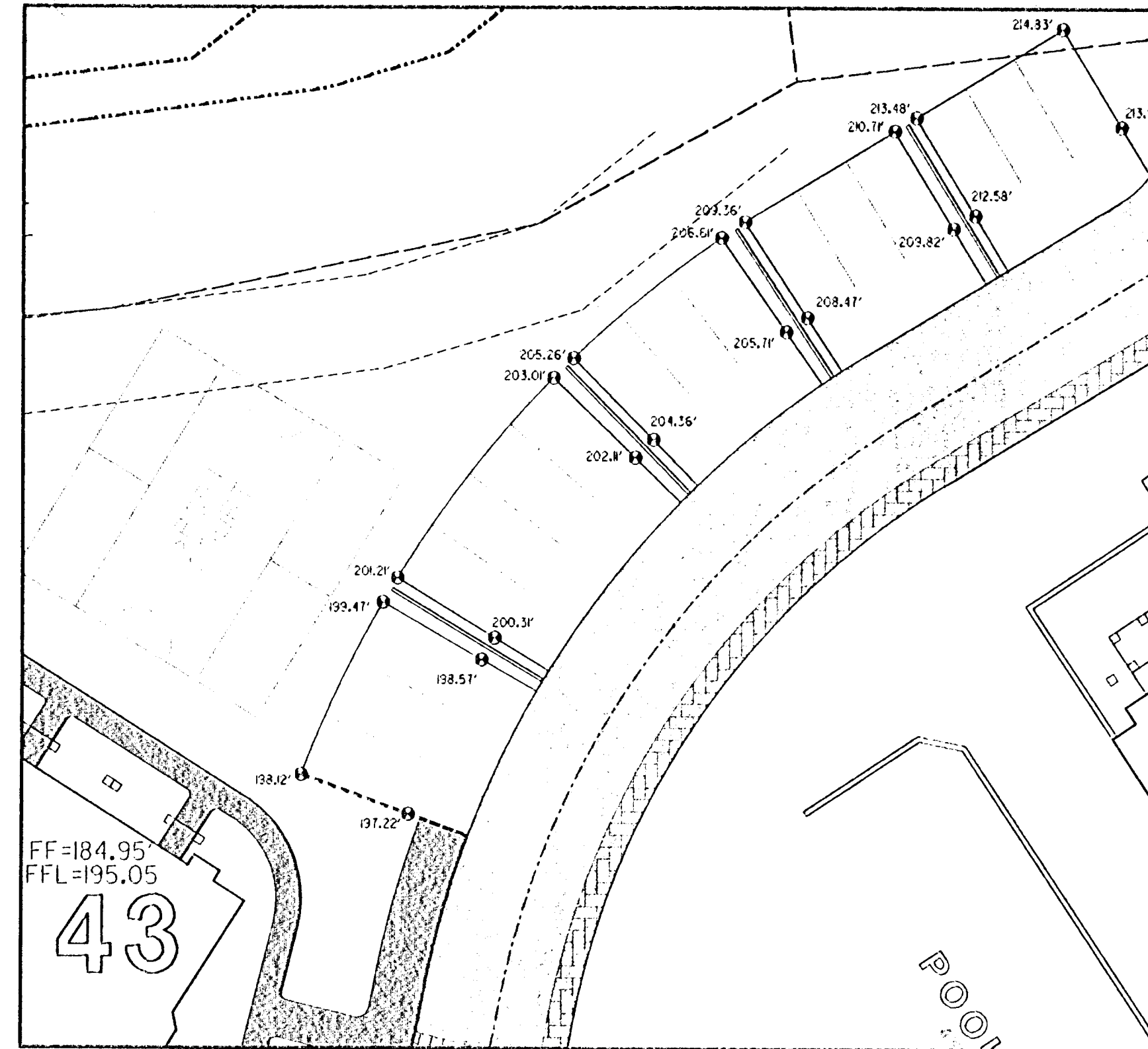


2:17:53



LEGEND

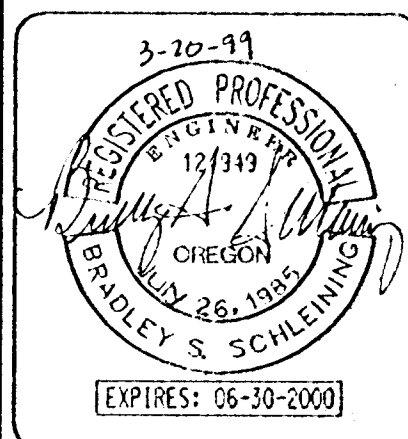
- EXISTING CONTOUR
- DESIGN CONTOUR
- SPOT ELEVATION
- FF - FINISH FLOOR ELEVATION
- FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
- FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
- GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY
- EXTRUDED CURB



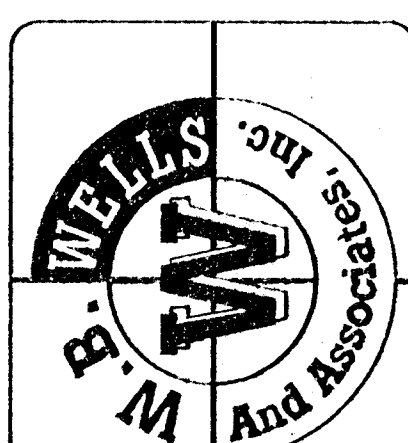
APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

DR-97-38/ZC-97-04/MISC-97-59/MISC-97-60

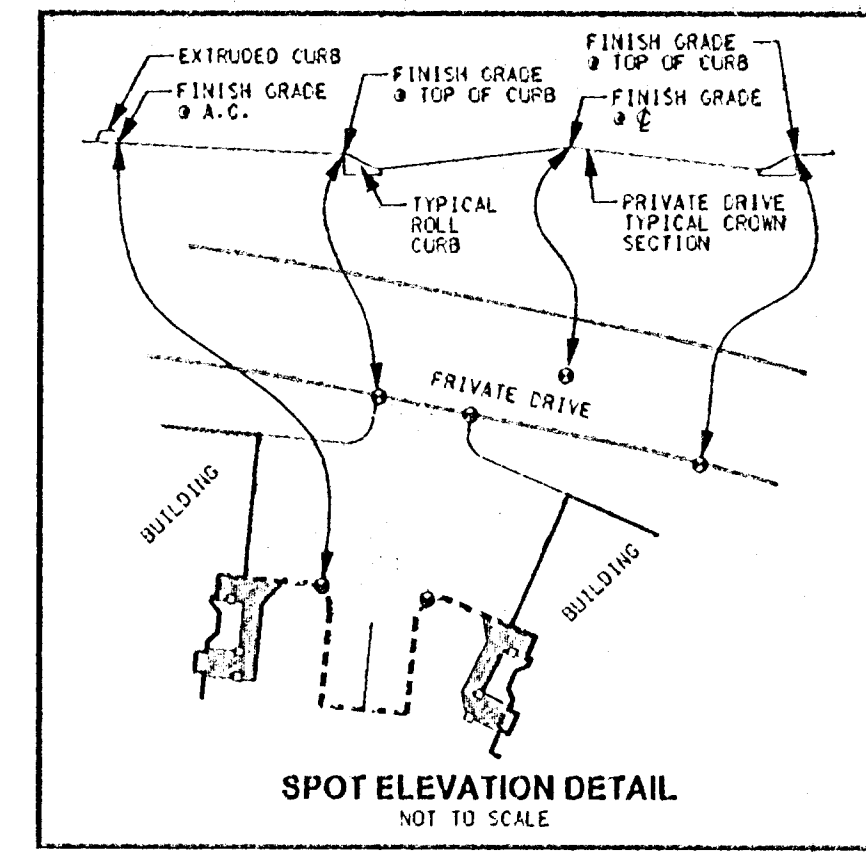
GRADING PLAN DETAILS - SHEET 1 OF 4 for SUMMERLINN APARTMENTS CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



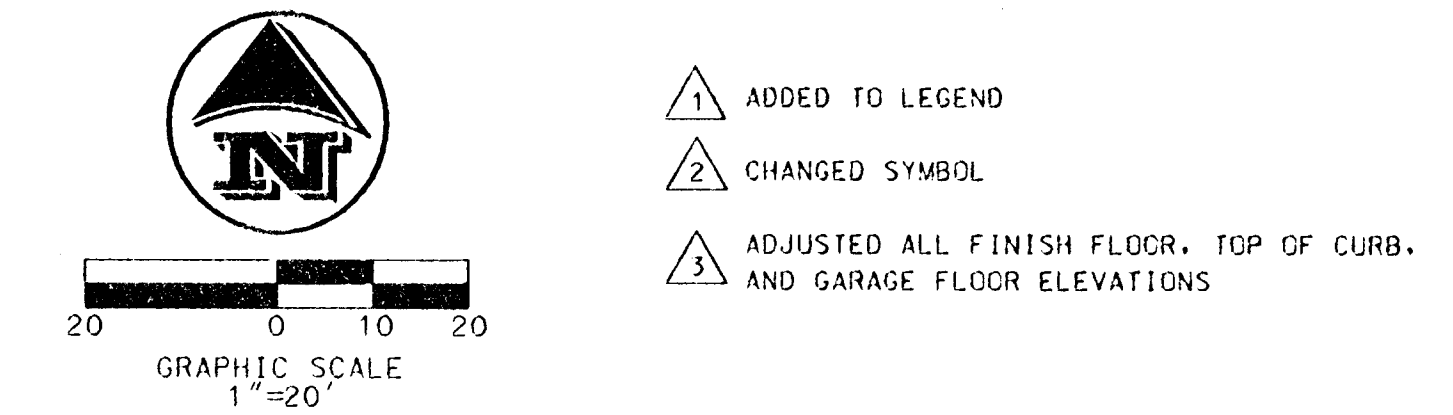
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PORTLAND, OREGON 97213
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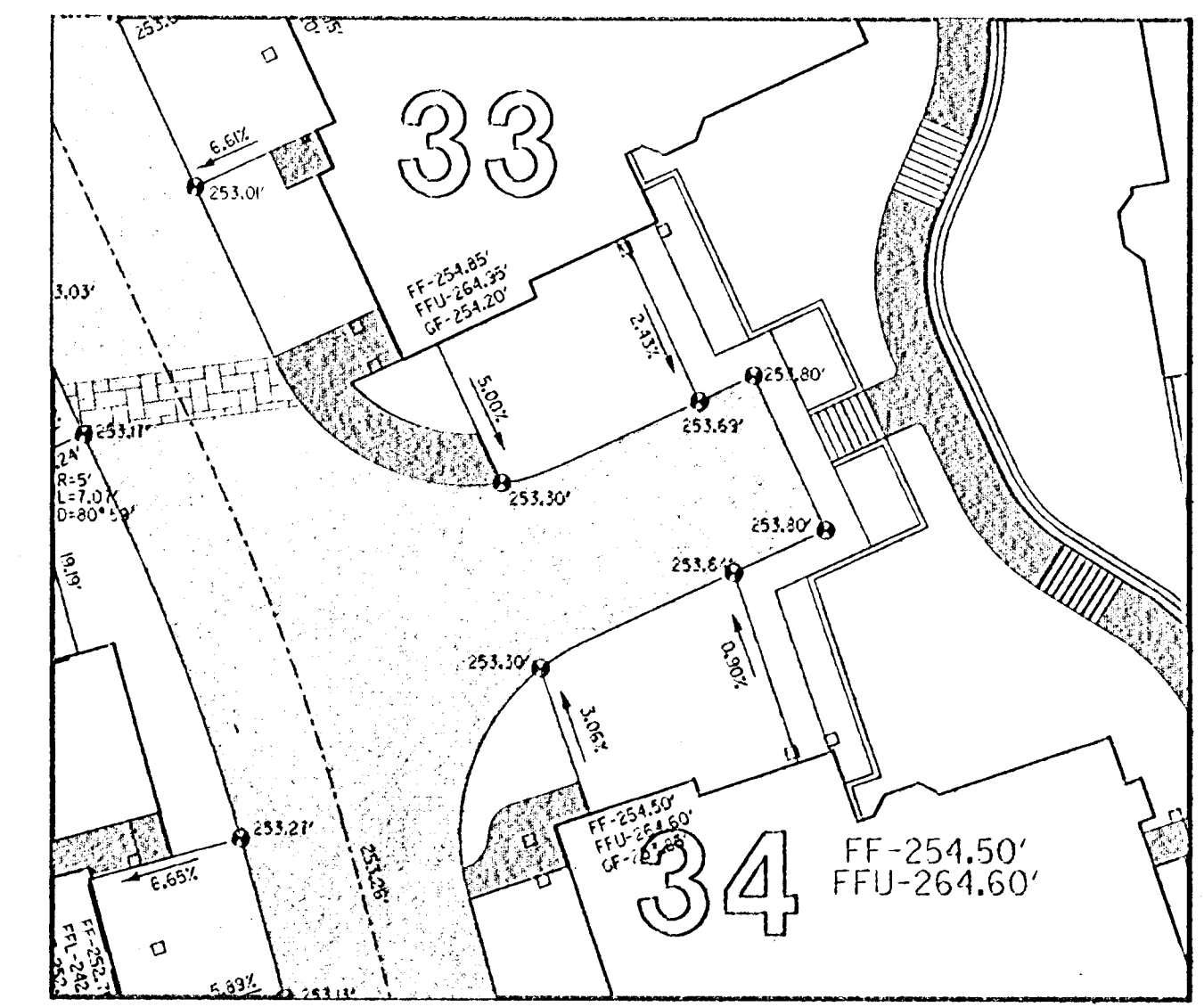
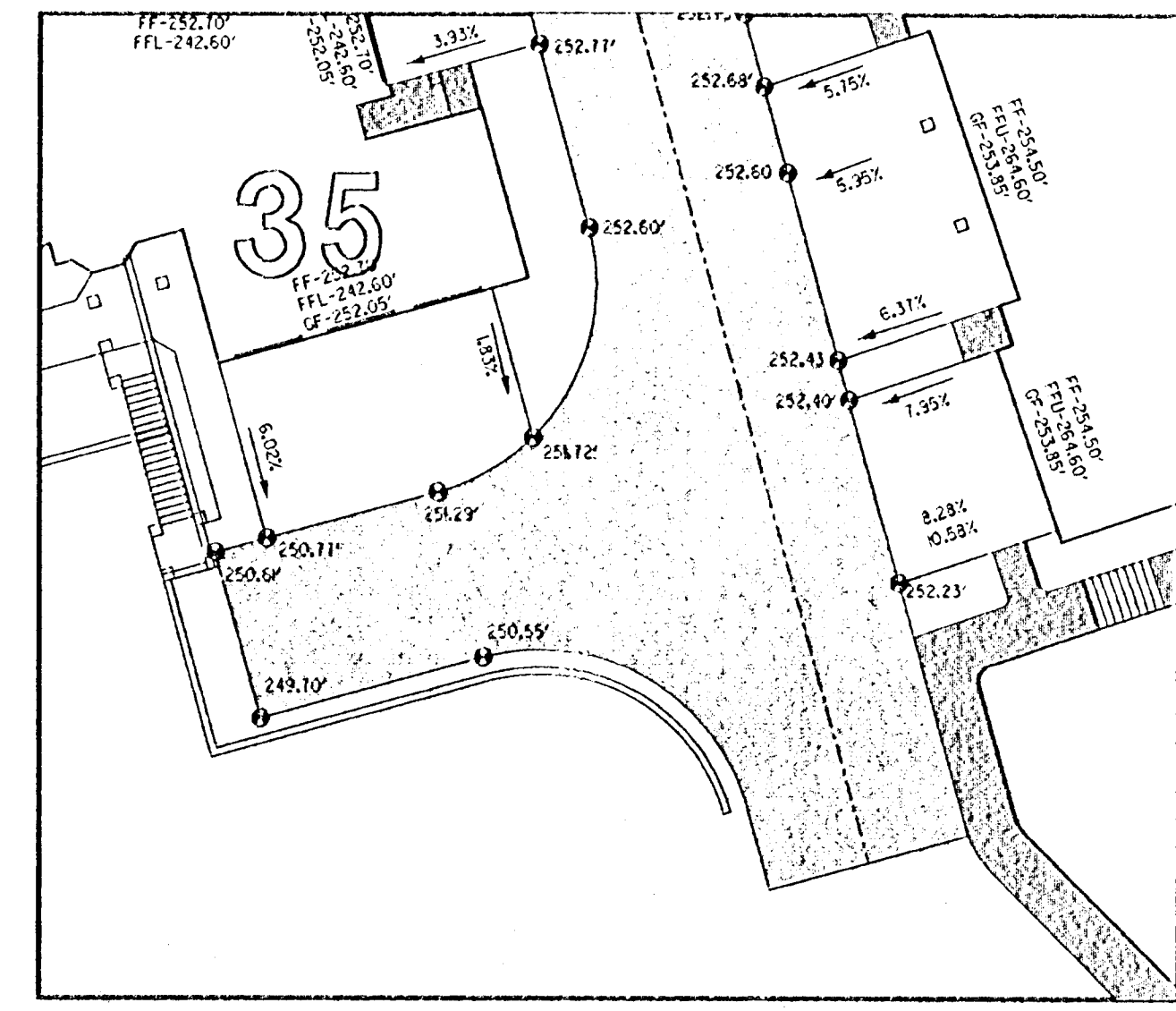
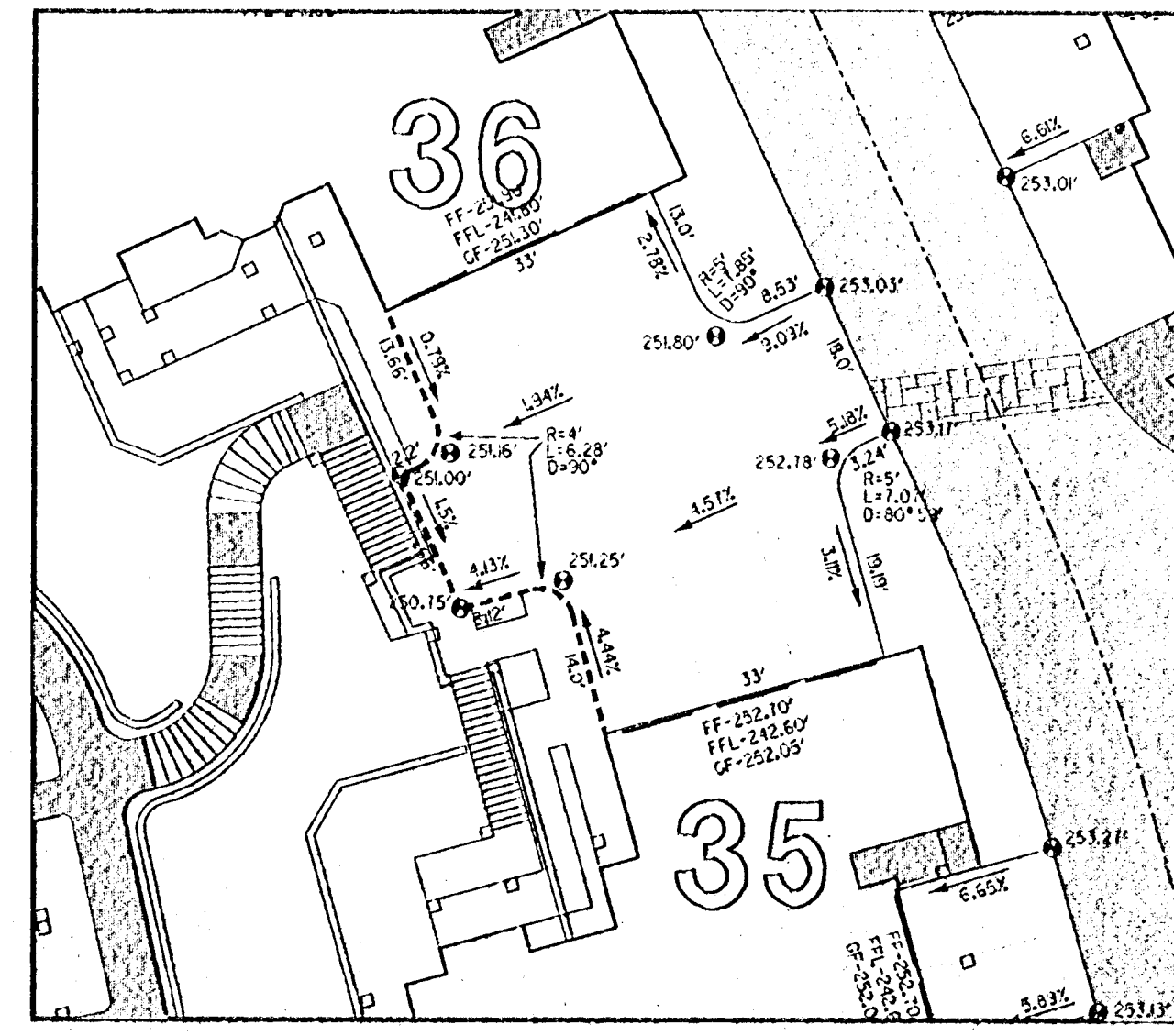
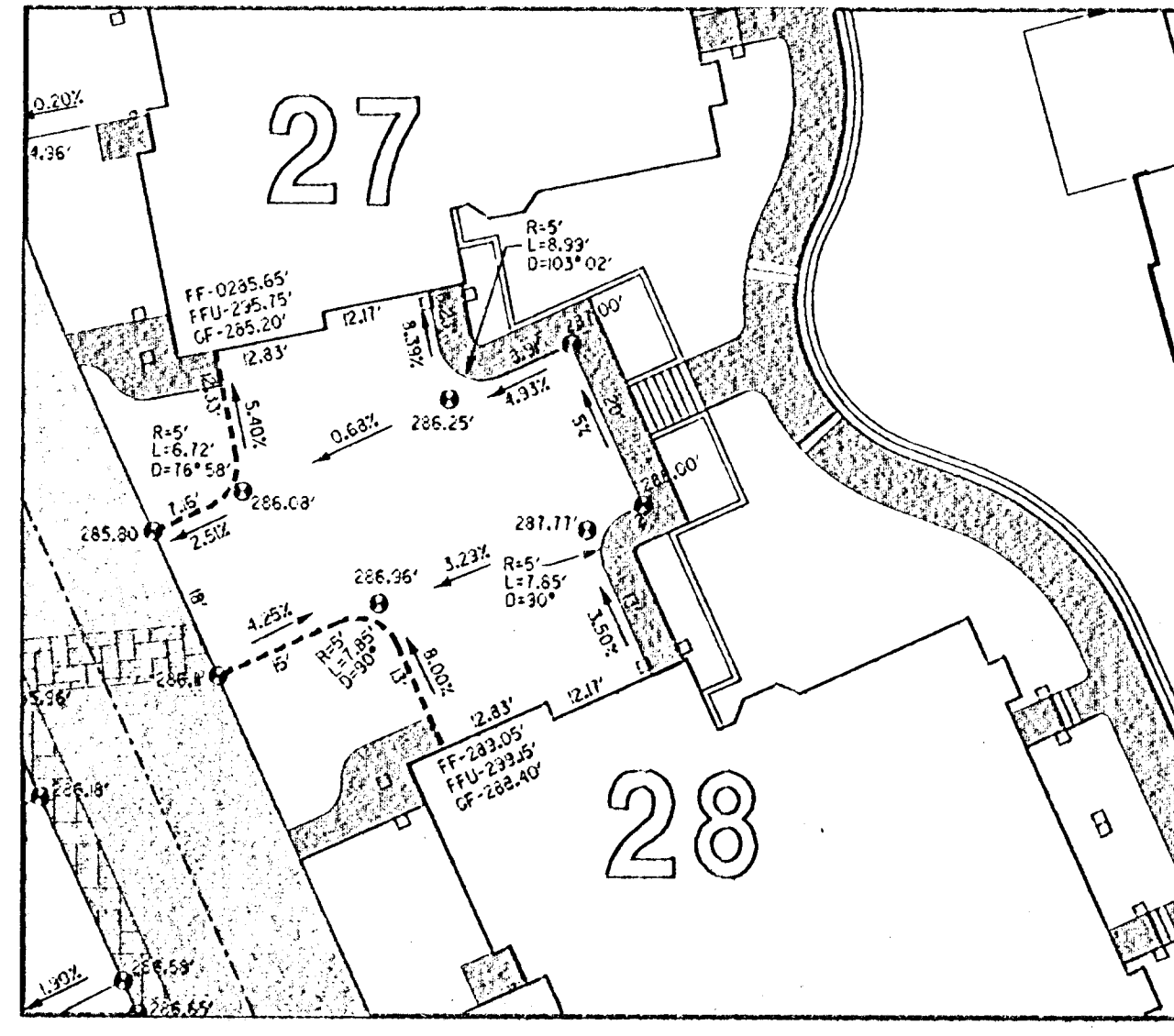
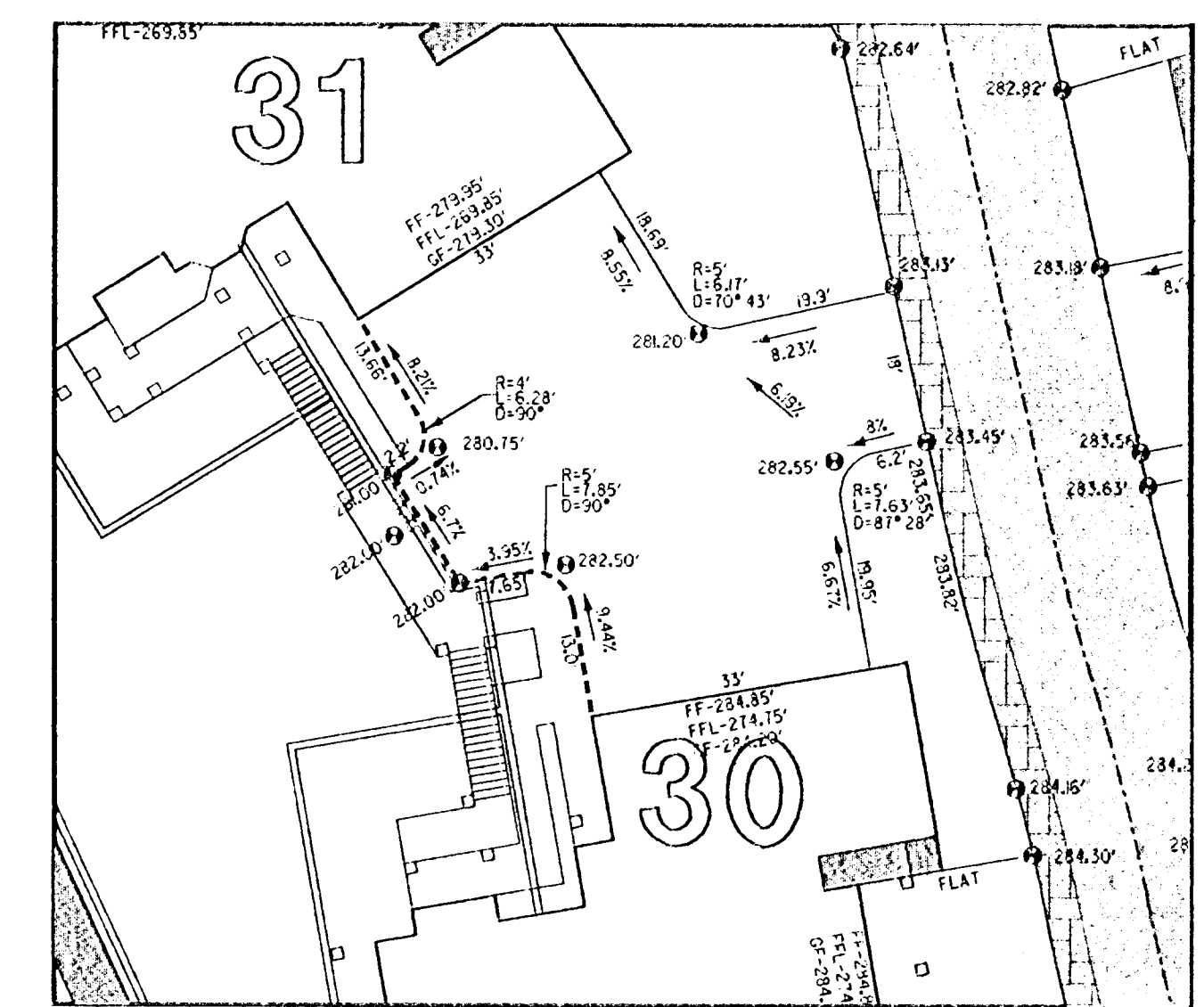
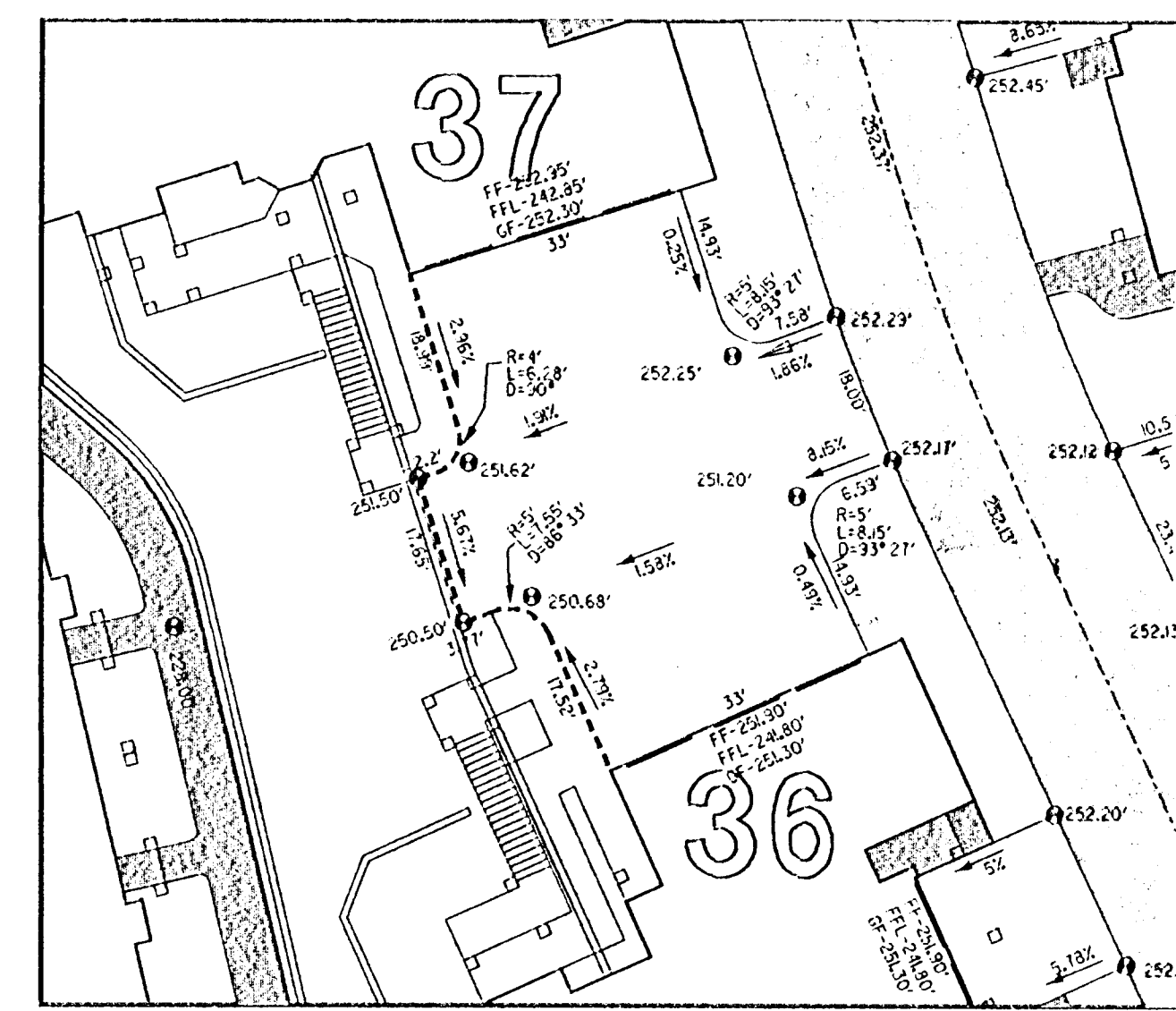
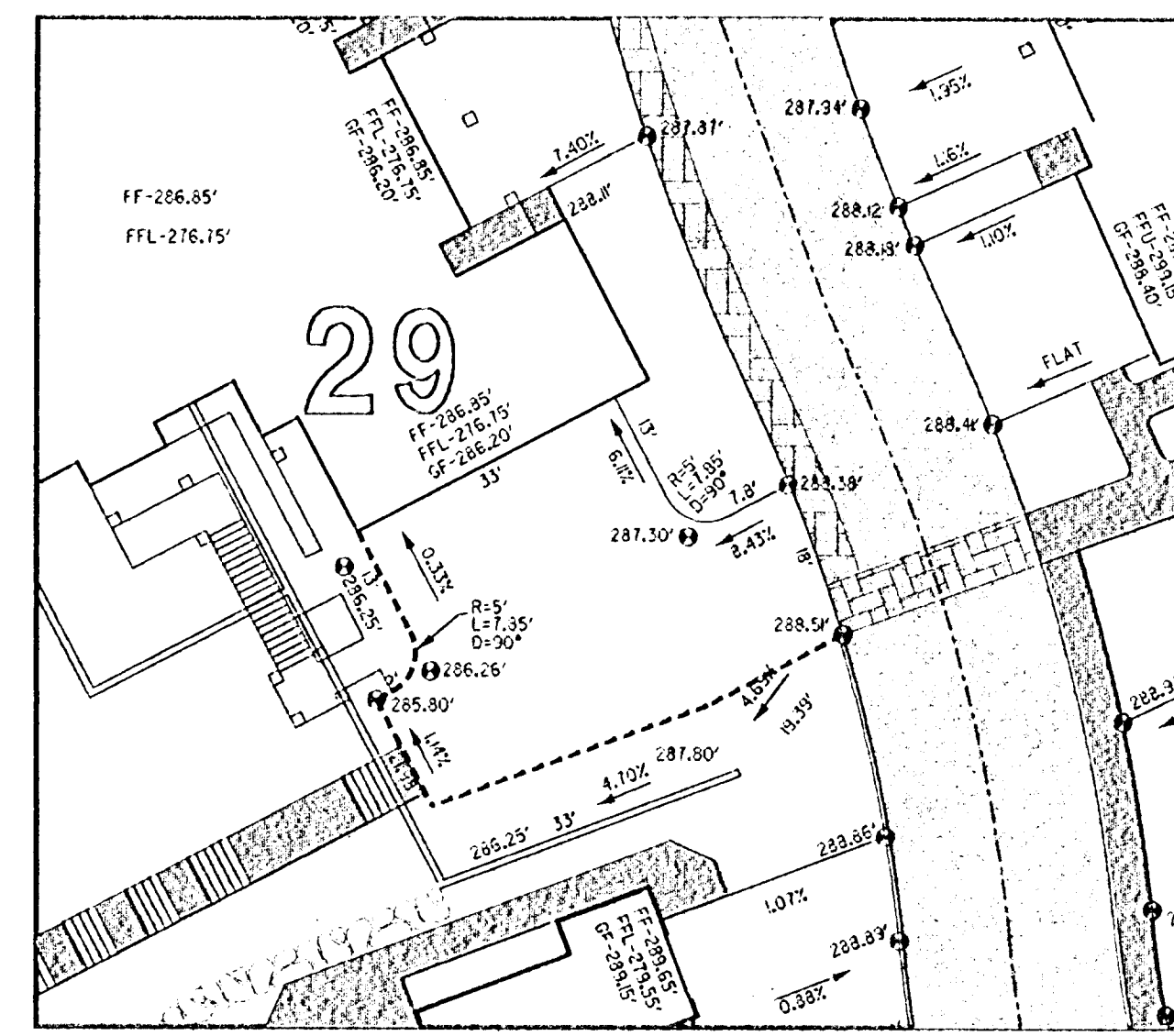
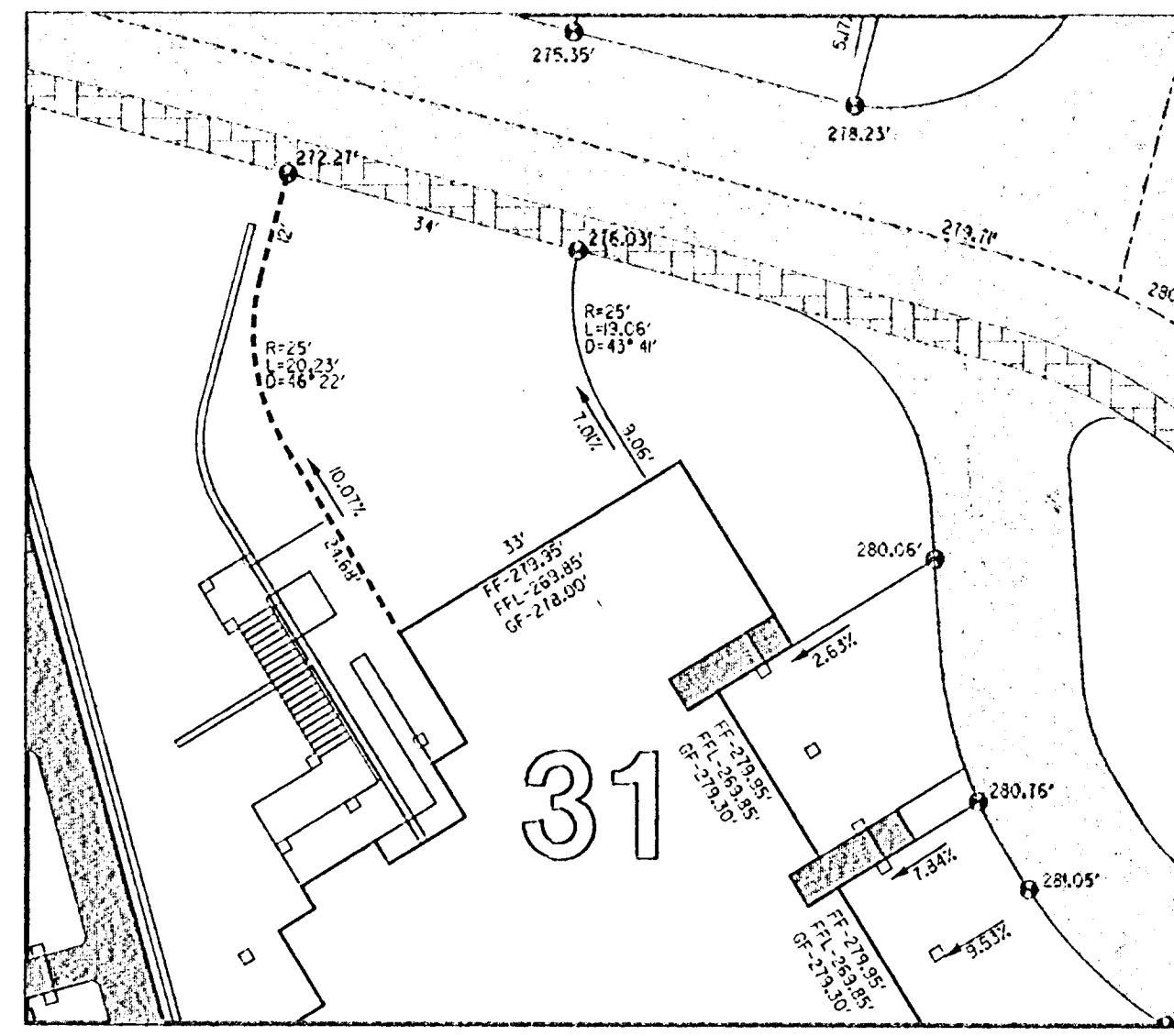
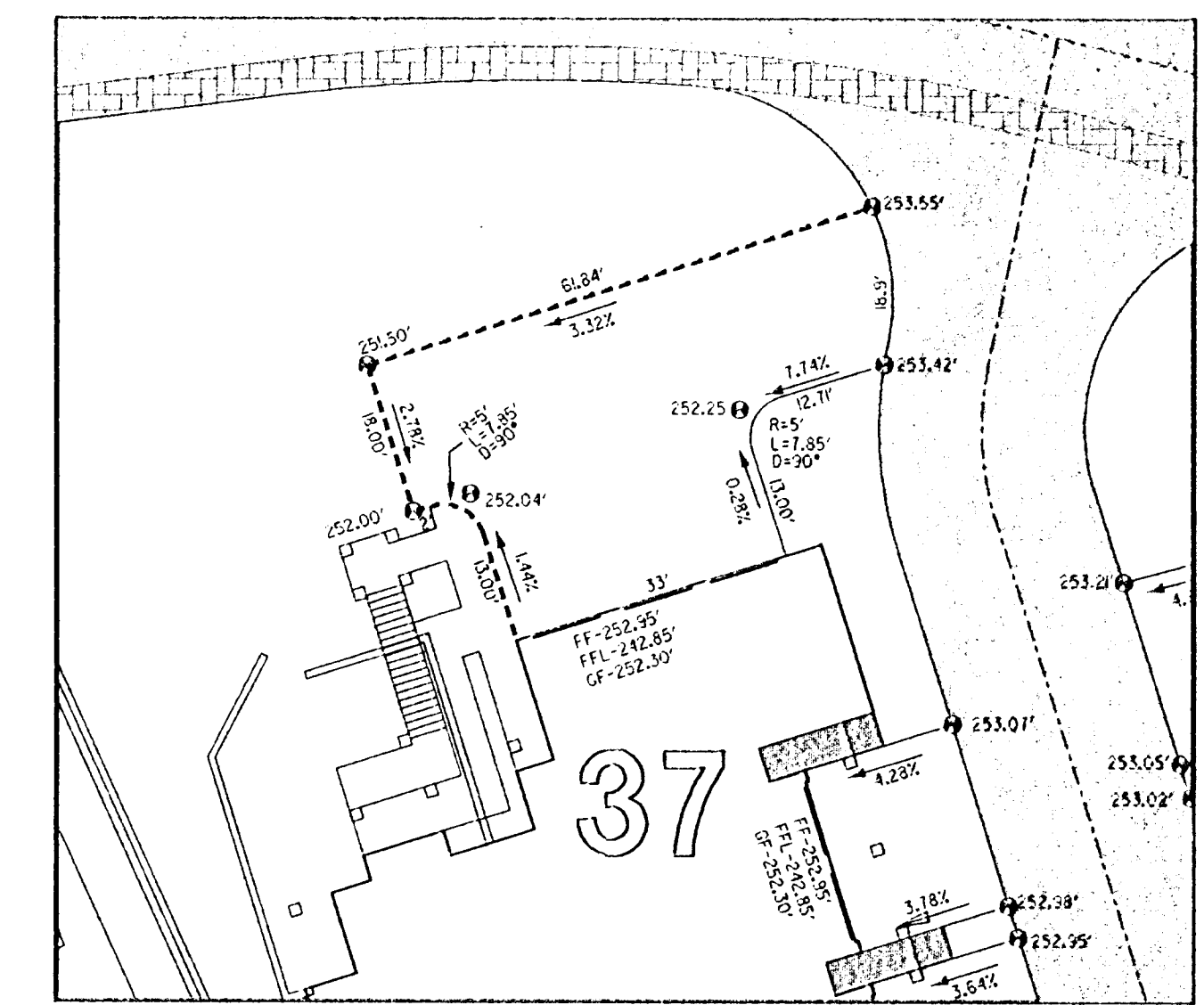
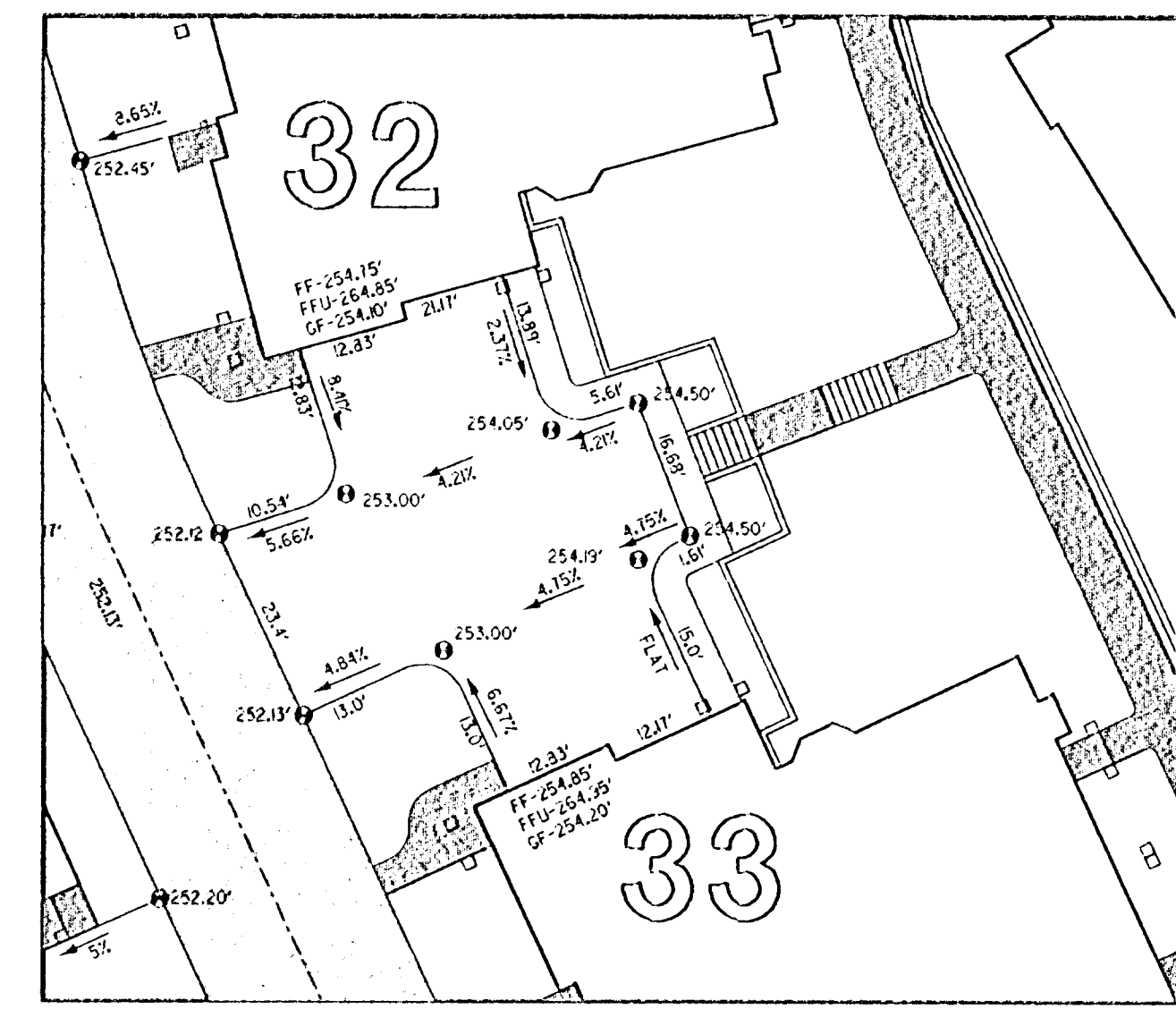
SHEET
17 of 42



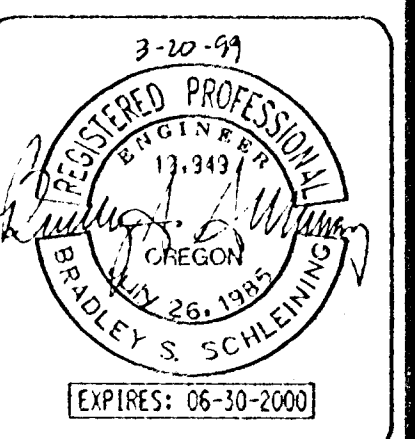
- LEGEND**
- 176° - EXISTING CONTOUR
 - 176° - CATCH POINT
 - DESIGN CONTOUR
 - 183.48 - SPOT ELEVATION
 - FF - FINISH FLOOR ELEVATION
 - FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
 - FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
 - GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY
 - EXTRUDED CURB



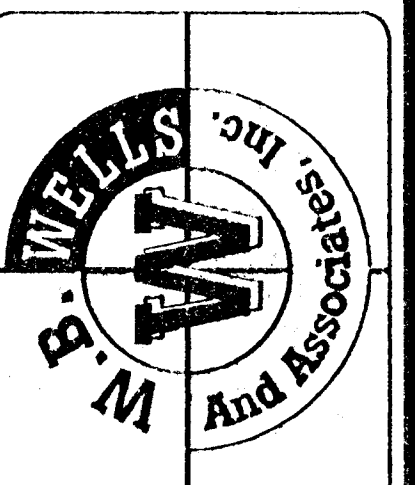
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- 2 CHANGED SYMBOL
- 3 ADJUSTED ALL FINISH FLOOR, TOP OF CURB, AND GARAGE FLOOR ELEVATIONS

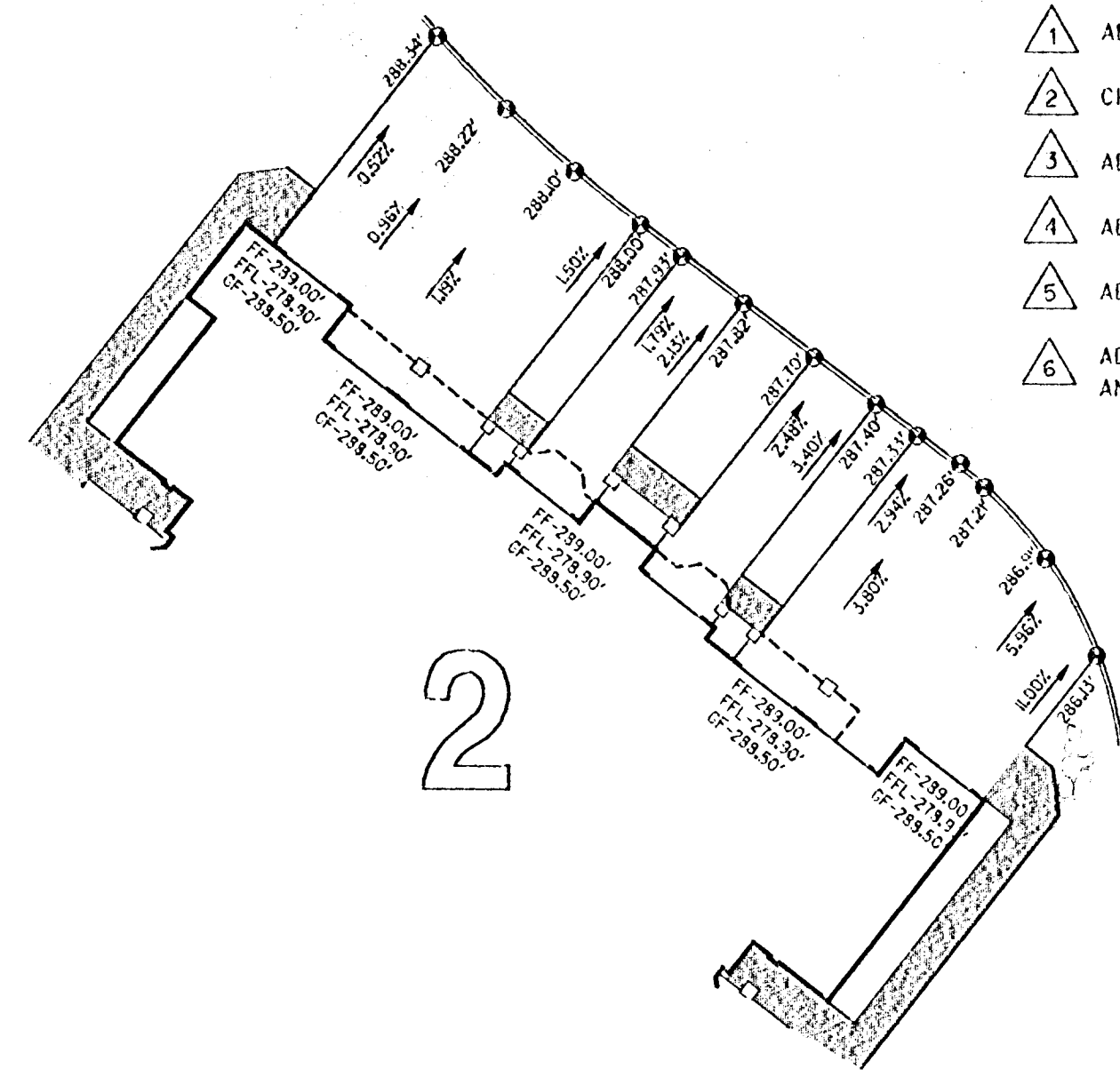


GRADING PLAN DETAILS - SHEET 2 OF 4
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

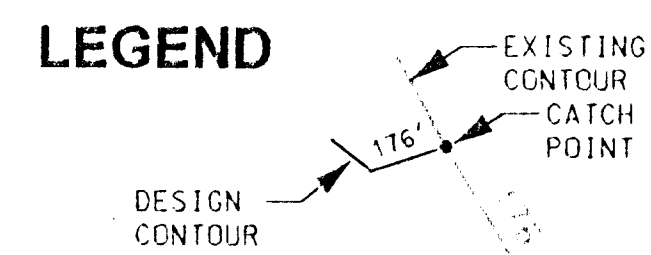


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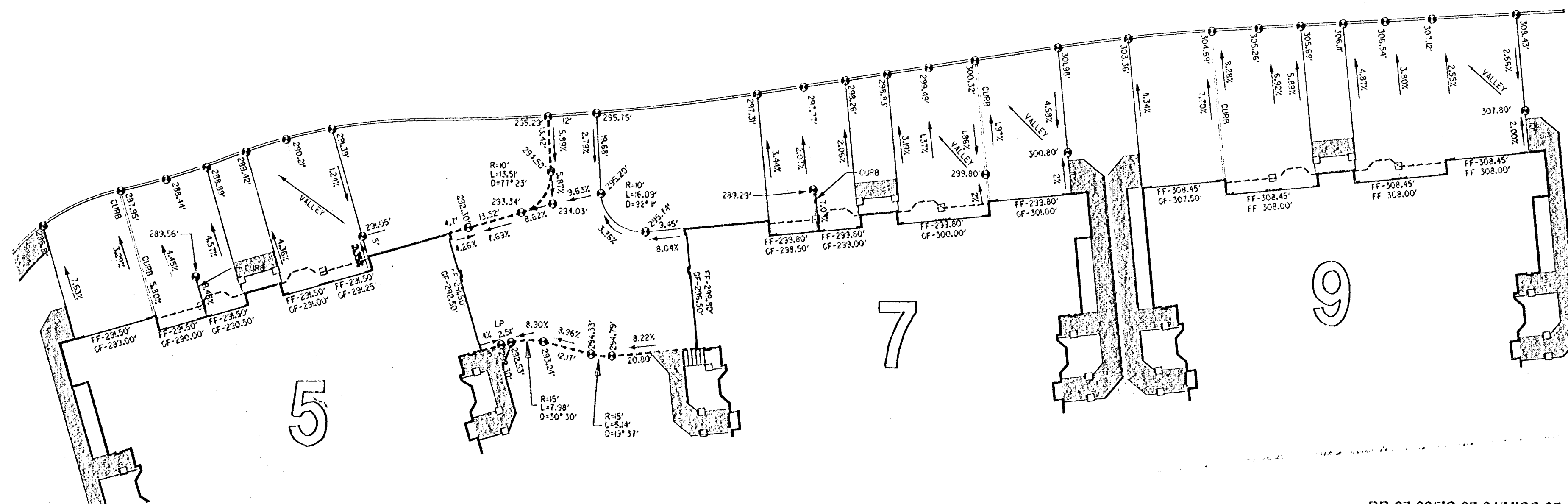
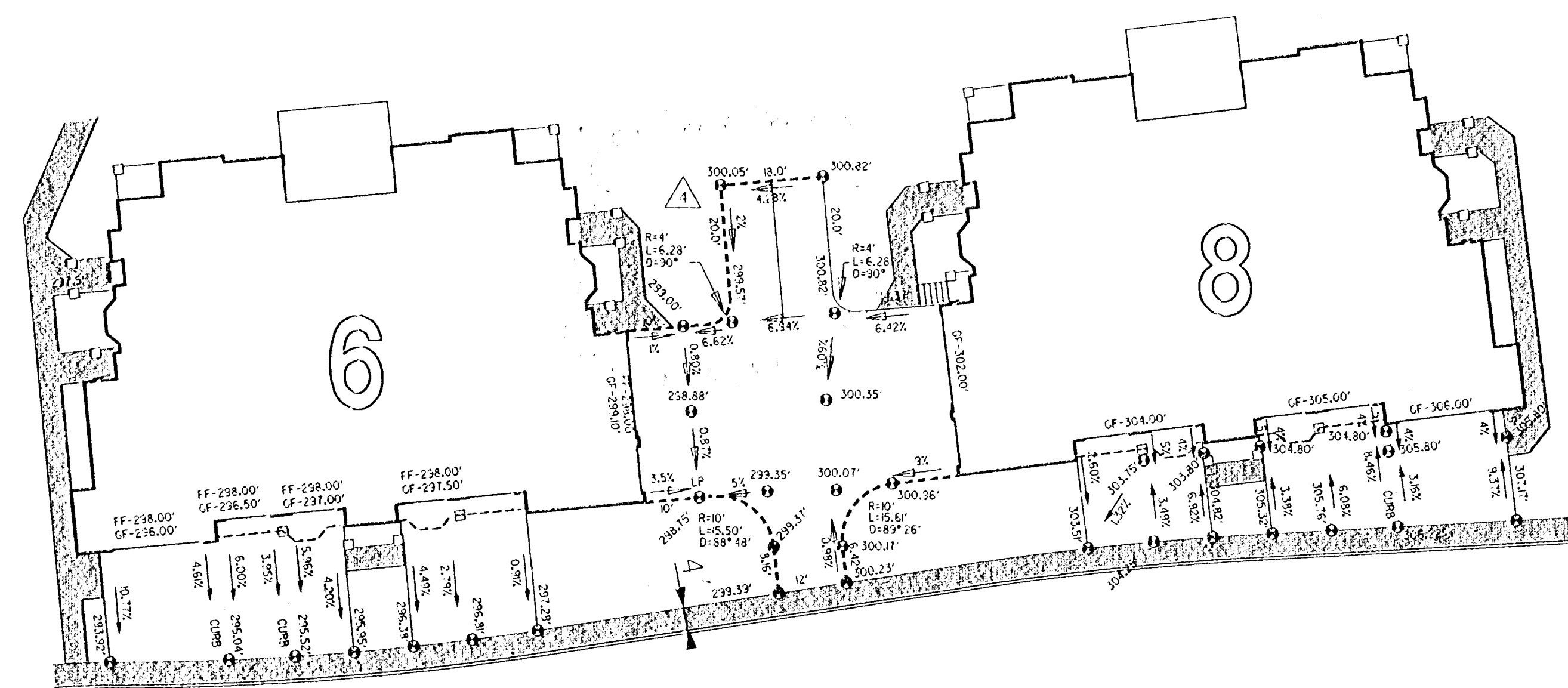
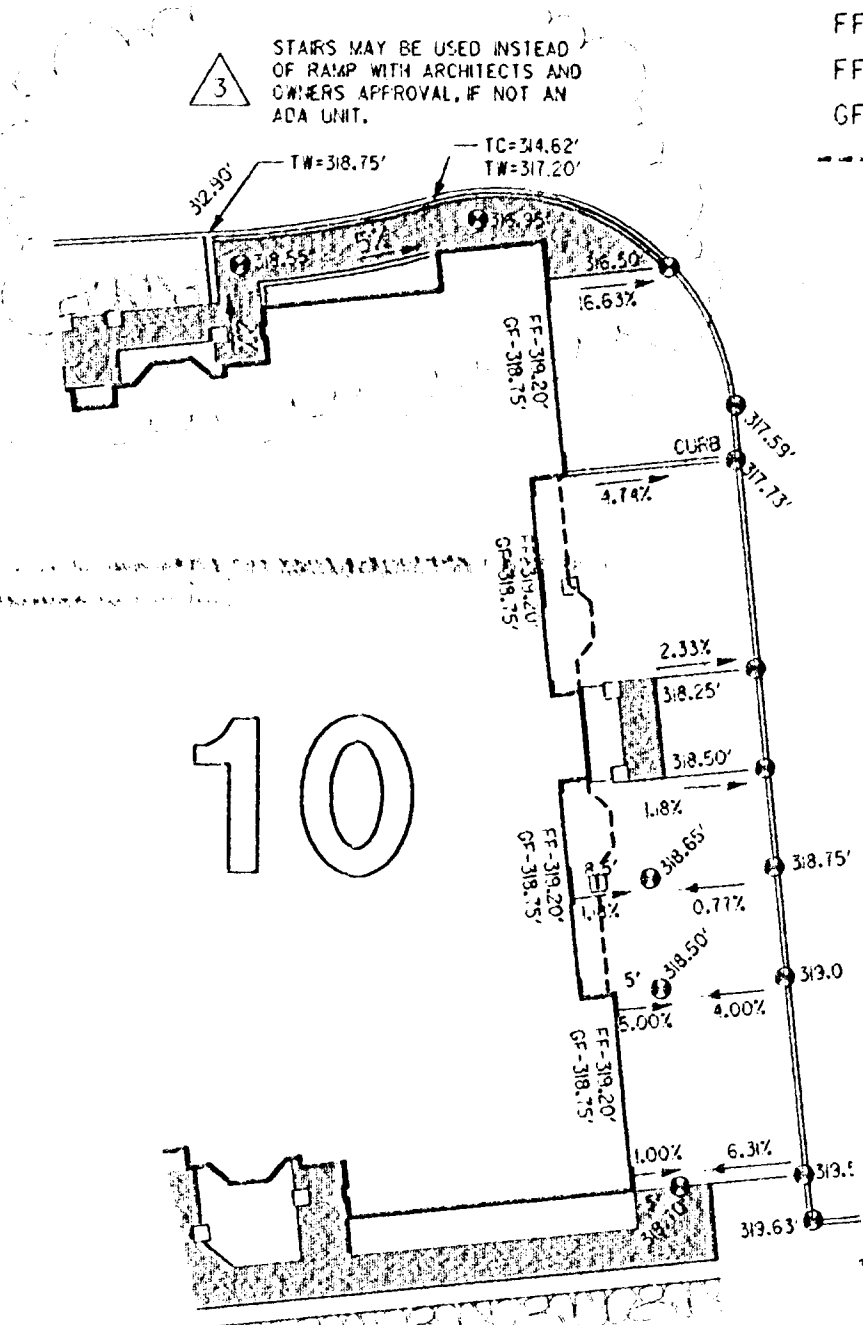
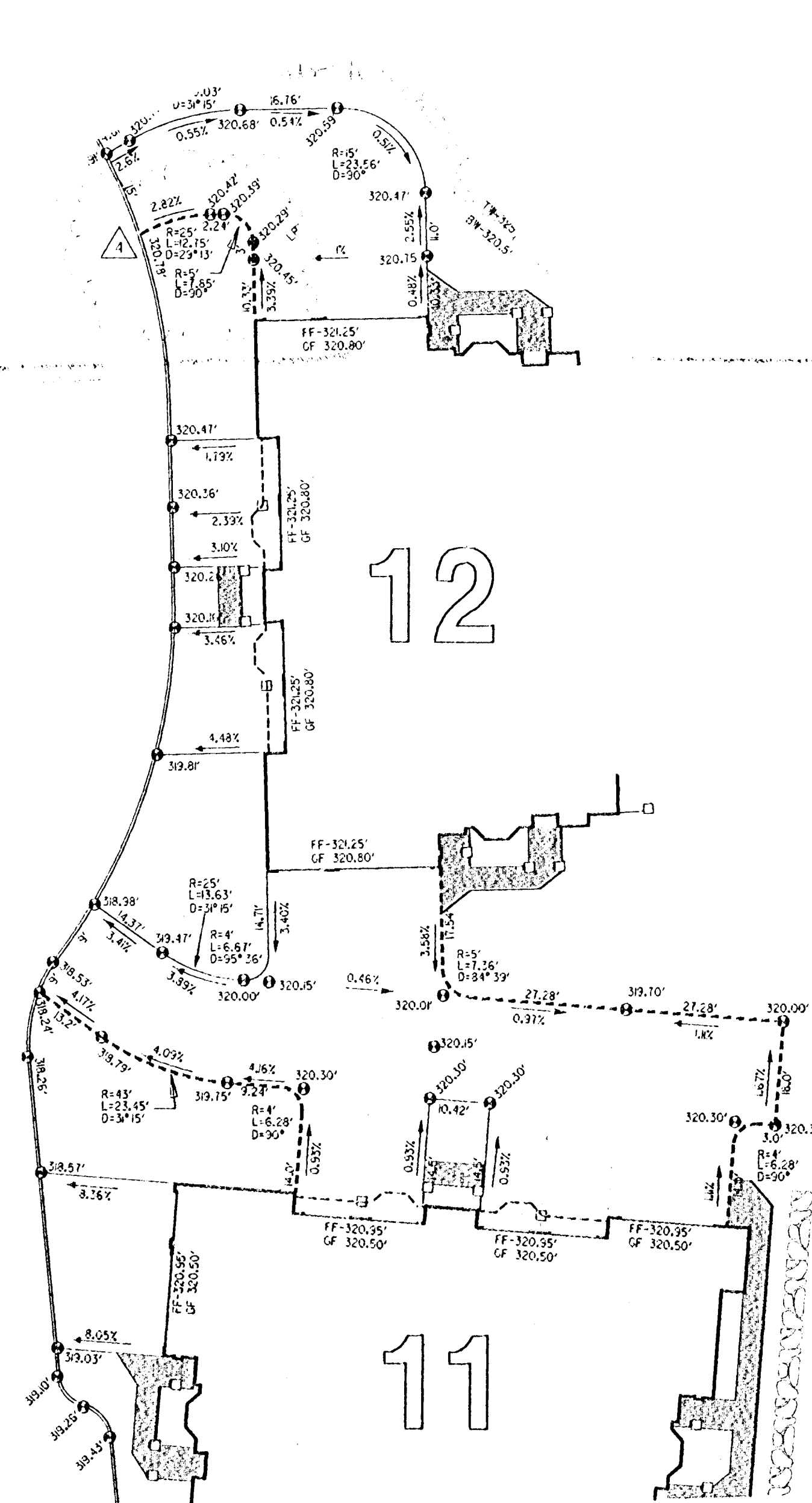
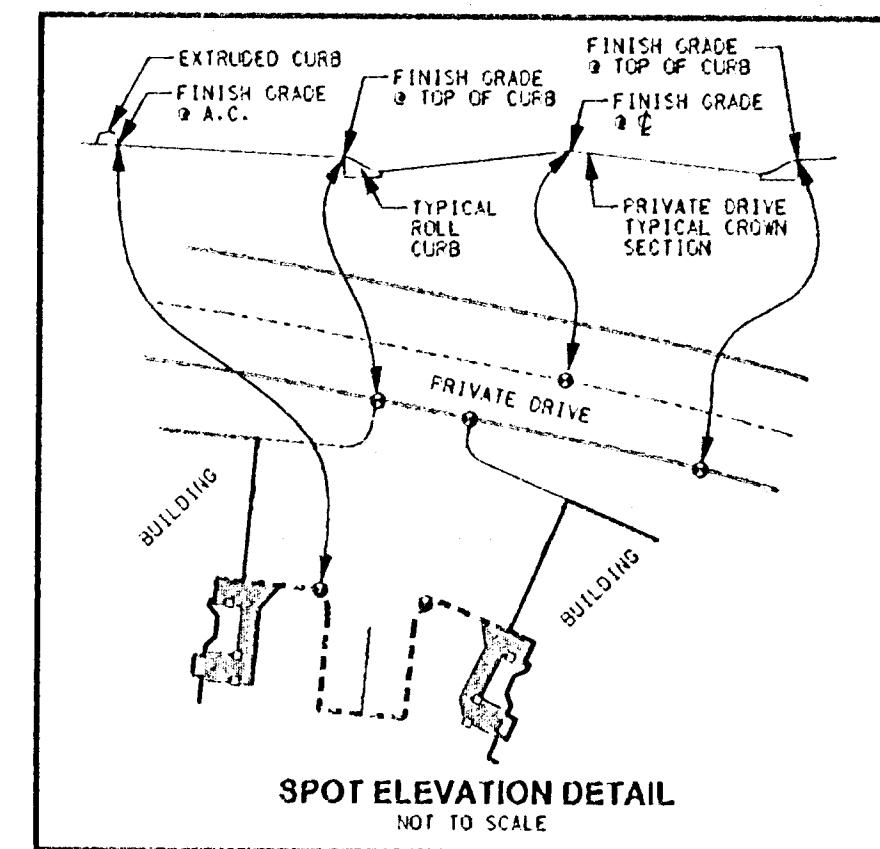
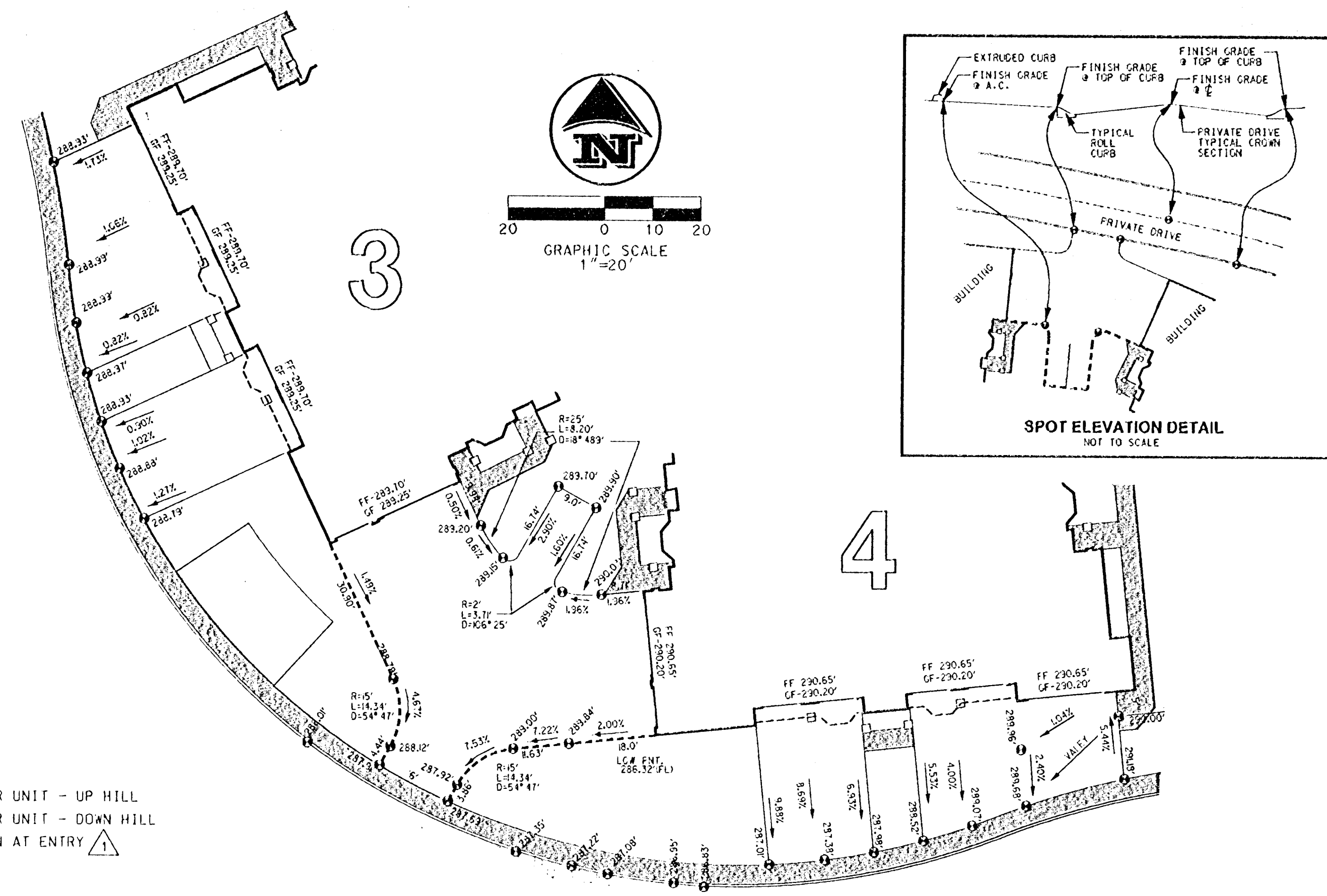




1. ADDED NOTE TO LEGEND
2. CHANGED SYMBOL
3. ADDED DETAILS
4. ADDED EXTRUDED CURB
5. ADDED MISCELLANEOUS DETAILS TO GRADING
6. ADJUSTED ALL FINISHED FLOOR, TOP OF CURB AND GARAGE FLOOR ELEVATIONS



185.40 SPOT ELEVATION
FF - FINISH FLOOR ELEVATION
FFU - FINISH FLOOR ELEVATION, UPPER UNIT - UP HILL
FFL - FINISH FLOOR ELEVATION, LOWER UNIT - DOWN HILL
GF - FINISH GARAGE FLOOR ELEVATION AT ENTRY
----- EXTRUDED CURB



APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

GRADING PLAN DETAILS - SHEET 3 OF 4

SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

3-20-99

REGISTERED PROFESSIONAL
ENGINEER
12,343
BRADLEY S. SCHLENNING
OREGON
JULY 26, 1995

EXPIRES: 06-30-2000

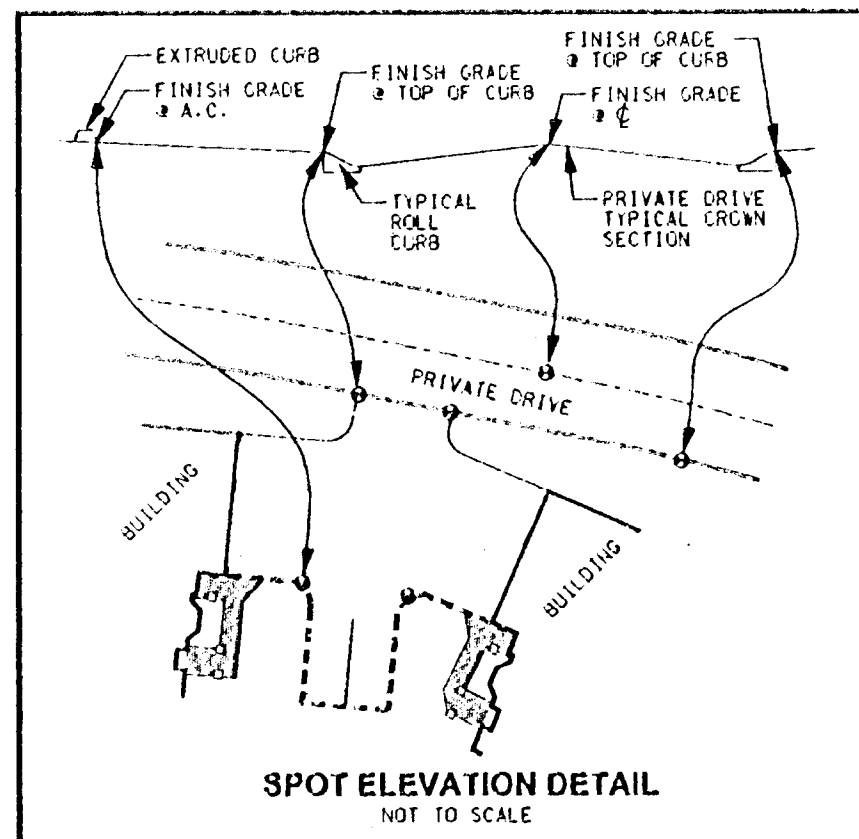
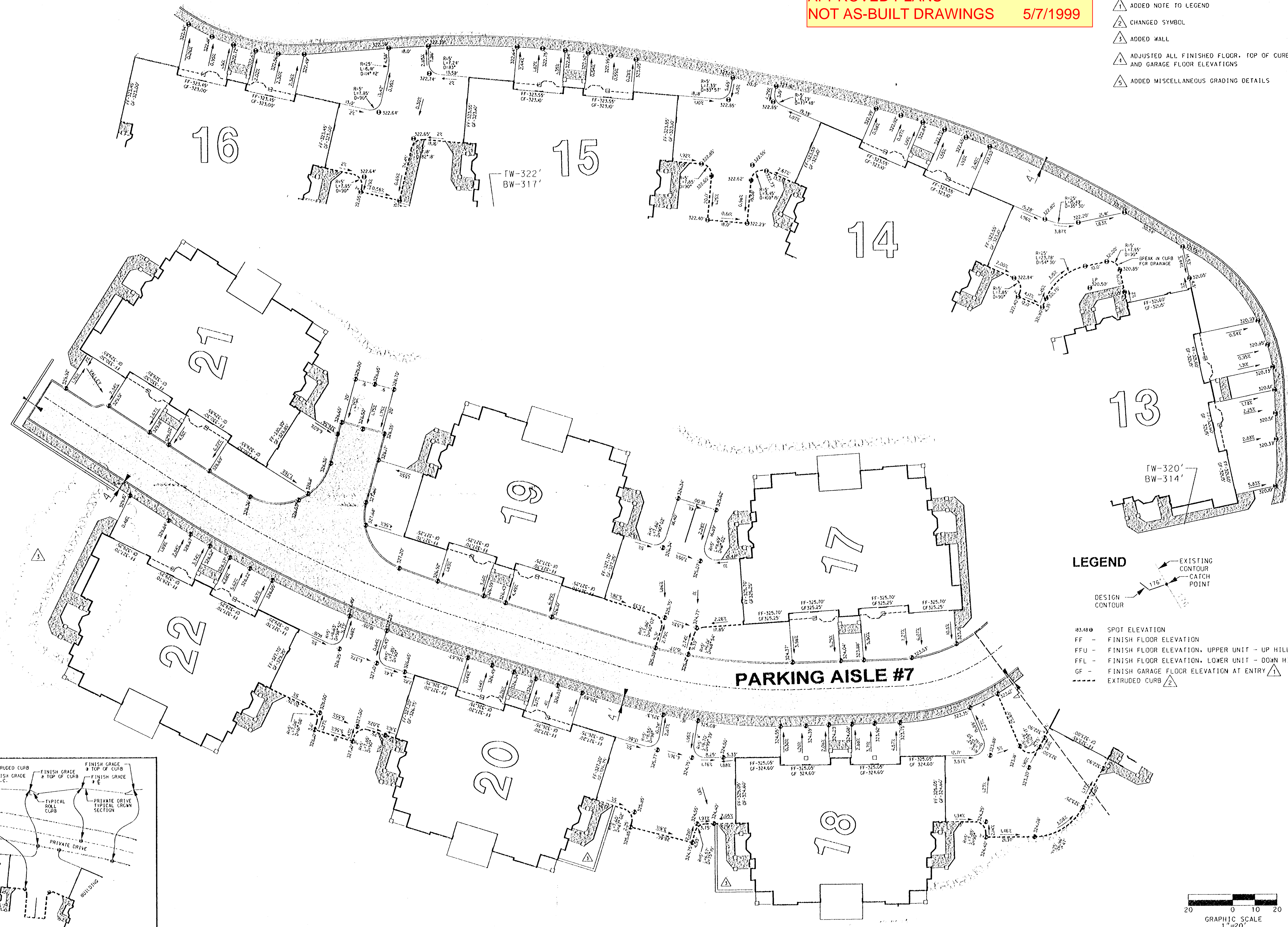
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SHEET
19 of 42

DR-97-36/ZC-97-04/MISC-97-59/MISC-97-60

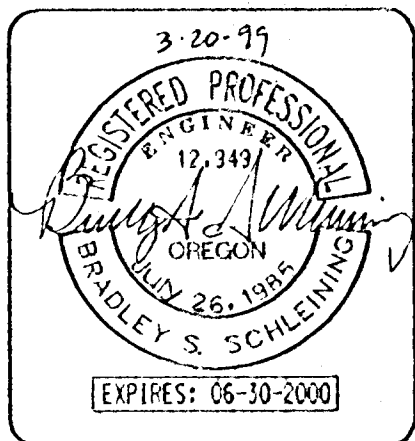
APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

- 1 ADDED NOTE TO LEGEND
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- 5 ADDED MISCELLANEOUS GRADING DETAILS

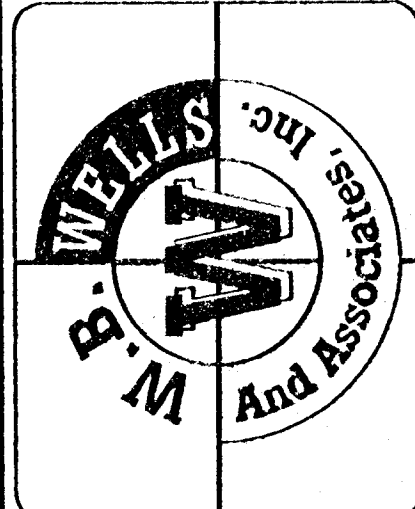


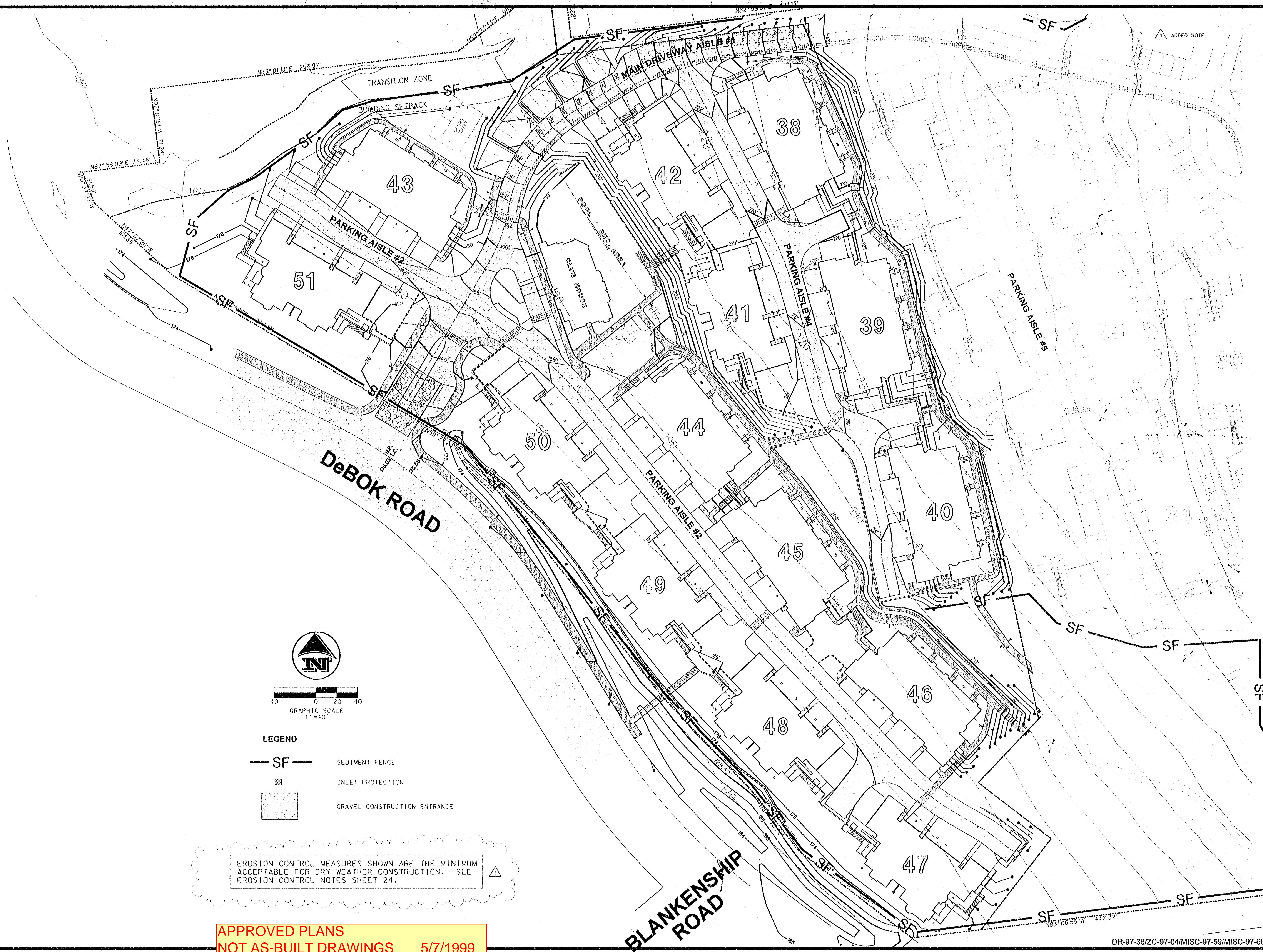
GRADING PLAN DETAILS - SHEET 4 OF 4

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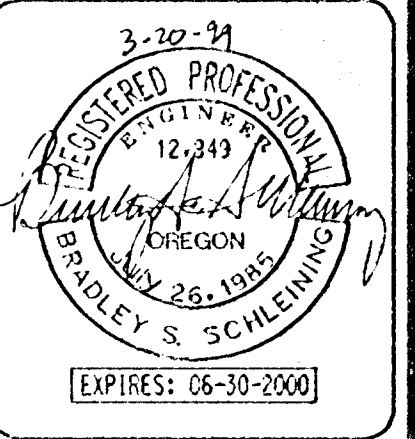




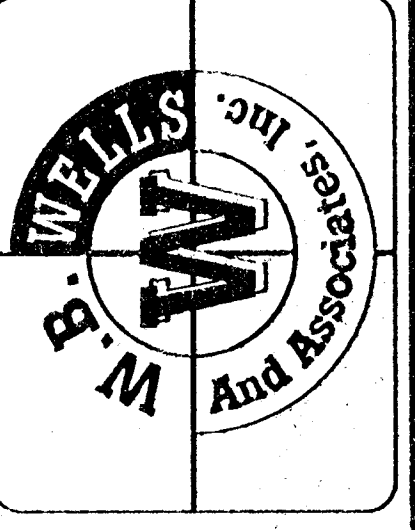
APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

| REVISIONS | | | |
|-----------------|----------------|--|--|
| 3-20-99 | | | |
| DESIGNED: BSS | CHECKED: BSS | | |
| DRAWN: BSS/RT | DATE: 11-20-98 | | |
| JOB NO.: 96-340 | SCALE: | | |

EROSION CONTROL PLAN - SHEET 1 OF 3
for
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CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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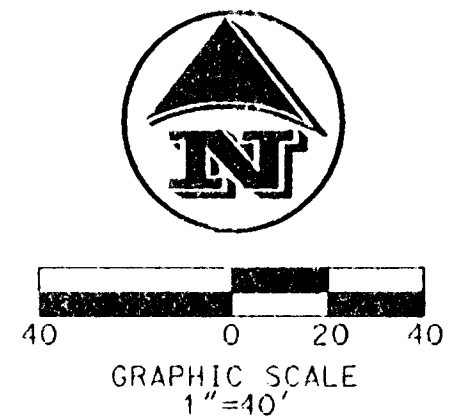


EROSION CONTROL MEASURES SHOWN ARE THE MINIMUM ACCEPTABLE FOR DRY WEATHER CONSTRUCTION. SEE EROSION CONTROL NOTES SHEET 24.

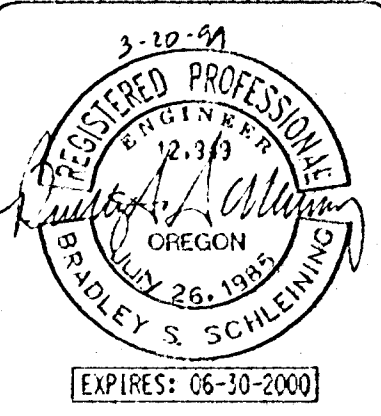
LEGEND

- SF — SEDIMENT FENCE
INLET PROTECTION
GRAVEL CONSTRUCTION ENTRANCE

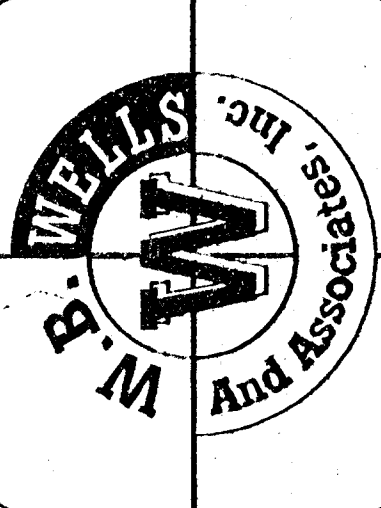
APPROVED PLANS
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EROSION CONTROL PLAN - SHEET 2 OF 3
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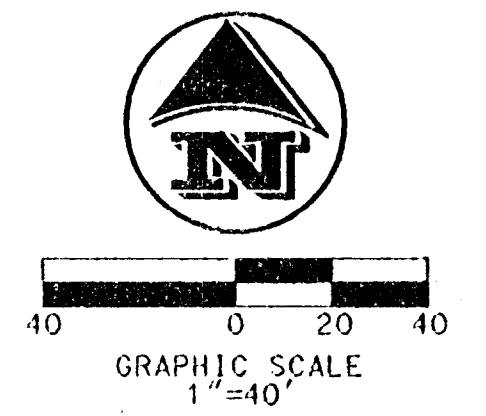
- △ ADDED NOTE
- △ CHANGED SYMBOL
- △ ADDED SEDIMENT FENCE

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IF THIS SITE IS UNDER CONSTRUCTION DURING WET WEATHER THE PONDS MUST BE CONSTRUCTED FIRST AND DEVELOPED INTO SEDIMENTATION PONDS. SITE SPECIFIC ADJUSTMENTS WILL BE NECESSARY

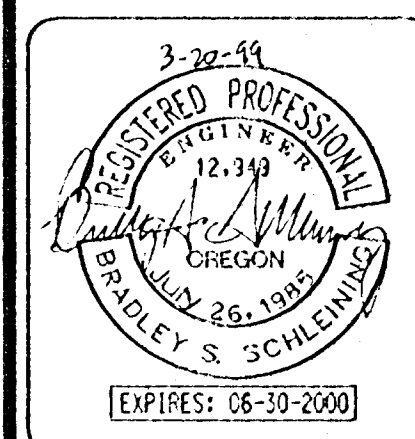
LEGEND

- SF — SEDIMENT FENCE
- ☒ INLET PROTECTION
- ▨ GRAVEL CONSTRUCTION ENTRANCE

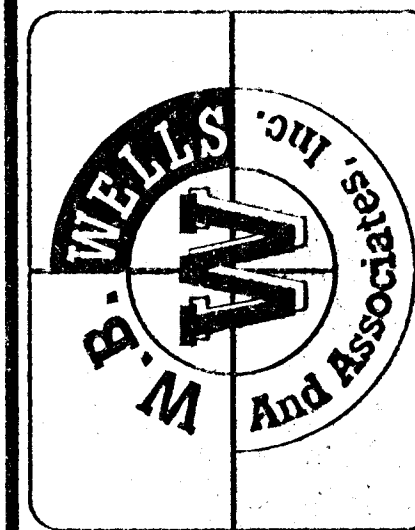


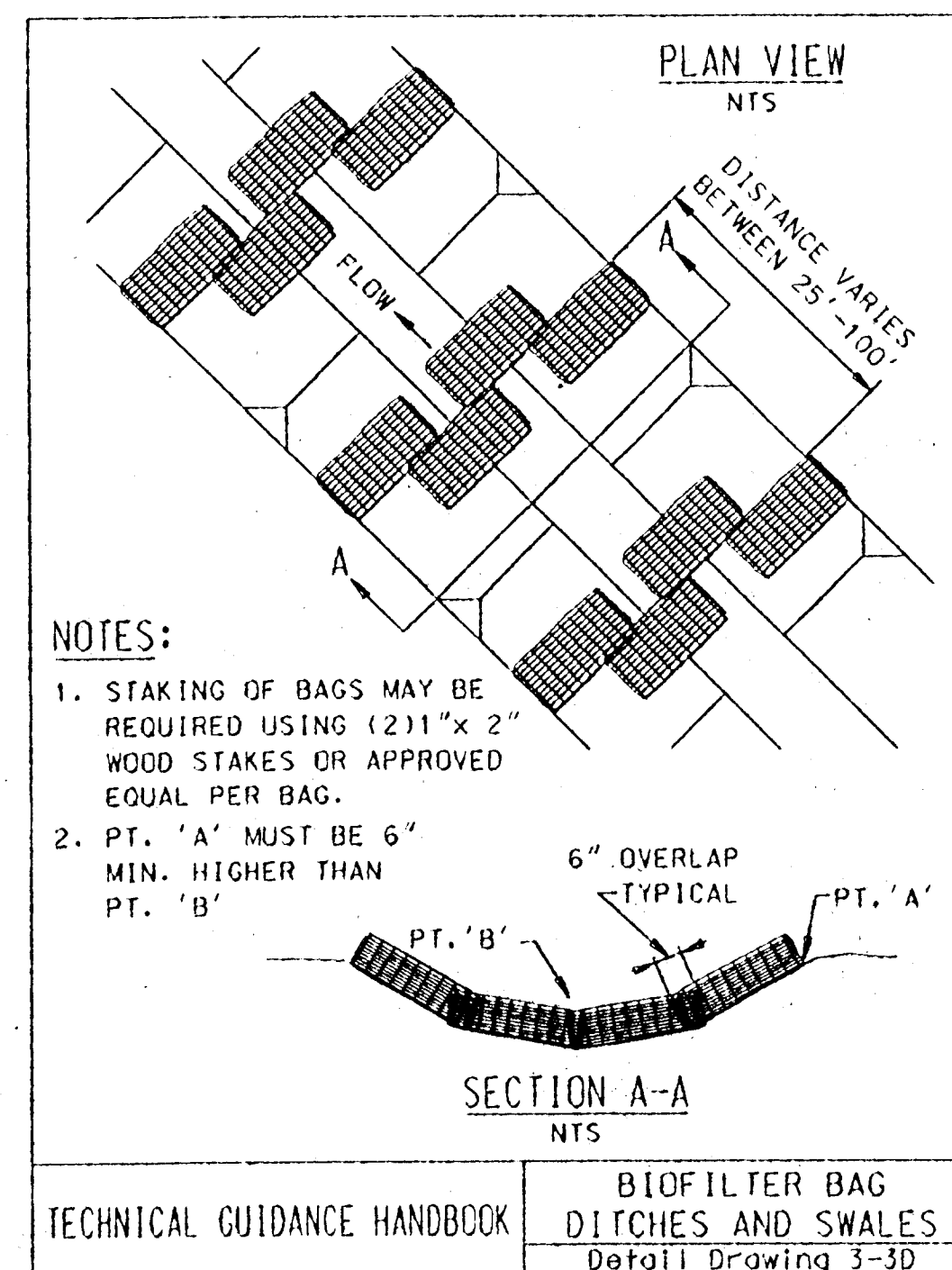
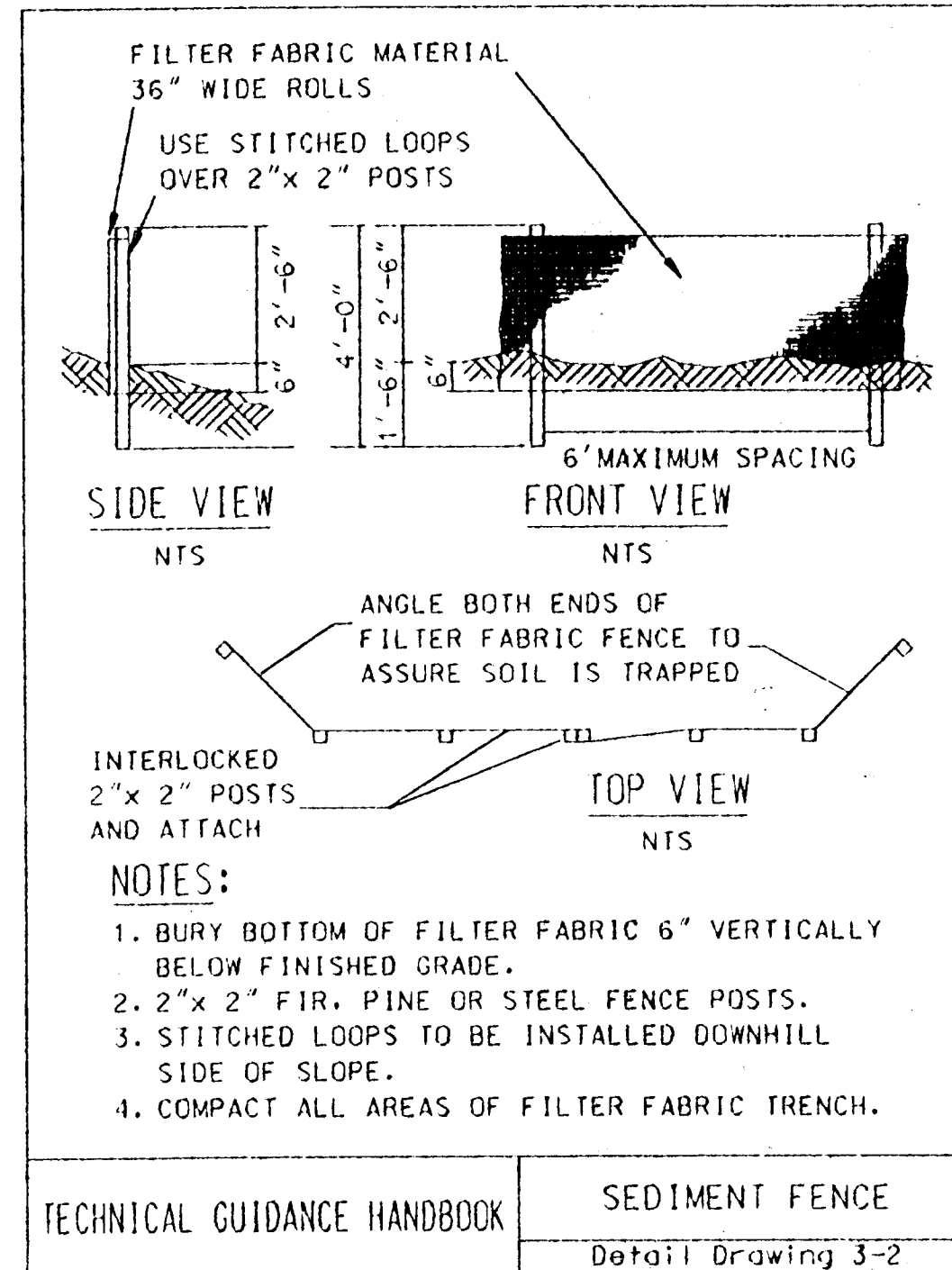
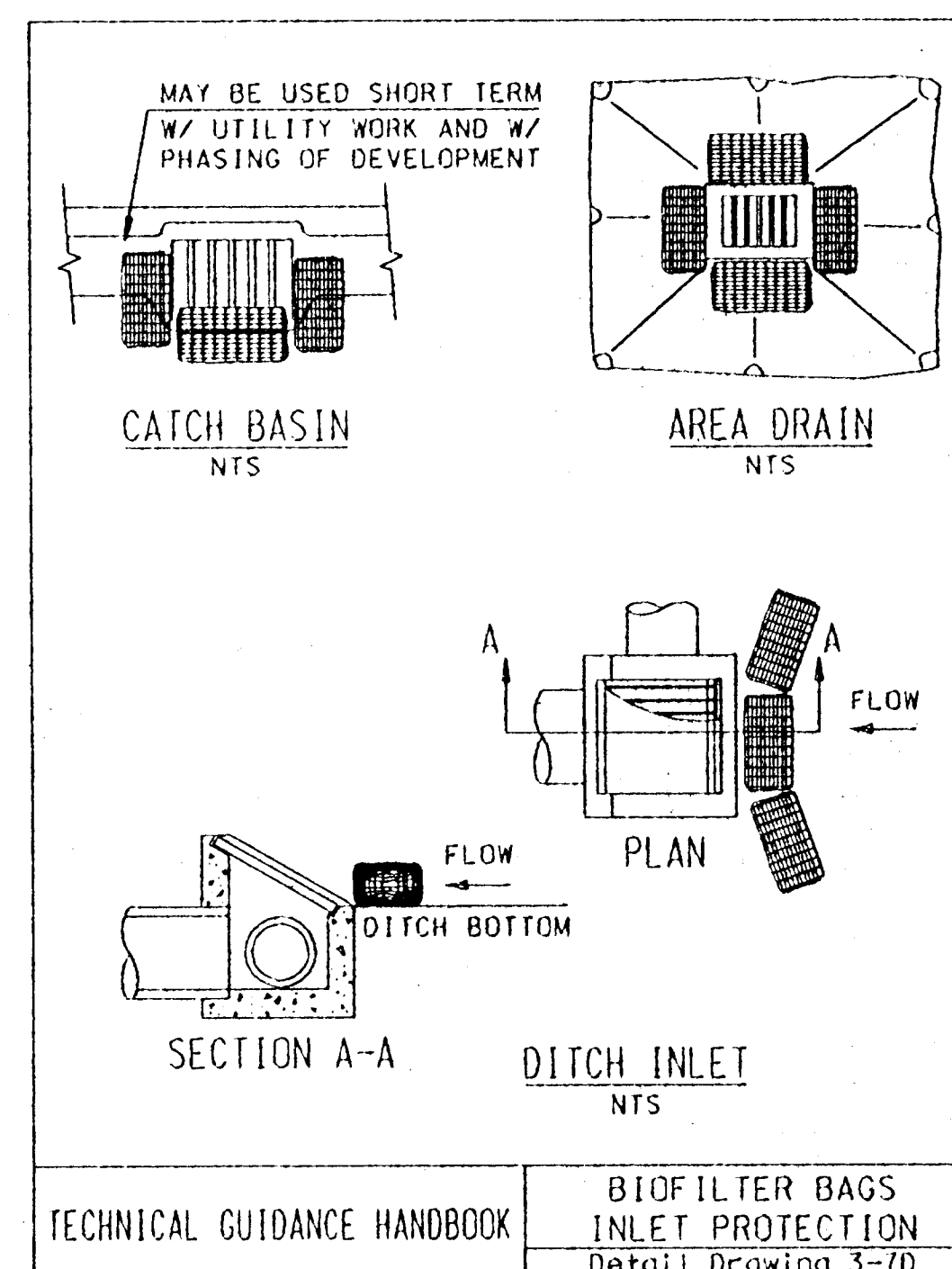
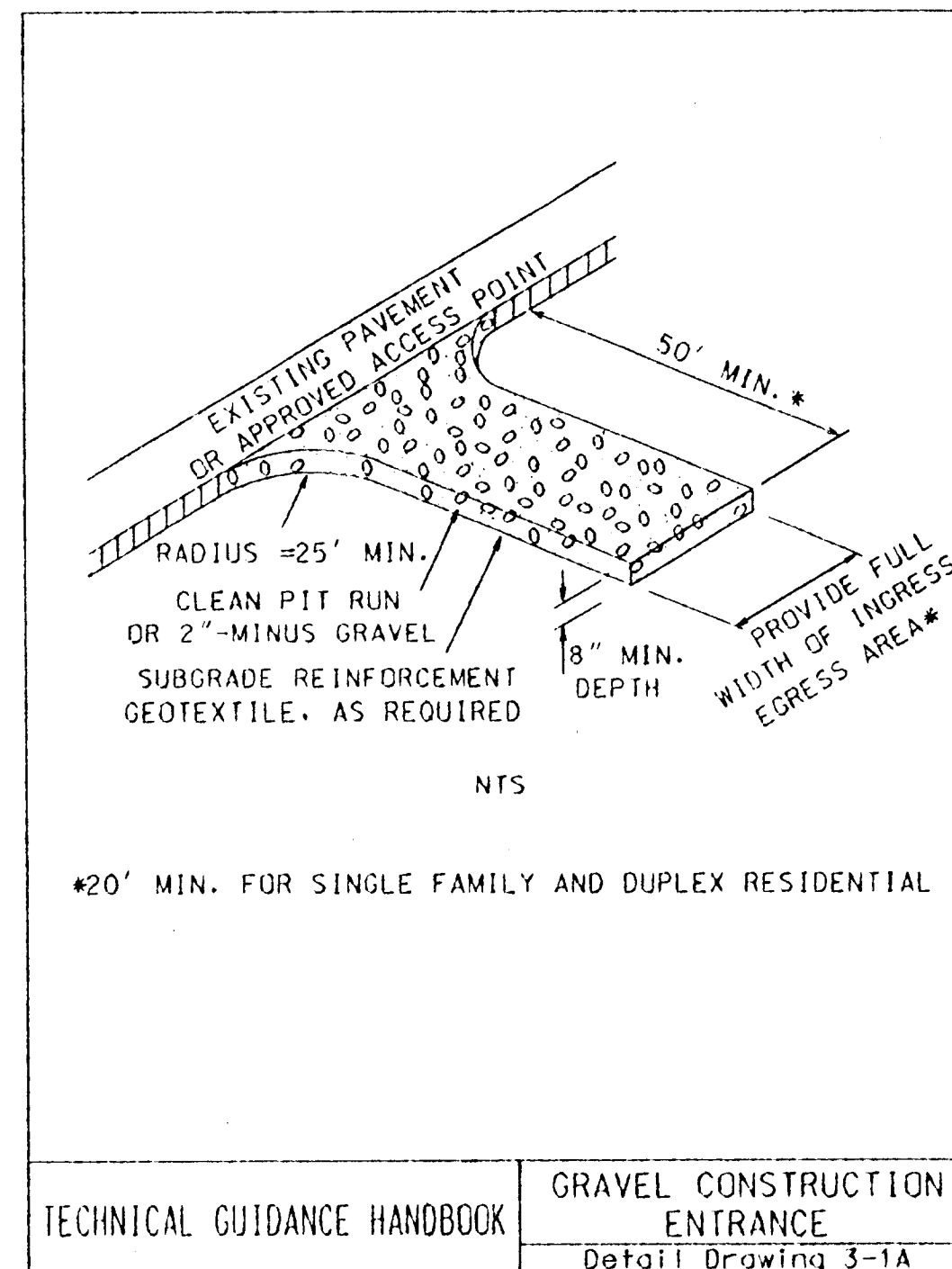
APPROVED PLANS
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EROSION CONTROL PLAN - SHEET 3 OF 3
for
SUMMERLINN APARTMENTS
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EROSION CONTROL NOTES

ALL EROSION CONTROL WORK WILL FOLLOW THE GUIDELINES ESTABLISHED IN OREGON SURFACE WATER QUALITY FACILITIES HANDBOOK, CLACKAMAS COUNTY AUGUST 1994.

APPROVAL OF THIS ESC PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G. SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).

THE IMPLEMENTATION OF THESE ESC PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED AND VEGETATION/LANDSCAPING IS ESTABLISHED.

THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED IN THE FIELD PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE FLAGGED CLEARLY LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE APPLICANT/CONTRACTOR FOR THE DURATION OF CONSTRUCTION.

THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED DRY WEATHER SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NECESSARY FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT-LOADED WATER DOES NOT LEAVE THE SITE.

THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.

THE ESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH, OR WITHIN 48 HOURS OF A STORM EVENT.

AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A TRAPPED CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LOADED WATER INTO THE DOWNSTREAM SYSTEM.

STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.

UNDISTURBED BUFFERS

DOWN HILL UNDISTURBED BUFFER ON PROPERTY OF EQUAL OR GREATER AREA TO DISTURBED AREA ON 10% OR LESS SLOPES MAY BE USED AS AN ALTERNATIVE TO SEDIMENT BARRIERS.

SEDIMENT FENCE

THE FILTER FABRIC SHALL BE PURCHASED ON A CONTINUOUS ROLL CUT TO LENGTH OF THE BARRIER TO AVOID USE OF JOINTS. WHEN JOINTS ARE NECESSARY, FILTER CLOTH SHALL BE SPICED ONLY AT A SUPPORT POST WITH A MINIMUM 6" OVERLAP, AND BOTH ENDS SECURELY FASTENED TO THE POST.

THE FILTER FABRIC FENCE SHALL BE INSTALLED TO FOLLOW THE CONTOURS, WHERE FEASIBLE. THE FENCE POSTS SHALL BE SPACED A MAXIMUM OF SIX FEET APART AND DRIVEN SECURELY INTO THE GROUND A MINIMUM OF 18".

A TRENCH SHALL BE EXCAVATED, ROUGHLY 8" WIDE BY 12" DEEP, UPSLOPE AND ADJACENT TO THE WOOD POST TO ALLOW THE FILTER FABRIC TO BE BURIED.

WHEN STANDARD STRENGTH FILTER FABRIC IS USED, A WIRE SUPPORT FENCE SHALL BE FASTENED SECURELY TO THE UPSLOPE SIDE OF THE POSTS USING HEAVY DUTY WIRE STAPLES AT LEAST 1" LONG. TIE WIRE OR HOO RINGS. THE WIRE SHALL EXTEND INTO THE TRENCH A MINIMUM OF 4" AND SHALL NOT EXTEND MORE THAN 30" ABOVE THE ORIGINAL GROUND SURFACE.

THE STANDARD STRENGTH FILTER FABRIC SHALL BE STAPLED OR WIRED TO THE FENCE, AND 6" OF THE FABRIC SHALL BE EXTENDED INTO THE TRENCH. THE FABRIC SHALL NOT EXTEND MORE THAN 30" ABOVE THE ORIGINAL GROUND SURFACE. FILTER FABRIC SHALL NOT BE STAPLED TO EXISTING TREES.

WHEN EXTRA-STRENGTH FILTER FABRIC AND CLOSER POST SPACING ARE USED, THE WIRE MESH SUPPORT FENCE MAY BE ELIMINATED. IN SUCH A CASE, THE FILTER FABRIC IS STAPLED OR WIRED DIRECTLY TO THE POSTS WITH ALL OTHER PROVISIONS OF THE ABOVE STANDARD NOTE FOR STANDARD STRENGTH FILTER FABRIC APPLYING.

SEDIMENT FENCES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFUL PURPOSE, AND AFTER THE UPSLOPE AREA HAS BEEN PERMANENTLY STABILIZED.

SEDIMENT FENCES SHALL BE INSPECTED BY APPLICANT/CONTRACTOR IMMEDIATELY AFTER EACH RAINFALL, AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.

AT NO TIME SHALL MORE THAN A ONE FOOT DEPTH OF SEDIMENT BE ALLOWED TO ACCUMULATE BEHIND STRAW BALE SEDIMENT BARRIERS AND/OR BIOFILTER BAGS. SEDIMENT SHOULD BE REMOVED OR REGRADED INTO THE SLOPE, OR NEW LINES OF BARRIERS INSTALLED UPHILL OF SEDIMENT-LOADED BARRIERS.

BIOFILTER BAGS

BIOFILTER BAGS SHALL BE CLEAN 100% RECYCLED WOOD PRODUCT WASTE. SIZE OF BAG SHALL BE 18" X 8" X 30" AND WEIGH APPROXIMATELY 45 POUNDS, AND MADE OF 1/2 INCH PLASTIC MESH.

SEEDING / MULCHING

RECOMMENDED SEED MIXTURE:

| TYPE | PERCENT BY WEIGHT |
|-------------------------------|-----------------------------|
| ELKA DWARF PERENNIAL RYEGRASS | 80 |
| CREeping RED FESCUE | 20 |
| APPLICATION RATE: | 100 POUNDS MINIMUM PER ACRE |

FERTILIZER SHALL BE 12-16-8 WITH 50% OF THE NITROGEN DERIVED FROM UREA-FORMALDEHYDE, AND APPLIED AT A RATE OF 400 POUNDS PER ACRE.

SEED AND MULCH AT A RATE OF 2000 POUNDS PER ACRE WITH HEAVY BONDING AGENT OR NETTING AND ANCHORS. MULCH SHALL BE A WOOD CELLULOSE FIBER OR OTHER MATERIAL SUITABLE FOR HYDROMULCHING.

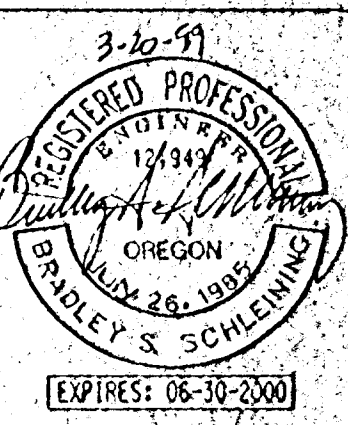
TEMPORARY/PERMANENT HYDROSEEDING OR ACCEPTABLE SEEDING AND MULCHING MUST BE PROVIDED WHENEVER PERENNIAL COVER CANNOT BE ESTABLISHED ON SITES WHICH WILL BE EXPOSED FOR 60 DAYS OR MORE.

DESIGN CRITERIA/SPECIFICATIONS: TEMPORARY EROSION CONTROL GRASSES

- TEMPORARY GRASS COVER MEASURES MUST BE FULLY ESTABLISHED BY NOVEMBER 1ST OR OTHER COVER MEASURES WILL HAVE TO BE IMPLEMENTED UNTIL ADEQUATE GRASS COVERAGE IS ACHIEVED. TO ESTABLISH AN ADEQUATE GRASS STAND FOR CONTROLLING EROSION BY NOVEMBER 1, IT IS RECOMMENDED THAT SEEDING AND MULCHING OCCUR BY SEPTEMBER 1. (ANY SEEDING PLANNED AFTER SEPTEMBER 1, MUST HAVE DISTRICT APPROVAL AND REQUIRES THE USE OF PRE-GERMINATED GRASS SEEDS.)
- HYDROMULCH SHALL BE APPLIED WITH GRASS SEEDS AT A RATE OF 2,000 LB /ACRE ON SLOPES STEEPER THAN 10%. HYDROSEED AND MULCH SHALL BE APPLIED WITH A BONDING AGENT (TACKIFIER). APPLICATION RATE AND METHODOLOGY TO BE IN ACCORDANCE WITH SEED SUPPLIER'S RECOMMENDATIONS.
- DRY, LOOSE, WEED-FREE STRAW USED AS MULCH SHALL BE APPLIED AT DOUBLE THE HYDROMULCH APPLICATION REQUIREMENT (4,000 LB/ACRE). ANCHOR STRAW BY WORKING IN BY HAND OR WITH EQUIPMENT (ROLLERS, CLEAT TRACKS, ETC.).
- MULCH SHALL BE SPREAD UNIFORMLY IMMEDIATELY FOLLOWING SEEDING.
- SOIL PREPARATION: TOP SOIL SHOULD BE PREPARED ACCORDING TO LANDSCAPE PLANS, IF AVAILABLE, OR RECOMMENDATIONS OF GRASS SEED SUPPLIER. IT IS RECOMMENDED THAT SLOPES BE ROUGHENED BEFORE SEEDING BY "TRACK-WALKING", (DRIVING A CRAWLING TRACTOR UP AND DOWN SLOPES TO LEAVE A PATTERN OF CLEAT IMPRINTS PARALLEL TO SLOPE CONTOUR) OR OTHER METHOD TO PROVIDE MORE STABLE SITES FOR SEEDS TO REST.
- FERTILIZER FOR GRASS SEEDS: IN ACCORDANCE WITH SUPPLIER'S RECOMMENDATIONS. DEVELOPMENT AREAS WITHIN 50 FEET OF WATER BODIES AND WETLANDS MUST USE A NON-PHOSPHORUS FERTILIZER.
- NETTING AND ANCHORS, AS NEEDED. FOR DISTURBED AREAS ON SLOPES AND IN DITCHES/SWALES, BIODEGRADABLE NETTING, OR JUTE IS DESIRABLE AND MAY BE USED INSTEAD OF BONDING AGENTS TO PROVIDE A STABLE AREA FOR SEEDING. NETTING SHOULD BE ANCHORED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- WATERING: SEEDING SHALL BE SUPPLIED WITH ADEQUATE MOISTURE TO ESTABLISH GRASS. SUPPLY WATER AS NEEDED, ESPECIALLY IN ABNORMALLY HOT OR DRY WEATHER OR ON ADVERSE SITES. WATER APPLICATION RATES SHOULD BE CONTROLLED TO PROVIDE ADEQUATE MOISTURE WITHOUT CAUSING RUNOFF.
- RE-SEEDING: AREAS WHICH FAIL TO ESTABLISH GRASS COVER ADEQUATE TO PREVENT EROSION SHALL BE RE-SEEDDED AS SOON AS SUCH AREAS ARE IDENTIFIED, AND ALL APPROPRIATE MEASURES TAKEN TO ESTABLISH ADEQUATE COVER.

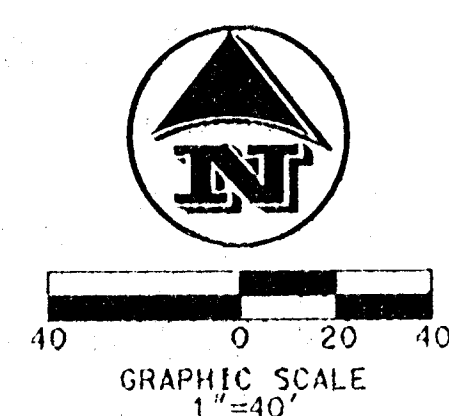
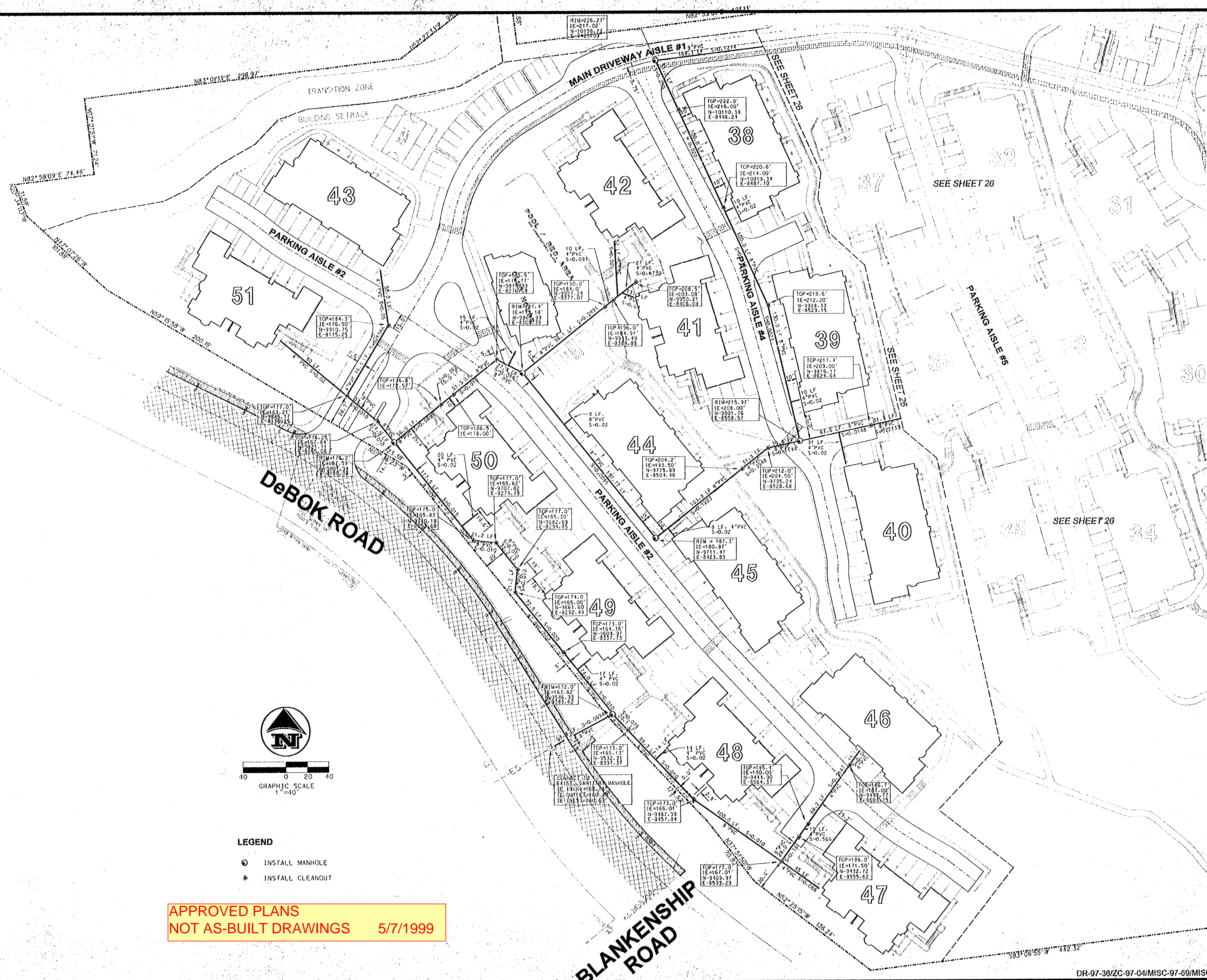
EROSION CONTROL NOTES AND DETAILS

for
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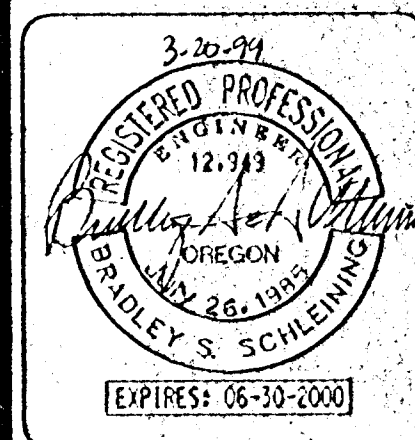


- LEGEND**
- INSTALL MANHOLE
 - INSTALL CLEANOUT

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

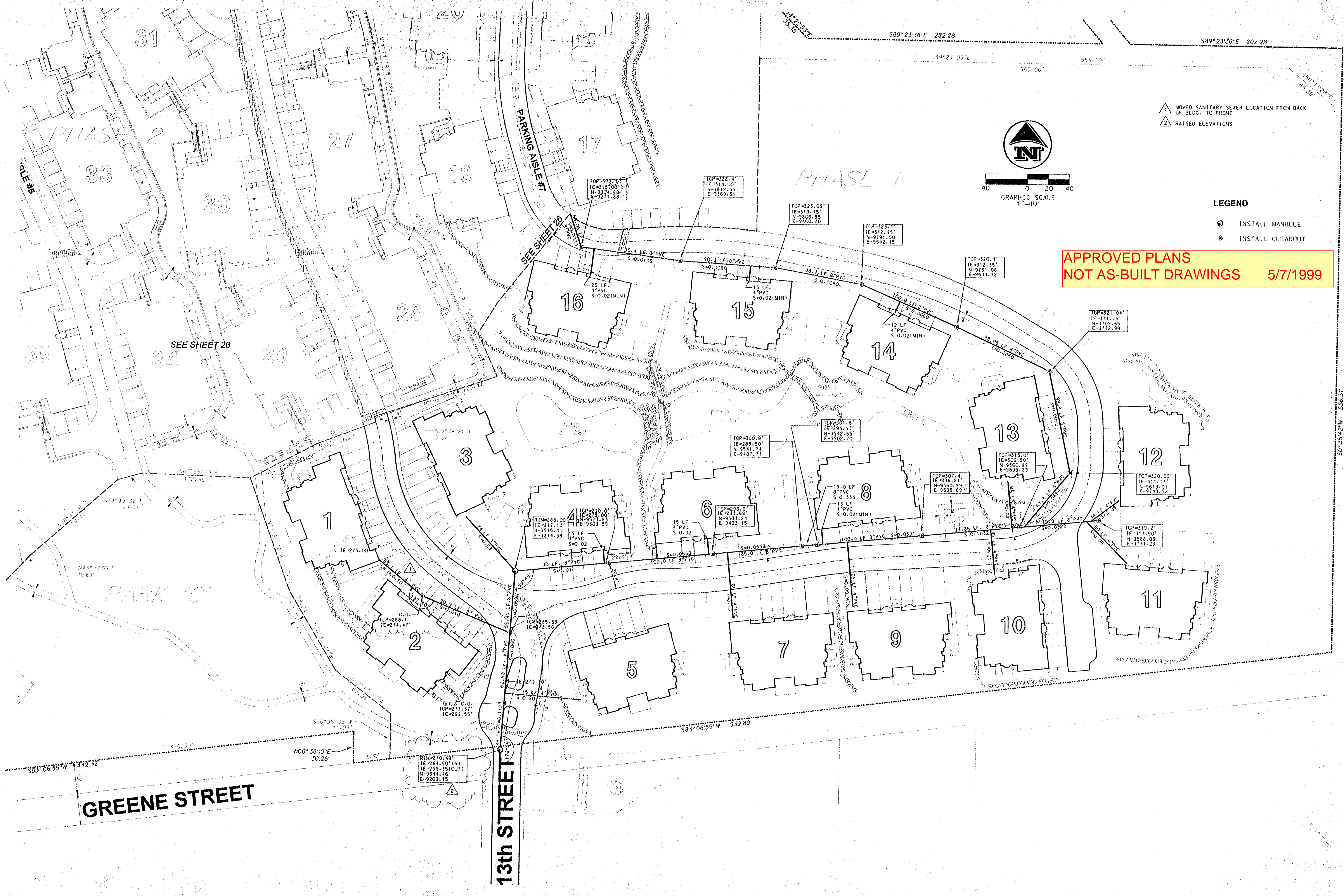
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|---------|-------------|------|
| 3-20-99 | | |

SANITARY SEWER PLAN SHEET 1 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

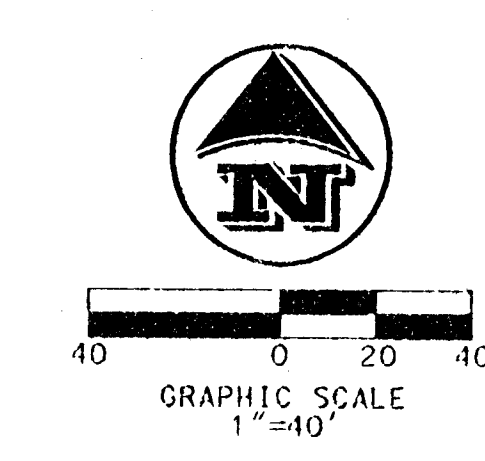


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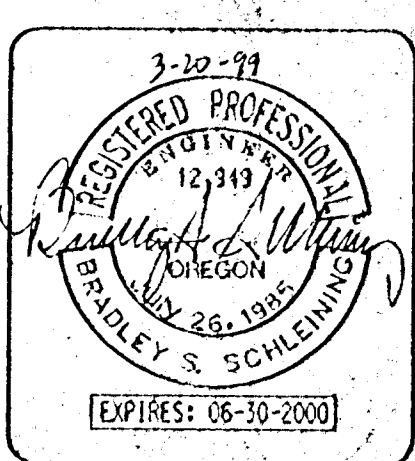
△ MOVED SANITARY SEWER LOCATION FROM BACK OF BLDG. TO FRONT
△ RAISED ELEVATIONS



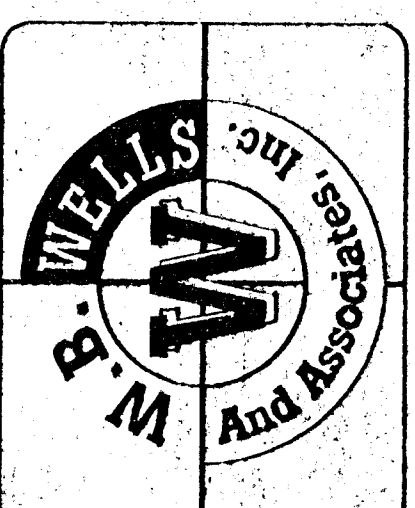
- LEGEND**
- INSTALL MANHOLE
 - INSTALL CLEANOUT

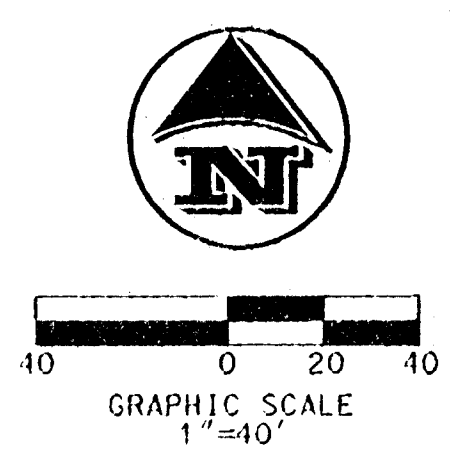
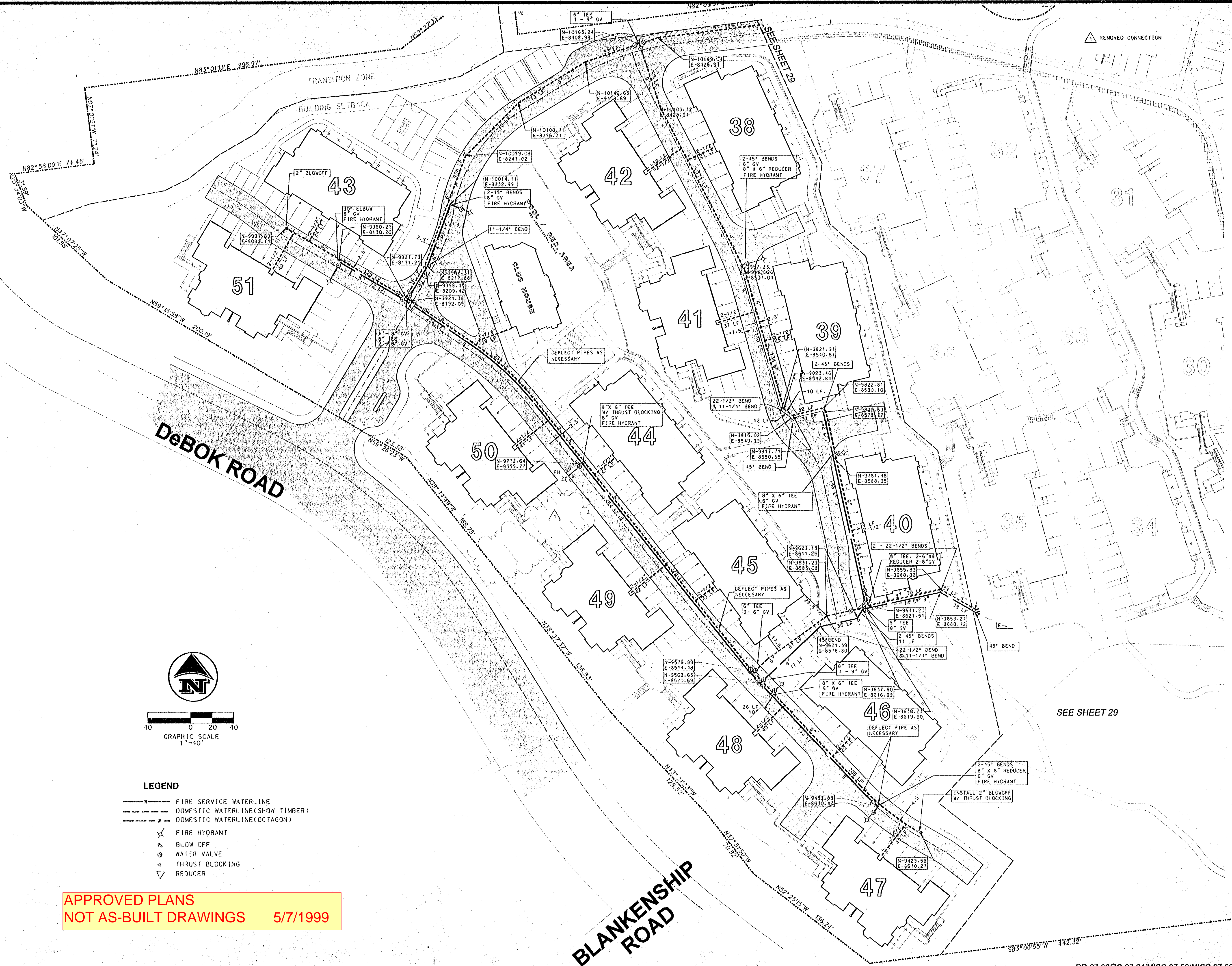
**APPROVED PLANS
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SANITARY SEWER PLAN SHEET 3 OF 3
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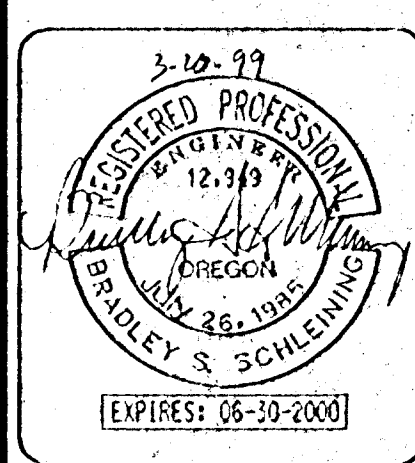


- LEGEND**
- FIRE SERVICE WATERLINE
 - - - DOMESTIC WATERLINE (SHOW FIMBER)
 - - - DOMESTIC WATERLINE (OCTAGON)
 - ⋈ FIRE HYDRANT
 - ⊙ BLOW OFF
 - ⊕ WATER VALVE
 - ⊖ THRUST BLOCKING
 - ▽ REDUCER

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

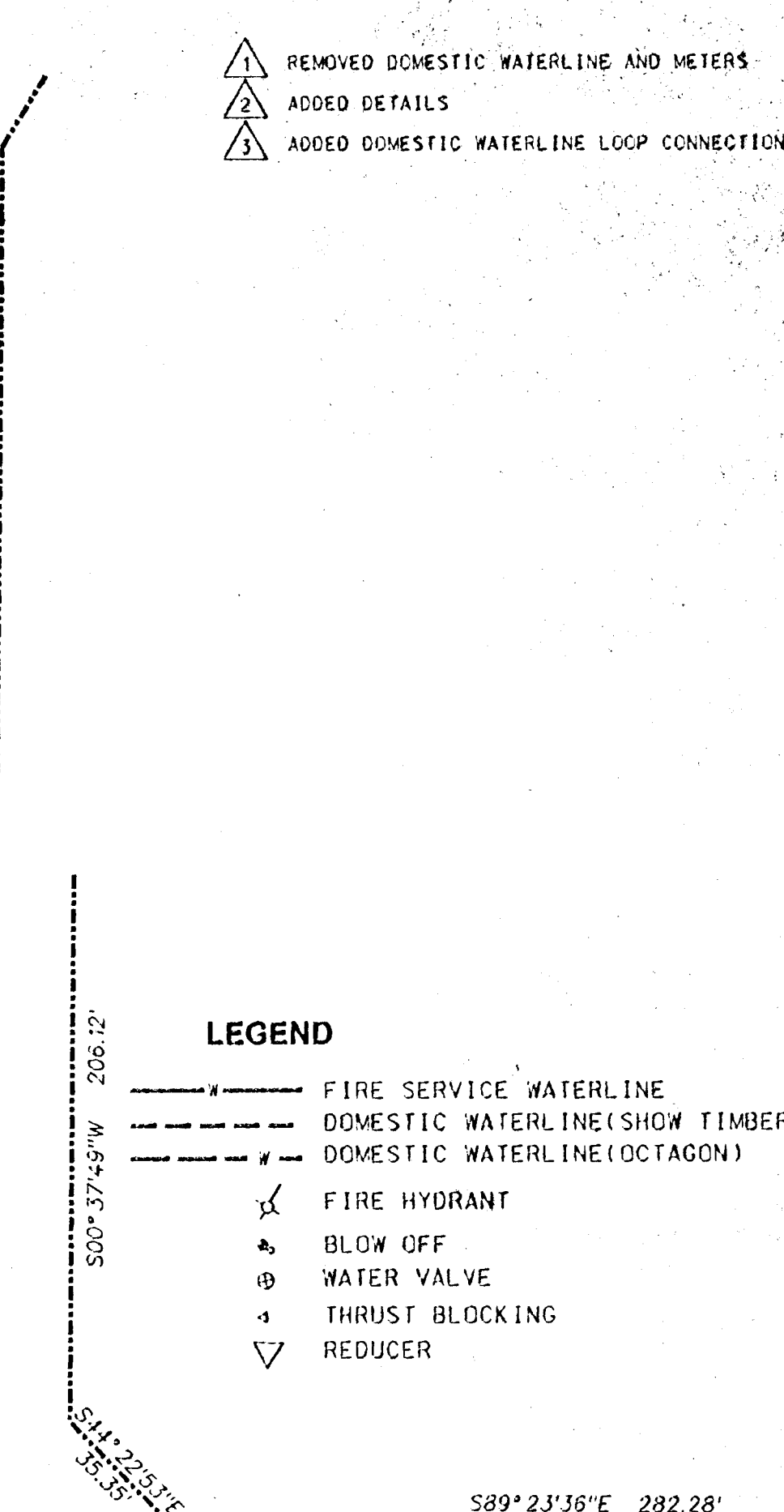
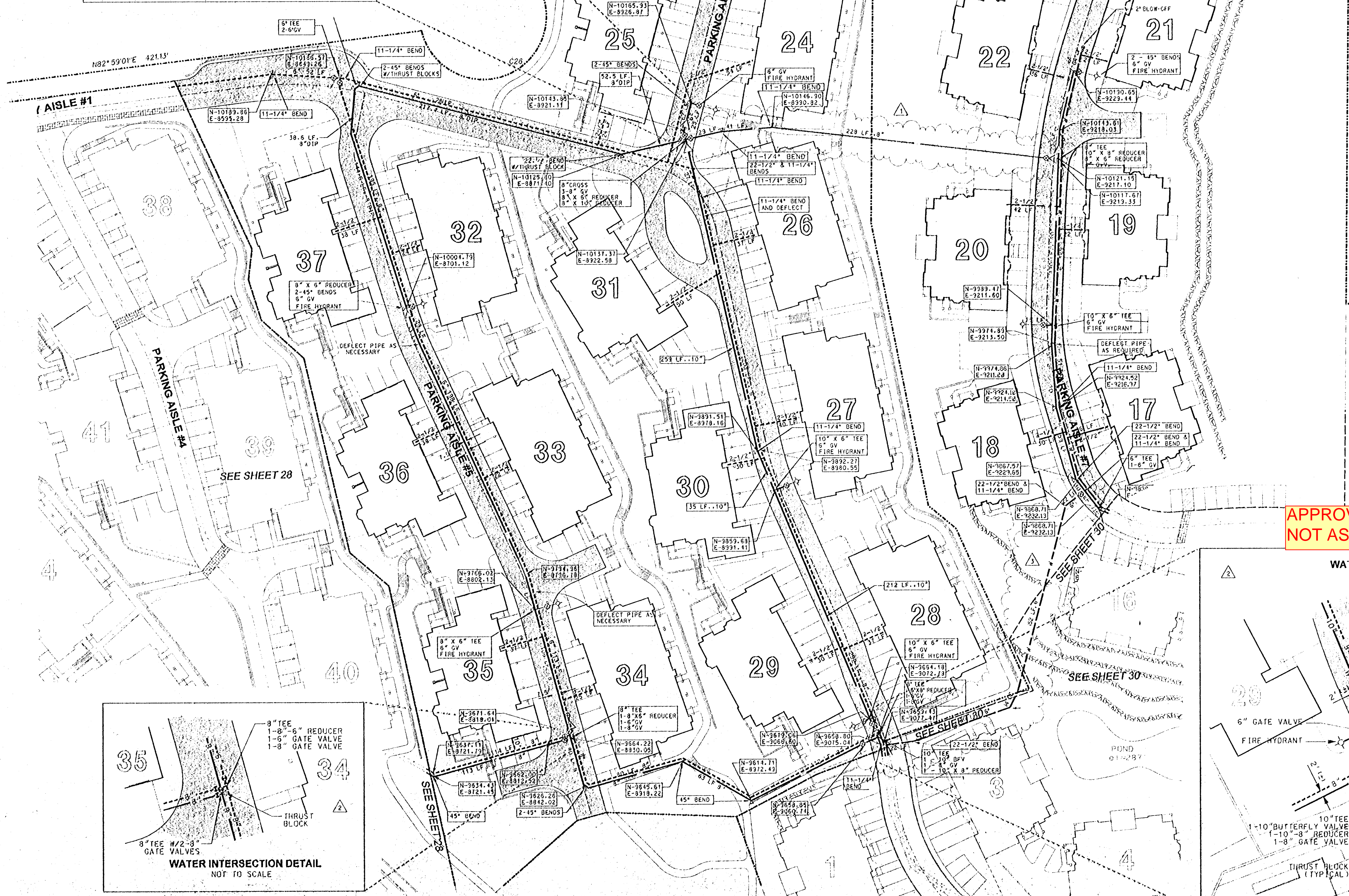
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|---------------|--------------|-----------------|--------|
| 3-20-95 | | | |
| DESIGNED: BSS | CHECKED: BSS | DATE: 11-20-98 | |
| DRAWN: BSS/RT | | JOB NO.: 56-340 | SCALE: |

WATER PLAN SHEET 1 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

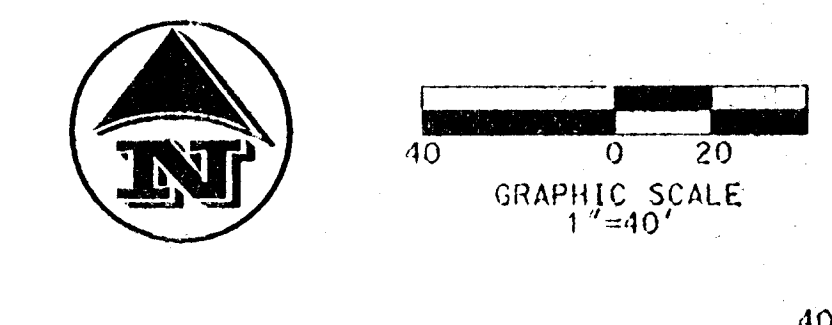


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and associates inc.
ENGINEERS • SURVEYORS • PLANNERS
4230 NE FREMONT STREET
PORTLAND, OREGON 97213
PHONE (503) 284-5896
FAX (503) 284-8530

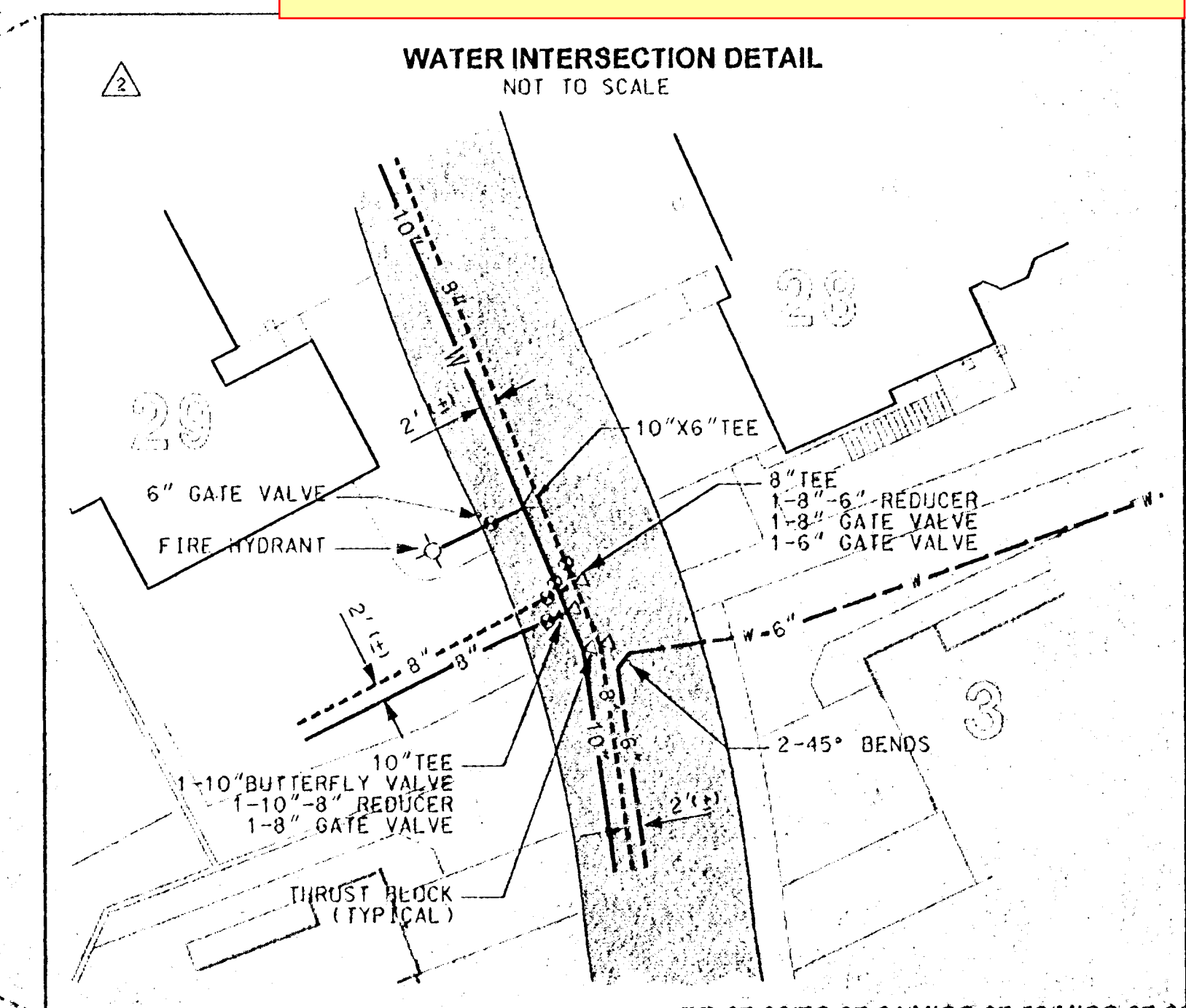




NOTE: BUILDINGS 24,25,26,27,28,29,30,31,32,
33,34,35,36, & 37 MAY REQUIRE WATER
PRESSURE BOOSTERS TO MEET POTABLE
WATER PRESSURE REQUIREMENTS. A SET
FORTH BY SYSTEM DESIGN CONSULTANTS, INC.
PRESSURE REQUIREMENTS ARE 50 psi (MINIMUM.)
SERVICING THE BUILDING.

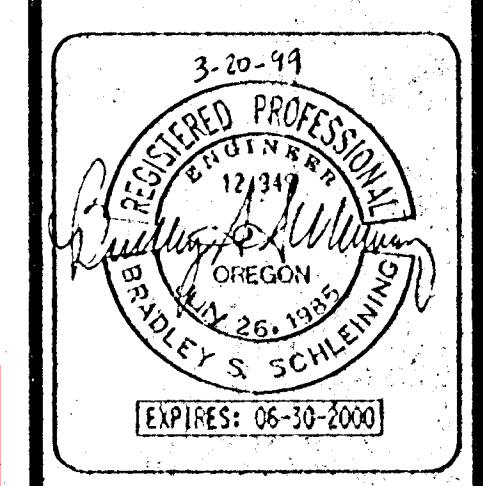


APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



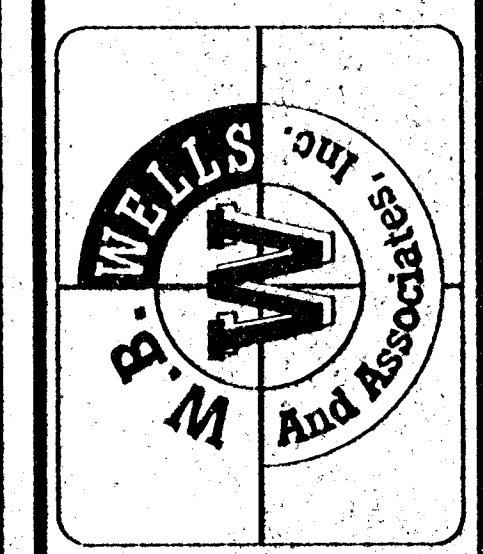
| | |
|-----------------|----------------|
| 3-20-99 | REVISIONS |
| | |
| | |
| | |
| | |
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| | |
| | |
| DESIGNED: BGS | CHECKED: BSS |
| DRAWN: BSS/RT | DATE: 11-20-98 |
| JOB NO.: 96-340 | SCALE: |

WATER PLAN SHEET 2 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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and associates inc.

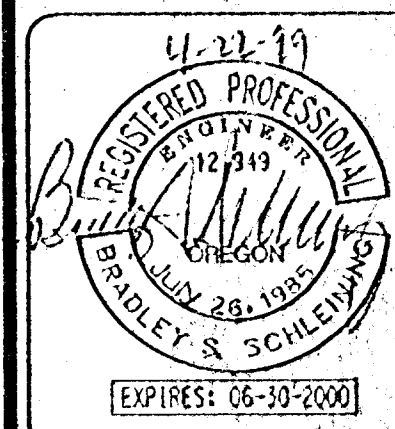
ENGINEERS • SURVEYORS • PLANNERS
4230 NE FREMONT STREET
PORTLAND, OREGON 97213
PHONE (503) 284-5896
FAX (503) 284-8530





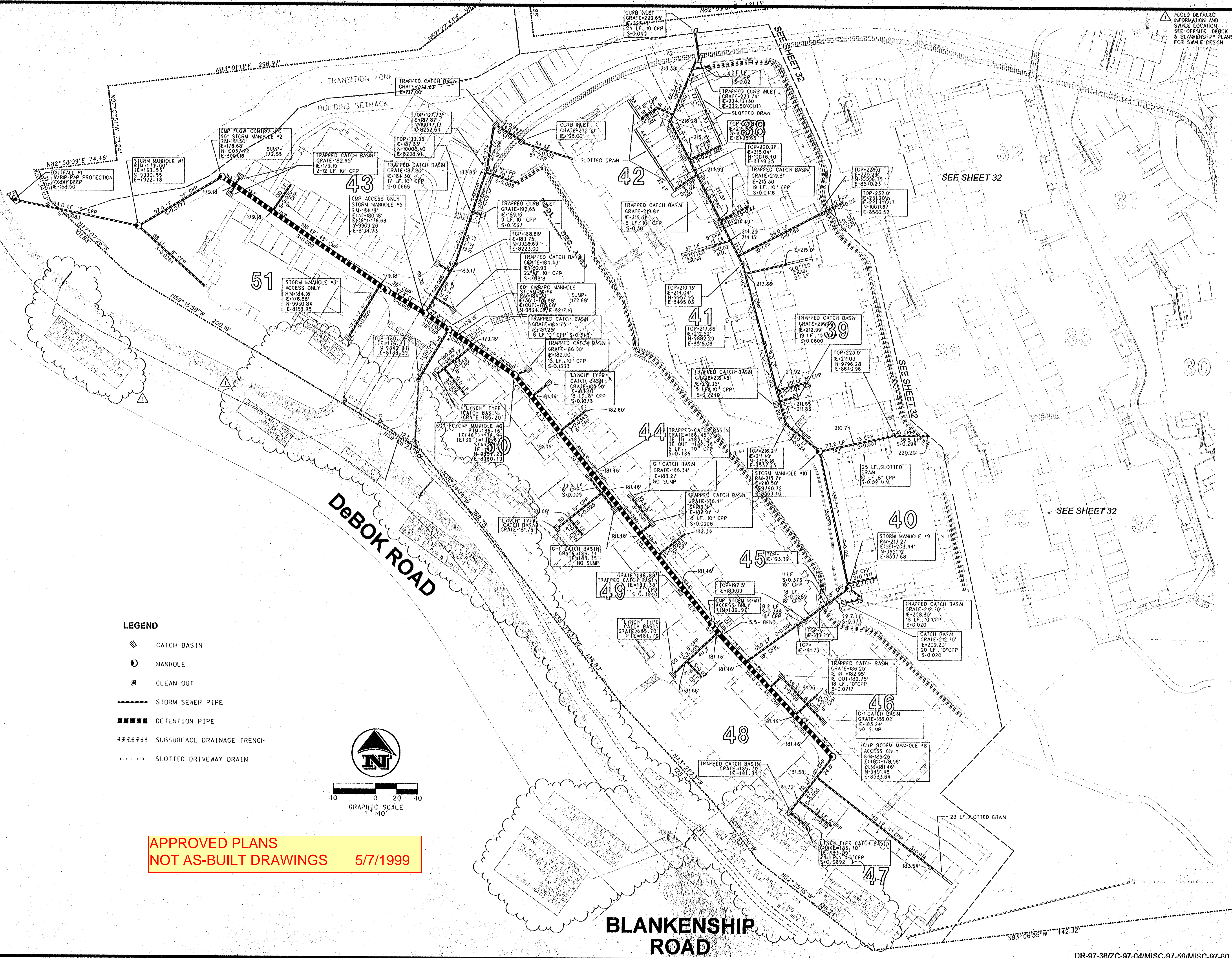
| REVISIONS | DATE | BY | CHKD | APPD |
|-----------------|----------------|----|------|------|
| 3-20-99 | | | | |
| DESIGNED: BSS | CHECKED: BSS | | | |
| DRAWN: BSS/RT | DATE: 11-20-98 | | | |
| JOB NO.: 96-340 | SCALE: | | | |

WATERLINE PLAN SHEET 3 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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LEGEND

- CATCH BASIN
- MANHOLE
- CLEAN OUT
- STORM SEWER PIPE
- DEFENTION PIPE
- SUBSURFACE DRAINAGE TRENCH
- SLOTTED DRIVEWAY DRAIN

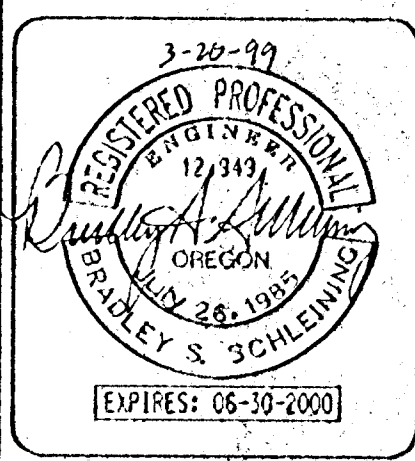


GRAPHIC SCALE
1"=40'

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

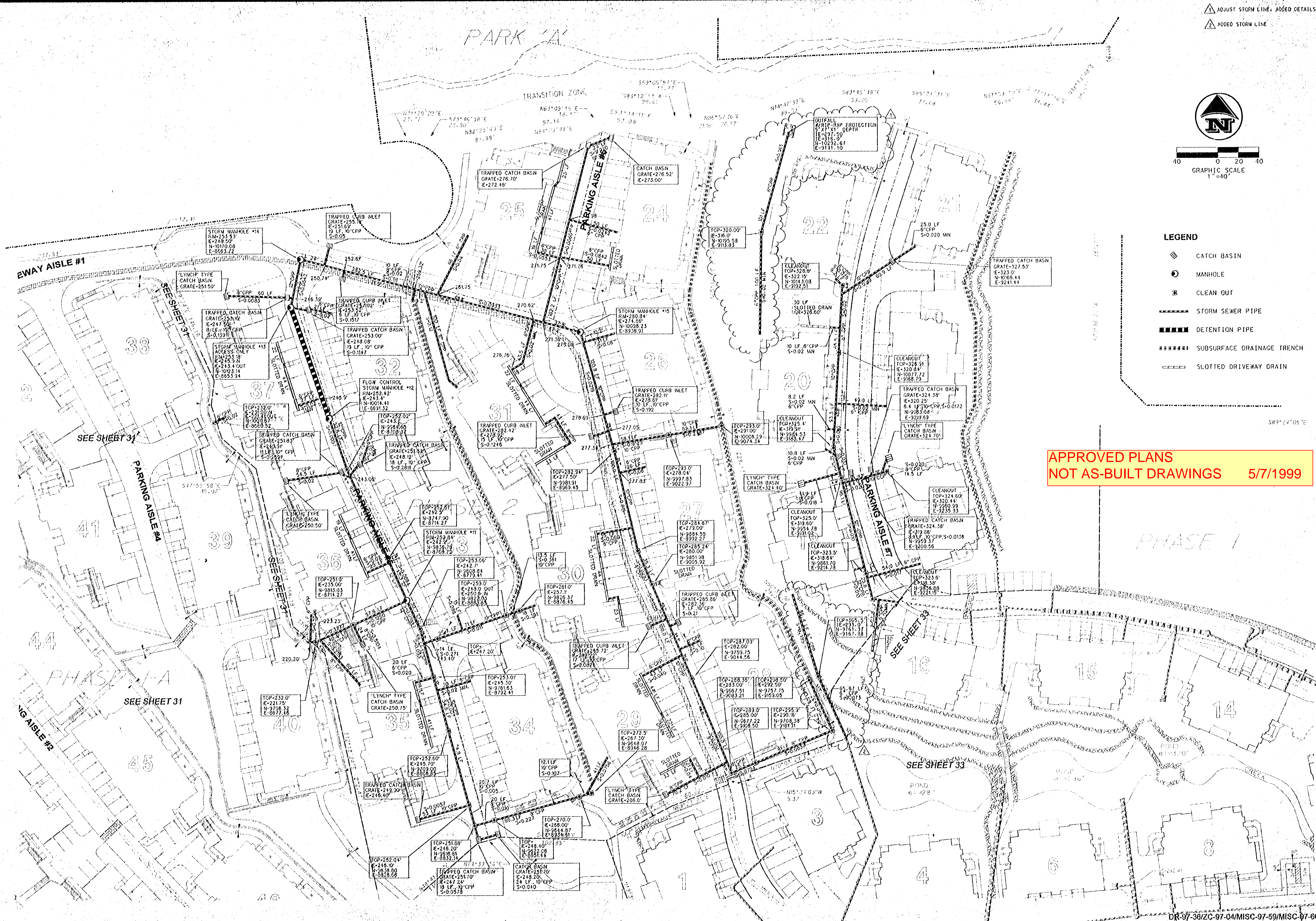
| REVISIONS | |
|------------------|----------------|
| 3-20-95 | |
| DESIGNED: BSS | CHECKED: BSS |
| DRAWN: BSS/RT | DATE: 11-20-98 |
| JOB NO. 1-96-340 | SCALE: |

STORM SEWER PLAN SHEET 1 OF 3
for
SUMMERLINN APARTMENTS
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△ ADJUST STORM LINE, ADDED DETAILS
△ ADDED STORM LINE



GRAPHIC SCALE
1"=40'

LEGEND

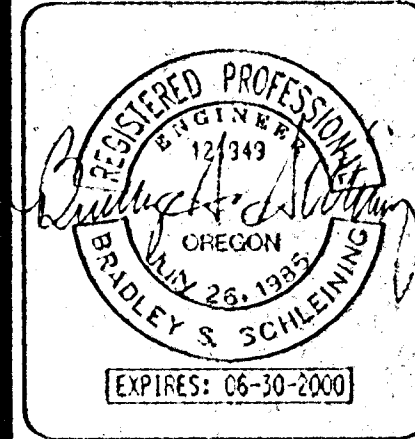
- CATCH BASIN
- MANHOLE
- CLEAN OUT
- STORM SEWER PIPE
- DETENTION PIPE
- SUBSURFACE DRAINAGE TRENCH
- SLOTTED DRIVEWAY DRAIN

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

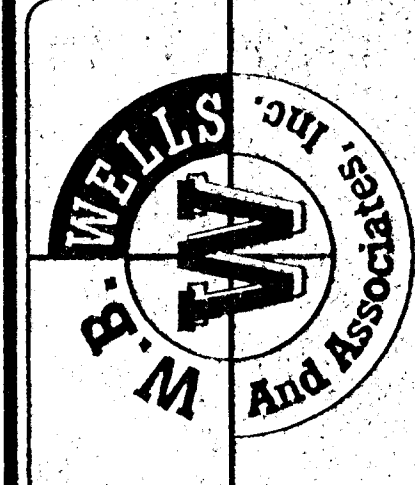
STORM SEWER PLAN SHEET 2 OF 3

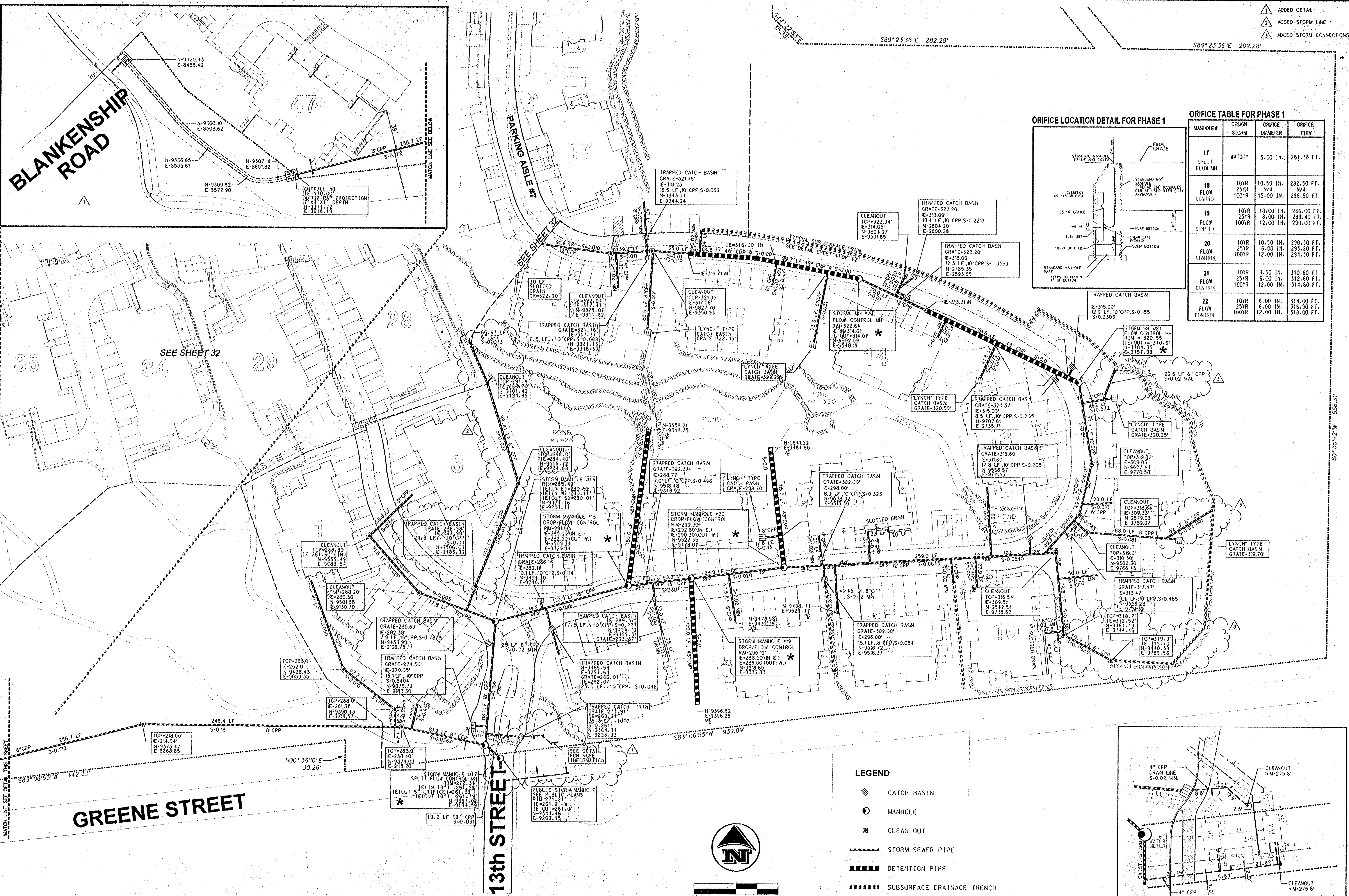
for
SUMMERLINN APARTMENTS

CITY OF WEST Linn, CLACKAMAS COUNTY, OREGON



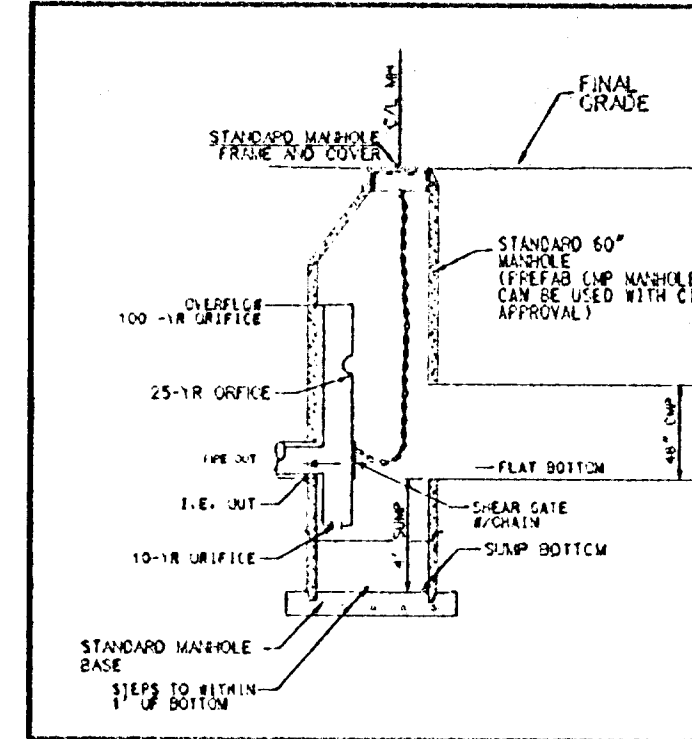
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FAX (503) 284-8530





- △ ADDED DETAIL
- △ ADDED STORM LINE
- △ ADDED STORM CONNECTIONS

ORIFICE LOCATION DETAIL FOR PHASE 1

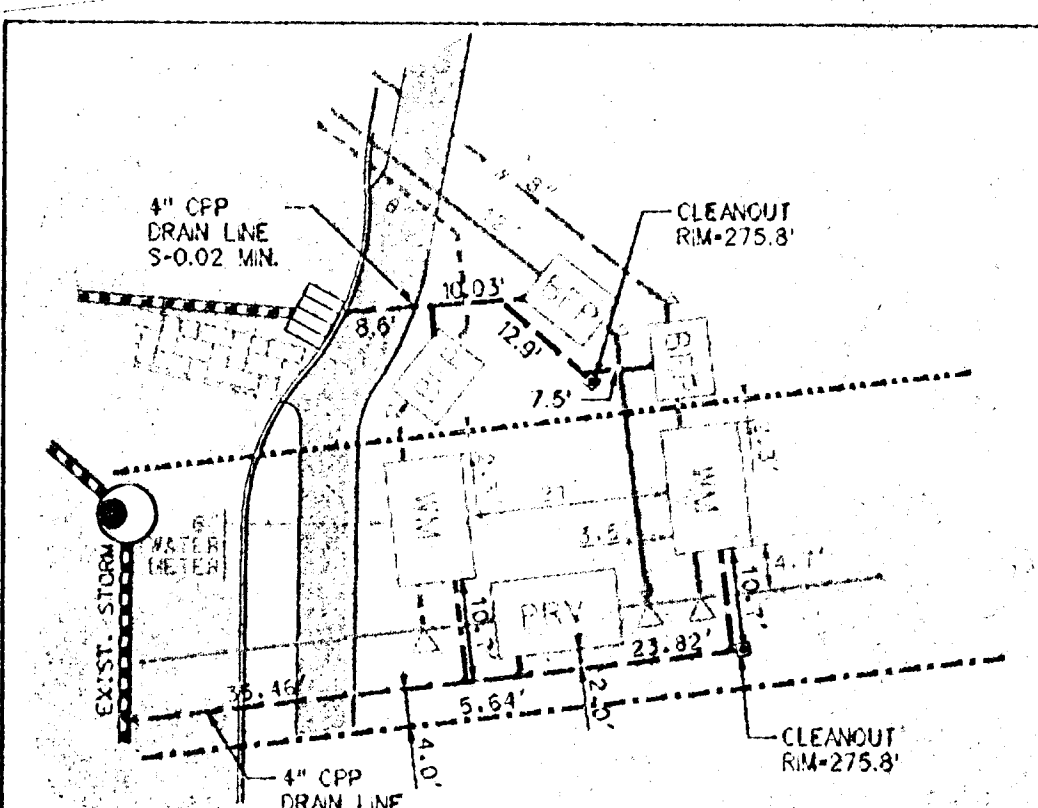


ORIFICE TABLE FOR PHASE 1

| MANHOLE # | DESIGN STORM | ORIFICE DIAMETER | ORIFICE ELEV. |
|-----------|-------------------|--------------------------|-----------------------|
| 17 | WATQTY | 5.00 IN. | 261.38 FT. |
| 18 | 10YR FLOW CONTROL | 10.50 IN. 25YR 15.00 IN. | 282.50 FT. N/A |
| 19 | 10YR FLOW CONTROL | 10.00 IN. 25YR 12.00 IN. | 286.00 FT. 283.40 FT. |
| 20 | 10YR FLOW CONTROL | 10.50 IN. 25YR 12.00 IN. | 230.30 FT. 231.20 FT. |
| 21 | 10YR FLOW CONTROL | 9.50 IN. 25YR 12.00 IN. | 310.60 FT. 312.60 FT. |
| 22 | 10YR FLOW CONTROL | 6.00 IN. 25YR 12.00 IN. | 314.00 FT. 318.00 FT. |

LEGEND

- CATCH BASIN
- MANHOLE
- CLEAN OUT
- STORM SEWER PIPE
- DETENTION PIPE
- ===== SUBSURFACE DRAINAGE TRENCH
- - - - SLOTTED DRIVEWAY DRAIN
- * SEE ORIFICE TABLE

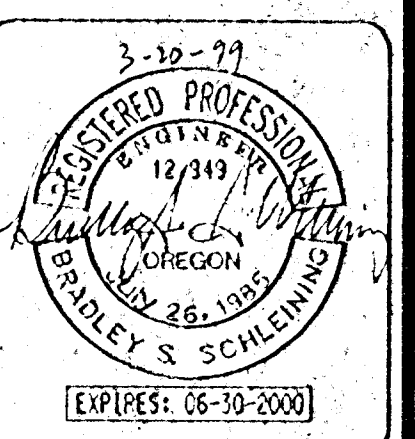


VAULT DRAIN LAYOUT DETAIL
SCALE: HORIZ. 1"=20'

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

| REVISIONS | DESIGNED: BSS | CHECKED: BSS |
|-----------|---------------|-----------------|
| 3-20-99 | DRAWN: BSS/RT | DATE: 11-20-98 |
| | | JOB NO.: 98-340 |
| | | SCALE: |

STORM SEWER PLAN SHEET 3 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



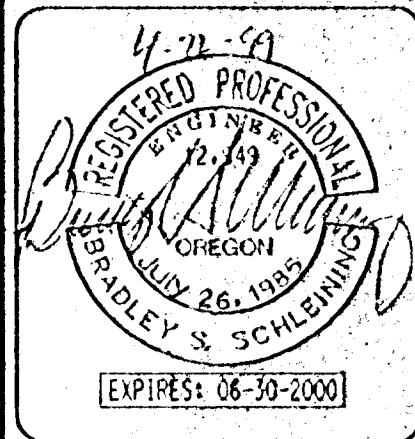
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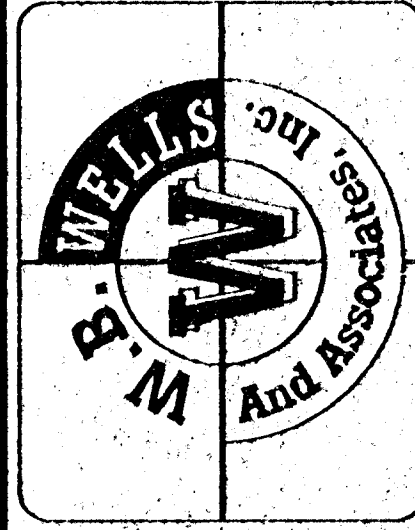
| | |
|---|----------------|
| REVISIONS | |
| 3-20-99 | |
| 4-22-99 Change Vertical to Curved Lines | |
| DESIGNED: BSS | CHECKED: BSS |
| DRAWN: BSS/RT | DATE: 11-20-98 |
| JOB NO.: 96-340 | SCALE: |



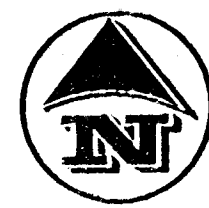
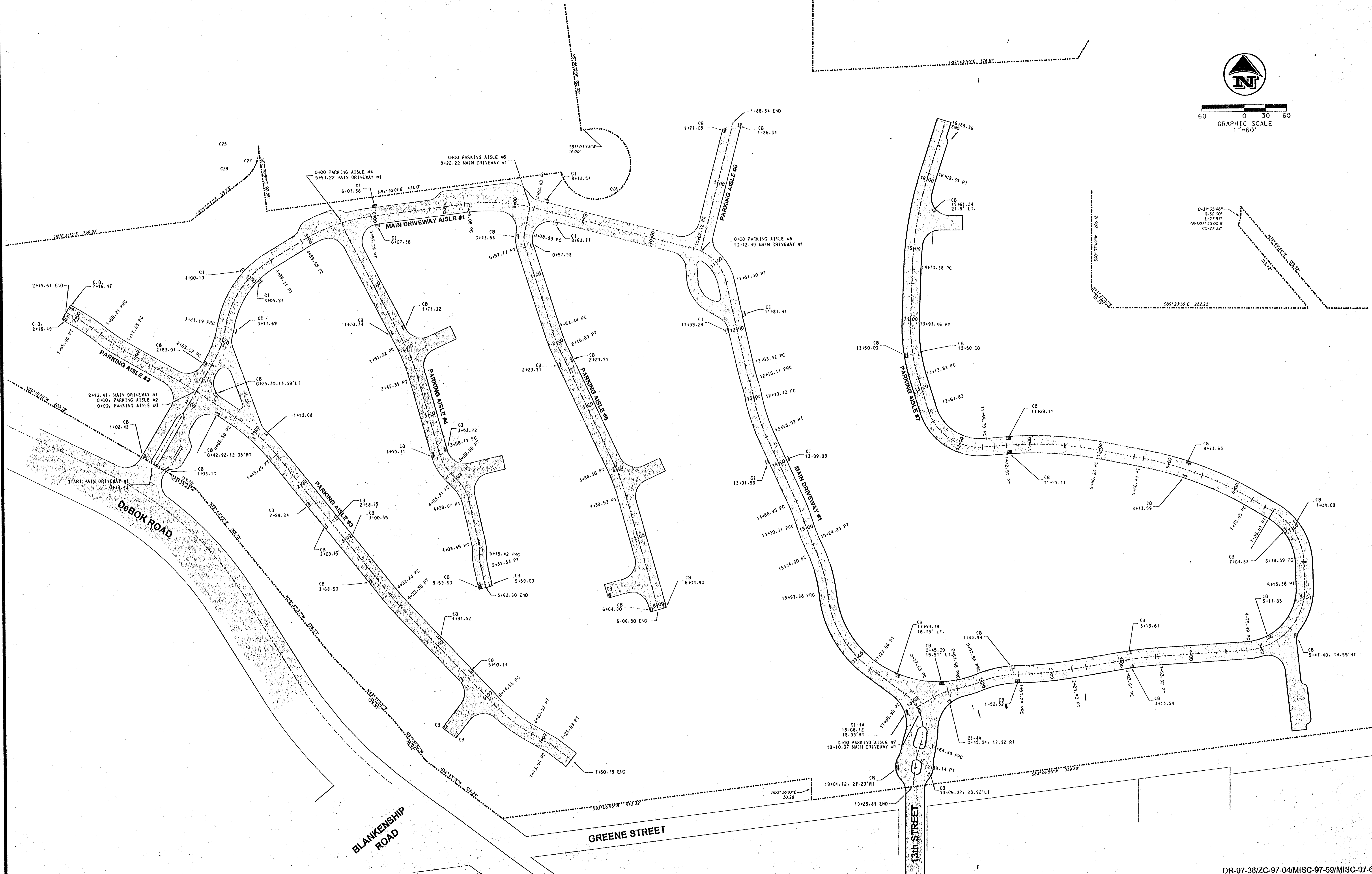
DRIVEWAY GEOMETRY PLAN
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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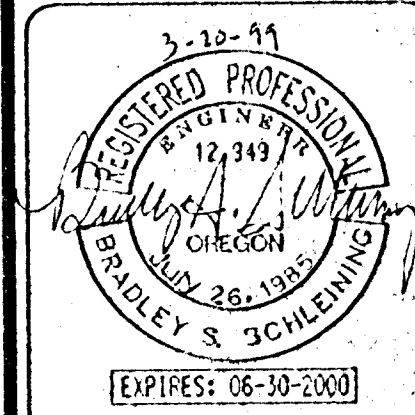
APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999



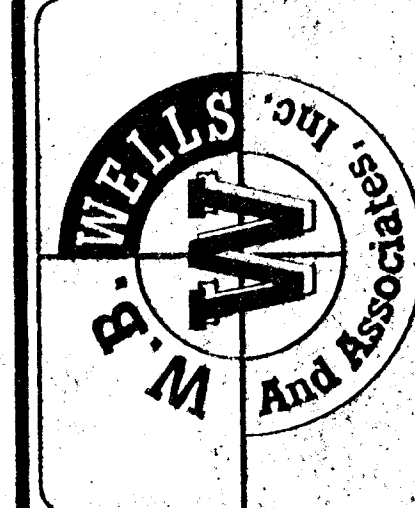
GRAPHIC SCALE
1"=60'

| REVISIONS | | | |
|-----------------|----------------|--|--|
| 3-20-99 | | | |
| DESIGNED: BSS | CHECKED: BSS | | |
| DRAWN: BSS/AT | DATE: 11-20-98 | | |
| JOB NO.: 56-340 | SCALE: | | |

STATIONING PLAN
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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DR-97-38/ZC-97-04/MISC-97-59/MISC-97-60

APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

VERTICAL CURVE DATA LENGTH 150
PVC STA 7+88.46 ELEV 252.2
PVI STA 8+63.46 ELEV 255.43
PVT STA 9+38.46 ELEV 266.68

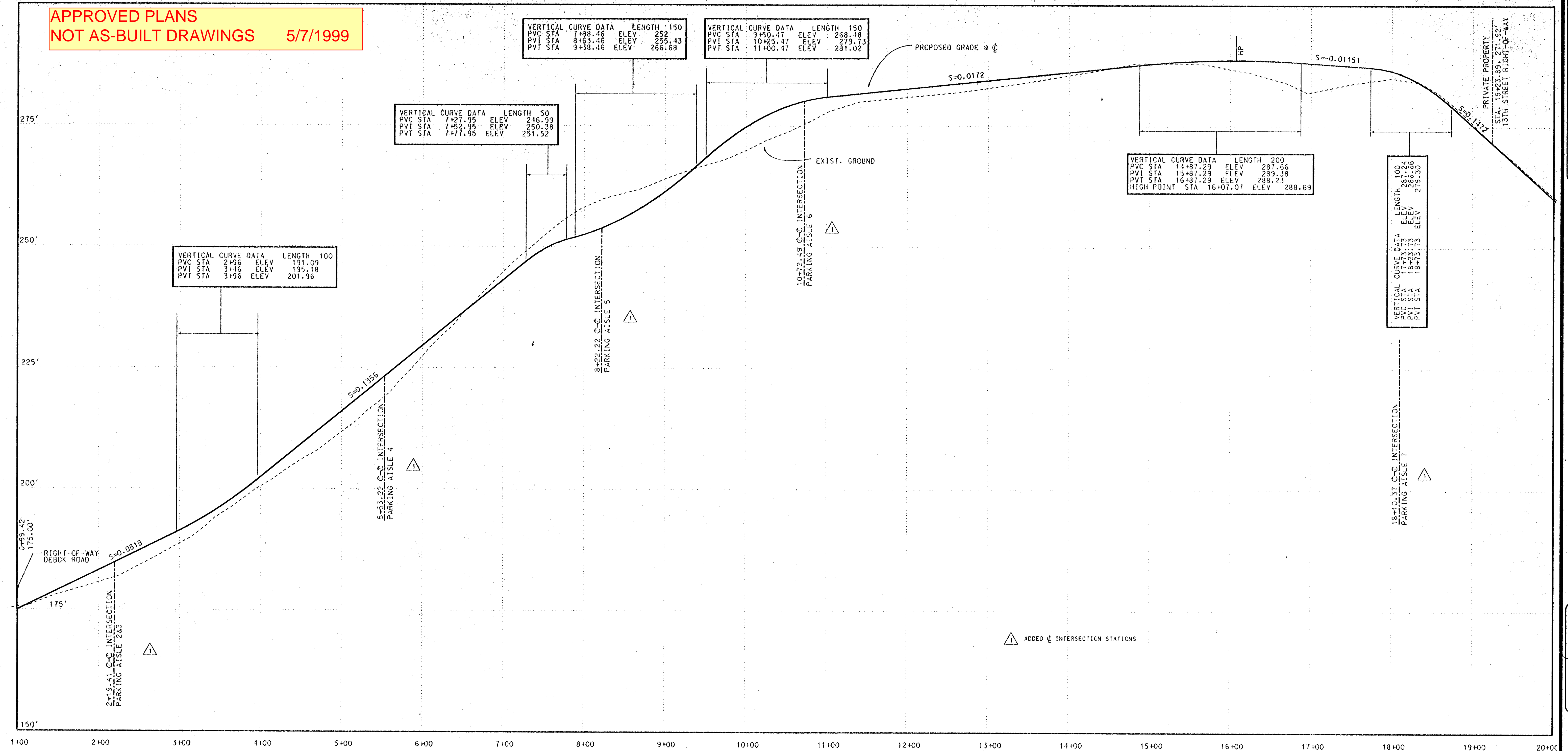
VERTICAL CURVE DATA LENGTH 150
PVC STA 9+50.47 ELEV 268.48
PVI STA 10+25.47 ELEV 279.73
PVT STA 11+00.47 ELEV 291.02

VERTICAL CURVE DATA LENGTH 50
PVC STA 7+27.95 ELEV 246.93
PVI STA 7+52.95 ELEV 250.38
PVT STA 7+77.95 ELEV 251.52

VERTICAL CURVE DATA LENGTH 100
PVC STA 2+96 ELEV 191.09
PVI STA 3+46 ELEV 195.18
PVT STA 3+96 ELEV 201.96

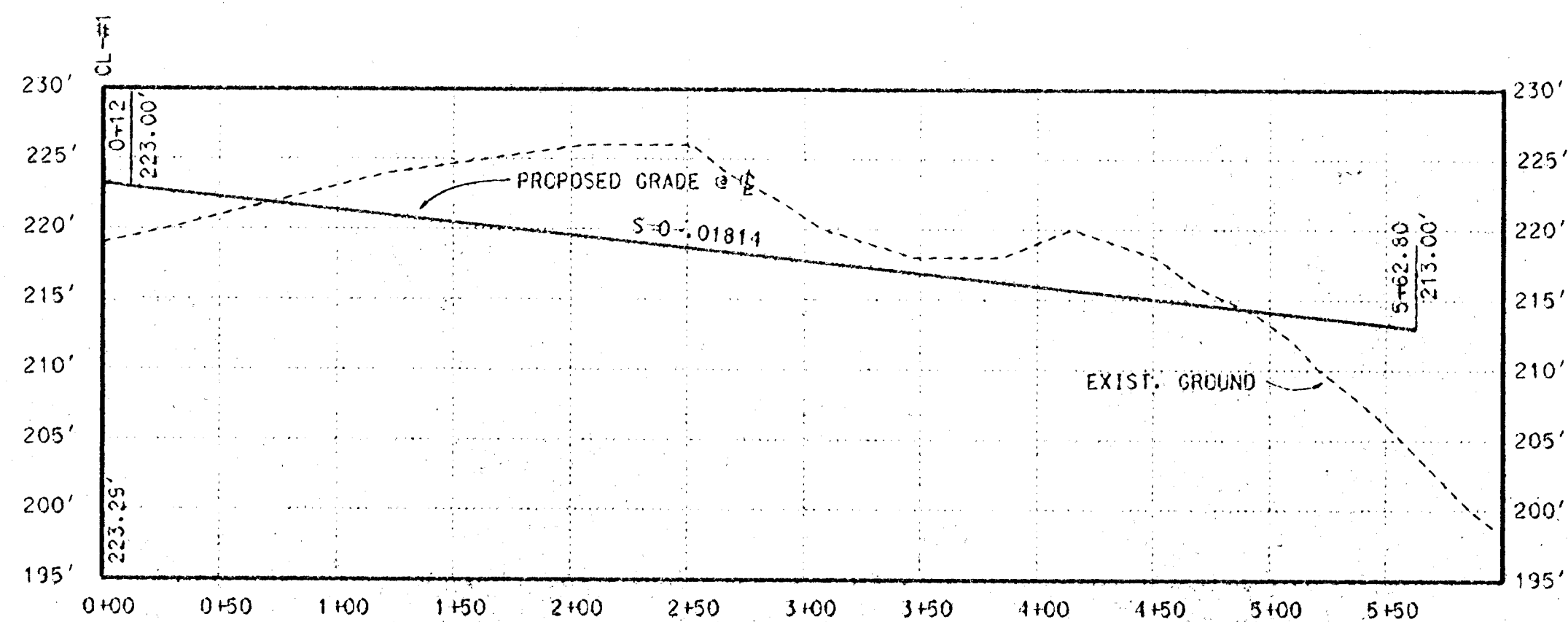
VERTICAL CURVE DATA LENGTH 200
PVC STA 14+87.29 ELEV 287.68
PVI STA 15+87.29 ELEV 289.38
PVT STA 16+87.29 ELEV 288.23
HIGH POINT STA 16+07.07 ELEV 288.69

VERTICAL CURVE DATA LENGTH 100
PVC STA 17+73.73 ELEV 286.06
PVI STA 18+23.73 ELEV 287.24
PVT STA 18+73.73 ELEV 288.42



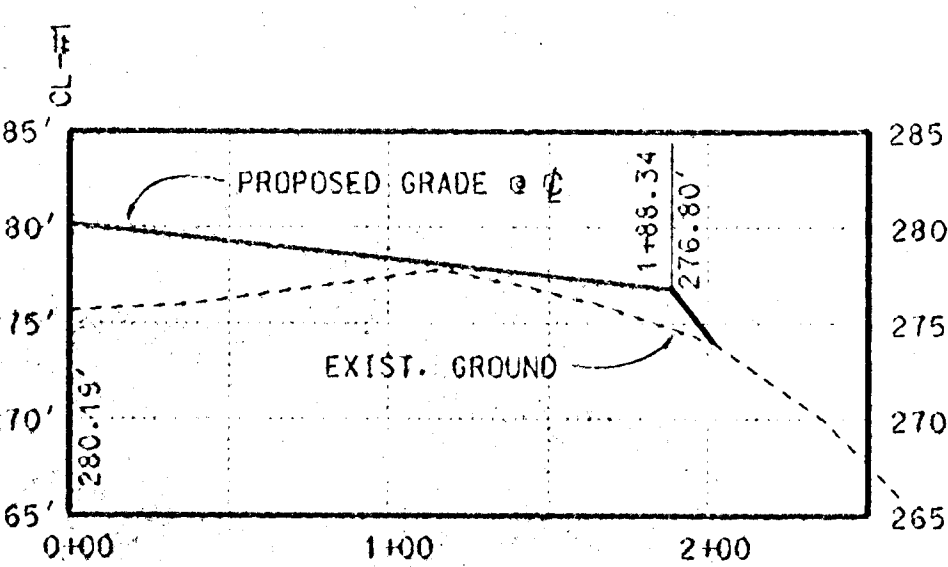
PROFILE FOR MAIN DRIVEWAY AISLE #1

SCALE : HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'



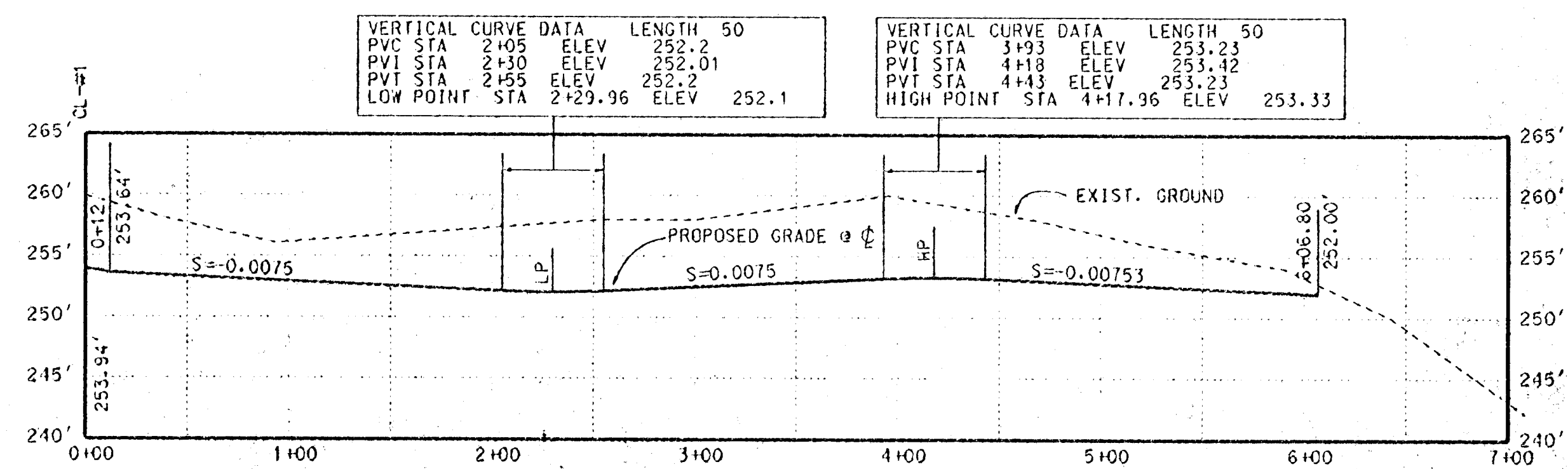
PROFILE FOR PARKING AISLE #4

SCALE : HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'



PROFILE FOR PARKING AISLE #6

SCALE : HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'

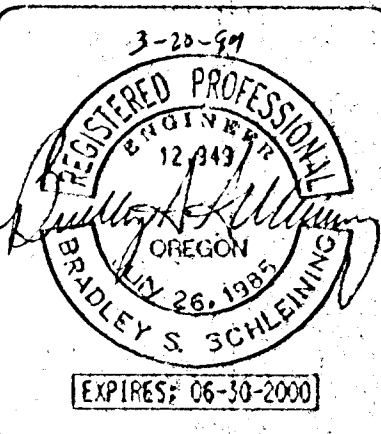


PROFILE FOR PARKING AISLE #5

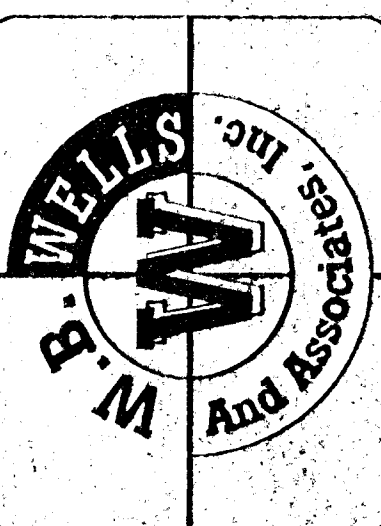
SCALE : HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'

DR-97-36/ZC-97-04/MISC-97-59/MISC-97-60

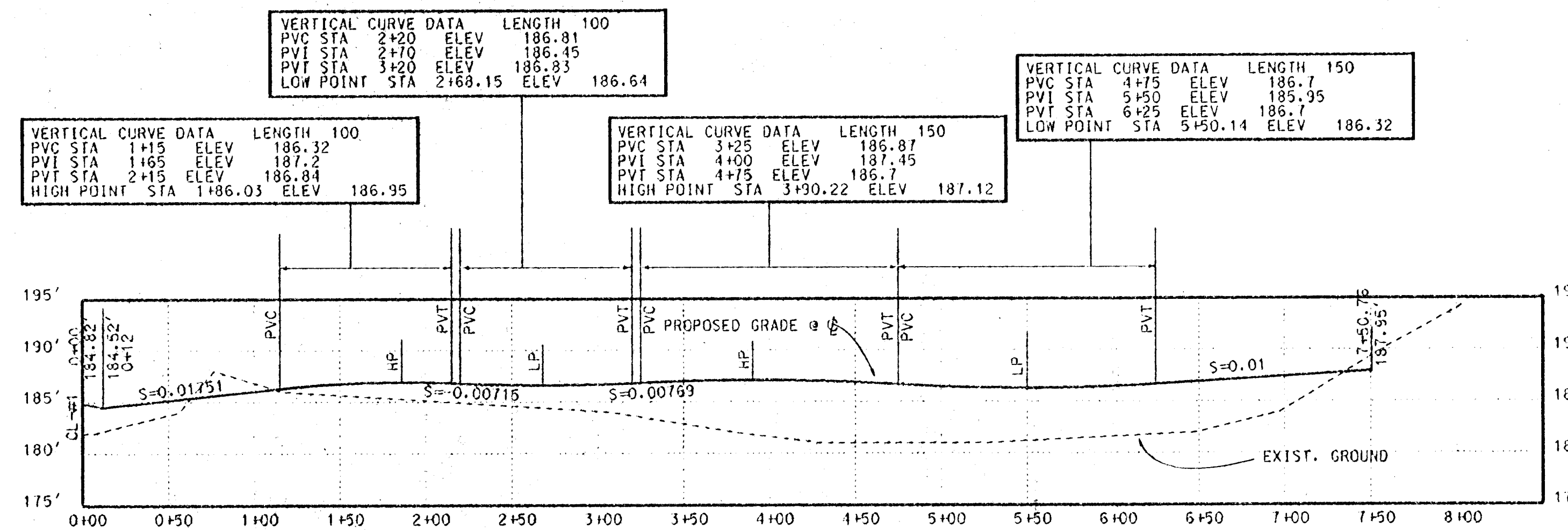
PARKING AISLE PROFILES SHEET 1 OF 2
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



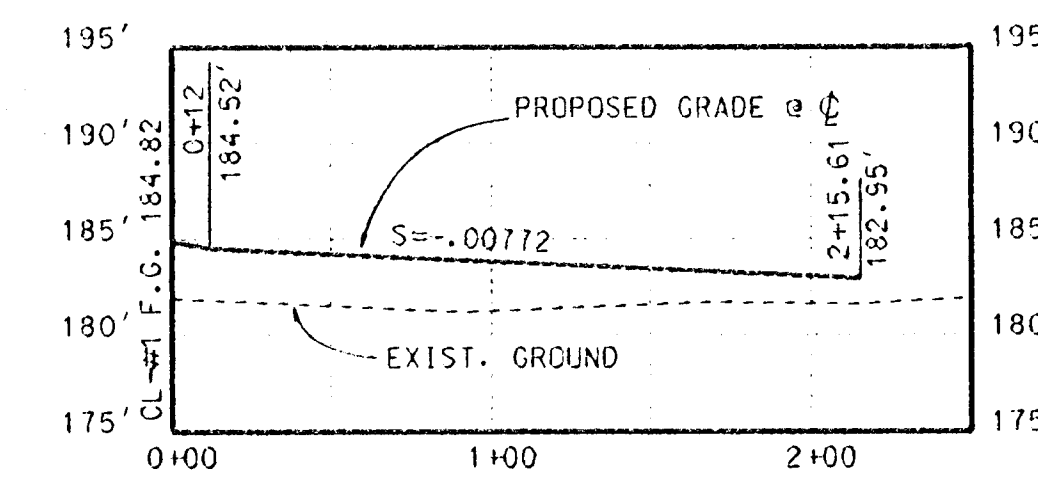
W.B. WELLS
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PORTLAND, OREGON 97213
PHONE (503) 284-5896
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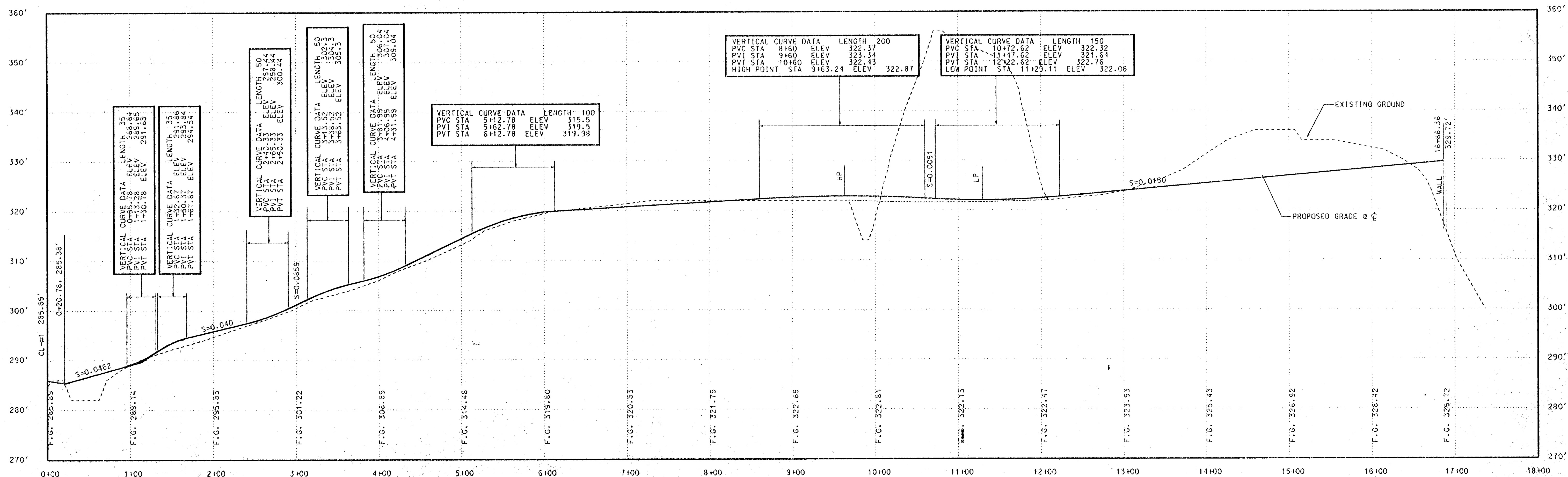
SHEET
36 of 42



PROFILE FOR PARKING AISLE #3
SCALE : HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'



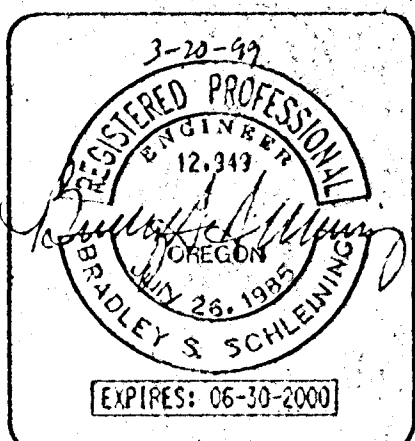
PROFILE FOR PARKING AISLE #2
SCALE : HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'



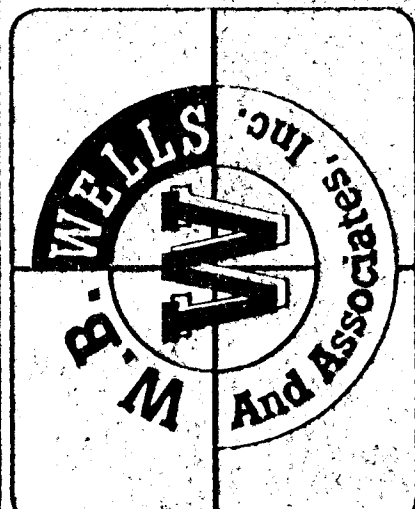
PROFILE FOR PARKING AISLE #7
SCALE : HORIZONTAL - 1" = 60'
VERTICAL - 1" = 10'

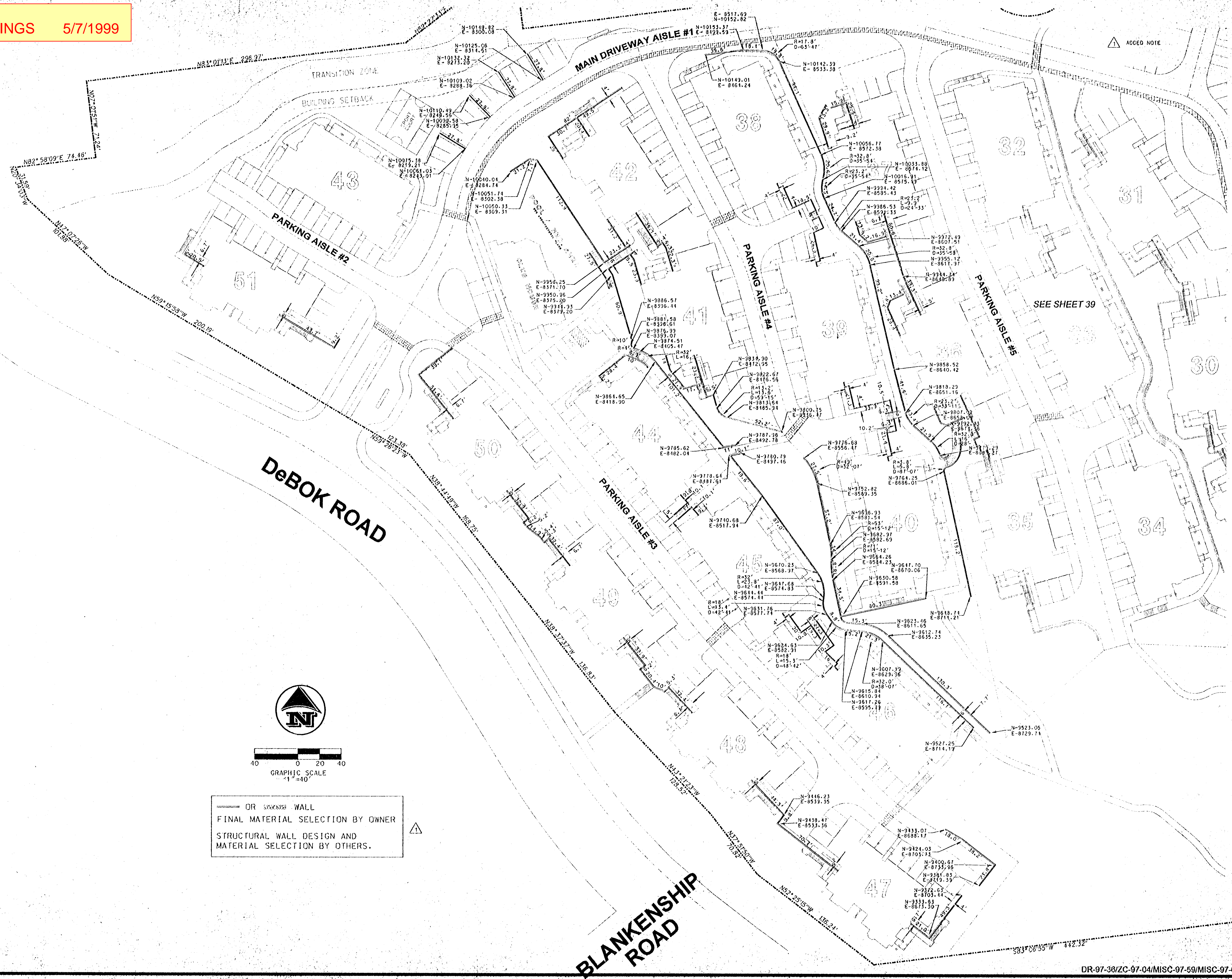
| REVISIONS | DESIGNED: BSS | CHECKED: BSS |
|-----------|-----------------|----------------|
| 3-20-99 | DRAWN: BSS/RT | DATE: 11-20-98 |
| | JOB NO.: 96-340 | SCALE: |

PARKING AISLE PROFILES SHEET 2 OF 2
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



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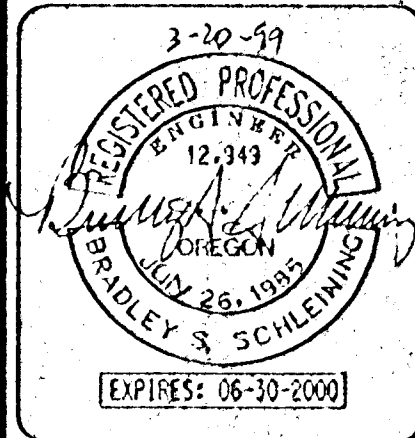




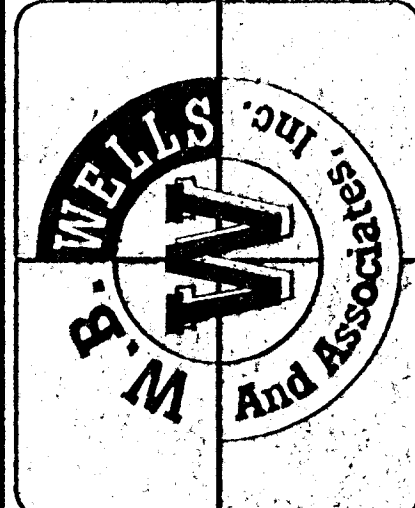
OR STRUCTURE WALL
FINAL MATERIAL SELECTION BY OWNER
STRUCTURAL WALL DESIGN AND
MATERIAL SELECTION BY OTHERS.

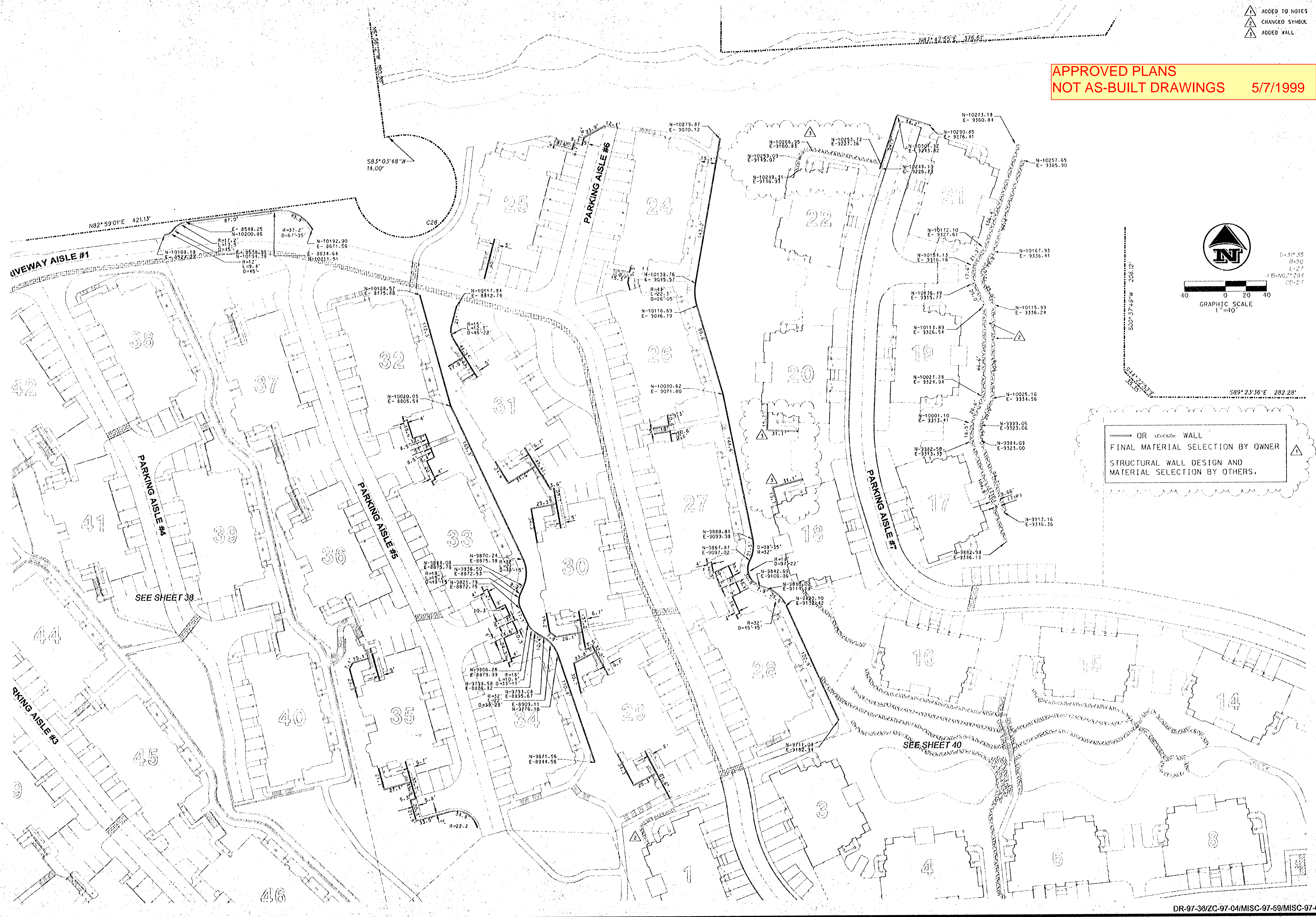
| REVISIONS | | | |
|---------------|--------------|----------------|--|
| 3-20-99 | | | |
| DESIGNED: BSS | CHECKED: BSS | DATE: 11-20-98 | |
| DRAWN: BSS/RT | | | |
| JOE NO. 1 | 96-340 | SCALE: | |

WALL GEOMETRY PLAN SHEET 1 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON



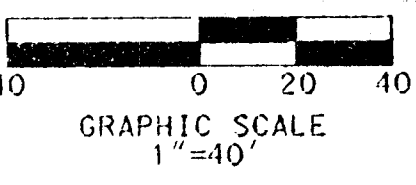
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PHONE (503) 284-5896
FAX (503) 284-8530





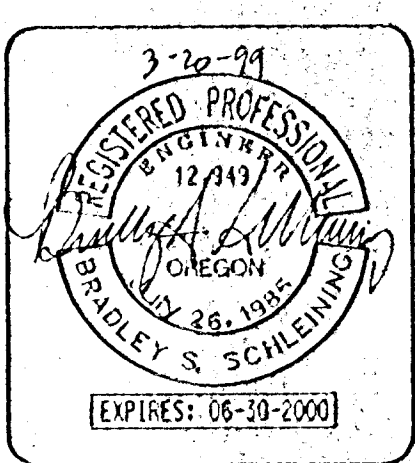
APPROVED PLANS
NOT AS-BUILT DRAWINGS 5/7/1999

- △ ADDED TO NOTES
- △ CHANGED SYMBOL
- △ ADDED WALL



OR VENEER WALL
FINAL MATERIAL SELECTION BY OWNER
STRUCTURAL WALL DESIGN AND
MATERIAL SELECTION BY OTHERS.

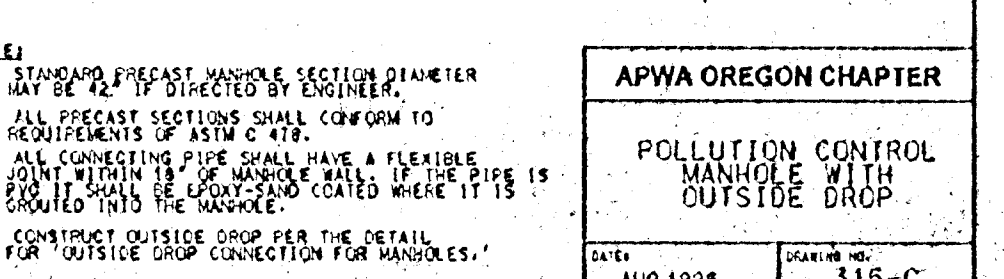
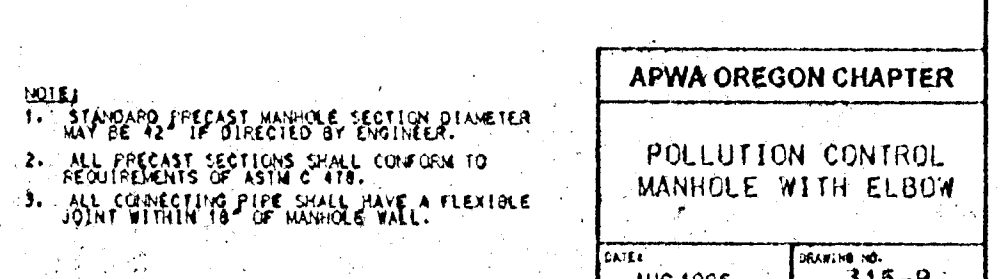
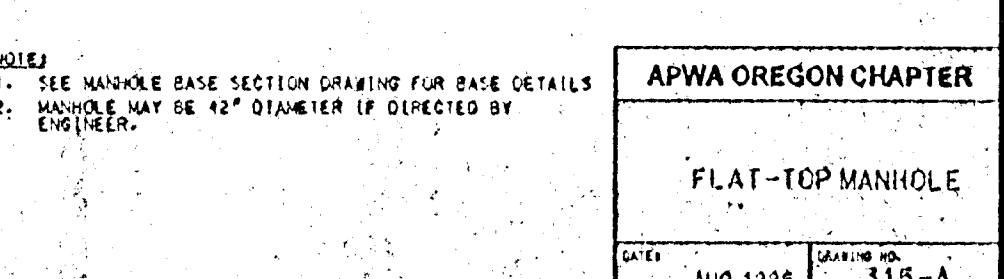
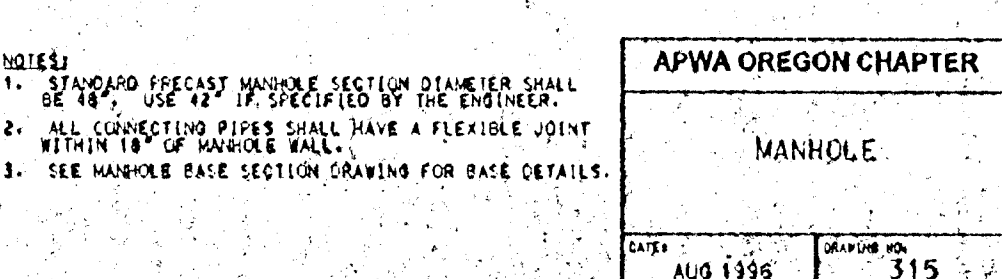
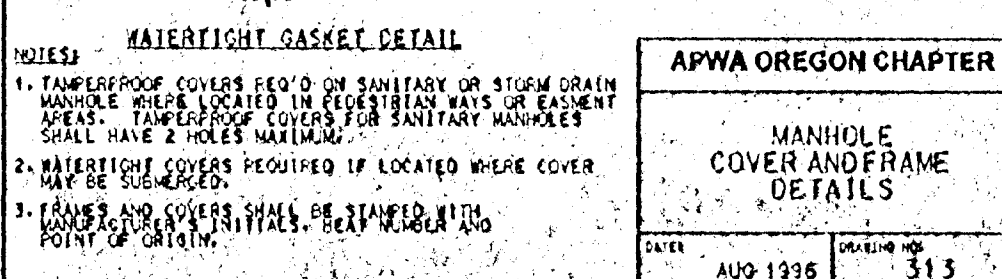
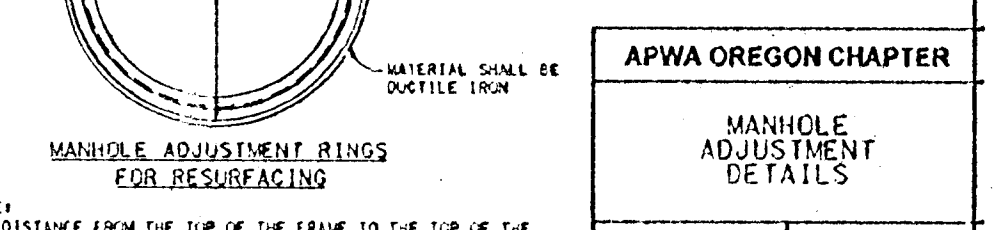
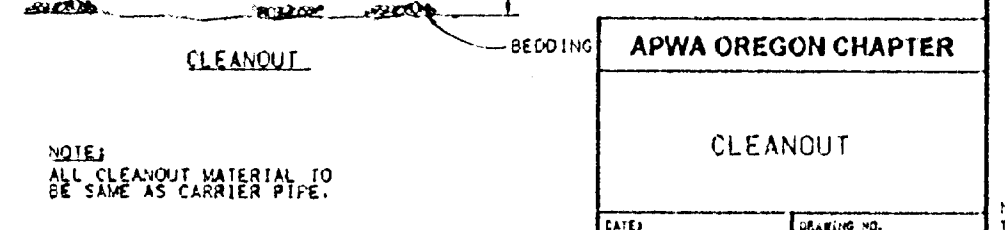
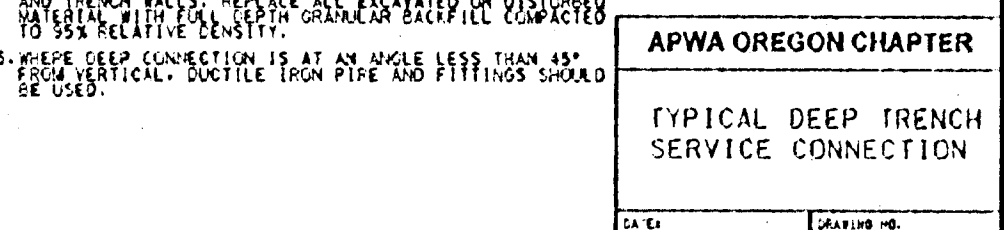
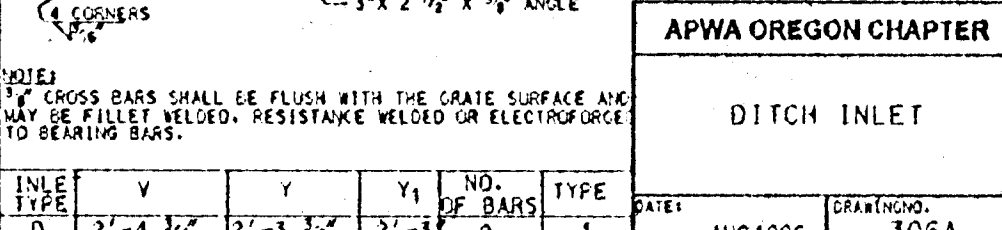
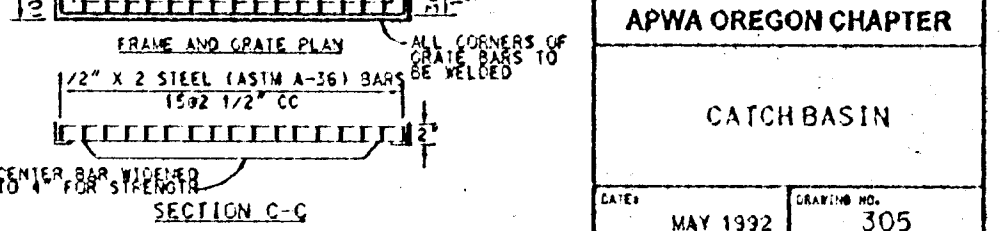
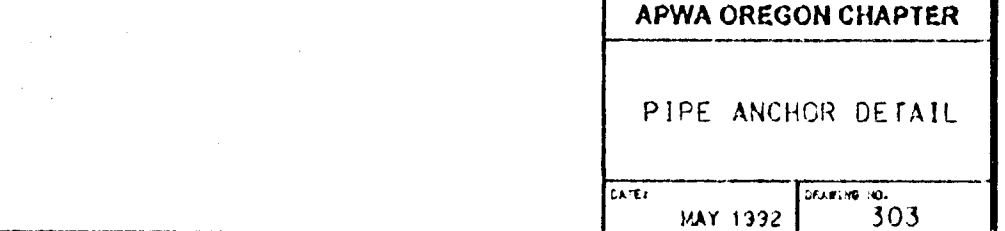
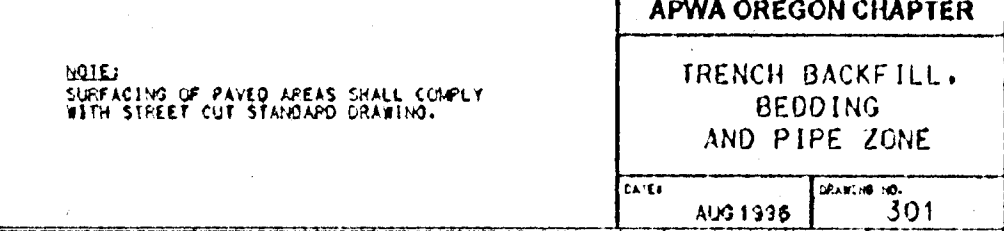
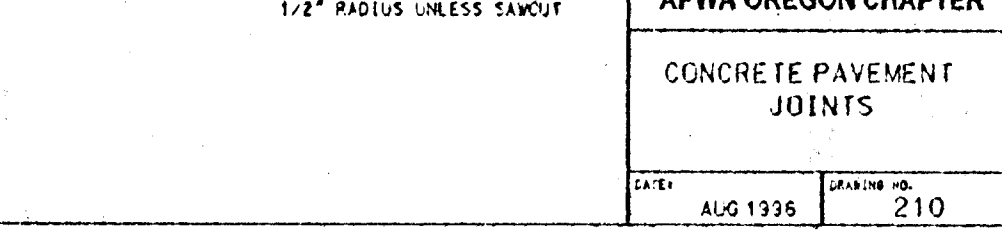
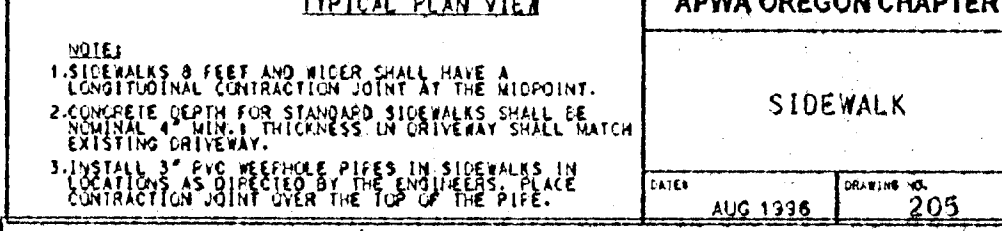
WALL GEOMETRY PLAN SHEET 2 OF 3
for
SUMMERLINN APARTMENTS
CITY OF WEST LINN, CLACKAMAS COUNTY, OREGON

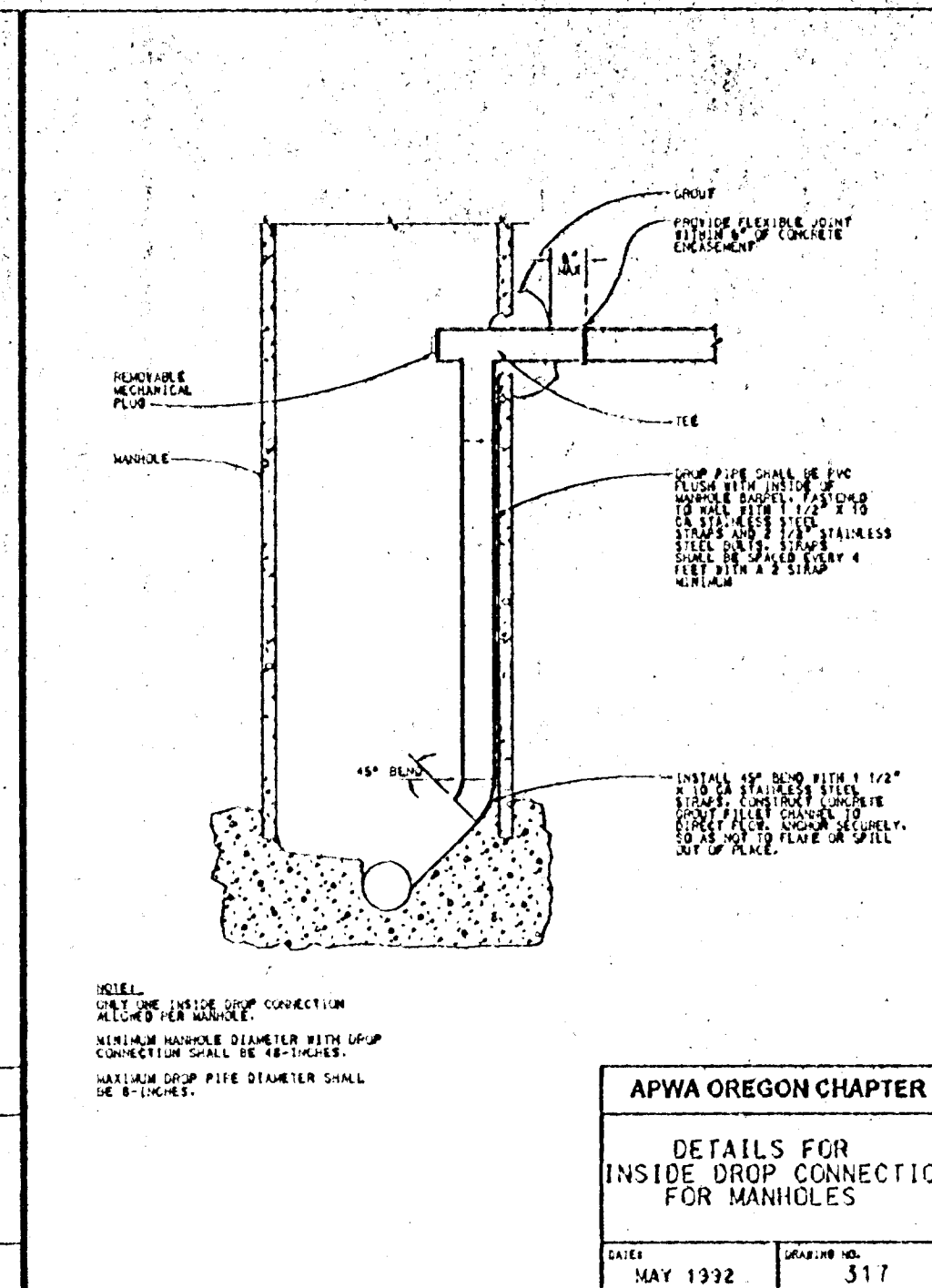
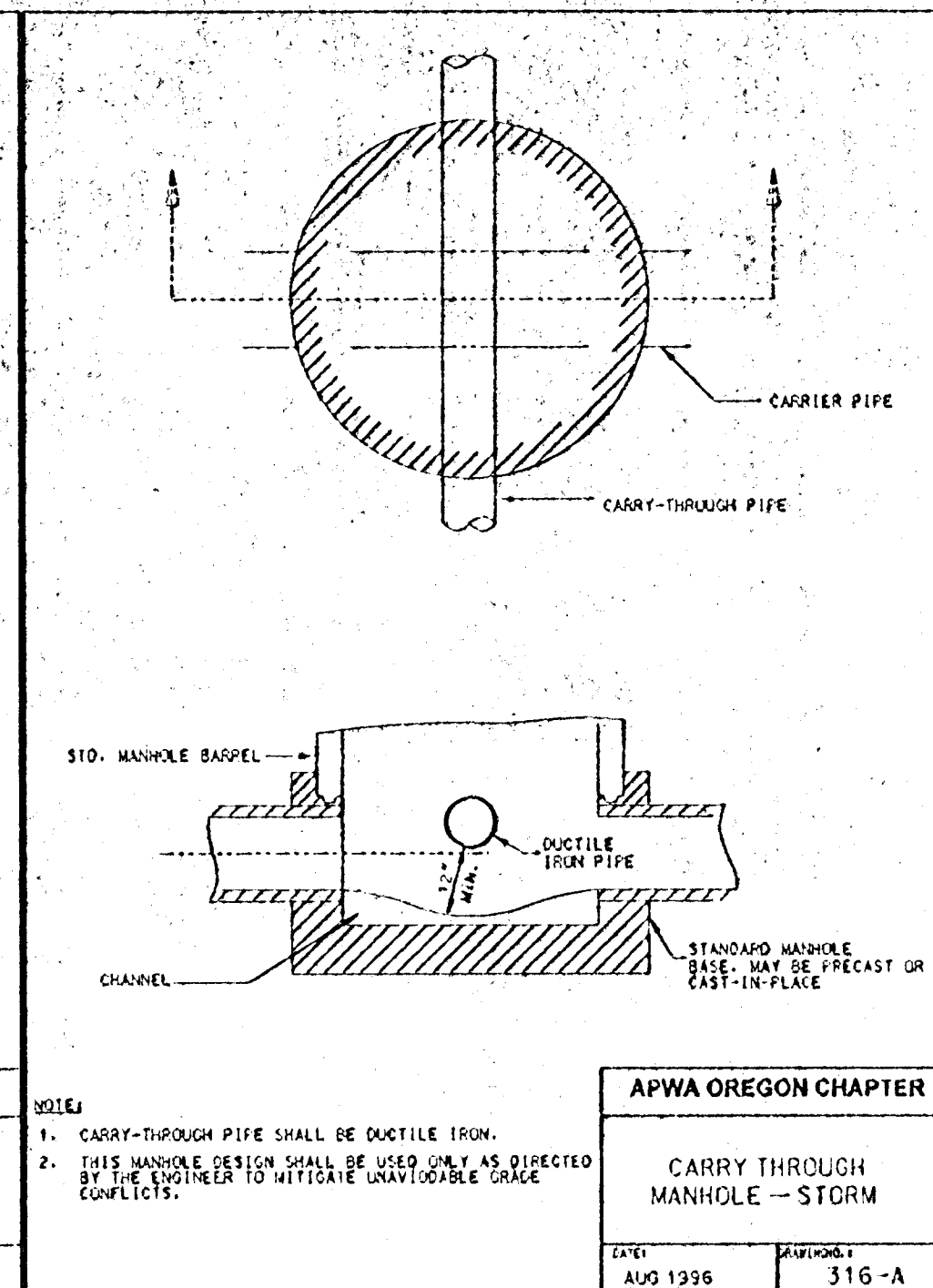
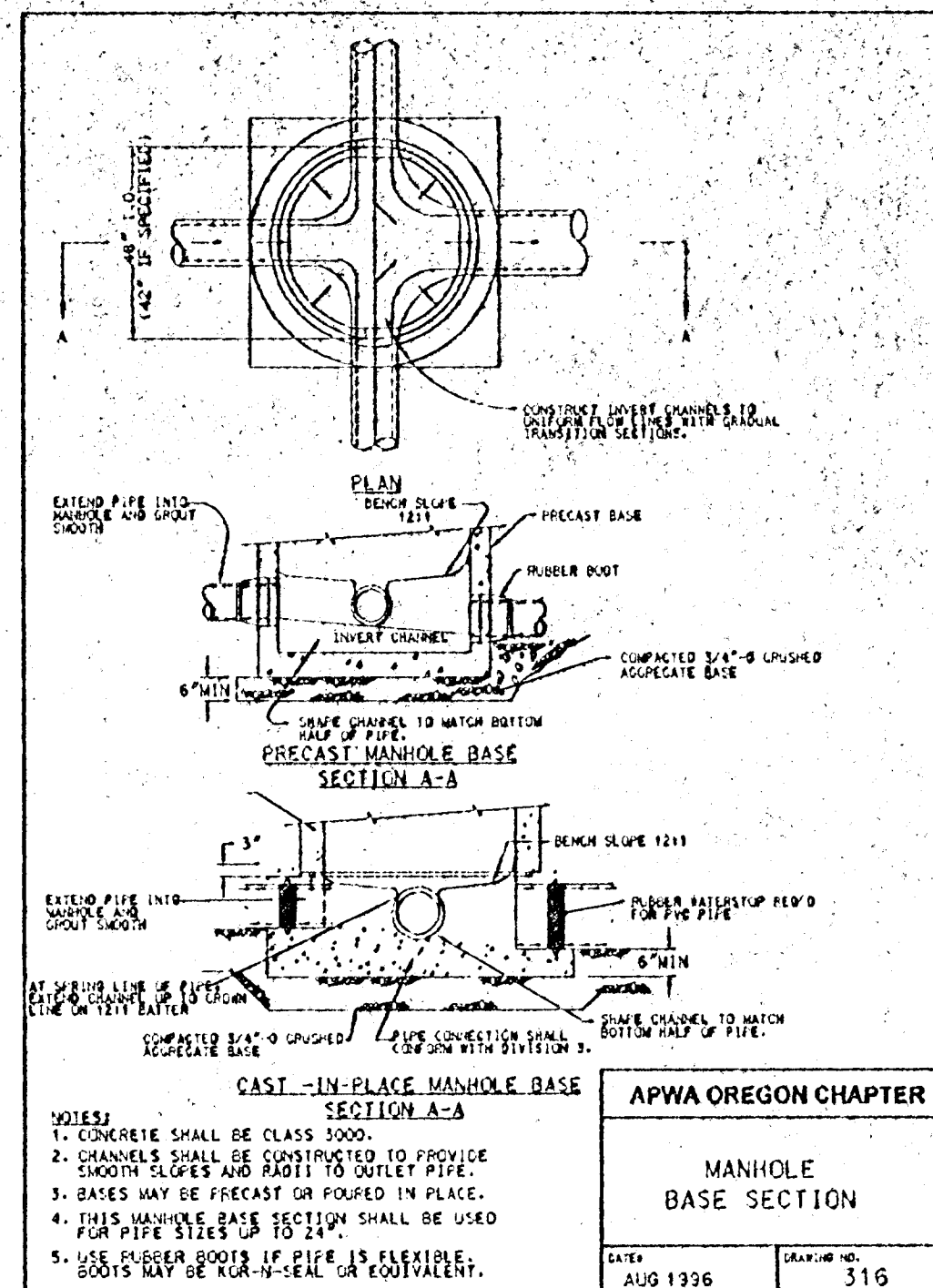


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**(HORIZONTAL)
BEARING AREA OF THRUST BLOCKS
IN SQUARE FEET**

**(VERTICAL)
VOLUME OF THRUST
BLOCK IN CUBIC YARDS**

| PIPE SIZE IN AND UNDER | 18" | 24" | 30" | 36" | 42" | 48" | 54" | 60" | 66" | 72" | 78" | 84" | 90" | 96" | 102" | 108" | 114" | 120" |
|---------------------------------|-------|------|------|------|------|-----|-----|-----|-----|-----|-----|-----|-----|-----|------|------|------|------|
| 18" | 1.0 | 1.8 | 1.4 | 1.9 | 1.4 | 1.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 24" | 2.1 | 3.7 | 3.0 | 4.0 | 3.0 | 1.6 | 1.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 30" | 3.2 | 5.6 | 4.5 | 6.0 | 4.5 | 2.1 | 1.5 | 1.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 36" | 4.3 | 7.7 | 6.2 | 8.4 | 6.2 | 2.8 | 1.9 | 1.3 | 1.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 42" | 5.4 | 9.7 | 7.8 | 10.4 | 7.8 | 3.4 | 2.4 | 1.6 | 1.2 | 1.1 | 1.0 | --- | --- | --- | --- | --- | --- | --- |
| 48" | 6.5 | 12.1 | 10.0 | 13.2 | 10.0 | 4.0 | 2.8 | 1.7 | 1.5 | 1.2 | 1.0 | 1.2 | --- | --- | --- | --- | --- | --- |
| 54" | 7.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 60" | 8.7 | 15.0 | 12.5 | 16.5 | 12.5 | 4.6 | 3.2 | 1.9 | 1.6 | 1.3 | 1.1 | 1.2 | 1.2 | --- | --- | --- | --- | --- |
| 66" | 9.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 72" | 10.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 78" | 12.0 | 23.5 | 20.0 | 26.5 | 20.0 | 5.2 | 3.6 | 2.2 | 1.9 | 1.5 | 1.3 | 1.4 | 1.4 | 1.4 | 1.4 | 1.4 | 1.4 | 1.4 |
| 84" | 13.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 90" | 14.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 96" | 15.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 102" | 16.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 108" | 17.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 114" | 18.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 120" | 19.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 126" | 20.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 132" | 21.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 138" | 23.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 144" | 24.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 150" | 25.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 156" | 26.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 162" | 27.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 168" | 28.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 174" | 29.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 180" | 30.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 186" | 31.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 192" | 32.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 198" | 34.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 204" | 35.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 210" | 36.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 216" | 37.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 222" | 38.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 228" | 39.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 234" | 40.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 240" | 41.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 246" | 42.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 252" | 43.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 258" | 45.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 264" | 46.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 270" | 47.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 276" | 48.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 282" | 49.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 288" | 50.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 294" | 51.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 300" | 52.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 306" | 53.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 312" | 54.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 318" | 56.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 324" | 57.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 330" | 58.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 336" | 59.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 342" | 60.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 348" | 61.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 354" | 62.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 360" | 63.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 366" | 64.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 372" | 65.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 378" | 67.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 384" | 68.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 390" | 69.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 396" | 70.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 402" | 71.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 408" | 72.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 414" | 73.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 420" | 74.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 426" | 75.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 432" | 76.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 438" | 78.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 444" | 79.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 450" | 80.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 456" | 81.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 462" | 82.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 468" | 83.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 474" | 84.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 480" | 85.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 486" | 86.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 492" | 87.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 498" | 89.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 504" | 90.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 510" | 91.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 516" | 92.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 522" | 93.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 528" | 94.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 534" | 95.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 540" | 96.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 546" | 97.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 552" | 98.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 558" | 100.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 564" | 101.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 570" | 102.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 576" | 103.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 582" | 104.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 588" | 105.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 594" | 106.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 600" | 107.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 606" | 108.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 612" | 109.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 618" | 111.0 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 624" | 112.1 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 630" | 113.2 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 636" | 114.3 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 642" | 115.4 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 648" | 116.5 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 654" | 117.6 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 660" | 118.7 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 666" | 119.8 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| 672" | 120.9 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | | | | | | | |